

# Council Meeting Agenda

October 23, 2018

Following the  
Organizational Meeting



ROCKY VIEW COUNTY  
Cultivating Communities  
262075 ROCKY VIEW POINT  
ROCKY VIEW COUNTY, AB  
T4A 0X2

PLEASE NOTE THAT THIS MEETING WILL BE HELD AT THE NEW COUNTY HALL:  
262075 Rocky View Point, Rocky View County, AB

CALL MEETING TO ORDER

UPDATES/ACCEPTANCE OF AGENDA

A CONFIRMATION OF MINUTES

1. October 16, 2018 Council Meeting

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B FINANCIAL REPORTS

- None

C APPOINTMENTS/PUBLIC HEARINGS

**NOTE:** As per Section 606(2)(a) of the *Municipal Government Act*, the Public Hearings for items C-1 and C-2 were advertised in the Rocky View Weekly on September 25, 2018 and October 2, 2018.

The Public Hearing for item C-3 was advertised in the Rocky View Weekly on October 2, 2018 and October 9, 2018.

## AFTERNOON APPOINTMENTS 1:30 P.M.

~~1. Division 7 – File: PL20180069 (6411017) – Bylaw C-7797-2018 –  
Redesignation Item – A Site Specific Amendment to Direct Control Bylaw C-  
6031-2005 (DC-99)~~

Application Withdrawn by Applicant

2. Division 5 – File: PL20170102 (05322016) – Bylaw C-7809-2018 –  
Redesignation Item – Residential Two District to Business – Highway Frontage  
District Outside of Business Areas

Staff Report

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3. Division 9 – File: 1042-155 – Rocky View County/Town of Cochrane –  
Annexation

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## D GENERAL BUSINESS

1. All Divisions – File: N/A – Economic Development 2017 Update Report

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2. All Divisions – File: 4050-100 – Snow and Ice Control Budget Adjustment for 2018

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3. Division 5 – File: 2025-600 – Property Tax Cancellation Request – Dalroy Gymkhana Club

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4. Divisions 1-5 – File: 1011-150 – PPC Recommendations for the Active Transportation Plan: South County

Staff Report Page 85

5. Division 9 – File: 1006-600 – Request for Capital Contribution - Friends of Westbrook School

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## E BYLAWS

1. Division 3 – File: PL20160018 – Bylaw C-7835-2018 – Revision to Road Closure Bylaw C-7745-2018

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2. Division 2 – File: PL20180045 (05714035) – Bylaw C-7824-2018 – Redesignation Item – Residential Two District to Residential One District

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## F UNFINISHED BUSINESS

- None

## G COUNCIL REPORTS

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ROCKY VIEW COUNTY, AB  
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## H MANAGEMENT REPORTS

- None

## I NOTICES OF MOTION

- None

## J SUBDIVISION APPLICATIONS

1. Division 1 – File: PL20180087 (03901008) – Subdivision Item – Residential One District

Staff Report

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2. Division 5 – File: PL20180066 (03329006) – Subdivision Item – Industrial – Industrial Activity and Public Services District

Staff Report

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## K COMMITTEE OF THE WHOLE/IN CAMERA

- None

ADJOURN THE MEETING

ROCKY VIEW COUNTY  
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October 16, 2018

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A regular meeting of the Council of Rocky View County was held in Council Chambers of the County Hall, 262075 Rocky View Point, Rocky View County, AB, T4A 0X2 on October 16, 2018 commencing at 8:59 a.m.

Present:

Division 6	Reeve G. Boehlke
Division 5	Deputy Reeve J. Gautreau
Division 1	Councillor M. Kamachi
Division 3	Councillor K. Hanson
Division 4	Councillor A. Schule
Division 7	Councillor D. Henn
Division 8	Councillor S. Wright
Division 9	Councillor C. Kissel

Absent: Division 2 Councillor K. McKylor

Also Present:

- R. McDonald, Interim County Manager
- K. Robinson, General Manager
- B. Riemann, General Manager
- C. O'Hara, General Manager
- A. Keibel, Manager, Legislative and Legal Services
- R. Barss, Manager, Intergovernmental Affairs
- L. Wesley-Riley, Manager, Enforcement Services
- M. Wilson, Planning Supervisor, Planning Services
- S. MacLean, Planner, Planning Services
- L. Ganczar, Planner, Planning Services
- K. Smigelski, Agricultural Services Officer, Agricultural and Environmental Services
- C. Satink, Deputy Municipal Clerk, Legislative and Legal Services
- T. Andreasen, Legislative Clerk, Legislative and Legal Services

**Call to Order**

The Chair called the meeting to order at 8:59 a.m. with all members present with the exception of Councillor McKylor.

**1-18-10-16-01**

**Updates/Acceptance of Agenda**

MOVED by Deputy Reeve Gautreau that the October 16, 2018 Council meeting agenda be accepted as presented.

Carried

**1-18-10-16-02**

**Confirmation of Minutes**

MOVED by Deputy Reeve Gautreau that the September 25, 2018 Council meeting minutes be accepted as presented.

Carried

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COUNCIL MEETING MINUTES  
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**1-18-10-16-03 (D-1)**  
**All Divisions – CAO Report**  
**File: N/A**

MOVED by Councillor Schule that Council accept the CAO report as presented for information.

Carried

**1-18-10-16-04 (D-2)**  
**All Divisions – Alternative Land Use Services (ALUS)**  
**File: 6000-300**

MOVED by Councillor Henn that the Terms of Reference for the Partnership Advisory Committee be approved as per Attachment 'A'.

Carried

**1-18-10-16-05 (D-3)**  
**All Divisions – Amending Terms of Reference for Intermunicipal Committees**  
**File: N/A**

MOVED by Councillor Kissel that the amended Terms of Reference for the Intermunicipal Committees be adopted as per Attachment 'B'.

AMENDING MOTION:

MOVED by Councillor Hanson that section 6 of the Intermunicipal Committees Terms of Reference in Attachment 'B' be amended as follows:

- 6) The Chair of the meeting will be as follows when Rocky View County is the host:
  - (1) The **Reeve Area Councillor** is the official Chair of the meeting;
  - (2) If the **Reeve Area Councillor** is not present, the Reeve or Deputy Reeve acts as Chair **if present;**

Carried

In Favour:

Councillor Hanson  
Reeve Boehlke  
Councillor Henn  
Councillor Wright  
Councillor Kissel

Opposed:

Councillor Kamachi  
Councillor Schule  
Deputy Reeve Gautreau

The Chair called for a vote on the motion as amended.

Carried

The Chair called for a recess at 9:39 a.m. and called the meeting back to order at 9:51 a.m. with all previously mentioned members present.

**1-18-10-16-06 (E-1)**  
**Division 4 – Bylaw C-7810-2018 – Redesignation Item – Ranch and Farm District to Agricultural Holdings District**  
**File: PL20180052 (03322005)**

Councillor Kamachi abstained from discussion and voting on item E-1 as he was not present during the public hearing and first and second readings of Bylaw C-7810-2018 at the September 25, 2018 Council meeting.

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MOVED by Councillor Schule that Bylaw C-7810-2018 be given third and final reading.

Carried  
Abstained: Councillor Kamachi

In Favour:

Councillor Schule  
Reeve Boehlke  
Deputy Reeve Gautreau  
Councillor Henn

Opposed:

Councillor Hanson  
Councillor Wright  
Councillor Kissel

**1-18-10-16-07 (E-2)**

**All Divisions – Bylaw C-7832-2018 – *Cannabis Consumption Bylaw***

**File: N/A**

The Chair called for a recess at 10:29 a.m. and called the meeting back to order at 10:40 a.m. with all previously mentioned members present.

MOVED by Councillor Hanson that an additional definition be added to Bylaw C-7832-2018 with the following wording:

**“Private Residence”** means any building or structure, and the land associated with it, used principally for human habitation on a permanent or temporary basis, but does not include common areas within campgrounds such as playgrounds, kitchen shelters, washrooms, parking areas, or roads.

AND that the remaining subsections be renumbered as necessary.

Carried

MOVED by Councillor Hanson that an additional section be inserted after section 6 in Bylaw C-7832-2018 with the following wording:

- 7) This Bylaw does not apply to areas licensed for cannabis consumption by the provincial or federal government.

AND that the remaining sections be renumbered as necessary.

Carried

MOVED by Councillor Hanson that the section reference for obstruction in Schedule ‘A’ of Bylaw C-7832-2018 be amended from section 12 to section 14.

Carried

MOVED by Councillor Hanson that Bylaw C-7832-2018 be given first reading as amended.

Carried

MOVED by Councillor Schule that Bylaw C-7832-2018 be given second reading as amended.

Carried

MOVED by Councillor Henn that Bylaw C-7832-2018 be considered for third reading as amended.

Carried

MOVED by Councillor Kamachi that Bylaw C-7832-2018 be given third and final reading as amended.

Carried

ROCKY VIEW COUNTY  
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**Adjournment**

MOVED by Councillor Schule that the October 16, 2018 Council meeting be adjourned at 10:46 a.m.

Carried

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REEVE

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CAO or Designate

## PLANNING SERVICES

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**TO:** Council  
**DATE:** October 23, 2018 **DIVISION:** 5  
**TIME:** Afternoon Appointment  
**FILE:** 05322016 **APPLICATION:** PL20170102  
**SUBJECT:** Redesignation Item – Residential Two District to Business – Highway Frontage District  
 Outside of Business Areas.

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### **<sup>1</sup>POLICY DIRECTION:**

The application was evaluated against the “Other Business Development” criteria of the County Plan and was found to be non-compliant:

- the subject property is located in the vicinity of two identified business areas (Conrich and Omni);
- the proposed development does not have direct and safe access from Highway 564; and
- the Applicant did not demonstrate how the proposed development would minimize adverse impacts on the surrounding residential and agricultural properties.

### **EXECUTIVE SUMMARY:**

The purpose of this application is to redesignate the subject land from Residential Two District to Business – Highway Frontage District to facilitate the development of a service station and a convenience store.

The subject land is located in an agricultural area and is in proximity to two business areas (Conrich located ± 3.2 km to the south, and Omni located ± 4 km to the west). The surrounding area is mainly agricultural land that is designated Ranch and Farm District, Agricultural Holdings District, and Farmstead District. The Hamlet of Delacour is located approximately 3.2 km to the east.

The closest commercial development (landscaping business) is located approximately 200 m to the east, at the corner of Highway 564 and Range Road 282. A Confined Feeding Operation (Egg Farm) is located approximately 400 m to the west, at the Highway 564 and Range Road 283 intersection.

The proposal is considered as ‘Other Business Development’ in accordance with Section 14 of the County Plan. ‘Other Business Development’ that is located outside of the business areas must justify why the proposed development cannot be located in a business area. The Applicant did not provide sufficient rationale to justify the proposed location, except for the fact that the subject land would provide services to the travelling public and to local agricultural and commercial operations.

The County Plan sets out four criteria for Other Business Development:

- *Be limited in size, scale, intensity, and scope;*
- *Have direct and safe access to a paved County Road or Provincial Highway;*
- *Provide a traffic impact and intersection assessment; and*
- *Minimize adverse impacts on existing residential, business, and agricultural uses.*

Overall, the proposal does not meet the above-listed criteria. Although the proposed development may be limited in size, the proposed development does not have direct and safe access from Highway 564.

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#### **<sup>1</sup> Administration Resources**

Johnson Kwan, Planning Services  
 Gurbir Nijjar, Engineering Services



In order to establish a new access point for the proposed development, the Applicant would need to close multiple existing accesses along Highway 564 and develop a service road. Furthermore, the proposed access would not be located on the subject property; rather, it would be located on the adjacent parcel to the east, which is also owned by Gas Plus Inc.

In addition, the Applicant did not demonstrate how the proposed development would minimize adverse impacts on the surrounding residential and agricultural properties (i.e. wetland impacts, site illumination, screening, and consideration for non-residential/residential transition).

Administration determined that the application does not meet policy.

**DATE APPLICATION RECEIVED:** July 6, 2017  
**DATE APPLICATION DEEMED COMPLETE:** June 20, 2018

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**PROPOSAL:** To redesignate the subject land from Residential Two District to Business – Highway Frontage District in order to facilitate the development of a service station and a convenience store.

**LEGAL DESCRIPTION:** Lot 2, Plan 9912511, within NE-22-25-28-W04M

**GENERAL LOCATION:** Located approximately 3.2 km (2 miles) west of Delacour, approximately 0.8 km (1/2 mile) west of Range Road 282 and on the south side of Highway 564.

**APPLICANT:** Gas Plus Inc.

**OWNERS:** 2044781 Alberta Ltd.

**EXISTING LAND USE DESIGNATION:** Residential Two District

**PROPOSED LAND USE DESIGNATION:** Business – Highway Frontage District

**GROSS AREA:** ± 2.83 hectares (± 6.99 acres)

**SOILS (C.L.I. from A.R.C.):** **Class 3W, I60, 3T40-** The land contains soil with moderate limitations for crop production due to excessive wetness/poor drainage, flooding, and adverse topography.

**PUBLIC & AGENCY SUBMISSIONS:**

The application was circulated to 18 adjacent landowners, and two letters in opposition were received in response (Appendix 'E'). The application was also circulated to a number of internal and external agencies. The responses are available in Appendix 'A'.

**HISTORY:**

**August 1999** Subdivision Plan 9912511 was registered, which created the subject land (7 acres) and the adjacent parcel (7 acres).

**September 1994** Land Use Redesignation (94-RV-162) was approved to facilitate the creation of two ± 7 acre parcels and two ± 20 acre parcels with a ± 23 acre remainder.

**BACKGROUND:**

The subject land is located approximately 3.2 km (2 miles) west of Delacour, approximately 0.8 km (1/2 mile) west of Range Road 282 and on the south side of Highway 564. The surrounding area is mainly



agricultural land designated as Ranch and Farm District, Agricultural Holdings District, and Farmstead District. The Hamlet of Delacour is located approximately 3.2 km to the east.

The County Plan identifies two business centres in this area:

- Conrich, located approximately 3.2 km (2 mile) to the south; and
- Omni, located approximately 4 km (2.5 mile) to the west.

The closest business development is located approximately 200 m to the east, at the corner of Range Road 282 and Highway 564. The 40 acre parcel was redesignated from Ranch and Farm District to Industrial – Industrial Activity District to facilitate a construction and landscaping company in the summer of 2015.

A Confined Feeding Operation (Egg Farm) is located approximately 400 m west of the subject land, at the southeast junction of Highway 564 and Range Road 283. The operation is regulated by the Natural Resources Conservation Board.

The Delacour Golf Course, the Delacour general store, and the Delacour Community Hall are located approximately 4 km (2.5 mile) to the east of the subject land. The Delacour Area Structure Plan supports small scale, locally oriented commercial development in the hamlet, and the hamlet expansion area is in proximity to the existing commercial establishments. Light industrial development is also allowed in the area in accordance with the Delacour Area Structure Plan policies.

**POLICY ANALYSIS:**

County Plan

The property is located in an agricultural area under the County Plan, and is not located in an area structure plan. For this reason, the proposal was considered as ‘Other Business Development’ in accordance with Section 14 of the County Plan.

The County Plan provides a number of business areas and development forms that accommodate the wide variety of businesses wishing to locate in the County. It encourages new businesses to locate within the business areas to provide for orderly growth and economic efficiencies in the development of the County’s transportation and infrastructure systems.

The following table outlines the selected County Plan policies, which are relevant for the assessment of this application:

Table 1: Analysis of County Plan policies

Selected County Plan Policies	Analysis
<b>Section 14 Business Development</b>	
14.19 Applications to redesignate land for business uses adjacent to, or in vicinity of, the boundaries of an identified business area shall not be supported.	<ul style="list-style-type: none"> <li>• The property is situated:                             <ul style="list-style-type: none"> <li>○ ± 3.2 km (2 miles) north of Conrich ASP’s proposed industrial development area; and</li> <li>○ ± 4 km (2.5 miles) east of Omni ASP’s proposed commercial and light industrial area.</li> </ul> </li> </ul>
14.20 Small scale value-added agriculture and agriculture services, natural resource extraction, and business as defined in relevant Federal or Provincial legislation may	<ul style="list-style-type: none"> <li>• The proposal is for a service station and convenience store. Therefore, it is not considered a small scale, value-added</li> </ul>

Selected County Plan Policies	Analysis
be supported adjacent to, or near, a business area.	agriculture and agricultural services business.
14.21 Applications to redesignate land for business uses outside of a business area shall provide a rationale that justified why the proposed development cannot be located in the business area (e.g. requirement for unique infrastructure at the proposed location).	<ul style="list-style-type: none"> <li>The Applicant indicated that the proposed service station and convenience store is to provide services to the travelling public and any local agricultural and commercial operations. However, the Applicant did not provide any justification as to why the proposed development cannot be located in the business areas.</li> </ul>
14.22 Proposals for business development outside of a business area should: <ol style="list-style-type: none"> <li>be limited in size, scale, intensity, and scope;</li> </ol>	<ul style="list-style-type: none"> <li>The subject land is ± 6.99 acres in size. The proposed development is for a service station and a convenience store. However, Business – Highway Frontage District also allows for a range of other uses (see Appendix B).</li> </ul>
<ol style="list-style-type: none"> <li>have direct and safe access to a paved County road or Provincial Highway;</li> </ol>	<ul style="list-style-type: none"> <li>The property currently has direct access onto Highway 564. However, the Applicant would need to close the existing access on the subject land, close multiple accesses on adjacent properties along Highway 564, upgrade the adjacent property's access to a Type III b intersection, and construct a service road to provide access via the adjacent property to facilitate the proposed development;</li> <li>Alberta Transportation commented that there are currently five direct accesses to the two quarter sections north and south of Highway 564, plus the two public road intersections at Range Road 282 and Range Road 283;</li> <li>Highway 564 is classified as a major two-lane highway, and only one direct access per quarter section will be permitted (see Appendix A for details).</li> </ul>
<ol style="list-style-type: none"> <li>provide a traffic impact and intersection assessment; and</li> </ol>	<ul style="list-style-type: none"> <li>The applicant submitted a Traffic Impact Assessment (JCB Engineering, May 2018);</li> <li>The assessment recommends a Type III intersection upgrade along Highway 564, closures of three existing accesses along Highway 564, and construction of a service road to provide access to the subject land through the adjacent parcel.</li> <li>The Applicant indicated that they are in the process of working with the surrounding landowners to resolve the access situation.</li> </ul>



Selected County Plan Policies	Analysis
	However, no written confirmation was provided.
d) minimizes adverse impacts on existing residential, business, or agricultural uses	The Applicant did not provide any mitigation strategy to minimize the adverse impacts on existing residential and agricultural lands in the surrounding area.

In terms of technical assessments, the applicant submitted the following as part of the application:

- A cover letter describing the application’s intent (Gas Fuels, dated June 19, 2018);
- A site plan with proposed on-site stormwater feature (dated June 19, 2018); and
- A Traffic Impact Assessment (JCB Engineering, dated May 25, 2018).

The Applicant indicated that the property’s existing water well would provide all potable water, and that all wastewater would be stored on site and then removed by truck to an appropriate off-site treatment facility. Specific sizes and locations of all utilities on site would be determined at the development permit stage.

Alberta Health Services (AHS) recommends that the Applicant confirms there is an adequate water supply available for the proposed use, and that use of the proposed volume of water required would not adversely affect supply or quality of neighbouring properties’ water. The Applicant did not provide a groundwater report or servicing study in support of the application.

AHS also noted that consideration should be given to the types and volume of chemicals that would be stored on site. The proposed method and location of chemical storage must ensure there would be no adverse impacts on local groundwater or surface water. The County’s Fire Services department also commented that the site is on a water body and close to a water body. Extra measures may need to be taken into consideration in the development and set up of the service station. The Applicant did not provide a Wetland Impact Assessment, Environmental Assessment, or Geotechnical study in support of the application.

Land Use Bylaw C-4841-97

The proposal is to redesignate the subject land from Residential Two District to Business – Highway Frontage District. Appendix B outlines the list of permitted and discretionary uses in the Business – Highway Frontage District for reference.

The following table outlines the purpose and intent of the existing and proposed land use districts.

Table 2: Land Use Bylaw Details

Land Use District	Purpose and Intent
<b>Residential Two District</b> Existing parcel: ± 2.83 hectares (± 6.99 acres)	<ul style="list-style-type: none"> <li>• To provide a residential use on a small parcel of land which accommodates minor agricultural pursuits and required accessory buildings.</li> </ul>
<b>Business – Highway Frontage District</b> Minimum parcel size: 1.01 ha (2.50 ac)	<ul style="list-style-type: none"> <li>• To allow for development along primary and secondary highways and major transportation links. Development will provide services to the</li> </ul>

Land Use District	Purpose and Intent
	travelling public and tourists, and include businesses that require a high degree of visibility and/or ease of access to transportation routes. These high profile areas represent 'gateway' development within the County and shall maintain a high standard of visual quality.

The minimum parcel size in Business – Highway Frontage District is 1.01 hectares (2.50 acres). The subject property is approximately 6.99 acres; therefore, the proposal complies with the minimum parcel sizes for the proposed district, and has the potential for future subdivision.

**CONCLUSION:**

The proposal does not meet the County Plan's criteria for 'Other Business Development'. The subject land is located in proximity to two business areas identified in the County Plan ( $\pm$  3.2 km from Conrich's future industrial development, and  $\pm$  4 km from Omni's future commercial and light industrial development), as well as the Hamlet of Delacour which allows for this type of business.

Although the proposed development may be limited in size, the proposed development does not have direct and safe access from Highway 564. In order to establish a new access point for the proposed development, the Applicant would need to close multiple existing accesses along Highway 564 and develop a service road. Furthermore, the proposed access would not be located on the subject property; rather, it would be located on the adjacent parcel to the east.

In addition, the Applicant did not demonstrate how the proposed development would minimize adverse impacts on the surrounding residential and agricultural properties (i.e. traffic impact, wetland impacts, site illumination, screening, and consideration for non-residential/residential transition).

**OPTIONS:**

- Option # 1:    Motion #1    THAT Bylaw C-7809-2018 be given first reading.
- Motion #2    THAT Bylaw C-7809-2018 be given second reading.
- Motion #3    THAT Bylaw C-7809-2018 be considered for third reading.
- Motion #4    THAT Bylaw C-7809-2018 be given third and final reading.

- Option # 2:    THAT application PL20170102 be refused.

Respectfully submitted,

Concurrence,

"Chris O'Hara"

"Rick McDonald"

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General Manager

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Interim County Manager

JKwan/rp

**APPENDICES:**

APPENDIX 'A': Application Referrals

APPENDIX 'B': Extract from Land Use Bylaw

APPENDIX 'C': Bylaw C-7809-2018 and Schedule A

APPENDIX 'D': Map Set

APPENDIX 'E': Landowner comments

**APPENDIX A: APPLICATION REFERRALS**

<b>AGENCY</b>	<b>COMMENTS</b>
<b><i>School Authority</i></b>	
Rocky View Schools	No objection.
Calgary Catholic School District	No comments.
Public Francophone Education	No comments.
Catholic Francophone Education	No comments.
<b><i>Province of Alberta</i></b>	
Alberta Energy Regulator	No comments received.
Alberta Health Services	<p>Please note that the property must be maintained in accordance with the Alberta Public Health Act, Nuisance and General Sanitation Regulation 243/2003, which stipulates:</p> <p><i>No person shall create, commit or maintain a nuisance. A person who creates, commits or maintains any condition that is or might become injurious or dangerous to the public health or that might hinder in any manner the prevention or suppression of disease is deemed to have created, committed or maintained a nuisance.</i></p> <p>Based on the information provided, AHS would like to make the following comments for your consideration:</p> <ol style="list-style-type: none"> <li>1. The application indicates that the existing well will be used to supply water for the proposed development. AHS recommends that the Applicant confirms there is an adequate water supply available for the proposed use, and that use of the proposed volume of water required will not adversely affect supply or quality of neighbouring properties' water.</li> </ol> <p>It should also be noted that the water must be potable and conform to the most recent Canadian Drinking Water Quality Guidelines as well as the Alberta Public Health Act, Nuisance and General Sanitation Regulation 243/2003, which states:</p> <ol style="list-style-type: none"> <li>a) <i>a person shall not locate a water well that supplies water that is intended or used for human consumption within</i></li> <li>b) <i>10 metres of any watertight septic tank, pump out tank or other watertight compartment of a sewage or water system,</i></li> <li>c) <i>15 metres of a weeping tile field, an evaporative treatment mound or an outdoor toilet facility with a pit,</i></li> </ol>

AGENCY	COMMENTS
	<p>d) 30 metres of a leaching cesspool,  e) 50 metres of sewage effluent on the ground surface,  f) 100 metres of a sewage lagoon, or  g) 450 metres of any area where waste is or may be disposed of at a landfill within the meaning of the Waste Control Regulation (AR 192/96).</p> <p>2. The application specifies that all wastewater will be stored on site and removed by truck for off-site treatment. Note that the storage facility should be completely contained within the property boundaries, be adequately sized for the proposed use and comply with all regulatory requirements.</p> <p>3. Consideration should be given to the types and volume of chemical that will be stored onsite. The proposed method and location of chemical storage must ensure there will be no adverse impacts on local groundwater or surface water.</p> <p>4. A waste management plan for the proposed operation should specify the type and volume of any waste materials. The manner in which waste materials are handled, stored and disposed of must not create a public health nuisance.</p> <p>5. Finalized building plans for this facility should be forwarded to AHS for approval before the building permit is granted and construction takes place. This will ensure that the proposed facility will meet the requirements of the Public Health Act and its regulations. To arrange for a plan review, applicants should contact Alberta Health Services, Environmental Public Health directly at (403) 943-2296, or email <a href="mailto:calgaryzone.environmentalhealth@ahs.ca">calgaryzone.environmentalhealth@ahs.ca</a>.</p> <p>If any evidence of contamination or other issues of public health concern are identified at any phase of development, AHS wishes to be notified.</p>
Alberta Environments and Parks	No comments received.
Alberta Transportation	<p>The area of land subject of this proposal is located within 300 metres of Highway 564, and therefore, is within Alberta Transportation's area of jurisdiction as outlined in the Highways Development and Protection Act. The department offers the following comments regarding the proposal:</p> <p>1. Currently there are five direct accesses to the two quarter section north and south of Highway 564 that is not including the two public road intersections. The two approaches in SE-27-25-28-W4M are located within Blk. 1, Plan 9010160. The balance of the quarter section gains access from Range Road 282. Proposing a Type IIIb intersection for the proposed development will create an additional north access onto Highway 564.</p>



AGENCY	COMMENTS
	<ol style="list-style-type: none"> <li>2. Highway 564 is classified as a major two-lane highway and one direct access per quarter section will be permitted. Alberta Transportation grandfathered all accesses that were constructed when the highway was under the jurisdiction of the County. All new accesses are required to follow the department's access management guidelines. Temporary access may be considered at 400 metre spacing from other accesses. The spacing between the proposed commercial accesses to the west property (Lot 1) access is approximately 200 metres and does not conform to the department's access management guidelines.</li> <li>3. The proposed access road (service road) needs to be extended to provide access to Lot 1 and remove their existing access. The proposed access road must be surveyed, registered and constructed to a municipal standard. A letter of support from the County is required.</li> <li>4. The access construction must be implemented as a condition of development approval and at no cost to Alberta Transportation.</li> <li>5. The proposed development will require the benefit of a Roadside Development Permit from Alberta Transportation.</li> </ol>
<b><i>Public Utility</i></b>	
ATCO Gas	No objection.
ATCO Pipelines	No objection.
AltaLink Management	No comments received.
FortisAlberta	No comment.
Telus Communications	No objection.
TransAlta Utilities Ltd.	No comments received.
<b><i>Other External Agencies</i></b>	
EnCana Corporation	No comments received.
<b><i>Rocky View County Boards and Committees</i></b>	
ASB Farm Members and Agricultural Fieldmen	No comments.

AGENCY	COMMENTS
Chestermere Conrich Recreation Board	Given that Municipal Reserves were previously provided on Plan 9812469, the Chestermere Conrich Recreation Board has no comments on this circulation.
<b><i>Internal Departments</i></b>	
Agricultural Services	If this application is approved, the application of the Agricultural Boundary Design Guidelines will be beneficial in buffering the Business – Highway Frontage District from the agricultural land surrounding the parcel. The guidelines would help mitigate areas of concern including: trespass, litter, pets, noise and concern over fertilizers, dust & normal agricultural practices.
Municipal Lands	The Municipal Lands Office has no concerns with this application as parks, open space or active transportation networks are not affected.
Development Authority	No comments.
Enforcement Services	No concerns.
GeoGraphics	No comments.
Building Services	No comments.
Emergency Services	1. The Rocky View Fire Services does not administer Part IV of the Alberta Fire Code; therefore, application will need to be made to the Petroleum Tank Management Association of Alberta (PTMAA) for the registering of the tanks and the site.
Infrastructure and Operations - Engineering Services	<p data-bbox="656 1337 760 1367"><u>General</u></p> <ul data-bbox="656 1388 1451 1921" style="list-style-type: none"> <li data-bbox="656 1388 1409 1518">• The review of this file is based upon the application submitted. These conditions/recommendations may be subject to change to ensure best practices and procedures;</li> <li data-bbox="656 1524 1451 1822">• As a condition of future DP, the applicant will be required to submit a construction management plan addressing noise mitigation measures, traffic accommodation, sedimentation and dust control, management of stormwater during construction, erosion and weed control, construction practices, waste management, firefighting procedures, evacuation plan, hazardous material containment and all other relevant construction management details;</li> <li data-bbox="656 1829 1386 1921">• As a condition of future DP, the applicant shall be responsible to dedicate all necessary easements and ROWs for utility line assignments and provide for the</li> </ul>

AGENCY	COMMENTS
	<p>installation of all underground shallow utilities with all necessary utility providers to the satisfaction of the County.</p> <p><u>Geotechnical</u> - Section 300.0 requirements:</p> <ul style="list-style-type: none"> <li>• ES has no requirements at this time;</li> <li>• As a condition of future DP, the applicant will be required to conduct an onsite geotechnical investigation, conducted by a qualified geotechnical professional, to provide geotechnical related recommendations for the future development of the subject lands</li> </ul> <p><u>Transportation</u> - Section 400.0 requirements:</p> <ul style="list-style-type: none"> <li>• The applicant provided a Transportation Impact Assessment prepared by JCB Engineering Ltd. dated May 25, 2018. The TIA analyzed the impacts of the proposed development onto Highway 564 and makes the following recommendations: <ul style="list-style-type: none"> <li>○ Upgrade of the proposed site access (located on the adjacent parcel to the east) to a Type IIIb standard;</li> <li>○ Closure of the two existing accesses to the subject lands and adjacent lands to the east; and</li> <li>○ Closure of the existing field access to the parcel directly north of the Highway 564 (this parcel has two existing approaches from Highway 564; the approach to be removed is not being utilized at this time).</li> </ul> </li> <li>• As a condition of future DP, the applicant will be required to enter into a Development Agreement with the County for the upgrade of the site access to a Type IIIb standard and removal of the existing approaches as indicated above in accordance with Alberta Transportation Standards as these improvements are necessary to support the proposed development;</li> <li>• As a condition of future DP, the applicant will be required to obtain a roadside Development Permit from AT for the access improvements to Highway 564;</li> <li>• As the proposed development is to access Highway 564 via the existing approach on the adjacent parcel to the east (also owned by the applicant), as a condition of the future DP, the applicant will be required to provide a ROW plan and access easement to place on title for each lot for the mutual use of the existing approach;</li> <li>• As a condition of future subdivision or DP, the applicant will be required to provide payment of the Transportation Off-Site Levy in accordance with the applicable levy at time of approval for the total gross acreage of the lands proposed to be subdivided or developed. In accordance with the current bylaw, should the entire area to be redesignated to B-HF be developed, the estimated levy</li> </ul>

AGENCY	COMMENTS
	<p>owed as a condition of future DP is \$32,120.</p> <p><u>Sanitary/Waste Water</u> - Section 500.0 requirements:</p> <ul style="list-style-type: none"> <li>• ES has no requirements at this time;</li> <li>• As per the application, the applicant is proposing on utilizing holding tanks with a trucked service to dispose of wastewater from the proposed development. ES has no further concerns</li> </ul> <p><u>Water Supply And Waterworks</u> - Section 600.0 &amp; 800.0 requirements:</p> <ul style="list-style-type: none"> <li>• ES has no requirements at this time;</li> <li>• ES generally recommends the use cisterns with a trucked service to service industrial/commercial type development. Should the applicant choose to source groundwater, the applicant will be required to obtain the appropriate licensing from AEP for the use of a groundwater well for a non-residential use as a condition of future DP.</li> </ul> <p><u>Storm Water Management</u> – Section 700.0 requirements:</p> <ul style="list-style-type: none"> <li>• The applicant did not provide a conceptual stormwater management plan as part of the application however has indicated that overland flows from the future development are to be directed into an onsite evaporative stormwater management pond to be located near the NE corner of the subject lands;</li> <li>• The applicant further indicates that any required wetland mitigation will conform to all applicable directives, regulations, and provincial laws;</li> <li>• As a condition of future DP, the applicant is required to submit detailed engineering drawings for the stormwater management system (SSIP), prepared by a qualified professional, in accordance with the conceptual stormwater management concept and County Servicing Standards;</li> <li>• As a condition of future DP, the applicant is required to provide a sediment and erosion control plan, prepared by a qualified professional, addressing ESC measures to be implemented during construction in accordance with the requirements of the County’s Servicing Standards.</li> </ul> <p><u>Environmental</u> – Section 900.0 requirements:</p> <ul style="list-style-type: none"> <li>• There appears to be existing altered wetlands along the eastern boundary of the subject lands. As part of the stormwater management plan to be submitted at time of DP, the applicant will be required to address all impacts to these wetlands due to the proposed development and maintain all necessary setbacks if proposing to maintain the wetland areas. Should the wetlands be disturbed or</li> </ul>



AGENCY	COMMENTS
	lost, as a condition of future DP, the applicant will be required to obtain all necessary approvals from AEP under the Water Act for all disturbances to these wetlands
Infrastructure and Operations - Maintenance	No issues
Infrastructure and Operations - Capital Delivery	No concerns.
Infrastructure and Operations – Utility Services	No concerns.
Infrastructure and Operations – Road Operations	No concerns. Access to be determined by Alberta Transportation.
Agriculture and Environmental Services - Solid Waste and Recycling	No comments.

Circulation Period: June 21, 2018 – July 13, 2018

**APPENDIX B: EXTRACT FROM LAND USE BYLAW****Business – Highway Frontage District (B-HF)****Uses, Permitted**

Automotive services  
Accessory Buildings  
Commercial Communications Facilities (Types A, B)  
Government Services  
Restaurant  
Restaurant, Drive Through  
Service Station  
Signs  
Tourist Information services and facilities

**Uses, Discretionary**

Accommodation Units, compatible with available servicing  
Banks or Financial Institutions  
Car wash (with internal bays only)  
Conference Centre  
Dealership/Rental Agency, Automotive  
Dealership/Rental Agency, Implement and Equipment  
Dealership/Rental Agency, Recreational Vehicle  
Drinking establishment  
Gaming Establishment, Bingo  
Gaming Establishment, Casino  
Grocery stores, Local (Floor Area up to 600 m<sup>2</sup> (6458.35 ft<sup>2</sup>))  
Grocery stores, Regional Liquor Sales  
Offices  
Outdoor display area  
Patio, accessory to the principal business use  
Personal Services Business  
Truck stop  
Truck Trailer Service  
Any use that is similar, in the opinion of the Development Authority, to the permitted or discretionary uses described above that also meets the purpose and intent of this district.



## BYLAW C-7809-2018

### A Bylaw of Rocky View County to amend Bylaw C-4841-97, being the Land Use Bylaw

The Council of Rocky View County enacts as follows:

**PART 1 – TITLE**

This Bylaw shall be known as Bylaw C-7809-2018.

**PART 2 – DEFINITIONS**

In this Bylaw, the definitions and terms shall have the meanings given to them in Land Use Bylaw C-4841-97 and the *Municipal Government Act*.

**PART 3 – EFFECT OF BYLAW**

**THAT** Part 5, Land Use Maps No. 53 of Bylaw C-4841-97 be amended by redesignating Lot 2, Plan 9912511 within NE-22-25-28-W04M from Residential Two District to Business – Highway Frontage District as shown on the attached Schedule 'A' forming part of this Bylaw.

**THAT** Lot 2, Plan 9912511 within NE-22-25-28-W04M is hereby redesignated to Business – Highway Frontage District as shown on the attached Schedule 'A' forming part of this Bylaw.

**PART 4 – TRANSITIONAL**

Bylaw C-7809-2018 comes into force when it receives third reading, and is signed by the Reeve/Deputy Reeve and CAO or Designate, as per the *Municipal Government Act*.

**Division: 5**

**File: 05322016- PL20170102**

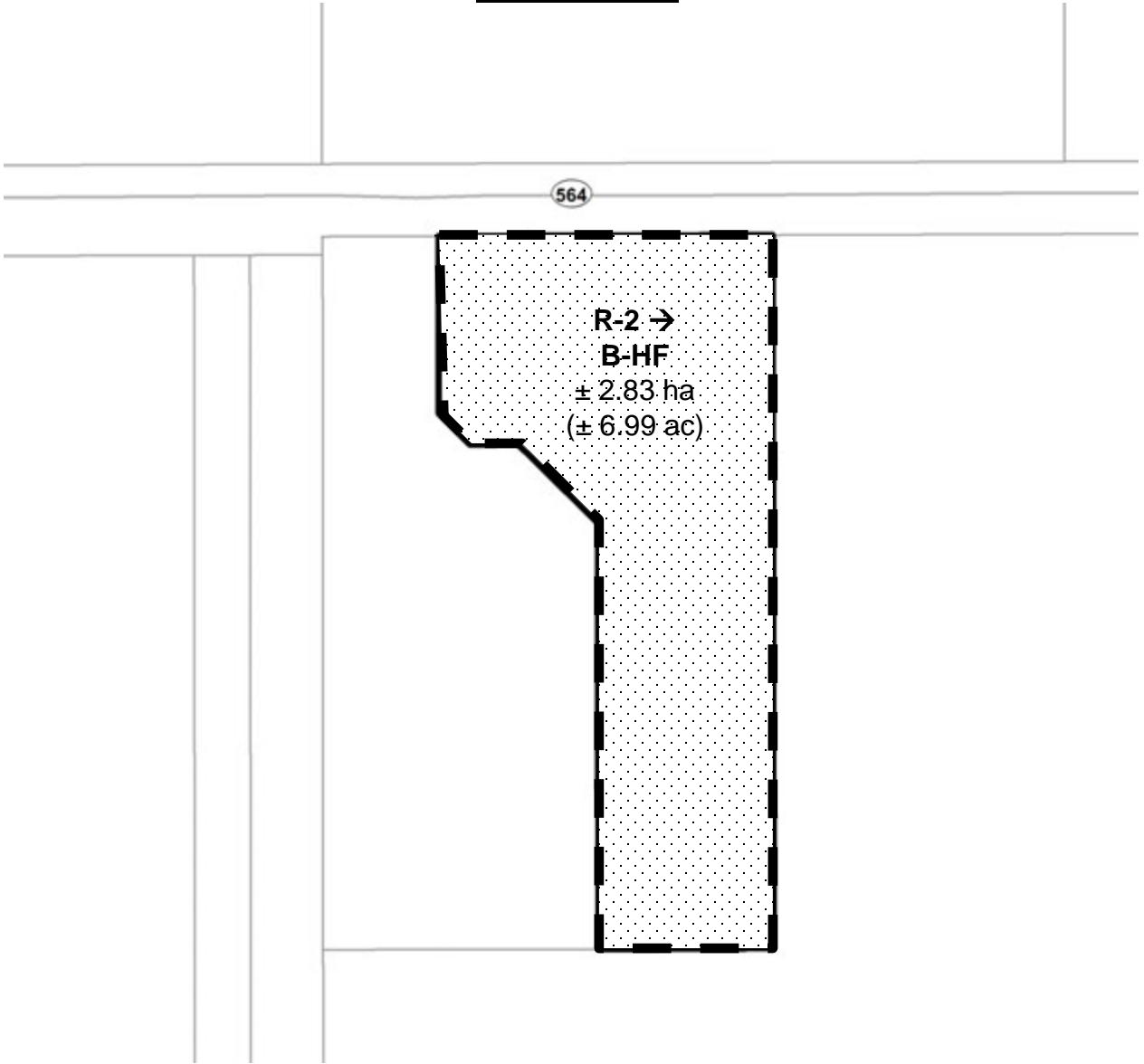
<i>PUBLIC HEARING WAS HELD IN COUNCIL this</i>	<i>day of</i>	<i>, 2018</i>
READ A FIRST TIME IN COUNCIL this	day of	, 2018
READ A SECOND TIME IN COUNCIL this	day of	, 2018
<i>UNANIMOUS PERMISSION FOR THIRD READING</i>	<i>day of</i>	<i>, 2018</i>
READ A THIRD TIME IN COUNCIL this	day of	, 2018

\_\_\_\_\_  
Reeve

\_\_\_\_\_  
CAO or Designate

\_\_\_\_\_  
Date Bylaw Signed

BYLAW: C-7809-2018

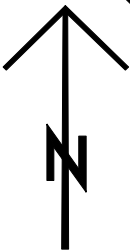


**AMENDMENT**

FROM Residential Two District TO Business – Highway Frontage District



Subject Land \_\_\_\_\_



LEGAL DESCRIPTION: Lot 2, Plan 9912511  
within NE-22-25-28-W04M

FILE: 05322016 – PL20170102

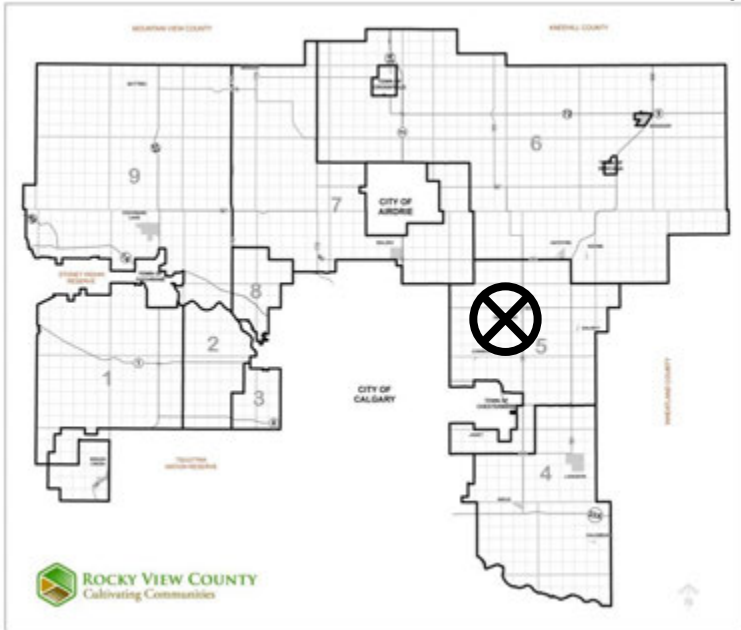
DIVISION: 5



ROCKY VIEW COUNTY  
Cultivating Communities

**AGENDA**



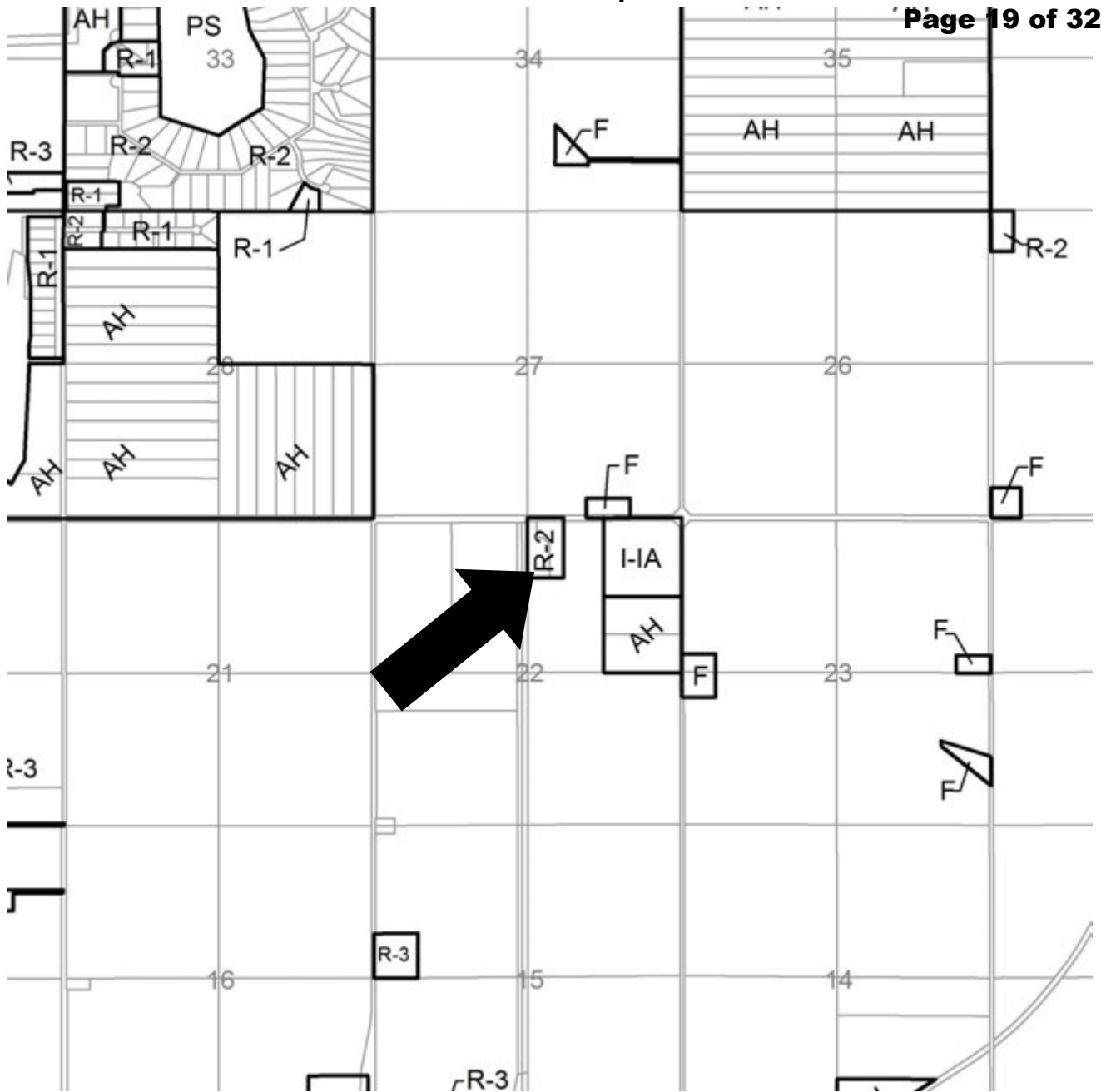


LOCATION PLAN

NE-22-25-28-W04M

Lot:2 Plan:9912511





Ranch and Farm	B-1 Highway Business
RF2 Ranch and Farm Two	B-2 General Business
RF3 Ranch and Farm Three	B-3 Limited Business
AH Agricultural Holding	B-4 Recreation Business
F Farmstead	B-5 Agricultural Business
R-1 Residential One	B-6 Local Business
R-2 Residential Two	NRI Natural Resource Industrial
R-3 Residential Three	HR-1 Hamlet Residential Single Family
DC Direct Control	HR-2 Hamlet Residential (2)
PS Public Service	HC Hamlet Commercial
	AP Airport

# LAND USE MAP

**NE-22-25-28-W04M**  
**Lot:2 Plan:9912511**





#### Adopted Statutory Plans in the area

- Conrich ASP located  $\pm 3.2$  km (2 miles) to the south;
- Omni ASP located  $\pm 4$  km (2.5 miles) to the east ;
- Delacour ASP located  $\pm 3.2$  km (2 miles) to the west.

#### Surrounding area:

- County Residential Subdivision and Fragmented Quarter sections.
- 40 acres I-IA parcel located at the intersection of Rge Rd 282 and Hwy 564;
- Confined Feeding Operation (Egg Farm) at the south east junction of Highway 564 and Range Road 283.

## CONTEXT MAP

**NE-22-25-28-W04M**

**Lot:2 Plan:9912511**

Date: June 19, 2018

Division # 5

File: 05322016

**AGENDA**

**Page 27 of 446**

**Land Use Redesignation Proposal:** to redesignate the subject land from Residential Two District (R-2) to Business – Highway Frontage District (B-HF) in order to facilitate the development of a Service Station and Convenience Store.



## DEVELOPMENT PROPOSAL

Note: Post processing of raw aerial photography may cause varying degrees of visual distortion at the local level.



**NE-22-25-28-W04M**

**Lot:2 Plan:9912511**

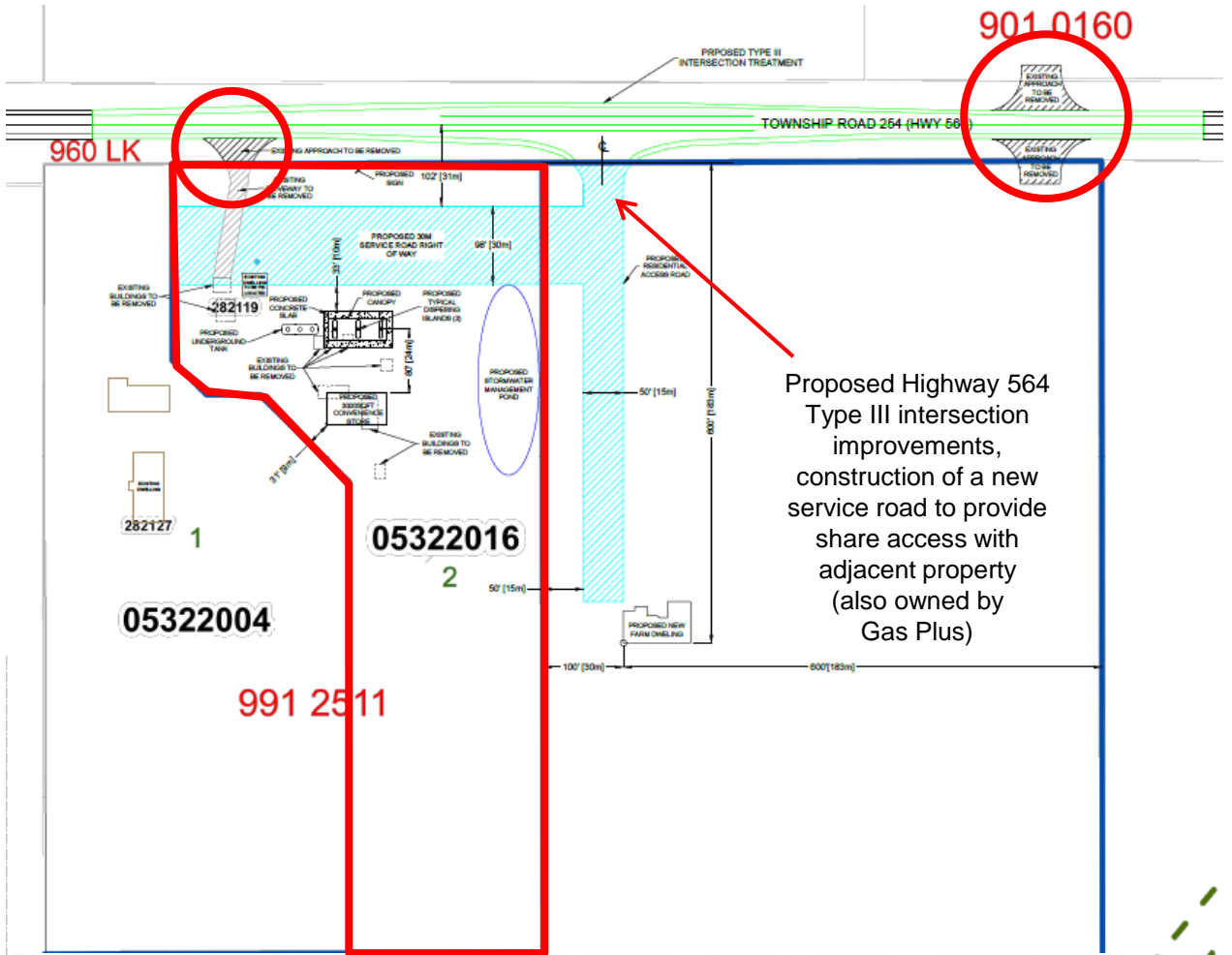
Date: June 19, 2018

Division # 5

File: 05322016

**AGENDA**

**Page 28 of 446**



Proposed Highway 564 Type III intersection improvements, construction of a new service road to provide share access with adjacent property (also owned by Gas Plus)



**County Policy 14.22**

- a) Be limited in size, scale intensity and scope
- b) Have direct and safe access to a paved Country road or Provincial Highway;

**TENTATIVE SITE PLAN**

**NE-22-25-28-W04M**  
**Lot:2 Plan:9912511**





Contours are generated using 10m grid points, and depict general topographic features of the area. Detail accuracy at a local scale cannot be guaranteed. They are included for reference use only.

## TOPOGRAPHY

Contour Interval 2 M



**NE-22-25-28-W04M**

**Lot:2 Plan:9912511**

Date: June 19, 2018

Division # 5

File: 05322016

**AGENDA**

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Facing West along Highway 564



Facing East along Highway 564

## SITE PHOTOS



**NE-22-25-28-W04M**

**Lot:2 Plan:9912511**

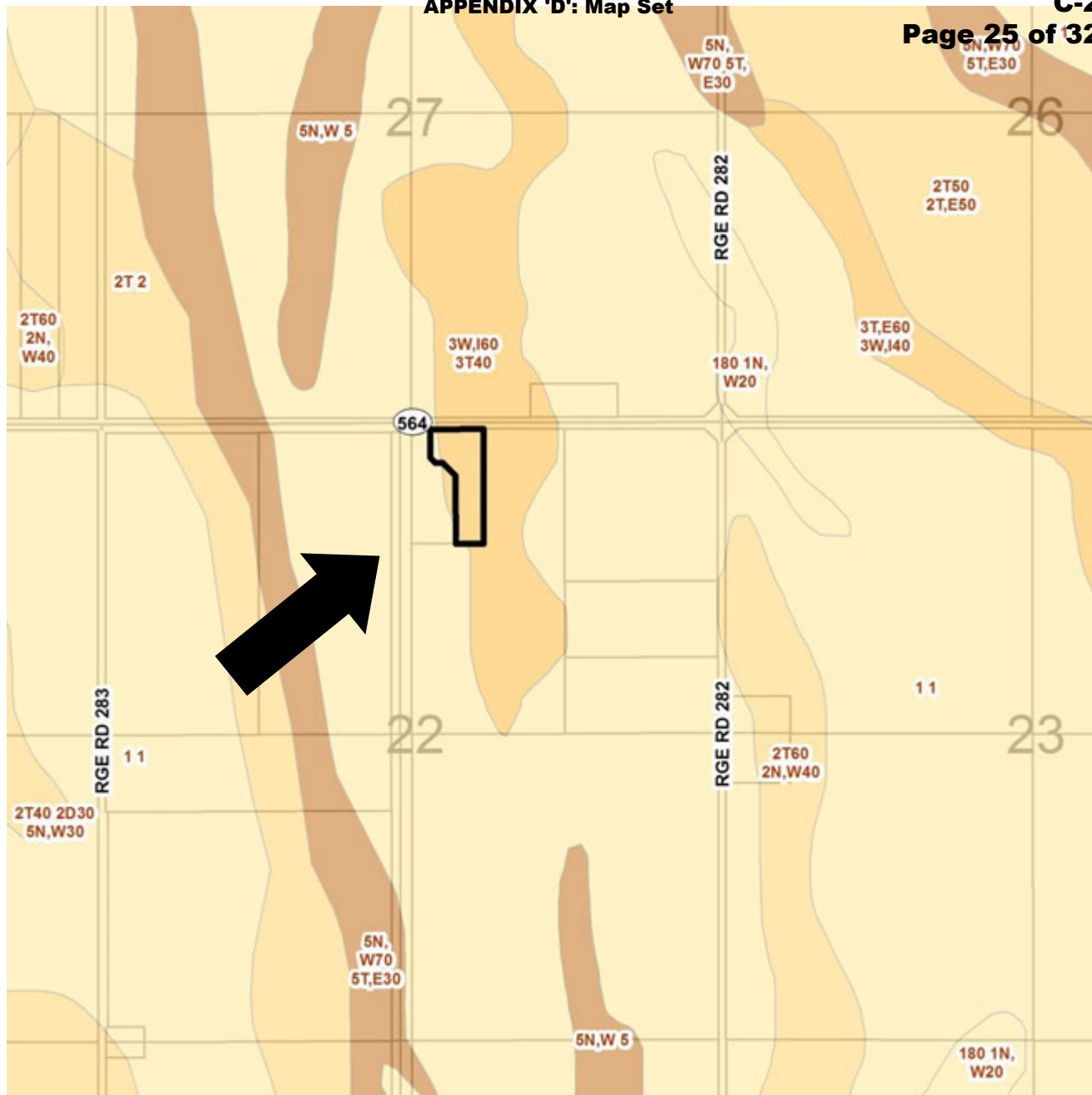
Date: June 19, 2018

Division # 5

File: 05322016

**AGENDA**

**Page 31 of 446**



**LAND CAPABILITY CLASSIFICATION LEGEND**

*Limitations refer to cereal, oilseeds and tame hay crops*

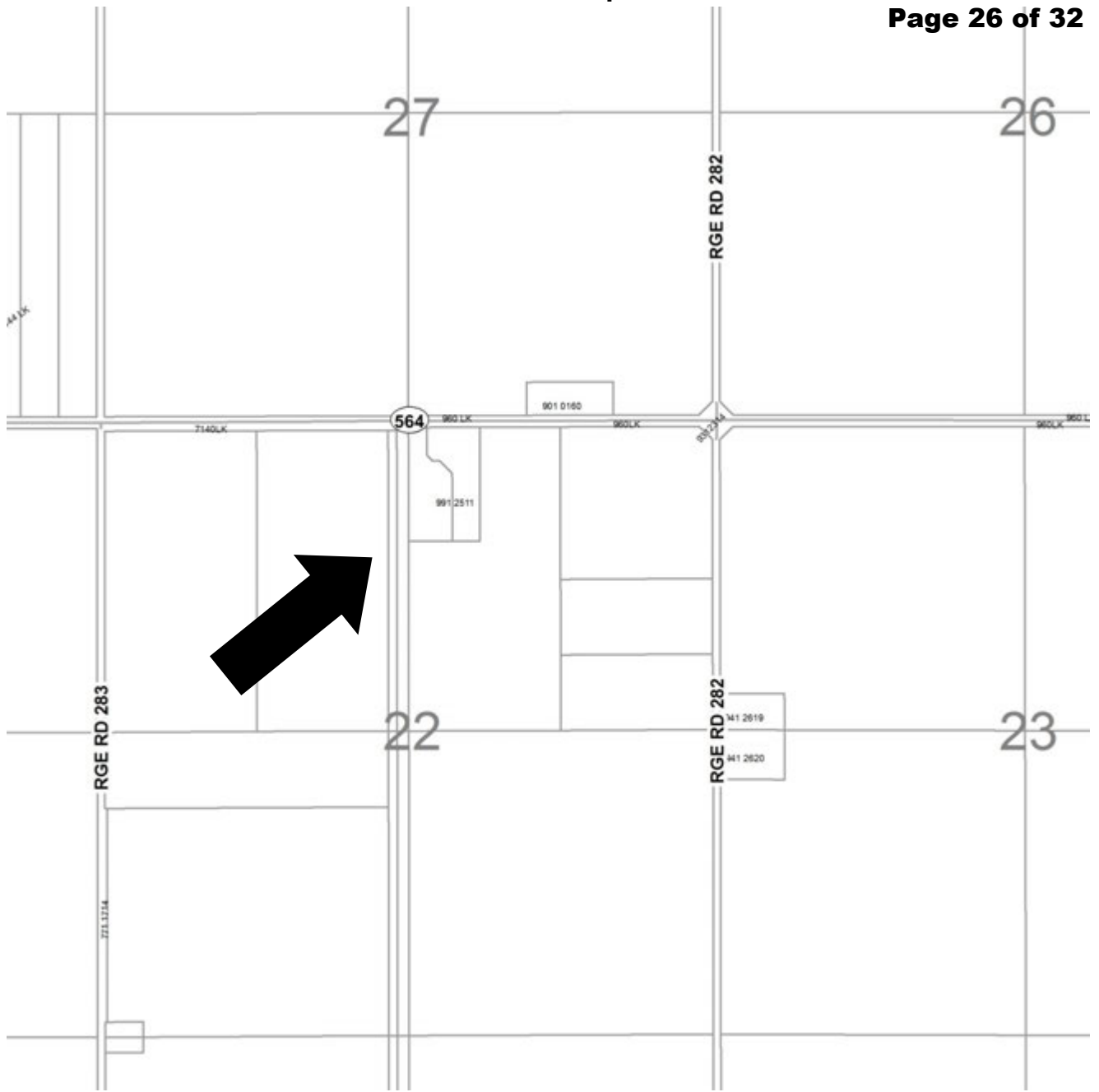
CLI Class	Limitations	
1 - No significant limitation	B - brush/tree cover	N - high salinity
2 - Slight limitations	C - climate	P - excessive surface stoniness
3 - Moderate limitations	D - low permeability	R - shallowness to bedrock
4 - Severe limitations	E - erosion damage	S - high sodicity
5 - Very severe limitations	F - poor fertility	T - adverse topography
6 - Production is not feasible	G - Steep slopes	U - prior earth moving
7 - No capability	H - temperature	V - high acid content
	I - flooding	W - excessive wetness/poor drainage
	J - field size/shape	X - deep organic deposit
	K - shallow profile development	Y - slowly permeable
	M - low moisture holding, adverse texture	Z - relatively impermeable

**SOIL MAP**

**NE-22-25-28-W04M**

**Lot:2 Plan:9912511**





**Legend – Plan numbers**

- First two numbers of the Plan Number indicate the year of subdivision registration.
- Plan numbers that include letters were registered before 1973 and do not reference a year

# HISTORIC SUBDIVISION MAP

**NE-22-25-28-W04M**

**Lot:2 Plan:9912511**

Date: June 19, 2018

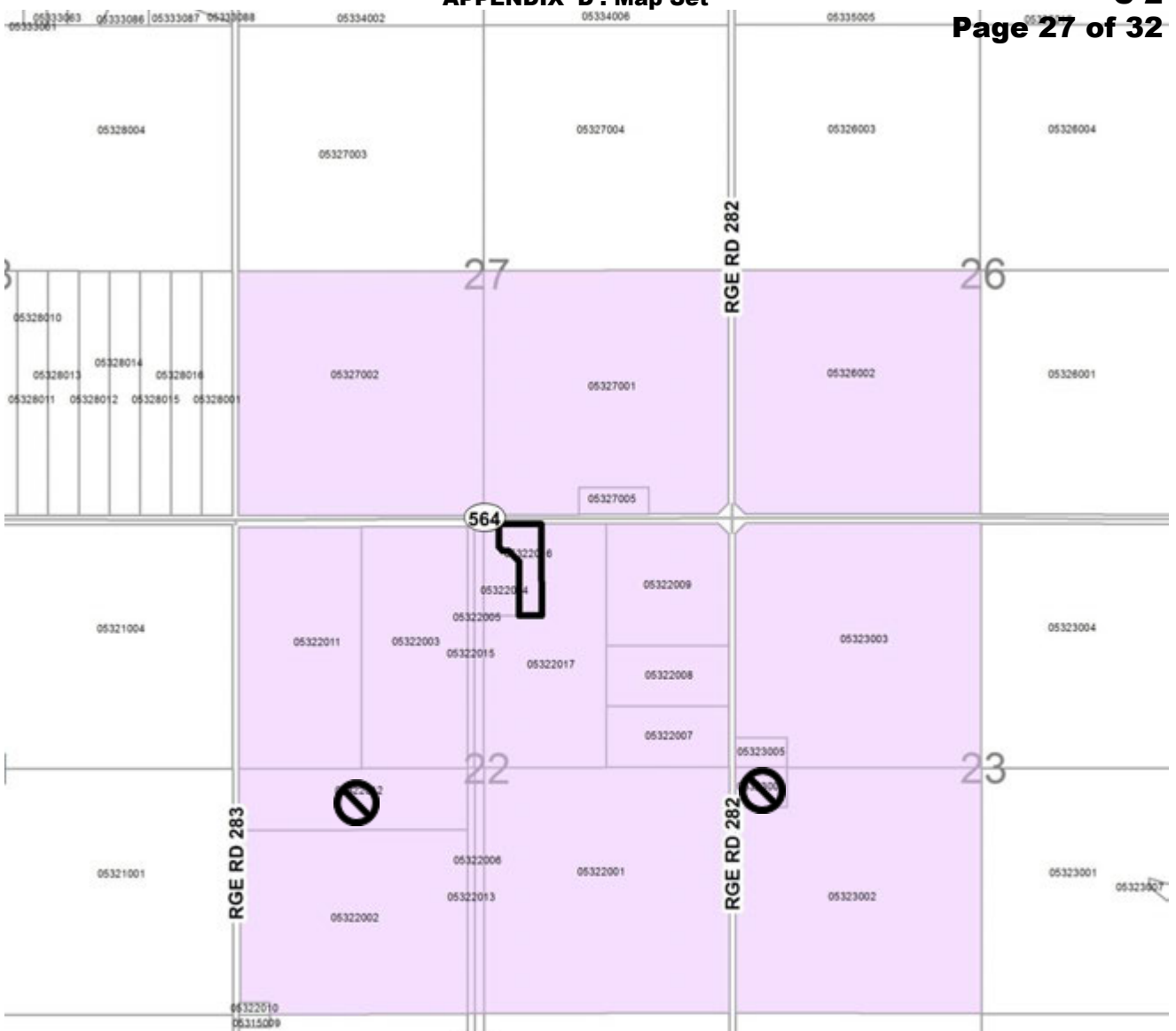
Division # 5



File: 05322016



**AGENDA**

**Page 33 of 446**

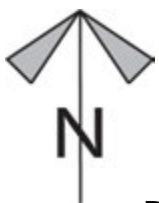




 Letters in Opposition  
 Letters in Support

Legend	
	Circulation Area
	Subject Lands

# LANDOWNER CIRCULATION AREA



**NE-22-25-28-W04M**  
**Lot:2 Plan:9912511**



June 19, 2018

Our file: 101.03152114.18.01  
Your file: PL20170102

Mr. Johnson Kwan - Municipal Planner  
Rocky View County  
911 - 32 Avenue N.E.  
Calgary, Alberta  
T2E 6X6

Subject: Redesignation Commercial or Business Purpose Application - proposed land use designation change at 282119 Township Road 254 (Highway 654), Rocky View County, AB

Dear Mr. Kwan,

We are asking that the subject property's land use designation be changed to Business - Highway Frontage for the development of a Service Station and Convenience Store. The subject property is owned by our company and is not located in a business area identified in the County Plan but we have been informed that it can be assessed based on the County Plan's "Other Business Development Policies".

The subject property is located 5.7km East of Calgary on Provincial Highway 564. The next closest service stations are approximately 10km away at the Hamlet of Conrich and in Northeast Calgary at 52 Street and Country Hills Boulevard.

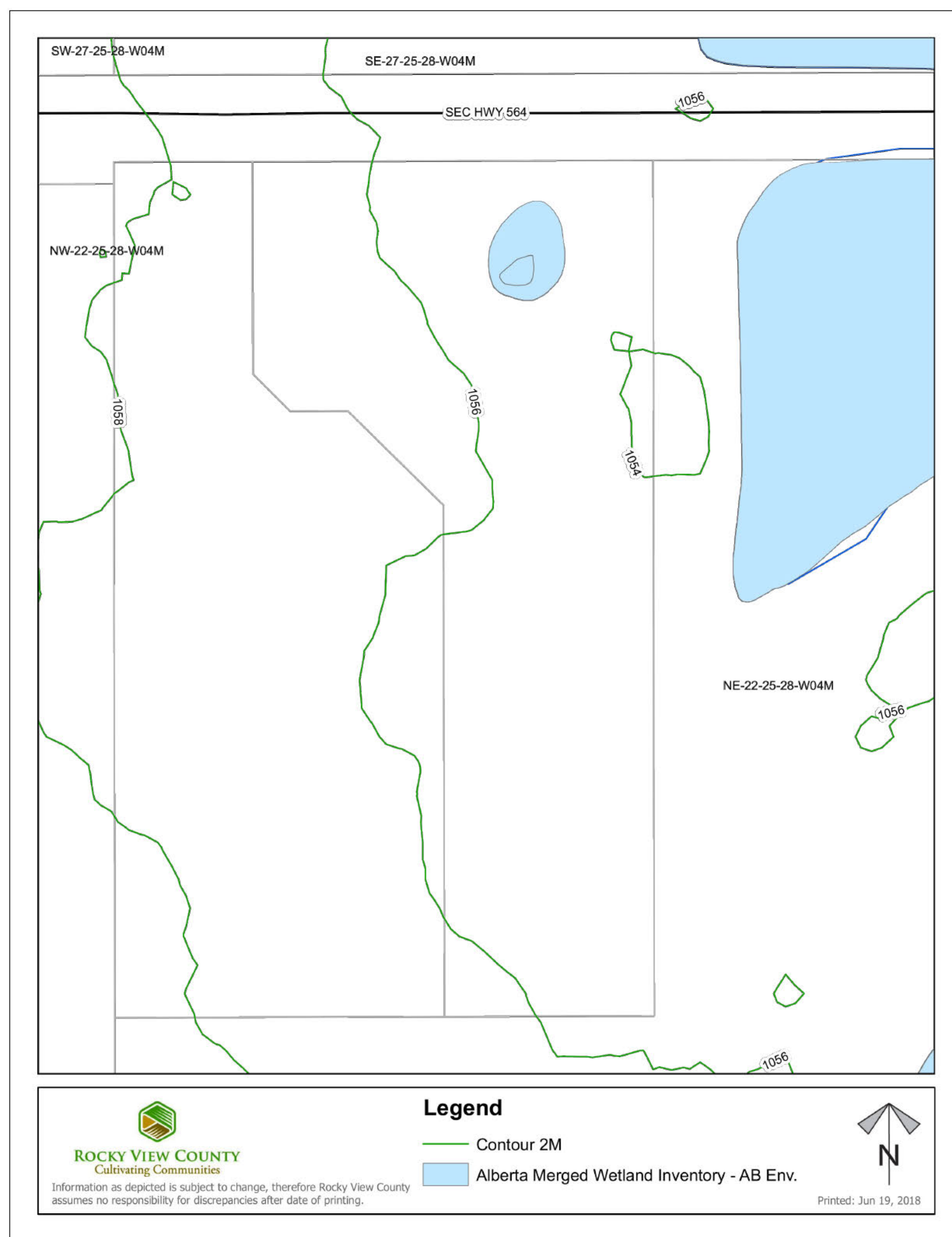
The purpose of our proposed Service Station and Convenience Store is to provide services to the traveling public and any local agricultural and commercial operations. The development is not in the vicinity of an identified business area but is small in scale, intensity, and scope, and will be an improvement that offers local business and employment opportunities.

The property's general topography slopes from West to the East down into a low area in the Northeast portion of the property. Overland drainage is to be directed into an onsite Stormwater Management Pond and any required wetland mitigation will conform to all applicable directives, regulations, and laws. The property's existing water well will provide all potable water. All waste water will be stored on site and then removed by truck to an appropriate off site treatment facility. Specific sizes and locations of all utilities on site will be determined at the development permit stage.

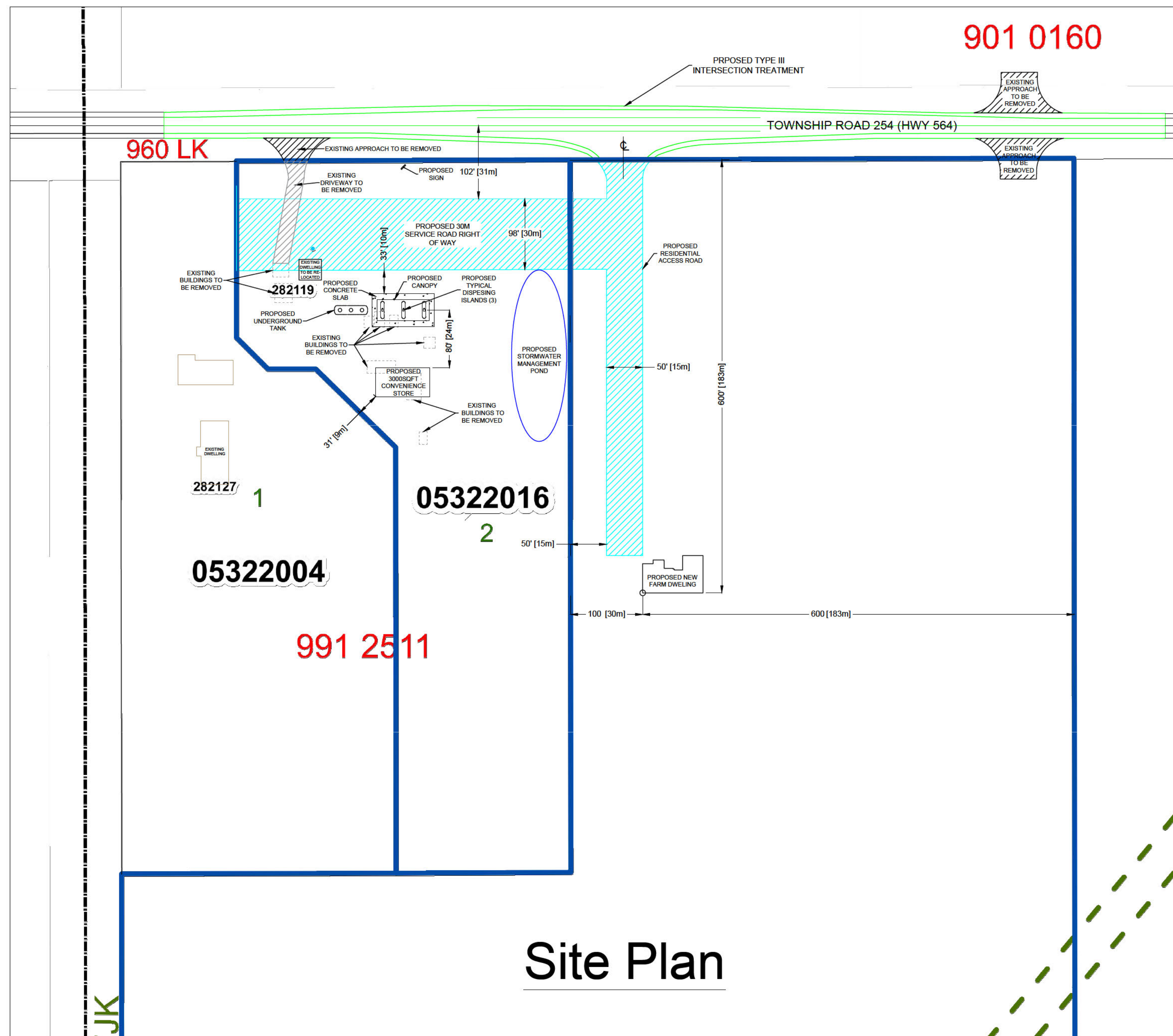
Please call or email me if you require any further information or have any questions you need answered.

Cordially,

Donald Handel



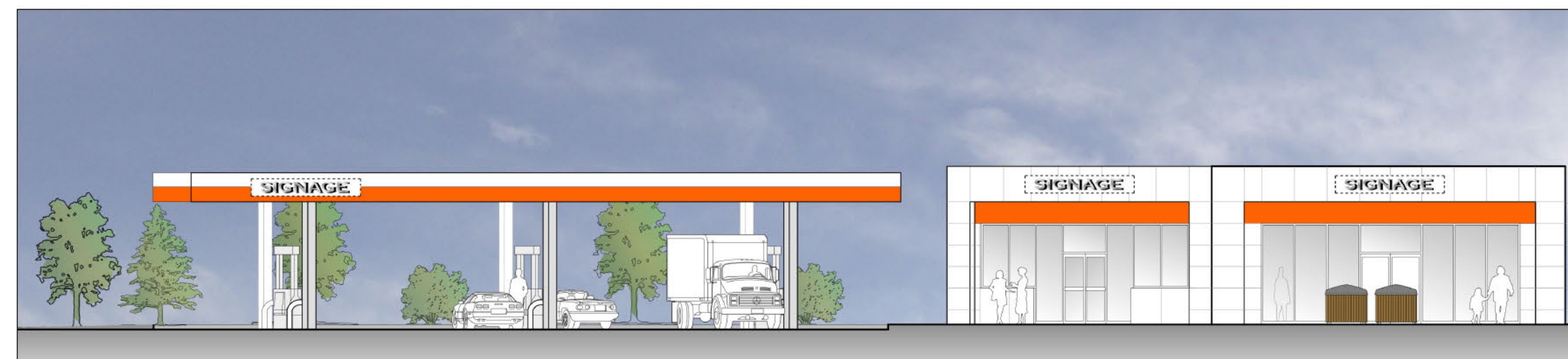
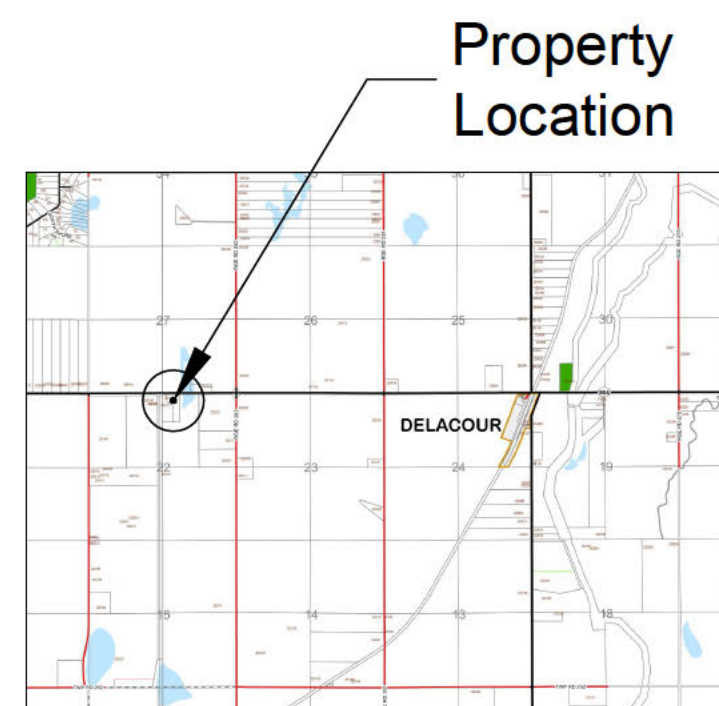
Area Topography



Site Plan



Site Picture



Typical Station and Store Elevation



GAS PLUS INC.  
5910 - 50 AVENUE S.E.  
CALGARY, ALBERTA, T2B 3C1  
PH: 403-273-4199 FAX: 403-569-0918  
Website: www.gasplusinc.com

Secs

GENERAL NOTES:

SITE INFORMATION

LEGAL DESCRIPTION -  
NE-22-25-28 W4M  
MUNICIPAL ADDRESS -  
TOWNSHIP ROAD 254 (HWY 564)  
ROCKY VIEW COUNTY, AB  
AREA OF LAND TO RECEIVE TOPSOIL -  
464.5 ACRES (426.53 HECTARES)  
  
ZONING  
  
LAND USE DESIGNATION -  
RF - RANCH AND FARM DISTRICT

No.	Revision/Issue	Date

Project Name and Address  
**GP SERVICE STATION & CONVENIENCE STORE  
HIGHWAY 564**  
282119 TOWNSHIP ROAD 254  
ROCKY VIEW COUNTY, ALBERTA

Drawing Title  
**SITE PLAN**

Project File 101.03152114.18.01	Sheet <b>A1</b>
Date JUNE 19, 2018	
Scale NOT TO SCALE	

**From:**  
**To:** [Johnson Kwan](#)  
**Cc:**  
**Subject:** File 05322016  
**Date:** Saturday, July 07, 2018 4:59:56 AM

---

Good morning

I would like to say that as landowners on RR 282 we are highly against any such proposal for a gas station on that corner. That is a blind intersection due to the hill and the semi's and cars that pull out onto Highway 564 have a history of causing issues.

There is already a convenience store down at the Railway tracks which is more than sufficient for the area. Conrich gas station is close by so is also ample for the area. Water tables are already being contaminated with the landowner for that area bringing in contaminated snow from his Gas Plus gas stations. (He was told to stop and remove it but allowed it melt instead of dealing with it). So absolutely not for a number of reasons!

If you have any questions regarding my concerns, please feel free to contact me at the below email or personal:

Or my husband, Don< @

Thank you for taking the time to address our concerns.

Karen Monaghan

This email may be privileged and/or confidential. Any distribution, use or copying of this email or the information it contains by other than an intended recipient is unauthorized. If you received this email in error, please advise the sender immediately. If you wish to no longer receive commercial electronic messages from the Calgary Exhibition and Stampede Limited, please respond and advise accordingly in your return email.

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**From:** karen  
**Sent:** Thursday, October 04, 2018 7:44 PM  
**To:** PAA\_ LegislativeServices  
**Cc:** jwan@rockyview.ca  
**Subject:** OPPOSE Bylaw C-7809-2018

Bylaw C-7809-2018 - a bylaw of Rockyview County for land use Bylaw C-4841-97

Good evening

I had previously submitted an email protesting the addition of a Gas Plus in this particular area. We live on Range Road 282 and would be directly affected by this addition.

Opposition points:

1. Traffic will be turning in and out of the station thus affecting flow of traffic as well as increasing the possibility of accidents.
2. There is a danger of tank leakage thus affecting water table quality
3. There is a Gas Plus in Conrich so there is no need for another in the area
4. There has already been a re-designation of land use on the SW corner of Range Road 282 and 564. This is an agricultural community and should not lose anymore land especially to a business that can affect the quality of the water and land.
5. We are directly affected by property value as our home has been directly affected by the land use change as noted in #4 as well as the CN railyard that was not there when we moved to the area in 1999.

We are unable to attend the hearing but hope that Rockyview will acknowledge our opposition and not allow our home to be affected by yet another business. We moved to Delacour area to be part of a rural community, not to be part of a business district.

Thank you for taking the time to listen to our concerns.

Karen and Don Monaghan

1126362 Alberta Ltd

June 25 2018

Rockyview County  
901 – 32 Ave NE  
Calgary, AB  
T2E 6X6

RE: File Number 05322016

Attn: Johnson Kwan

We are writing to comment on application PL20170102. My family has resided in the Delacour area for the past 4 decades. We are not in support of this proposed re-designation for the following reasons:

1. The site has common ownership with a 71 acre parcel adjacent to it. It is clear that the owner has larger plans in place given the previous attempts to add fill to the land without approvals in place
2. This is an agriculture district. Delacour is a growth node as is Conrich and both haven't seen their Area structure plans come to fruition. As a county we can't repurpose Agriculture land for non-agriculture purposes.
3. The proposed site is within 1km of a large confined feeding operation that is under reconstruction.
4. The site will increase the traffic in what is now a 100km an hour zone. Significant road upgrades would be necessary to support the proposed re-designation.

We would like to be circulated via fax on any documents that may be available on the proposed development going forward – please fax to me directly at \_\_\_\_\_ as we would appreciate more details to make a fair determination. We trust you will find everything you need. Please contact me with any questions.

Thanks,

A handwritten signature in blue ink, appearing to read "Muneer Gilani".

Muneer Gilani

## PLANNING SERVICES

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<b>TO:</b>	Council	
<b>DATE:</b>	October 23, 2018	<b>DIVISION:</b> 9
<b>TIME:</b>	Afternoon Appointment	
<b>FILE:</b>	06816005 & 06814007	<b>APPLICATION:</b> 1042-155
<b>RE:</b>	Rocky View County/Town of Cochrane – Annexation	

---

### **<sup>1</sup>POLICY DIRECTION:**

The proposed Rocky View County/Town of Cochrane annexation was reviewed in accordance with the County Plan and the Cochrane North Area Structure Plan and was found to be compliant with the growth and servicing policies and objectives of the County.

### **EXECUTIVE SUMMARY:**

The Town of Cochrane (the Town) formally notified Rocky View County (the County) and the Municipal Government Board (MGB) of a proposed annexation of 64.33 hectares (158.97 acres) of land located in the northwest of the County and immediately north of the town of Cochrane. Although negotiations proceeded through 2017, the timing of the 2017 municipal elections affected community engagement, representatives on the Negotiating Committee, and the inclusion of additional lands in the annexation. At its meeting on January 23, 2018, Council resolved to proceed with negotiations on the basis of the revised notification.

#### *Annexation proposal regarding SW-16-26-4-W5M*

The intent of the annexation of SW-16-26-4-W5M is to provide a site for development of a Rocky View Schools High School, as well as recreational uses and other public uses. The lands were the subject of an application for redesignation to Public Services District in order to facilitate the development of a public school, but the application was ultimately refused by Council on May 12, 2015.

#### *Annexation proposal regarding Block 1 Plan 1364LK within NW-14-26-04-W05M*

Correspondence was received from the Town on December 28, 2017, requesting the inclusion of another property for the purposes of the annexation. The additional property is situated to the northeast of the town, and is geographically separated from the property that is the subject of the initial request (see Appendix 'A'). The intent of the annexation of Block 1 Plan 1364LK is to provide for intersection upgrades associated with the development of the Cochrane Sunset Ridge community situated in the northeast of the town.

#### *Annexation Negotiations*

The Annexation Negotiation Committee commenced meeting with representatives from the Town of Cochrane on April 18, 2018, and reached an agreement in principle on the proposed annexation. The agreement in principle was presented to Town of Cochrane Council on May 14, 2018, and Rocky View County Council on May 22, 2018. Both Councils directed Administration to proceed with public engagement regarding the proposed annexation and report back to Council.

---

<sup>1</sup> **Administration Resources**  
 Matthew Wilson, Planning Services





### *Public Engagement*

On June 27, 2018, the Town of Cochrane hosted the 'Let's Talk Cochrane BBQ': a public engagement session intended to provide residents with information on a broad range of topics including the proposed annexation. The event was attended by 115 people who provided Administration with feedback on the annexation. People were supportive of the proposal, as it provided enhanced recreational opportunities, and provided a solution to a transportation challenge in Sunset Ridge (See Appendix C for details). Throughout the spring and summer, interested residents and stakeholders were also provided additional information on the proposed annexation via the 'Let's Talk Cochrane' website. As well, poster boards were placed in the Cochrane Visitors Information Centre and Town Administration offices. Staff representatives from both Rocky View County and the Town of Cochrane responded to questions on the proposed annexation during this period.

If Council approves the annexation, the Town of Cochrane would subsequently prepare the formal annexation application to the Province of Alberta for submission prior to the end of October 2018, thereby formally commencing the Provincial annexation process, which is anticipated to take until the Spring of 2019.

## **BACKGROUND AND ASSESSMENT**

### *Annexation proposal regarding SW-16-26-4-W5M*

On January 20, 2017, the Town notified the County and the MGB of a proposed annexation of 64.33 hectares (158.97 acres) of land located in the northwest of the County and immediately north of the town of Cochrane. The annexation area encompasses a single titled parcel comprising SW-16-26-4-W5M, at the northeast junction of Horse Creek Road and Township Road 262 (see Appendix 'A').

The subject land contains a dwelling and an accessory building in the southeastern portion of the property. The remainder of the land is currently used for pasture. The Cochrane Extraction Plant (gas plant) operated by Inter Pipeline is situated immediately to the northeast of the subject land on NE-16-26-4-W5M.

The Town identified that the annexation of these lands would provide for the logical extension of servicing, transportation routes, pathway and adjacent school sites within the Heritage Hills community located immediately to the south. The Town does not wish to commit resources to the purchase and servicing of lands outside its jurisdiction.

### *Annexation proposal regarding Block 1 Plan 1364LK within NW-14-26-04-W05M*

The intent of the annexation of Block 1 Plan 1364LK is to provide for intersection upgrades associated with the development of Cochrane's Sunset Ridge community, situated in the northeast of the town. The Cochrane North ASP identifies the subject land as a future growth area intended for higher densities with appropriate servicing. However, the related policies require the amendment of the ASP prior to any conceptual scheme planning, redesignation, subdivision or development applications. Further, the conceptual road network (figure 8 of the ASP) identifies these lands as encumbered by a proposed main road and an intersection with Highway 22. While the Town of Cochrane acknowledges that the newly proposed intersection arrangement deviates from the original Sunset Ridge transportation network proposal, the Town considers the revised arrangement to be a better solution.



The County recognizes the growth pressures facing the region and the need for additional land for urban centers. The proposed annexation provides an opportunity to develop mutually beneficial solutions to growth needs. Map 1 within the County Plan defines the key growth areas for Rocky View County, which were considered in the process of the annexation negotiations. SW-16-26-4-W5M is located in the Agricultural Area of the County Plan, in which institutional uses are considered on the basis of justification for the proposed location, benefit to broader public, compatibility with surrounding lands uses, and infrastructure capacity. It is considered that the proposed development of the lands for institutional uses would be consistent with the objectives and policies of the County Plan. Block 1 Plan 1364LK is situated within the area of the Cochrane North Area Structure Plan, and is identified as an area for future growth (figure 6 of the ASP). The future growth area recognizes long-term residential development at higher densities with appropriate transportation and utility services. The proposed inclusion of these lands within the Town of Cochrane is consistent with the development objectives of the Area Structure Plan.

**OPTIONS**

Option # 1: Motion #1: THAT the proposed Rocky View County/Town of Cochrane annexation regarding SW-16-26-4-W5M and Block 1, Plan 1364 LK within NW-14-26-04-W05M be approved.

Motion #2: THAT Administration be directed to proceed with the formal submission of the proposed annexation to the Municipal Government Board.

Option # 2 THAT alternative direction be provided.

Respectfully submitted,

Concurrence,

“Chris O’Hara”

“Rick MacDonald”

\_\_\_\_\_  
General Manager

\_\_\_\_\_  
County Manager

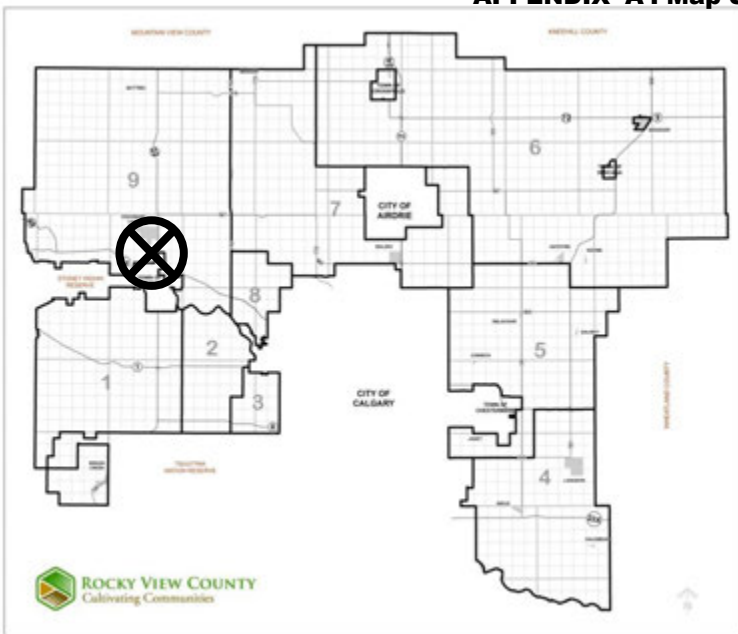
MW/rp

**APPENDICES:**

APPENDIX ‘A’: Map Set

APPENDIX ‘B’: Report of Negotiations – Annexation 2018

APPENDIX ‘C’: WHAT WE HEARD REPORT – Annexation 2018



LOCATION PLAN

SW-16-26-04-W05M &

Block:1 Plan:1364 LK within NW-14-26-04-W05M

Date: Jan 11, 2018

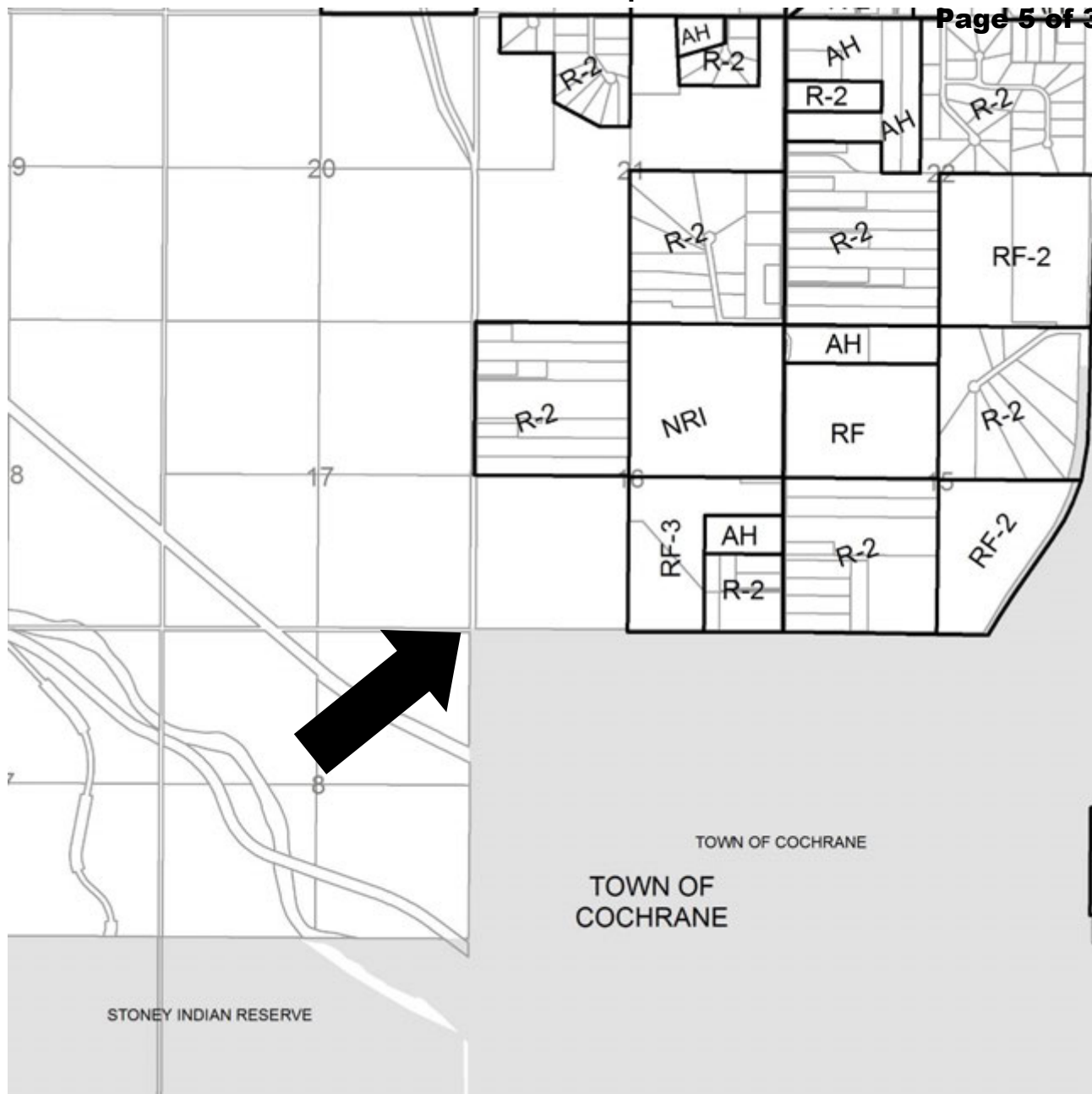
Division # 9

File: 06814007 & 06816005

AGENDA

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STONEY INDIAN RESERVE

TOWN OF COCHRANE

TOWN OF COCHRANE

RF2	Ranch and Farm Two	B-1	Highway Business
RF3	Ranch and Farm Three	B-2	General Business
AH	Agricultural Holding	B-3	Limited Business
F	Farmstead	B-4	Recreation Business
R-1	Residential One	B-5	Agricultural Business
R-2	Residential Two	B-6	Local Business
R-3	Residential Three	NRI	Natural Resource Industrial
DC	Direct Control	HR-1	Hamlet Residential Single Family
PS	Public Service	HR-2	Hamlet Residential (2)
		HC	Hamlet Commercial
		AP	Airport

# LAND USE MAP



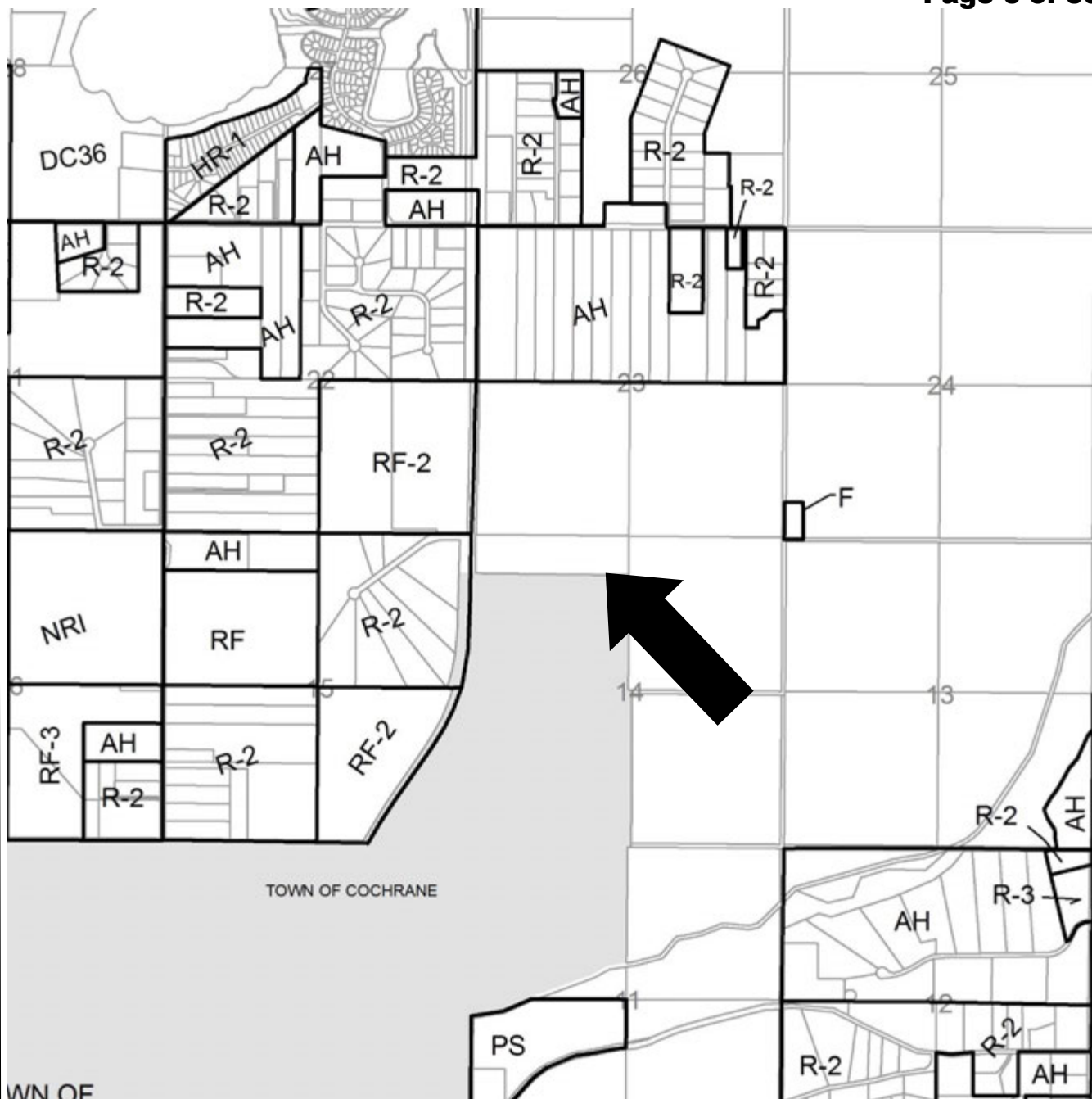
**SW-16-26-04-W05M &  
Block:1 Plan:1364 LK within NW-14-26-04-W05M**

Date: Jan 11, 2018

Division # 9

File: 06814007 & 06816005

**AGENDA**



RF2	Ranch and Farm Two	B-1	Highway Business
RF3	Ranch and Farm Three	B-2	General Business
AH	Agricultural Holding	B-3	Limited Business
F	Farmstead	B-4	Recreation Business
R-1	Residential One	B-5	Agricultural Business
R-2	Residential Two	B-6	Local Business
R-3	Residential Three	NRI	Natural Resource Industrial
DC	Direct Control	HR-1	Hamlet Residential Single Family
PS	Public Service	HR-2	Hamlet Residential (2)
		HC	Hamlet Commercial
		AP	Airport

# LAND USE MAP



**SW-16-26-04-W05M &  
Block:1 Plan:1364 LK within NW-14-26-04-W05M**

Date: Jan 11, 2018

Division # 9

File: 06814007 & 06816005

**AGENDA**

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Contours are generated using 10m grid points, and depict general topographic features of the area. Detail accuracy at a local scale cannot be guaranteed. They are included for reference use only.

## TOPOGRAPHY

*Contour Interval 2 M*



### SW-16-26-04-W05M & Block:1 Plan:1364 LK within NW-14-26-04-W05M

Date: Jan 11, 2018

Division # 9

File: 06814007 & 06816005

**AGENDA**

**Page 47 of 446**



Note: Post processing of raw aerial photography may cause varying degrees of visual distortion at the local level.

**AIR PHOTO**

*Spring 2016*

**SW-16-26-04-W05M &**

**Block:1 Plan:1364 LK within NW-14-26-04-W05M**

Date: Jan 11, 2018

Division # 9

File: 06814007 & 06816005

**AGENDA**

**Page 48 of 446**







Note: Post processing of raw aerial photography may cause varying degrees of visual distortion at the local level.

**AIR PHOTO**

Spring 2016



**SW-16-26-04-W05M &  
Block:1 Plan:1364 LK within NW-14-26-04-W05M**

Date: Jan 11, 2018

Division # 9

File: 06814007 & 06816005

**AGENDA**

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**TOWN OF COCHRANE / ROCKY VIEW COUNTY**

**REPORT ON NEGOTIATIONS**

**PROPOSED ANNEXATION**

**OCTOBER 2018**

In accordance with the provisions of the Municipal Government Act, the following report has been prepared to provide a detailed description of the results of the negotiations on the proposed annexation of lands from Rocky View County by the Town of Cochrane.

## BACKGROUND

The Town of Cochrane and Rocky View County have been in discussions regarding the proposed annexation since 2015, when Rocky View Schools first purchased approximately +/- 158 acres within the SW ¼ 16-26-4-W5M.

These lands are located immediately north of the current boundary of the Town of Cochrane, where Rocky View Schools identified the need for a future high school site. However, as part of their purchase in 2015, Rocky View Schools clarified that they would only require approximately +/- 25- 30 acres. Rocky View Schools subsequently approached the Town of Cochrane regarding an opportunity to purchase of the balance of the property which could be utilized for future recreational uses, with significant benefits for the Town and its residents.

The Town of Cochrane was attracted by the various opportunities this property could potentially provide the community, but Administration also cautioned against the purchase unless they were within the Town's jurisdiction. In order to make the financial commitment to purchase and develop these lands, it would be imperative for these lands to be within the Town's jurisdiction and control. Rocky View Schools also confirmed their desire for their future school to be located within the Town's boundary and to be a fully serviced site, ready for future development when required.

Throughout 2016 Administration had discussions with Rocky View County regarding the lands proposed for annexation and highlighted the unique opportunity for the Town and County to collaborate on a future regional recreation and public use solution.

In response, Town of Cochrane Council passed the following motion on January 9, 2017:

*RES # 12/01/17 That Council direct Administration to prepare a Notice of Annexation for the SW ¼ of 16-26-4-W5M in accordance with Section 116(1) of the Municipal Government Act and to begin collaborative and good faith negotiations with the Rocky View County regarding the proposed annexation of these lands into the boundary of the Town of Cochrane.*

On January 20, 2017, Administration provided to Rocky View County and the Municipal Government Board a Formal Notice of Annexation in accordance with Section 116 of the Municipal Government Act. Both organizations formally acknowledged the receipt of this correspondence and the annexation process commenced.

In February 2017, Rocky View County and Town of Cochrane Council appointed two members of Council and a member of Administration, or a designate, to the Annexation Negotiation Committee. This Committee was tasked with discussing the proposed annexation and reporting back to their respective Councils. The Committee met in April 2017 and directed both Administrations to investigate a number of different matters before reporting back to the Committee and proceeding with the annexation process.

However, once each matter was fully investigated and ready for further discussion by the Committee, summer had arrived. As the next steps in the annexation process involved community engagement, it was determined that proceeding at that time would not be appropriate. With the pending election in the Fall of 2017, it was also determined that it would be best to revisit this matter once the new Councils were in place in each municipality.

Following the 2017 Municipal Election and with new representation on both Councils, annexation discussions between Rocky View County and the Town of Cochrane began to also focus on a property immediately north of Sunset Ridge, where future long term access for both Town and County residents needs to be formally addressed.

In response, Town of Cochrane Council passed the following motion on December 11, 2017:

*RES # 330/12/17 That Council direct Administration to prepare a Notice of Annexation for Plan 1364LK, Block 1 in accordance with Section 116(1) of the Municipal Government Act and to begin collaborative and good faith negotiations with the Rocky View County regarding the proposed annexation of these lands into the boundary of the Town of Cochrane.*

On January 8, 2018, Cochrane Council passed the following motion in order provide new representation on the Town's Annexation Negotiation Committee:

*RES # 07/1/18 Moved by Councillor Reed that Council appoint Mayor Genung and Councillor Reed, with Councillor Flowers appointed as an alternate and the Senior Manager, Development Services, or a designate, to represent the Town of Cochrane on the Annexation Negotiation Committee with Rocky View County.*

**ANNEXATION NEGOTIATION**

The Annexation Negotiation Committee met on April 18, 2018 to commence the annexation process. Prior to initiating formal negotiations, the Committee was provided a comprehensive summary of the Provincial annexation process by Rick Duncan, Case Manager, from Municipal Affairs and signed off on Negotiation Protocols that had been mutually developed. This presentation and the Negotiation Protocols helped establish the foundation for the negotiation process.

Following preliminary negotiations, the Annexation Negotiation Committee successfully reached an Agreement in Principle on *all matters* related to the proposed annexation and there were no matters of no agreement between the two parties. **Section 118(1) a, a.1**

All the matters outlined in the Agreement in Principle are listed below:

**Area A: SW ¼ 16-26 -4-W5M**

- Transfer of these lands from the jurisdiction of Rocky View County to the Town of Cochrane

***Terms:***

- Town of Cochrane agrees to the approval of a future stormwater line within the Horse Creek Road Right of Way (line assignment) intended to provide a stormwater solution for future development within the Cochrane Lakes area provided the outfall is located west of the Town boundaries
- The future capacity of the future stormwater line will be determined at the design stage
- Rocky View County agrees to offer the Town of Cochrane access to the future stormwater line for the development of the SW ¼ 16-24-4-W5M, subject to design and cost considerations.
- The future capacity the Town of Cochrane has access to is to be determined
- Town of Cochrane to explore the use of stormwater from SW ¼ 16-24-4-W5M and Cochrane Lakes for the irrigation of future recreational fields proposed onsite
- As Rocky View County currently does not collect taxes for this property owned by Rocky View Schools, there will be no monetary compensation for the annexation of these lands

**Area B: Plan 1364LK Block 1**

- Transfer of these lands from the jurisdiction of Rocky View County to the Town of Cochrane

***Terms:***

- Town of Cochrane ensures that ultimate access and intersection on Highway 22 is provided through the annexation Plan 1364LK Block 1
- Town of Cochrane to ensure that access to the future proposed roadway is provided for future access to policy lands within the Cochrane North Area Structure Plan immediately north of Plan 1364LK Block 1
- Rocky View County collects approximately \$8,000 annually in taxes on the subject property, as such, the Town of Cochrane agrees to compensate Rocky View County for the loss of this tax assessment for a period of 10 years as part of the formal annexation agreement (\$80,000)

**Key Aspects**

- Cooperative and collaborative agreement that benefits both municipalities
- **Area A** – Protects the Town of Cochrane drinking water while meeting the stormwater needs of Rocky View County to allow for future development in the Cochrane Lakes area
- **Area B** – Provides the opportunity to ensure the ultimate access to Sunset Ridge in constructed in the interests of Rocky View County and the Town of Cochrane residents (enhanced safety)
  - Will allow the Sunset Ridge Stage 3 Neighbourhood Plan to proceed
  - Will provide enhanced future access to lands located to the north and east in Rocky View County and in the Cochrane North Area Structure Plan

The Agreement in Principle was presented to Town of Cochrane Council on May 14, 2018 and Rocky View Council on May 22, 2018, where both Councils accepted this as information, and directed Administration to proceed with public engagement regarding the proposed annexation.

**PUBLIC CONSULTATION**

Please see attached Let's Talk Cochrane – Annexation 2018 for a comprehensive summary of the Public Consultation process completed for the proposed annexation. **Section 118(1) b, c**

## Municipal Authority Confirmation

I hereby confirm that the Report of Negotiations and the attached *What We Heard – Annexation 2018* document accurately reflect the results of the negotiations on the proposed annexation between the Town of Cochrane and Rocky View County.

### **Section 118(2)**

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Dave Devana, CAO  
Town of Cochrane  
Initiating Municipal Authority

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Rick McDonald, Interim CAO  
Rocky View County  
Responding Municipal Authority

**WHAT WE HEARD REPORT**  
Proposed Annexation  
Public Consultation Process

**July 2018**





## ABOUT THE PROJECT

Cochrane is one of the fastest growing municipalities in Alberta and has experienced considerable growth and development over the past decade. This rapid growth has presented Cochrane with new and diverse development challenges with an ever-increasing level of complexity.

In response and through discussions with Rocky View County, The Town of Cochrane has identified two parcels, comprised of approximately 200 acres of land within the County, that are proposed to be annexed into the Town's boundary. Unlike traditional annexations, the proposed annexation is not based on future growth, but rather intended to overcome technical development challenges associated with the provision of secondary access for the community of Sunset Ridge and the unique opportunity for the provision of a future high school and a large recreation property for the benefit of both Town of Cochrane and Rocky View County residents.

### **Subject Parcel 1: SW 1/4 of 16-26-4-W5M**

The Town envisions that a portion of the subject quarter section would be developed as a future High School Site by Rocky View Schools, while the balance of the property would be developed for recreational & public uses to meet the future recreational needs of Cochrane and the surrounding community.

The Town also recognizes there are also other inherent benefits that support the annexation of these lands into Cochrane, including the logical extension of utility servicing, transportation routes, pathway and adjacent school sites within the Heritage Hills community located immediately to the south. In addition, this would eliminate the need to service a site outside the boundary of the Town of Cochrane.

### **Subject Parcel 2: Plan 1364 LK**

The community of Sunset Ridge has experienced significant growth and requires a secondary access for the continued growth of the community. The Developer of Sunset Ridge, Melcor, had proposed to provide an interim secondary access located within the Town's current boundaries as part of their Stage 3 Neighbourhood Plan. However, Rocky View County, the Town of Cochrane, Sunset Ridge residents and adjacent landowners within the County expressed concerns with the interim proposal. As a result, the Town of Cochrane, Rocky View County, Melcor and the owner of the subject lands have collaborated on an annexation proposal intended to provide a permanent solution for secondary access for this community. Once the access has been provided, the balance of the lands will be planned for future residential development within the Town of Cochrane.

## LET'S TALK COCHRANE BBQ - OVERVIEW

On June 27, 2018, the Town of Cochrane hosted the Let's Talk Cochrane BBQ. This public event was held for Town of Cochrane and Rocky View residents, business owners and other affected stakeholders to provide comments on a variety of different proposals, including the proposed annexation.

The event was advertised on the Town of Cochrane & Rocky View County websites, through social media, and via signage in the Town, and a total of **115 participants** attended.



At the event, participants were provided the opportunity to discuss the proposed annexation with planning staff and members of Council from both the Town of Cochrane and Rocky View County.

All comments were recorded, and participants also had the opportunity to write down any additional comments they had. The following chart provides an overview of all comments received.

<b>General Comments:</b>
Make trail from Sunset to Cochrane High/Tri Schools area
Need more bike trails
Finish trails from Sunset to Town before 1A/22 work
More Rec Spaces needed
Open walkway/roads between Heartland and West Valley
Build a fieldhouse for multi-sport use
Expand main highway to avoid/lessen traffic
Sidewalks by 1A at Heartland
More trails, parks, schools
More Fire personnel, fire equipment and fire halls prior to anymore development
Bike/stroller connection from Sunset to Town
We need a water park!!
More transit
Connect Heartland to Town with trail
Schools
<b>Specific to Proposed Annexation:</b>
Highly agree with both annexations. More sports fields/diamonds are really needed
Agree with more fields and sporting areas
Yes. But no more further development
Is Site A the right site for a High School? What about Sunset?

## Summary

There was a tremendous response to the first annual Let's Talk Cochrane BBQ with 115 participants in attendance. Overall participants were in support of the proposed annexation, with one lone expressing concerns as he would have preferred to see a future high school site within the community of Sunset Ridge.

The comments provided have been summarized above into two categories, one for general comments provided and another for those specific to the proposed annexation. Overall the comments reflect the common desire for more recreation spaces and amenities within the Town, which is a central aspect of this proposal. Therefore, it appears that there is significant community support for the proposed annexation.

## ADDITIONAL PUBLIC CONSULTATION EFFORTS

Throughout the annexation process, the Town of Cochrane and Rocky View County have been committed to ongoing public consultation efforts to raise awareness and increase transparency regarding the proposed annexation.

The following list provides a summary of the additional public consultation efforts that were undertaken:

- **Let's Talk Cochrane website**
- **Project Signage**
  - **Town of Cochrane Administration Building**
  - **Town of Cochrane Visitor Information Centre**
- **Non-Statutory Public Hearings**
  - **Town of Cochrane – Tuesday, October 9, 2018**
  - **Rocky View County – Tuesday, October 23, 2018**
- **Advertising of the Non-Statutory Public Hearings**
  - **Cochrane Eagle**
  - **Rocky View Weekly**
- **Direct mail out to adjacent landowners in Rocky View County to advise the previously scheduled Non-Statutory Public Hearings**

### Let's Talk Cochrane Website

In February 2018, the Town of Cochrane launched LetsTalkCochrane.ca – a new public engagement portal. The vision for Let's Talk Cochrane was to provide an online tool to help the community connect on a wide variety of topics, obtain critical project information and provide community feedback on those proposals.

The proposed annexation project was added to the website in May 2018 and has been regularly updated throughout the process. For more information, please visit:

<http://www.letstalkcochrane.ca/annexation-north/>



**Project Signage**

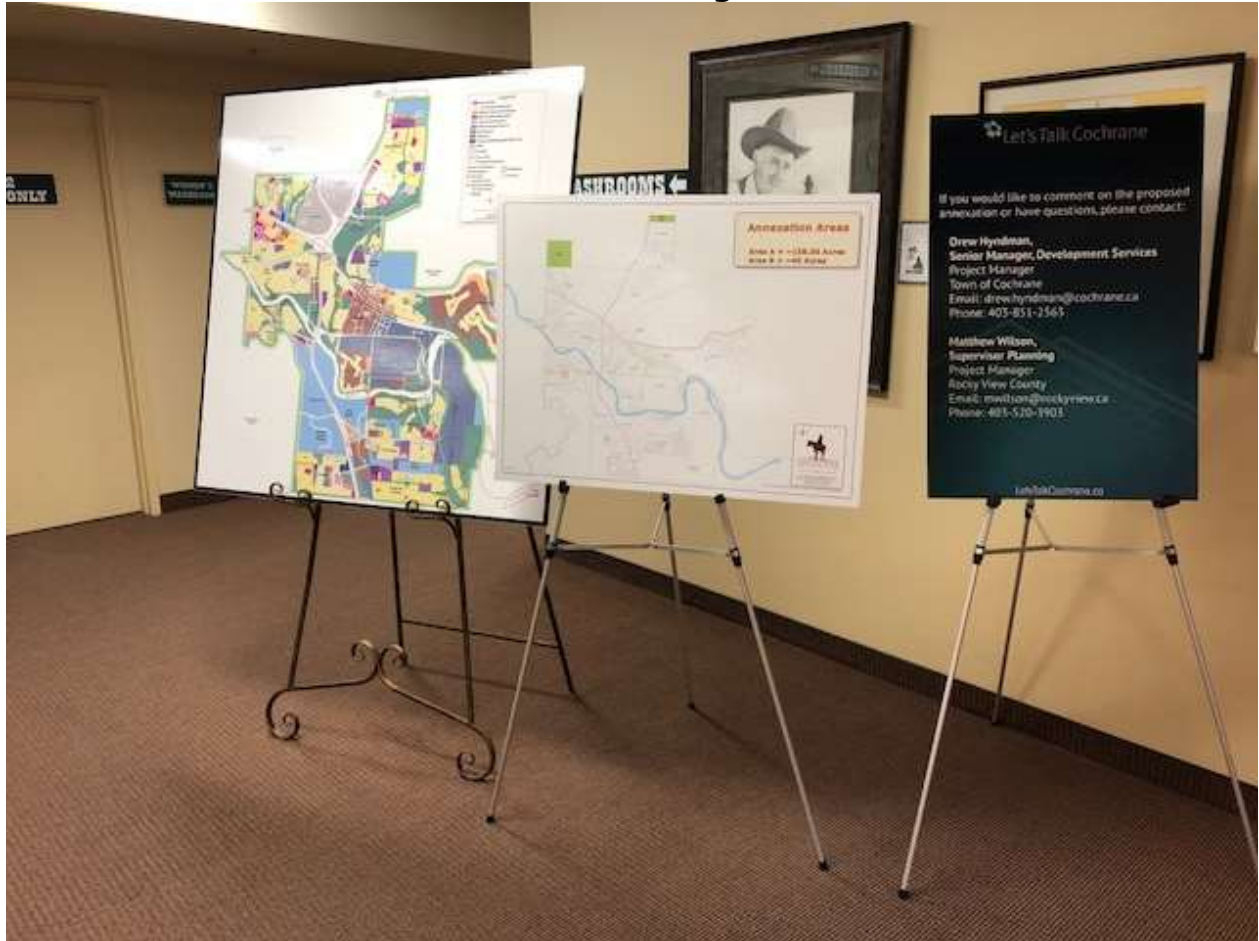
Since May 2018, the Town of Cochrane has utilized signage regarding the proposed annexation at two strategic locations, within the lobby of the Administration Building adjacent to reception and at the Visitors Information Centre in downtown Cochrane.

Both locations are regularly frequented by residents, businesses and affected stakeholders from Cochrane and Rocky View County. Over the past several months, these signs have raised awareness and allowed for people to get more information on the proposed annexation.

Attached you will find a rendering on the main signage used along with a map of the proposed annexation. Photos of the two locations utilized have also been included.



**Town of Cochrane – Administration Building**



**Town of Cochrane – Visitors Information Centre**



**Non-Statutory Public Hearings:**

While not a legislated requirement of the annexation process, the Annexation Negotiation Committee for the Town of Cochrane and Rocky View County wanted to ensure that residents, businesses and affected stakeholders had the opportunity to provide each Council their input before a final decision was made.

As a result, two separate Non-Statutory Public Hearings took place on October 9, 2018 at the Town of Cochrane and October 23, 2018 at Rocky View County.

In advance of these Non-Statutory Public Hearings, advertisements were taken out in both the Cochrane Eagle and the Rocky View Weekly, published in accordance with the regulations of the Municipal Government Act. Please see below for a copy of the advertisements from each paper.

Notification of the previously scheduled Non-Statutory Public Hearings were also mailed to the directly affected residents within Rocky View County. Please see the Appendix for a copy of the letters sent, as well as the circulation area where affected residents received this notification.

**MUNICIPAL***matters*  
www.cochrane.ca | Ph: 403-851-2500 | Fax: 403-932-6032 | 101 RancheHouse Road, Cochrane, AB T4C 2K8  
cochrane  
HOW THE WEST IS NOW

---

**NON STATUTORY PUBLIC HEARING NOTICE**

 **Non-Statutory Public Hearing - Proposed Annexation**  
Tue Oct 9, 2018 | 6pm | Cochrane RancheHouse  
Any member of the public may attend to hear comments.

**What does this mean?**  
This is a Non-Statutory Public Hearing for residents, stakeholders and affected parties to provide Council with input on the proposed annexation of two parcels from Rocky View County.

**How can I get more information?**  
Any member of the public may review the proposed Annexation and ask questions. Come to the Cochrane RancheHouse (101 RancheHouse Rd) between 8:30am and 4:30pm, Mon to Fri. Planning staff can explain what's proposed, how it might impact you and how the public hearing process works.

**Can I provide comments?**  
Interested parties can submit oral and written comments about the proposed Annexation in person at the public hearing or by email before the public hearing.

**Annexation Area**  
Area A is ~150.00 Acres  
Area B is ~40 Acres



**Questions?**  
Call us:  
403-851-2570



**Speak at the Public Hearing (5 minute time limit)**  
Please arrive before the start time and sign the speakers' list. If you're making a presentation, please send us a copy by 10am Thu, Oct 4, 2018: Town of Cochrane, 101 RancheHouse Rd or [planning@cochrane.ca](mailto:planning@cochrane.ca) (quote Annexation).



**By email:** Send comments or questions: [planning@cochrane.ca](mailto:planning@cochrane.ca) (quote Annexation)

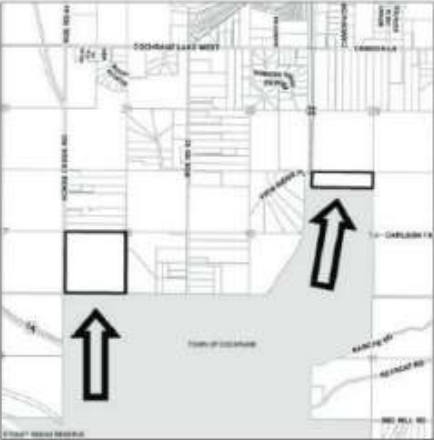
This notice is given pursuant to Section 606 of the Municipal Government Act, Revised Statutes of Alberta, 2000, Chapter M-26, as amended. Advertised Sep 27, 2018 and Oct 4, 2018

# PUBLIC NOTICE

## PUBLIC HEARING - First Notice Proposed Annexation

Public Hearing Date: **Tuesday, October 23, 2018**  
Written submission Deadline: **8:00 a.m., Tuesday, October 23, 2018**

**DIVISION 9** – Public hearing to consider the proposed annexation by the Town of Cochrane of Block 1 Plan 1364LK within NW-14-26-04W05M for intersection upgrades and SW-16-26-4-W05M for public purposes. SW-16-26-4-W05M is located at the northeast junction of Horse Creek Road and Township Road 262 immediately north of the Town of Cochrane, and Block 1 Plan 1364LK within NW-14-26-04W05M is located east of the junction of Highway 22 and View Ridge Place immediately north of the Town of Cochrane.  
**FILE: 1042-155**




**Please note that this public hearing will be held at the new County Hall:  
262075 Rocky View Point, Rocky View County, AB**

**Important information:**

1. A public hearing will be held at Rocky View County Council Chambers, 262075 Rocky View Point, Rocky View County, AB, on the date indicated.
2. The proposed annexation may be viewed beginning on the Wednesday before the hearing date at [www.rockyview.ca/CouncilAgenda](http://www.rockyview.ca/CouncilAgenda) or inspected at the Rocky View County Municipal Building between the hours of 8:30 a.m. and 4:30 p.m., excluding weekends and holidays.
3. Anyone who believes they are affected by the proposed annexation will be given an opportunity to be heard on the matters contained in the bylaw that is the subject of the hearing. To be heard, attend the public hearing at the time, date, and location indicated. If your verbal presentation includes a written component, you must bring 20 copies for distribution.
4. If you are unable to attend the public hearing you may make a written submission on the proposed annexation. Written submissions must be received by the deadline indicated to be included in the public hearing agenda. Rocky View County Council may vote to accept late submissions, but meeting the deadline ensures your input is part of the public record. Your name, address or legal land description, and the bylaw number must be included with your submission.
5. Your written submission must be sent before the indicated deadline to the Deputy Municipal Clerk by one of these methods:
  - **Mail or deliver** to the Deputy Municipal Clerk's Office, Rocky View County, 262075 Rocky View Point, Rocky View County, AB, T4A 0X2; OR
  - **E-mail** to [legislative@rockyview.ca](mailto:legislative@rockyview.ca) with the bylaw number in the subject line.
6. Your response to this advertisement will be considered your consent to the public distribution of your written or verbal submission as part of the public record for this hearing, including any personal information you provide.
7. If you have any questions about the proposed annexation contact Planning Services at 403-520-8166. If you have questions on the public hearing process contact Legislative & Legal Services at 403-520-8197 or 403-520-1651.

[www.rockyview.ca](http://www.rockyview.ca)



**ROCKY VIEW COUNTY**  
Cultivating Communities



**Non-Statutory Public Hearing Feedback:**

**To be completed following the Town of Cochrane & Rocky View County Public Hearings.**

**APPENDIX**

- Let's Talk Cochrane website documents
- Non-Statutory Public Hearing Notice
- Landowner Circulation Area – Rocky View County



October 4, 2018

**Drew Hyndman, Senior Manager, Development Services**

Project Manager

Town of Cochrane

Email: [drew.hyndman@cochrane.ca](mailto:drew.hyndman@cochrane.ca)

Phone: 403-851-2563

**Matthew Wilson, Supervisor Planning**

Project Manager

Rocky View County

Email: [mwilson@rockyview.ca](mailto:mwilson@rockyview.ca)

Phone: 403-520-3903

Re: Town of Cochrane (the "**Town**") Application for Annexation (north) SW ¼ 16-26-4 W5M  
(the "**Southwest Quarter Section**") and Plan 1364 LK

We are writing to your office to indicate our support with respect to the Town's proposed annexation of the above referenced lands.

Our support is based on our discussion with your respective offices that the Southwest Quarter Section of the Cochrane Extraction Plant (the "**Extraction Plant**") proposed for annexation shall be used as: (i) a high school (Rocky View Schools); (ii) a recreational field for uses ancillary to the high school; and (iii) a property for the school bus yard/shop.

It is our preference that future development on the subject site should be conditional upon the following:

1. The building for the high school shall not be developed within the one (1) kilometre emergency response planning zone around the Extraction Plant. Such restriction does not apply to the recreational field and bus yard/shop.
2. There will be no residential or overnight uses on the Southwest Quarter Section lands.
3. Egress away from the Extraction Plant will be incorporated into the plan of sub-division for the Southwest Quarter Section lands.

Our approach to the potential development on the subject site represents a "baseline" condition. Given the location of the subject site, a comprehensive review of the planning framework will be required by the Town and we look forward to being part of such review/process.

Thank you for working with us on this initiative and if you require anything else please feel free to give me a call at (403) 290-2643.

Yours truly,

**INTER PIPELINE LTD.**

Michelle Dawson,  
Director, Public & Regulatory Affairs

**AGREED AND ENDORSED BY:**

Town of Cochrane

\_\_\_\_\_  
Jeff Genung, Mayor



Delivery Services, Transportation  
Southern Region, Calgary District  
2nd Floor, 803 Manning Road NE  
Calgary, Alberta T2E 7M8  
Canada  
Telephone 403-297-6311  
[www.transportation.alberta.ca](http://www.transportation.alberta.ca)

Our File: 2100-COCH/Annexation2018

October 9, 2018

Drew Hyndman  
Senior Manager, Development Services  
Town of Cochrane  
101 Ranchehouse Road  
Cochrane AB T4C 2K8

Dear Mr. Hyndman:

**RE: PROPOSED ANNEXATION FROM ROCKY VIEW COUNTY**

This will acknowledge receipt of your recent email regarding annexation of lands from Rocky View County to the Town of Cochrane.

Alberta Transportation previously completed a Functional Planning and Access Management Study for Highway 22, which identifies access locations and adjacent road connections along Highway 22, which presently occur outside of the Town of Cochrane on the land proposed for annexation.

The annexation is supported by Alberta Transportation, as the local road connections from Highway 22 to the lands within the Town of Cochrane can be achieved as a result of the annexation, forming part of the Town's local road network.

If you have any questions or require additional information, please contact this office.

Yours truly,

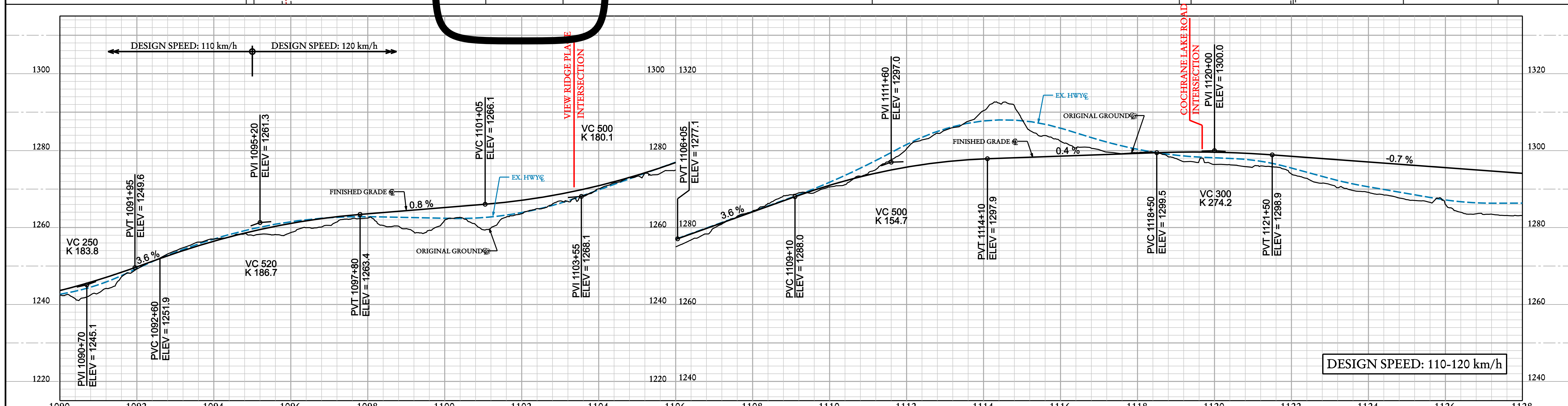
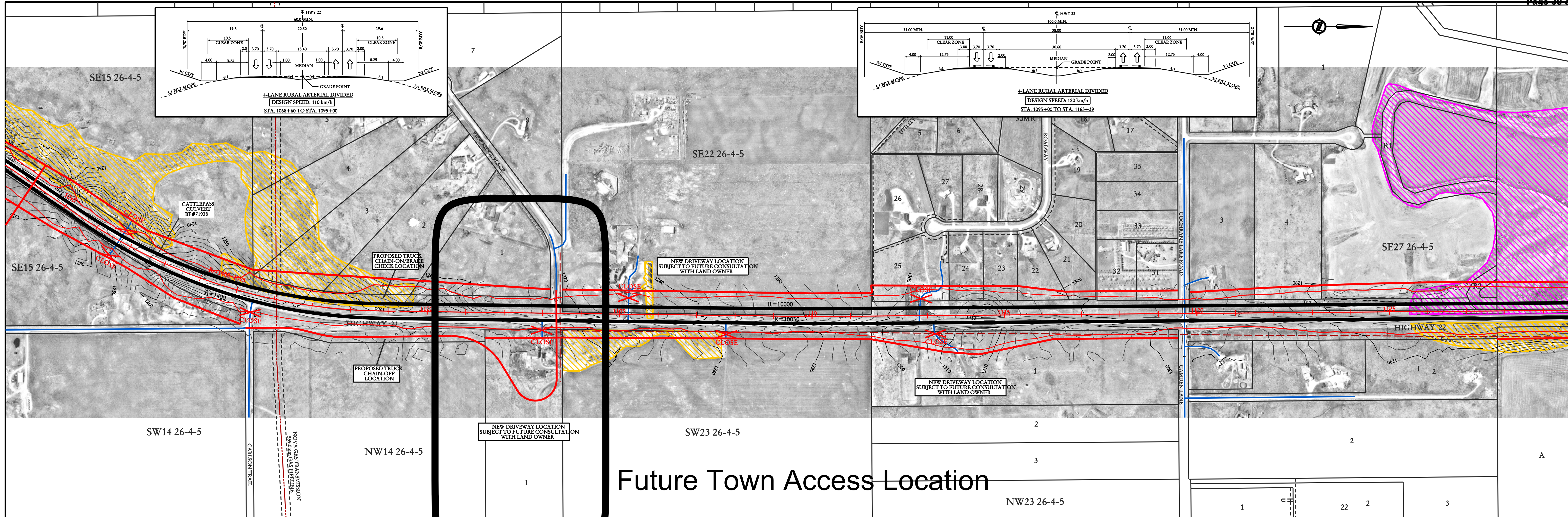
Trevor Richelhof  
Development and Planning Technologist

TBR/pf

cc Rick Duncan  
Case Manager, Municipal Government Board

Richard Barss, Manager of Intergovernmental Affairs  
Rocky View County

Jerry Lau  
Infrastructure Manager, Alberta Transportation



**LEGEND:**

- EXISTING HIGHWAY CENTERLINE
- PROPOSED TWINNING CENTERLINES
- PROPOSED HIGHWAY
- PROPOSED ROAD
- EXISTING ROAD/DRIVEWAY
- PROPOSED RIGHT-OF-WAY BOUNDARY
- POWERLINES
- MINOR ENVIRONMENTAL CONSTRAINTS
- MODERATE ENVIRONMENTAL CONSTRAINTS
- MAJOR ENVIRONMENTAL CONSTRAINTS
- ACCESS CLOSED
- TEMPORARY ACCESS TO BE REMOVED WHEN HIGHWAY UPGRADED, REQUIRES NEW ACCESS
- CLOSE
- TOP OF CUT LIMIT
- BOTTOM OF FILL LIMIT

**KEY PLAN**

**McElhanney Consulting Services Ltd.**

**Alberta TRANSPORTATION**

Highway 22:16 - STA 1090+00 to 1128+00  
Proposed Twinning and Access Management  
Highway 22 & 1A  
Functional Planning Study

Drawn by: CO/MC    Designed by: BJ/BD    Checked by: DM    Approved by: \_\_\_\_\_    Date: 2004-06-23

100m 0 200m    Plan No.: P-3278-04



## ECONOMIC DEVELOPMENT

**TO:** Council  
**DATE:** October 23, 2018 **DIVISION:** All  
**FILE:** N/A  
**SUBJECT:** Economic Development Update Report - 2017

**<sup>1</sup>POLICY DIRECTION:**

A policy currently does not exist that provides guidance on reporting activities and accomplishments of Economic Development within Rocky View County.

**EXECUTIVE SUMMARY:**

The purpose of this report is to provide Council with an update on the activities and accomplishments of Economic Development in RVC. The report highlights current economic trends, a regional assessment, significant developments, and prospective developments in Rocky View County, as well as performance measurement and outcomes/successes.

**TRENDS TO WATCH:**

CAD-USD Exchange Rate	Western Canada Select	West Texas Intermediate	Natural Gas AECO Spot
<b>1 CAD = 75¢ USD</b> 1 USD = \$1.33 CAD	<b>\$57.89 (\$C/bbl)</b> \$43.56 (\$US/bbl)	\$91.68 (\$C/bbl) <b>\$68.98 (\$US/bbl)</b>	<b>\$1.62 (\$C/mcf)</b> \$1.22 (\$US/mcf)
<i>Source: Petroleum Services Association of Canada, as of June 25<sup>th</sup>, 2018. <a href="http://www.pvac.ca/business/GMPFirstEnergy/">http://www.pvac.ca/business/GMPFirstEnergy/</a> and <a href="http://www.xe.com">www.xe.com</a></i>			

The price of oil has increased and stabilized since this time last year. Overall, the price of WTI (West Texas Intermediate) is up 70% over the past 12 months. That said, higher prices are set to moderate as OPEC countries like Saudi Arabia as well as Russia increase production in order to bolster government revenues.

**REVIEWING TRENDS FROM 2017:**

In last year’s Economic Development Report, the following “Top Five Trends” were discussed. All of these trends will likely continue in 2018:

- Improving oil prices;
- Continued lower Canadian dollar;
- Increasing competition with the County’s urban neighbours;
- Slowing retail spending in the Calgary Region; and
- Government of Alberta striving to diversify and stabilize the economy

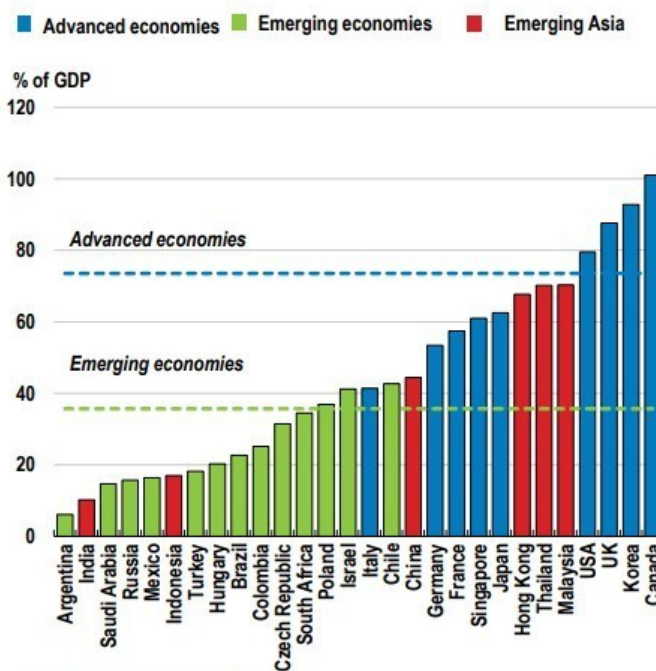
<sup>1</sup> **Administration Resources**  
David Kalinchuk, Economic Development

**NEW TRENDS FOR 2018:**

*Credit bubbles are a real risk*

Global debt continues to concern many economists and central bankers around the world. A recent article in the *Financial Post* stated, “mortgage debt, credit card debt, student loan debt, and car loan debt are all, once again, at record levels and growing.”

**Credit to households**  
2016Q4



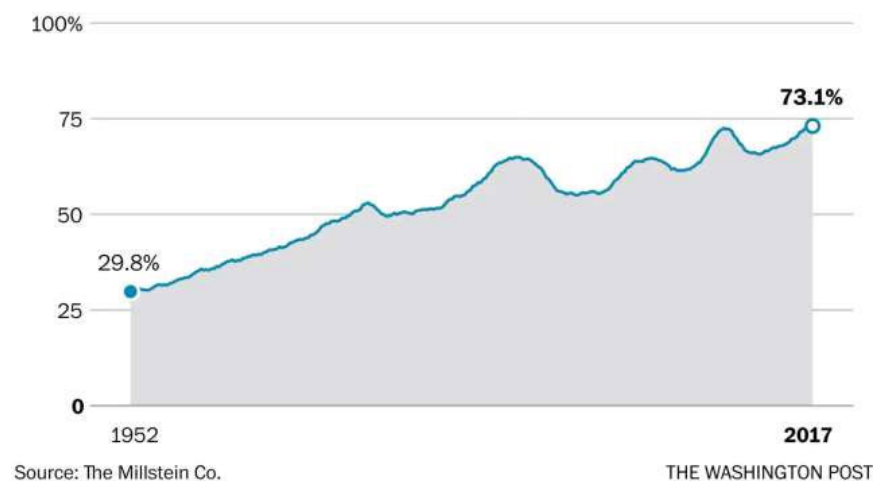
Source: Bank of International Settlements.

Source: World Economic Forum, “Canada’s household debt levels are the highest in the world”, December 2017.

In a *Financial Post* article, they expanded on this topic, “For the bigger reality is that the global economy is now awash in debt – not just corporate debt but also record amounts of government debt, household debt and investor debt – at a time when interest rates are rising from historically low levels.”

### Corporate debt is at a record high – and still rising

Total credit to nonfinancial corporations, as a percent of GDP



Source: “The ‘mother of all credit bubbles’ is brewing — and this time it isn’t household debt”, *Financial Post*, June 15, 2018.

Here in Alberta, the Provincial government reported that the fiscal year end in 2017 ended in an \$8 billion dollar deficit, \$2.5 billion lower than forecast. Total government debt in Alberta is currently \$43.4 billion and is paying \$1.42 billion in annual interest costs. Provincial government debt is forecast to reach \$96 billion by 2023.<sup>2</sup>

How could global debt bubbles impact Rocky View County? As Canada experienced in the 2007-08 global financial crisis, investment and development dropped dramatically. This rapid economic change is best described in a quote: In a telephone call from U.S. Treasury Secretary, Hank Paulson to Jim Flaherty, Canada’s Finance Minister during that period, “Well, good news and bad news. The bad news is there’s a huge credit crisis. The good news is we know what the problem is.”

Rocky View County weathered the storm with prudent and wise fiscal restraint, maintaining low tax rates and no business tax. As in years past, the County should position itself as an “island of stability amidst an ocean of economic uncertainty”, the County will likely continue to attract investment to Alberta and create jobs for the Calgary Region.

#### Consumers want high quality local products

Canada’s ongoing trade war with the United States over American steel and aluminum tariffs, has prompted an increasing number of Canadians to start buying local and supporting Canadian producers. The US President also has his sights on taxing Canadian dairy. This will impact primarily dairy producers in Ontario and Quebec. However, Rocky View County’s Economic Development office would not rule out American tariffs on softwood lumber and beef, two commodities that Alberta produces and exports. Fortunately for Alberta, the United States still needs Canadian oil. That said, it is prudent to continue the development of pipelines for export markets, other than the USA.

Producers and food processors in Rocky View County should capitalize on the burgeoning demand for local products.<sup>3</sup> It is also strategic to continue developing export markets for Alberta products in

<sup>2</sup> Source: CBC “Alberta deficit \$2.5B lower than forecast thanks to higher revenues”, June 2018.

<sup>3</sup> Source: Maclean’s magazine, “A patriot’s guide to shopping during a Canada-U.S. trade war”, June 2018.





Europe, Asia and the Middle East. Rocky View's Economic Development office works very closely with Provincial and Federal staff on these export development opportunities. These include free-trade zone development, inland port promotion (CN's Calgary Logistics Park at Conrich) and foreign direct investment (FDI).

*Global consumption will help move oil prices higher*

Increasing global demand for oil is good for the price of the commodity and the economic prospects in Alberta. Increasing petroleum demand in non-OECD countries (OECD is the Organization for Economic Co-operation and Development and consists of 34 developed western nations, including Canada) also bodes well for Canada's pipeline developments and oil exports.

"Consumption has risen by an average of 1.7 million bpd (barrels per day) in the last three years since oil prices slumped (2015-2017) compared with an average of just 1.1 million bpd in the three previous years (2012-2014)." The article continued, "Non-OECD economies now account for the majority of global oil consumption (51 million bpd) compared with the OECD countries (47 million bpd)." China and India are primary consumers.<sup>4</sup>

Opponents to pipeline development have said that there is no room for pipeline growth and that tankers are unsafe. The Port of Vancouver recently reported, "The Port of Vancouver sees about 30 to 50 crude oil tankers per year, out of a total of about 3,160 vessel calls annually. With the Kinder Morgan pipeline expansion project approved, this number could increase to about 400 tankers per year, or about 11 per cent of our total vessel traffic. Other ports see far more such traffic. The Port of Rotterdam in the Netherlands sees about 8,200 tankers each year and Singapore hosts about 22,200 tankers annually."<sup>5</sup>

New export market development for Alberta's oil and natural gas products will help reduce the discount on Western Canadian Select oil (discount to West Texas Intermediate). This would result in hundreds of millions of dollars of new-found income for Alberta producers and much needed market diversification. This would be good for Calgary head offices and the Calgary Region, including Rocky View County.

**OTHER SIGNIFICANT DEVELOPMENTS:**

Economic Development has created a comprehensive list of prospective developments. This is a cumulative list of ongoing, current developments that have expressed an interest in Rocky View County. The data points in this list will also help track performance in the following areas:

- Number of prospective developments attracted to Rocky View;
- Potential assessment base to be developed; and
- Potential tax revenues to be generated for Rocky View County.

**MEASURING SUCCESS:**

Every municipality in Alberta employs slightly different programs or strategies in order to execute economic activities and achieve overall economic development success. Tax growth that has resulted from successful economic development is a key measurement. All comparative benchmarked data comes from Alberta Municipal Affairs. This allows Rocky View County the opportunity to compare data with other municipalities using a very reliable and neutral, third-party source.

The County's Economic Development success is currently measured in two ways:

1. Prospective investments or developments, as described in the document,

<sup>4</sup> Source: Reuters, "Commentary: Higher oil prices set to moderate consumption growth", June 2018.

<sup>5</sup> Source: Port of Vancouver, "Petroleum products and tanker safety", April 2018.



- “2017 Prospect Values”; and
- Existing tax base analysis, using statistics published by Alberta Municipal Affairs. Data found in these community profiles are consistent and comparable (“apples to apples”) with all other municipalities in Alberta.

Each of the items, in the chart below, is a tangible measurement of the County’s economic well-being and the cumulative success of ongoing Economic Development initiatives. Currently, as of the end of 2017, all economic indicators are indicating positive, balanced growth in the County.

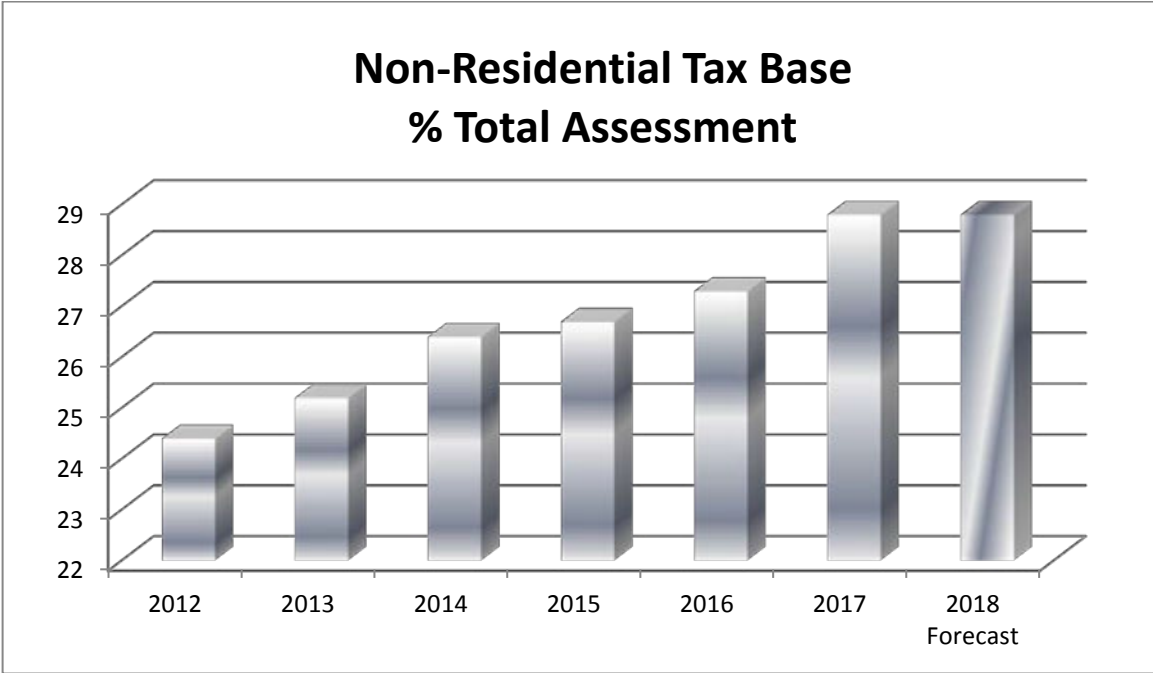
<b>STATISTICS:</b>	
<u>Rocky View Municipal Tax Rates: +1.43%</u> 2017 Res/Farm 2.5140, Non-Res 7.5420 2016 Res/Farm 2.4785, Non-Res 7.4355 2015 Res/Farm 2.4420, Non-Res 7.3260	<u>Current total assessment: +2.71%</u> \$17,682,704,860 (2017) \$17,216,189,240 (2016) \$16,778,795,330 (2015)
<u>Total residential assessment: +1.22%</u> \$12,674,790,050 (2017) \$12,522,307,200 (2016) \$12,304,356,080 (2015)	<u>Total non-res. assessment: +7.66%</u> \$4,322,021,000 (2017) \$4,014,478,350 (2016) \$3,794,053,640 (2015)
<i>Source: Rocky View County Tax Rate Bylaws, 2012 to 2018 and Rocky View Chief Assessor, June 29, 2018.</i>	
<u>Equity in tangible capital assets: up +24.9% since 2011</u> 2017 data not available from Municipal Affairs \$503,816,575 (2016) +15.74% \$435,317,303 (2015) +0.59% \$432,779,887 (2014) -7.75% \$469,159,512 (2013) +6.7% \$439,796,204 (2012) +9.0% \$403,386,021 (2011)	<u>Long term County debt: down - 21.46% since 2011</u> 2017 data not available from Municipal Affairs \$59,116,863 (2016) -1.86% \$60,238,596 (2015) -7.36% \$65,025,981 (2014) +3.79% \$62,649,274 (2013) -6.3% \$66,860,808 (2012) -11.2% \$75,271,753 (2011)
<i>Source: Alberta Municipal Affairs – Rocky View County Profile, June 22, 2018.</i>	



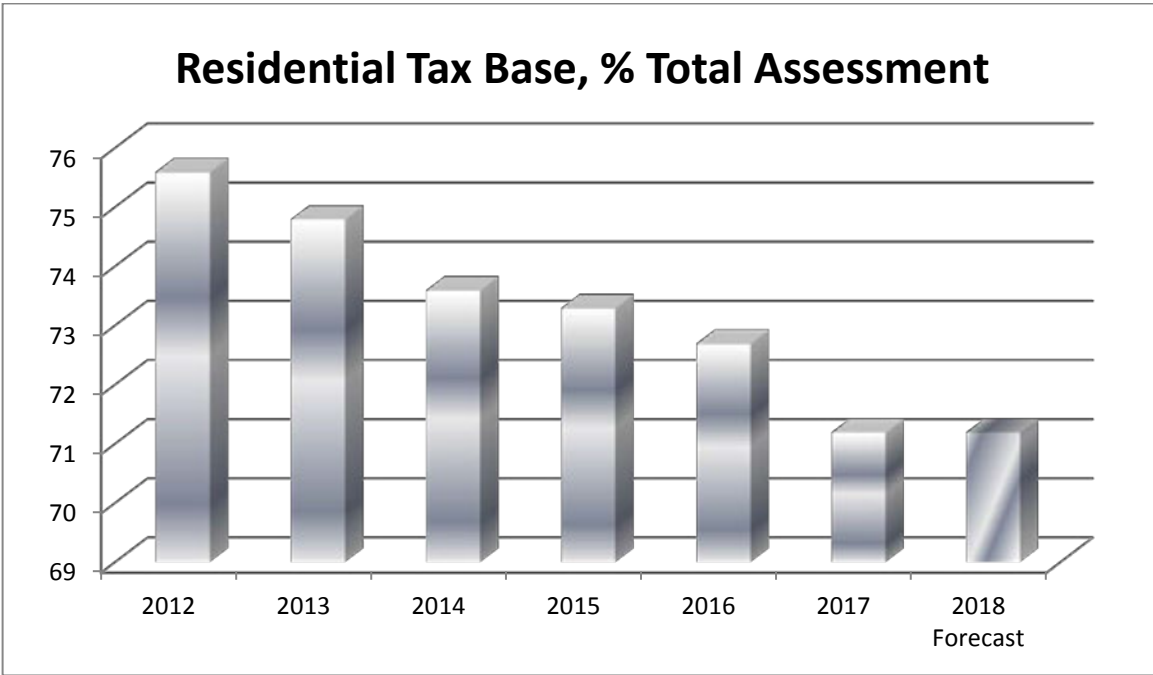
## ASSESSMENT GROWTH:

Highlighting long-term trends in non-residential tax-base growth.

<b>ROCKY VIEW COUNTY</b>						
Total Assessment Summary Comparison						
<b>2012 Total Assessment</b>	<b>2013 Total Assessment</b>	<b>2014 Total Assessment</b>	<b>2015 Total Assessment</b>	<b>2016 Total Assessment</b>	<b>2017 Total Assessment</b>	<b>2018 Forecast</b>
<i>Non Res. \$1,809 million 12.5%</i>	<i>Non Res. \$2,009 million 13.3%</i>	<i>Non Res. \$2,316 million 14.4%</i>	<i>Non Res. \$2,591 million 15.4%</i>	<i>Non Res. \$2,817 million 16.4%</i>	<i>Non Res. \$3,055 million 17.3%</i>	<i>Non Res. \$3,260 million 18.1%</i>
Residential \$10,942 million 75.6%	Residential \$11,317 million 74.8%	Residential \$11,815 million 73.6%	Residential \$12,304 million 73.3%	Residential \$12,522 million 72.7%	Residential \$12,675 million 71.2%	Residential \$12,825 million 71.2%
Farmland \$153 million 1.1%	Farmland \$153 million 1.0%	Farmland \$153 million 1.0%	Farmland \$152 million 0.9%	Farmland \$152 million 0.9%	Farmland \$152 million 0.9%	Farmland \$152 million 0.8%
M&E \$491 million 3.4%	M&E \$521 million 3.4%	M&E \$543 million 3.4%	M&E \$528 million 3.1%	M&E \$527 million 3.1%	M&E \$533 million 3.0%	M&E \$522 million 2.9%
Linear \$1,071 million 7.4%	Linear \$1,127 million 7.4%	Linear \$1,232 million 7.7%	Linear \$1,202 million 7.2%	Linear \$1,197 million 7.0%	Linear \$1,266 million 7.2%	Linear \$1,250 million 6.9%
Total \$14,466 million	Total \$15,128 million	Total \$16,059 million	Total \$16,778 million	Total \$17,216 million	Total \$17,683 million	Total \$18,009 million
Ratio of 75.6 to 24.4	Ratio of 74.8 to 25.2	Ratio of 73.6 to 26.4	Ratio of 73.3 to 26.7	Ratio of 72.7 to 27.3	Ratio of 71.2 to 28.8	Ratio of 71.2 to 28.8
Looking at economic forecasts for the Province of Alberta and discussing those forecasts with Rocky View's Chief Assessor, the Economic Development Office is forecasting the following future growth trends:						
2018 Forecast = + 1%		2019 Forecast = + 1.5%		2020 Forecast = + 2%		
<i>Source: Rocky View County Tax Rate Bylaws, 2012 to 2018 and Rocky View Chief Assessor – July 3, 2018</i>						
<i>Note: Percentage shown is the % of Total Assessment in that specific year.</i>						



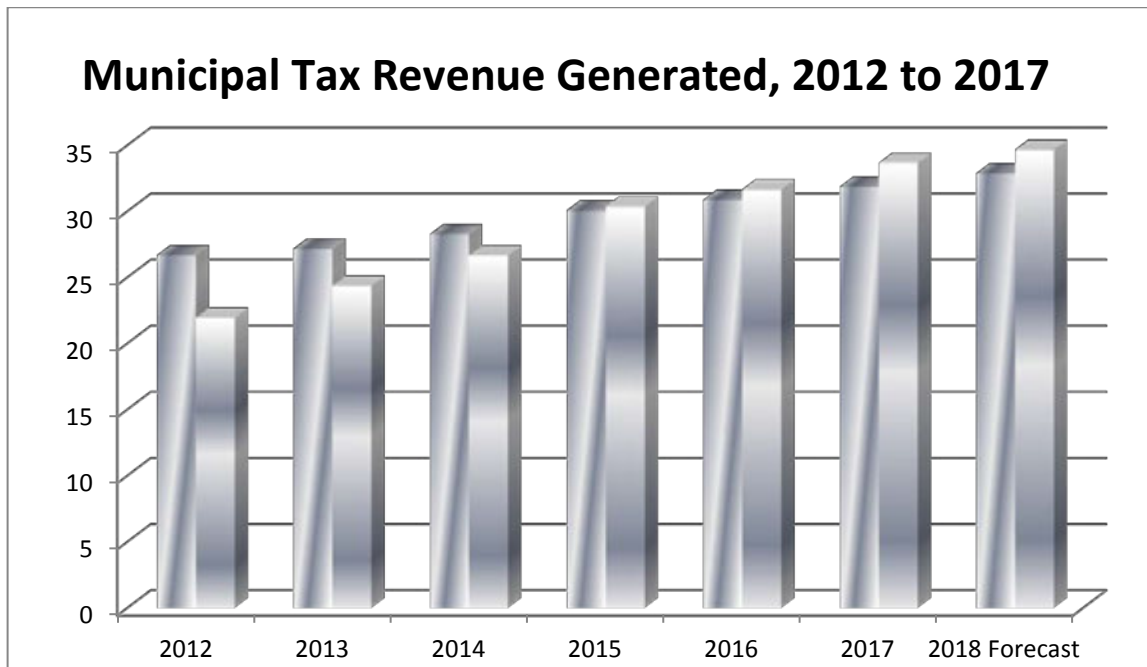
Since 2012, Rocky View County's Non-Residential Tax Base has grown from \$3,524,374,380 to \$5,007,914,810 in 2017. This represents a 42.1% increase and approximately \$11 million additional tax dollars for the County to spend on services (calculated using the 2017 Municipal tax rate of 7.4355 mills).



Since 2012, Rocky View's Residential Tax Base has grown from \$10,942,002,720 to \$12,674,790,050 in 2017. This represents a 15.8% increase in assessment and approximately \$4.29 million additional tax dollars for the County (calculated using the 2017 Municipal tax rate of 2.4785 mills). More significantly however, the Residential to Non-Residential assessment-split changed from 78:22 to



72:28. This demonstrates less reliance on the residential tax base, in order to pay for municipal services.



In 2017, Rocky View’s Economic Development office forecast Residential Tax Revenue would be \$31M, it was \$31.9M (+2.9%). This was accurately projected and a positive indicator, considering the sluggish residential real estate market in the Calgary region. In 2017 we forecast Non-Residential Tax Revenue would be \$32.5M; it was \$33.7M, higher that predicted (+3.7%). This indicates continued economic confidence in Rocky View County, and an improving economy in Alberta.

Municipal Tax Rev. Generated	2012	2013	2014	2015	2016	2017
Res. and Farm	\$26.7 million	\$27.2 million	\$28.3 million	\$30.1 million	\$30.9 million	\$31.9 million
Non-Residential	\$17.3 million	\$19.6 million	\$21.6 million	\$26.4 million	\$27.8 million	\$29.8 million
Machinery and Equipment	\$4.7 million	\$4.8 million	\$5.1 million	\$4.0 million	\$3.9 million	\$3.9 million

*Source: Corporate Services at Rocky View County  
Rocky View County, Tax Rate Bylaw – June 13, 2018*

**CONCLUSION:**

Rocky View County continues to work extremely hard increasing the total Non-Residential Tax Base. And while the Residential Tax Base continues to grow, the Non-Residential Tax Base continues to outpace all other categories including Farmland, M&E (Machinery and Equipment) and Linear Assessment.

Economic Development has forecasted continued flat assessment growth for 2018 at +1%. This reflects the current economic environment and the economic headwinds that Alberta is experiencing



with low oil prices. That being said, into 2019 and 2020, Rocky View will play a major role in the economic diversification of the Province, with growth expected in warehousing and distribution, value added processing, tourism and agribusiness. This is reflected in the positive assessment growth forecasts of +1.5% (2019) and +2% (2020).

As noted in this report, strategic and targeted efforts allow the Economic Development Office to focus on increasing the Total Non-Residential Tax Base through business and commercial development. The current Residential to Non-Residential assessment-split is 72:28. As such, Total Non-Residential Tax Base will continue to be a primary measurement of success for Rocky View County.

**BUDGET IMPLICATION(S):**

There are no implications to the budget resulting from this report.

**OPTIONS:**

Option #1                    THAT the 2017 Economic Development Update Report be received for information.

Option #2                    THAT alternative direction be provided.

Respectfully submitted,

Concurrence,

“Chris O’Hara”

“Rick McDonald”

\_\_\_\_\_

General Manager

\_\_\_\_\_

County Manager



## ROADS MAINTENANCE

**TO:** Council  
**DATE:** October 23, 2018  
**FILE:** 4050-100  
**SUBJECT:** Snow and Ice Control Budget Adjustment For 2018

**DIVISION:** All  
**APPLICATION:** N/A

### **<sup>1</sup>POLICY DIRECTION**

Council is the approving authority for all budget adjustments as per the MGA. A budget adjustment of \$1.2M is required to ensure snow and ice control is funded to the end of December 2018.

### **EXECUTIVE SUMMARY:**

The purpose of this report is to request a budget adjustment in the amount of \$1.2M for additional snow and ice control funding.

As a result of a particularly severe winter season in early 2018, the operating budget approved for fiscal year 2018 snow and ice control (SNIC) was entirely depleted by mid-May. An additional \$1.2M will be required to support SNIC activities to the end of December.

### **BACKGROUND:**

Council was initially apprised of a potential budget shortfall on April 27, 2018 when Administration outlined the severe snow accumulations and multiple freeze-thaw cycles experienced in the early months of the year as the primary factors causing the shortfall.

Once actual costs were known, Council was further updated on July 4, 2018 with the following 5-year table of data:

	Kms road (2-lane equivalent)	SNIC Expense, Jan-May	Cm snow, Jan-May	SNIC Expense, Oct-Dec	Cm snow, Oct-Dec	Total SNIC Expense
FY 2013	845	\$ 1,174,470	61.7	\$ 1,350,032	84.6	\$ 2,524,502
FY 2014	870	\$ 1,663,610	95.1	\$ 1,224,029	80.2	\$ 2,887,639
FY 2015	875	\$ 1,116,250	65.4	\$ 1,176,446	38.7	\$ 2,292,696
FY 2016	875	\$ 1,006,998	19.5	\$ 1,267,111	42.2	\$ 2,274,109
FY 2017	909	\$ 2,125,591	83.6	\$ 1,153,905	61.0	\$ 3,279,496
2018 YTD	921	\$ 2,634,000	120.8	\$1,200,000 est.	-	\$ 3,834,000 est

This indicates an average expenditure in the October to December timeframe of \$1.2M. It also indicates that the overall seasonal cost for SNIC activities is trending upwards from the \$2.5M annual average; a trend which will be reflected in future budgets.

<sup>1</sup>**Administration Resources**

Howard Bell, Manager, Roads Maintenance



**BUDGET IMPLICATION(S):**

The overall SNIC budget for 2018 will be increased from \$2.54M to \$3.74M using a transfer of \$1.2M from the Tax Stabilization Fund. Additional savings (3,834,000-3,740,000 = \$94,000) will be found within other Roads Maintenance programs.

**OPTIONS:**

- Option #1 THAT the budget adjustment of \$1.2m for additional snow and ice control funding be approved as per Attachment 'A'.
- Option #2 THAT alternative direction be provided.

Respectfully submitted,

Concurrence,

Byron Riemann

Rick McDonald

\_\_\_\_\_  
General Manager

\_\_\_\_\_  
Interim County Manager

**ATTACHMENTS:**

ATTACHMENT 'A' – Snow and Ice Control - Budget Adjustment



**ROCKY VIEW COUNTY**  
**INTERIM BUDGET ADJUSTMENT REQUEST FORM**  
**BUDGET YEAR: 2018**

Description	Budget Adjustment
EXPENDITURES: SNIC expenses, in addition to the 2018 operating budget. October to December 2018.	1,200,000
<b>TOTAL EXPENSE:</b>	1,200,000
REVENUES: Transfer from tax stabilization reserve	(1,200,000)
<b>TOTAL REVENUE:</b>	(1,200,000)
<b>NET BUDGET REVISION:</b>	0
REASON FOR BUDGET REVISION: To cover higher than expected SNIC costs as a result of severe weather in January to April 2018	
<b>AUTHORIZATION:</b>	
County Manager: _____ Rick McDonald	Council Meeting Date: _____
Gen. Manager Corp. Services: _____ Kent Robinson	Council Motion Reference: _____
Manager: _____ Byron Riemann	Date: _____

Budget AJE No: \_\_\_\_\_

Posting Date: \_\_\_\_\_



**FINANCIAL SERVICES**

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**TO:** Council  
**DATE:** October 23, 2018 **DIVISION:** 5  
**FILE:** 2025-600  
**SUBJECT:** Property Tax Cancellation Request – Dalroy Gymkhana Club

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**<sup>1</sup>POLICY DIRECTION:**

A policy currently does not exist that would provide guidance when tax relief requests are received. Council recently directed Administration to prepare a policy regarding municipal tax cancellation requests due to fire and will include all municipal tax cancellation requests in the same policy for consideration by the Policy & Priorities Committee for January 2019.

**EXECUTIVE SUMMARY:**

Under section 347 of the *Municipal Government Act*, Council retains the ability to cancel taxes. In the absence of policy in this regard, the County has assessed each request on a case by case basis. In order for a property to qualify for a tax cancellation, the owner would need to provide a community service and unrestricted community access.

Tax Roll # 05214014 and 05214030 are properties that have been used for community purposes for a number of years for the benefit of Rocky View County residents and have previously had the Municipal taxes cancelled by Rocky View County Council. The properties are leased by the Dalroy Gymkhana Club, a non-profit community based organization whose mission is to have family fun on horseback.

**BACKGROUND:**

The Dalroy Gymkhana Club currently does not qualify for exemption under the Community Organization Property Tax Exemption Regulation “COPTER” due to non-registration of the community organization with the province of Alberta. The group will undertake efforts in the future to qualify for COPTER which will allow them property tax exemption for a period of three years under the current legislation.

A summary of the tax cancellation request is outlined as follows:

Roll #	Property Owner	Division	Year	Municipal Tax	School/RV Foundation	Total Taxes
05214014	Starosta, Mary Douglas, Carol & Elizabeth	5	2018	\$256.93	\$264.43	\$521.36

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<sup>1</sup> Administration Resources  
Barry Woods, Financial Services



Roll #	Property Owner	Division	Year	Municipal Tax	School/RV Foundation	Total Taxes
05214030	Cairns, Mary-Anne Hubregts, Elizabeth Starosta, Carol, Douglas & Steven	5	2018	\$342.15	\$352.14	\$694.29
	<b>TOTAL 2018</b>			<b>\$599.08</b>	\$616.57	\$1,215.65

**BUDGET IMPLICATION(S):**

Option # 1 – Although no budget adjustment is required, if Council directs that the request be approved, the 2018 municipal property tax revenue will be reduced by \$599.08.

**OPTIONS:**

- Option #1                    THAT the 2018 municipal tax levy, in the amount of \$599.08 for roll numbers 05214014 and 05214030, be cancelled.
- Option #2                    THAT alternative direction be provided.

Respectfully submitted,

Concurrence,

“Kent Robinson”

“Rick McDonald”

\_\_\_\_\_  
General Manager

\_\_\_\_\_  
Interim County Manager

**ATTACHMENTS:**

ATTACHMENT ‘A’ – 2018 Request Letter

Sent by email to [tax@rockyview.ca](mailto:tax@rockyview.ca)

August 30, 2018

Property Tax Department  
**Rocky View County**  
911 – 32 Avenue N.E.  
Calgary AB T2E 6X6

Dear Sir/Madam:

**TAX NOTICE ROLL # 05214014 – LOT 18**  
**TAX NOTICE ROLL # 05214030 – LOT 40**  
**DALROY PLAN 2166 W**

On behalf of the Dalroy Gymkhana Club, could you please forward a request to Council to exempt the 2018 property taxes for the above noted lands. As you know, the Dalroy Gymkhana Club is a non-profit, community-based group who has leased these lands for the past 24 years. The Club's mission is to have 'family fun on horseback'.

Thank you for your consideration in this matter. If you need anything further please call the undersigned at [REDACTED].

Yours truly,



Mary Anne Cairns



## **POLICY & PRIORITIES COMMITTEE**

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**TO:** Council

**DATE:** October 23, 2018 **DIVISION:** 1-4, PORTION OF 5

**FILE:** N/A

**SUBJECT:** PPC Recommendations for the Active Transportation Plan: South County

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### **<sup>1</sup>POLICY DIRECTION:**

The Active Transportation Plan: South County was developed as a result of recommendations provided in Rocky View County's Parks and Open Space Master Plan. At the October 2, 2018 Policy & Priorities Committee meeting, PPC recommended that the Active Transportation Plan: South County be approved by Council.

### **EXECUTIVE SUMMARY:**

The purpose of the Policy & Priorities Committee (PPC) is to advise Council on a variety of matters impacting or potentially impacting Rocky View County. When required, the Committee makes recommendations to Council.

The Active Transportation Plan: South County (the Plan) project is a direct result of a recommendation and plan priority provided in the Parks and Open Space Master Plan (POSMP) directing a need for regional planning. As a result, Council approval and funding for the Plan was provided in 2017 to satisfy the aforementioned POSMP recommendation.

At the October 2, 2018 Policy & Priorities Committee meeting, the Committee passed the following motion regarding the Plan:

*MOVED by Councillor Schule that the Active Transportation Plan: South County draft plan and presentation by HDR Inc. be received as information;*

*AND that the Policy & Priorities Committee recommend to Council that the Active Transportation Plan: South County be approved and that any projects referenced in the plan are to be brought to Council for approval.*

PPC indicated a desire for greater clarity associated with implementation of the Plan through the process of project approvals. Administration has edited the Plan to remove references to the project cost estimates provided in the "Order of Cost Magnitude." These amounts were offered in the draft plan to serve as a representative cost schedule and were not intended to be initiated upon plan approval.

### **BACKGROUND:**

The POSMP, a foundation document approved by Rocky View County Council in 2011 provides a comprehensive list of Plan Priorities for the County to follow through the implementation of the plan. A specific recommendation indicates: "Prepare detailed plans for each region". Recognizing the POSMP as an overview document for the entire County, regional planning intends to provide for the unique needs and requirements inherent throughout the various regions of the County. Recent revisions to Area Structure Plans (ASP) for Langdon, Janet, Conrich, the current ASP review for Springbank, and the development of community planning documents such as the Bragg Creek Revitalization Plan have

---

<sup>1</sup> **Administration Resources**

Greg Van Soest, Agricultural and Environmental Services



identified an interest and need for active transportation planning. The preparation of a functional active transportation plan can be looked upon as a tool to assist County Council, Senior Leadership, Administration, and stakeholders alike when development decisions are required.

By definition, Active Transportation is any form of human-powered transportation. In Alberta, walking and cycling are the most popular forms of active transportation. This includes travel to and from particular destinations such as private residences, employment centres, educational institutions and commercial centres. Typical active transportation infrastructure includes elements that support active transportation, such as improved sidewalks, dedicated bike lanes, safer pedestrian crossing points, bicycle racks, and pathways and trails for walking and cycling.

The County will benefit from a comprehensive “ground level” plan addressing the built form including a decision making matrix to accommodate regional connectivity and related active transportation infrastructure requirements accordingly. Further, the plan serves to inform the County’s long term land acquisition plan to ensure that suitable Municipal Reserve lands are taken at the time of subdivision to accommodate integrated park and active transportation infrastructure.

The impact of having an active transportation plan in place for Rocky View County organizational operations is considered to be positive. The long term objective of a Rocky View County active transportation plan is to further ensure alignment with the Corporate Strategic Plan, permit informed, justifiable decision making, and to promote public safety as our communities grow.

A draft of the Plan was presented at the October 2, 2018 PPC meeting by a representative of the primary project consultant- HDR Inc. The draft Plan indicated projected costs, entitled “Order of Cost Magnitude” associated with implementation of the Plan. These amounts were offered in the draft plan to serve as a representative cost schedule and were not intended to be initiated upon plan approval.

The Committee indicated a desire for greater clarity associated with implementation of the Plan through the process of project approvals. The Committee in turn offered an amended motion which was approved. To further support clarity and the Plan implementation process, Administration has edited the Plan as presented, by removing any reference to “Order of Cost Magnitude”.

**BUDGET IMPLICATION(S):**

There are no budget implications at this time.

**OPTIONS:**

- Option #1                    THAT the Active Transportation Plan: South County be approved.
- Option #2                    THAT alternative direction be provided.

Respectfully submitted,

Concurrence,

“Byron Riemann”

“Rick McDonald”

\_\_\_\_\_  
General Manager

\_\_\_\_\_  
Interim County Manager



**ATTACHMENTS:**

- Attachment 'A' – Draft Minutes from the October 2, 2018 Policy & Priorities Committee Meeting
- Attachment 'B' – Active Transportation Plan: South County

ROCKY VIEW COUNTY  
POLICY AND PRIORITIES COMMITTEE  
October 2, 2018

Page 1

A regular meeting of the Policy and Priorities Committee of Rocky View County was held in Council Chambers of the Municipal Administration Complex, 911 – 32<sup>nd</sup> Avenue NE, Calgary, Alberta on October 2, 2018 commencing at 9:14 a.m.

Present:	Division 2	Councillor K. McKylor (Chair)
	Division 8	Councillor S. Wright (Vice Chair) (arrived at 9:36 a.m.)
	Division 3	Councillor K. Hanson (arrived at 9:30 a.m.)
	Division 4	Councillor A. Schule
	Division 5	Deputy Reeve J. Gautreau
	Division 6	Reeve G. Boehlke
	Division 7	Councillor D. Henn
	Division 9	Councillor C. Kissel (arrived at 10:11 a.m.)
Absent:	Division 1	Councillor M. Kamachi
Also Present:	R. McDonald, Interim County Manager	
	K. Robinson, General Manager	
	C. O'Hara, General Manager	
	H. Bell, Acting General Manager	
	A. Keibel, Manager, Legislative and Legal Services	
	S. Baers, Manager, Planning Services	
	C. Nelson, Manager, Agriculture and Environmental Services	
	L. Wesley-Riley, Manager, Enforcement Services	
	G. Van Soest, Parks Planner, Agriculture and Environmental Services	
	G. Nijjar, Municipal Engineer, Engineering Services	
	L. Ganczar, Planner, Planning Services	
	C. Satink, Deputy Municipal Clerk, Legislative and Legal Services	
	T. Andreasen, Legislative and Bylaw Coordinator, Legislative and Legal Services	

### Call to Order

The Chair called the meeting to order at 9:14 a.m. with all members present with the exception of Councillor Kamachi, Councillor Hanson, Councillor Wright, and Councillor Kissel.

### 1-18-10-02-01

#### Updates/Acceptance of Agenda

MOVED by Deputy Reeve Gautreau that the October 2, 2018 Policy and Priorities Committee meeting agenda be accepted as presented.

Carried  
Absent: Councillor Hanson  
Councillor Wright  
Councillor Kissel



ROCKY VIEW COUNTY  
POLICY AND PRIORITIES COMMITTEE  
October 2, 2018

**1-18-10-02-02**  
**Confirmation of Minutes**

MOVED by Councillor Henn that the September 4, 2018 Policy and Priorities Committee meeting minutes be accepted as presented.

Carried  
Absent: Councillor Hanson  
Councillor Wright  
Councillor Kissel

**1-18-10-02-05 (D-1)**  
**All Divisions – Policy C-409 – Road Brushing**  
**File: 2000-450 / 4050-100**

MOVED by Councillor Reeve Boehlke that Road Brushing Policy C-409 be approved as per Attachment 'A'.

Carried  
Absent: Councillor Hanson  
Councillor Wright  
Councillor Kissel

**1-18-10-02-06 (D-2)**  
**Divisions 1-5 – Rocky View County Active Transportation Plan: South County – Project Update**  
**File: N/A**

Presenter: Steve Power, HDR Inc.

Councillor Hanson arrived at the meeting at 9:30 a.m.

Councillor Wright arrived at 9:36 a.m.

MOVED by Councillor Schule that item D-2 be tabled until later in the meeting.

Carried  
Absent: Councillor Kissel

The Chair called for a recess at 10:04 a.m.

Councillor Kissel arrived at the meeting at 10:11 a.m.

The Chair called the meeting back to order at 10:13 a.m. with all previously mentioned members present.

**1-18-10-02-03 (C-1)**  
**All Divisions – Airdrie RCMP Presentation**  
**File: 3000-300**

Presenter: Insp. Kim Pasloske, Detachment Commander, Airdrie RCMP

MOVED by Reeve Boehlke that the Enhanced Policing Agreement (Crime Reduction) presentation by the Airdrie Royal Canadian Mounted Police (RCMP) be received as information.

Carried

ROCKY VIEW COUNTY  
POLICY AND PRIORITIES COMMITTEE  
October 2, 2018

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**1-18-10-02-04 (C-2)**  
**All Divisions – Cochrane RCMP Presentation**  
**File: 3000-300**

Presenters: Insp. Lauren Weare, Detachment Commander, Cochrane RCMP  
Sgt. Tom Kalas, Cochrane RCMP

MOVED by Councillor Schule that the 2018 Crime Reduction Update presentation by the Cochrane Royal Canadian Mounted Police (RCMP) be received as information;

AND that Administration be directed to work with the RCMP on the upcoming legalization of recreational marijuana in Canada.

Carried

The Chair called for a recess at 11:13 a.m. and called the meeting back to order at 11:23 a.m. with all previously mentioned members present.

**1-18-10-02-06 (D-2)**  
**Divisions 1-5 – Rocky View County Active Transportation Plan: South County – Project Update**  
**File: N/A**

MOVED by Councillor Schule that item D-2 be lifted from the table.

Carried

Councillor Kissel stated that she was not present during the presentation of the Rocky View County Active Transportation Plan: South County earlier in the meeting and would abstain from voting on the motion.

MOVED by Councillor Schule that the Active Transportation Plan: South County draft plan and presentation by HDR Inc. be received as information;

AND that the Policy & Priorities Committee recommend to Council that the Active Transportation Plan: South County be approved and that any projects referenced in the plan are to be brought to Council for approval.

Carried

Abstained: Councillor Kissel

**1-18-10-02-07 (D-3)**  
**Division 5 – Functional Planning Study – Glenmore Trail East Project**  
**File: 5000-680**

Presenters: Nick Ryan, Parsons Plus  
David Breu, ISL Engineering

ROCKY VIEW COUNTY  
POLICY AND PRIORITIES COMMITTEE  
October 2, 2018

MOVED by Councillor Schule that the functional planning study for the Glenmore Trail East Project prepared by Parsons Plus & ISL Engineering be received as information.

Carried

In Favour:

Reeve Boehlke  
Councillor Hanson  
Councillor McKylor  
Councillor Wright  
Councillor Schule  
Councillor Henn  
Councillor Kissel

Opposed:

Deputy Reeve Gautreau

**1-18-10-02-08 (D-4)**

**All Divisions – Draft Terms of Reference – Governance and Priorities Committee**

**File: N/A**

MOVED by Councillor Hanson that the following items be brought to a future Council meeting for consideration:

- 1) Updated Terms of Reference for a Governance and Priorities Committee; and
- 2) Rocky View County Bylaw #C-7827-2018, Committees Bylaw Amendment No. 4.

Carried

The Chair called for a recess at 12:22 p.m. and called the meeting back to order at 12:28 p.m. with all previously mentioned members present with the exception of Councillor Wright.

**1-18-10-02-09 (D-5)**

**All Divisions – Draft Cannabis Consumption Bylaw**

**File: N/A**

Councillor Wright returned the meeting at 12:29 p.m.

MOVED by Councillor Schule that the Cannabis Consumption Bylaw be brought to a future Council meeting for consideration.

Carried

**Adjournment**

MOVED by Deputy Reeve Gautreau that the October 2, 2018 Policy and Priorities Committee meeting be adjourned at 12:56 p.m.

Carried

\_\_\_\_\_  
CHAIR

\_\_\_\_\_  
CAO or Designate



# Active Transportation Plan

**SOUTH COUNTY**

**OCTOBER 2018**

Presented by:



## **ACKNOWLEDGEMENTS**

Rocky View County would like to thank everyone that contributed to this planning effort. Special thanks are extended to stakeholders, the community and County staff members of the Technical Review Committee and Rocky View City Council for their support.

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## EXECUTIVE SUMMARY

The Rocky View County Active Transportation Plan South County (the Plan) is Rocky View County's first ever active transportation plan. By taking the step to develop a comprehensive master plan means that Rocky View County is showing continued commitment to developing programs, policies, and infrastructure that serve the needs of all residents and visitors. The Plan is inspired by the myriad of trails and pathways that have been recommended in previous planning efforts and knits them together to provide a backbone active transportation network that is both connected and consistent with previous planning efforts.

The Plan resulted from months of public engagement, field work, and analysis. It involved numerous meetings with stakeholders and was continually refined to reflect the type of community where people want to live, work and play.

Chapter 1. Introduction – describes the plan background, framework and goals. A summary of the public process is also included.

Chapter 2. Baseline Analysis – includes a narrative that breaks down the types of active transportation facilities included within Rocky View County, describes their location, and explains how they relate to existing land use.

Chapter 3. Active Transportation Needs Analysis – provides a data-driven assessment of the existing infrastructure and identifies gaps in countywide infrastructure as well as within the communities of Springbank, Langdon, and Bragg Creek.

Chapter 4. Active Transportation Network – presents a toolbox of potential active transportation facilities that serve as building blocks for network development as well as comprehensive countywide recommendations and more detailed recommendations in focus areas of Springbank, Langdon, and Bragg Creek.

The Plan includes recommendations for over 40 discrete infrastructure projects and supporting education, encouragement and policy projects as well as recommended design guidance for on- and off- street facilities in rural and more urban areas to help ensure consistency in facility construction and maintenance.

The development of the network recommendations was an iterative and collaborative process that considered both existing data and feedback gathered through conversations with staff, stakeholders, and the public. The needs of people walking and bicycling are balanced with the safety and comfort of other users as well as roadway characteristics and corridor constraints.

Chapter 5. Implementation – introduces a recommended seven-step process for near-term (five-year) Plan implementation and other supporting infrastructure, programmatic, and policy recommendations that can be implemented opportunistically as well as potential funding sources.

The near-term steps include:

1. Develop enabling policies and procedures
2. Implement connections and programs to support active school travel in Langdon
3. Improvements in Bragg Creek on Balsam Avenue and Burnside Avenue
4. Apply for Walk Friendly Communities designation
5. Build continuous path on Centre Street in Landon
6. Active transportation policy for new developments
7. Build Range Road 33 shared-use pathway

CHAPTER 1

# Introduction

## PURPOSE OF THE ACTIVE TRANSPORTATION PLAN

Rocky View County is taking steps to develop a cohesive and comprehensive active transportation plan. While Rocky View has always benefitted from many initiatives to build pathways, sidewalks, and other places for people to walk and cycle. However, the efforts have lacked coordination and as a result, the infrastructure has been ad hoc and does not meet the needs of all user groups. Rocky View County understands this deficiency and has sought to address it in previous years through efforts such as the Parks and Open Space Master Plan (POSMP), which laid the ground work for a plan focused on pathways and recreation. That initial effort has grown into this planning effort that fills a larger need. The Active Transportation Plan: South County (ATPSC) provides direction on priorities for establishing a connected network of on- and off-street facilities where walking and cycling is a safe and accessible choice for all residents. The ATPSC includes infrastructure, policy, and programmatic recommendations that can be implemented strategically over time by the Rocky View County as it grows into its role as a provider of active transportation facilities and programs.

Guiding principles provide overall direction for the ATPSC and are described below. The principles support the overall County vision as articulated in the Rocky View County Plan (2013).

The ATPSC guiding principles state that the plan will:

- Provide clear guidance for active transportation facility development that incorporates best practices while being sensitive to the context in which facilities are developed.
- Contribute to quality of life by supporting economic development, environmental sustainability, and healthy lifestyles.
- Be a realistic transportation choice to safely and comfortably connect people with their day-to-day activities.
- Support access to features and amenities within Rocky View County.
- Create connections and spaces for social interaction within and between communities, contributing to a greater sense of community.
- Accommodate a range ages and abilities that reflects the needs of the surrounding community context.
- Identify year-round active transportation opportunities where there is high demand and where year-round maintenance can be realistically be applied and sustained.
- Be prioritized within the financial means of Rocky View County and will consider capital and life-cycle costs.

The Plan's guiding principles all support the overall County vision:

*“Rocky View is an inviting, thriving, and sustainable county that balances agriculture with diverse residential, recreational, and business opportunities.”*

# PLAN AREA

Rocky View County surrounds the City of Calgary to the north, east, and west. Between 2011 and 2016, Rocky View County grew by 110 percent, to reach 39,407 residents. The County is comprised of 14 hamlets and numerous country residential subdivisions. While agriculture shapes most of the landscape, most residents of Rocky View work outside of their home. The ATPSC addresses two regions in the County: the Elbow River Ranch Lands and Bow River Plains areas shown in Figure 1.

The Elbow River Ranch Lands region is located in the southwest portion of Rocky View County and includes the hamlet of Bragg Creek and the communities of

Springbank, Elbow Valley, and Harmony. The region is bounded by the City of Calgary to the east and has a direct connection to the Town of Cochrane to the north.

The Bow River Plains region is located in the southeast and includes the hamlets of Langdon, Indus, Conrich, Dalemead, and Janet. In addition to bordering the City of Calgary, this region also shares a boundary with the City of Chestermere.

Combined, the Bow River Plains and Elbow River Ranch Lands form the South County study area.

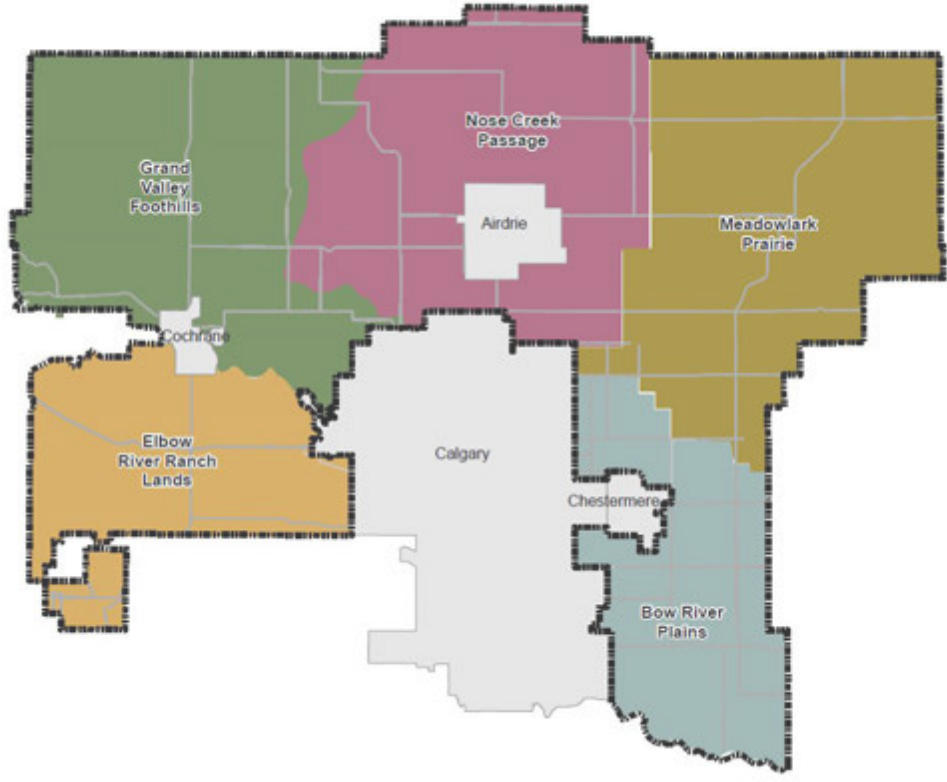


Figure 1: Rocky View Country regions developed as part of the Parks and Open Space Master Plan

# ACTIVE TRANSPORTATION USER TYPES

Active transportation is a term that is typically used to describe modes of travel that are people-powered. In the minds of most people, active transportation users typically fall into two categories: people who walk and people who cycle. Though there are many other active transportation user types (e.g., people on scooters, people on rollerblades, and people with strollers) this Plan focuses primarily on people who walk and cycle. These user types are explained here and then in more detail in the design guide in Appendix A.

## PEDESTRIANS

Walking is an everyday activity for the majority of people. People walk to go to school or work, to shop, for leisure, and for recreation or exercise. Suitable pedestrian facilities provide youth with a comfortable and safe walk home from school and allows community members to visit their neighbours.

Elderly people and others with mobility impairments may utilize mobility devices to navigate the pedestrian environment. While many trips are made by motor vehicle, bus, or train, most trips involve at least a small walking component.

## CYCLISTS

People ride bicycles for many reasons: it may be for recreational purposes, for training or fitness purposes, to get to work, school, or to run errands. For many, riding a bike may be a primary mode of transportation for at least some daily trips. Where people choose to ride, how long they ride for, and what destinations they access largely depend on their level of comfort and proximity to destinations. Cyclist comfort is often influenced by the availability (or lack thereof) of dedicated and separated cycling infrastructure. Generally, cyclists will fall into one of four categories shown in Figure 2.

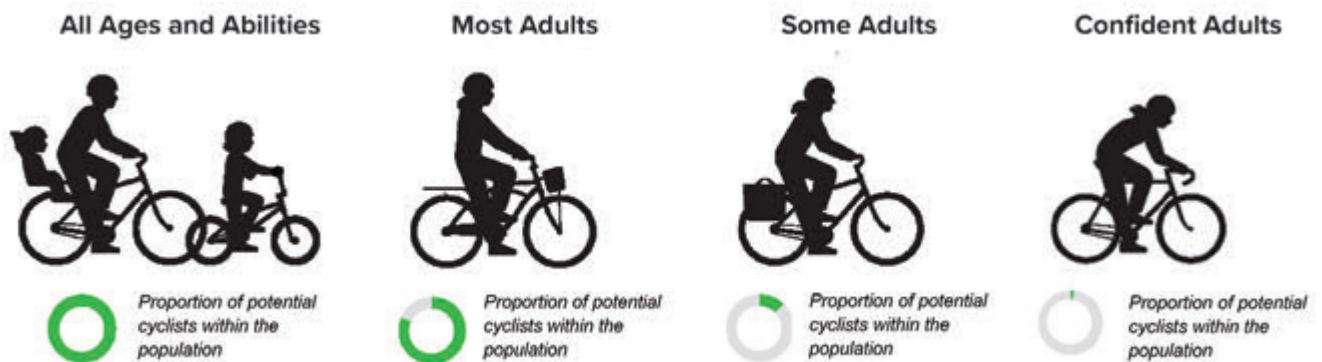


Figure 2: Four types of bicyclists

# PLAN DEVELOPMENT PROCESS

The development of the ATPSC was a collaborative effort that brought together residents, local leaders, regional staff, and adjacent municipalities through a Technical Advisory Committee and public / stakeholder engagement process. To develop a robust and comprehensive plan, the team first reviewed current Rocky View County background documents and representative plans from across North America, then developed the plan in three stages:

## Baseline Analysis

This stage reviewed land use, facility types, and existing infrastructure within Rocky View County and then analyzed the existing active transportation and road networks through a lens of quality, equity, and needs to develop a list of potential active transportation corridors.

## Active Transportation Network

The Active Transportation Network developed the types and application of active transportation facilities appropriate for Rocky View County and identified network connection priorities between and within communities.

## Recommendations

Recommendations provided an ordered list of steps that Rocky View County can take to build the foundation of an active transportation network in the near term. A listing of longer-term opportunities and funding sources was also generated.

These three stages are illustrated in Figure 3.

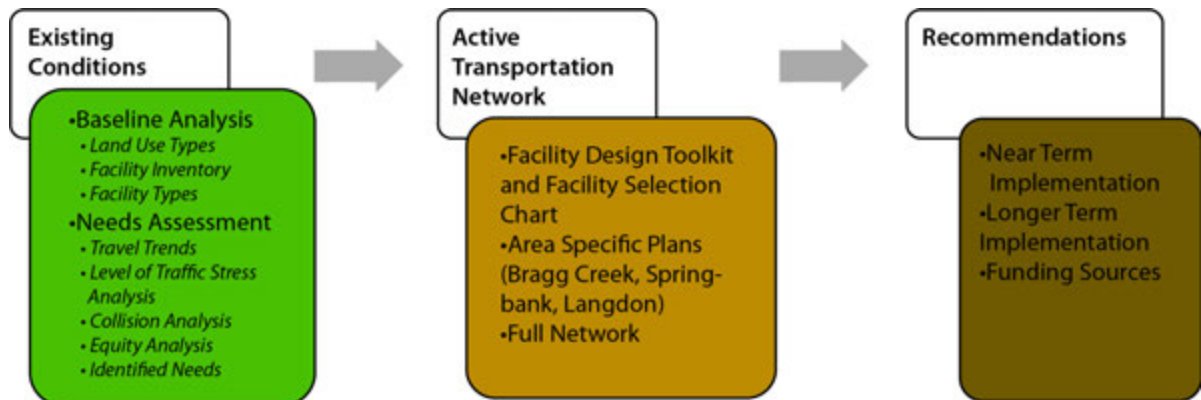
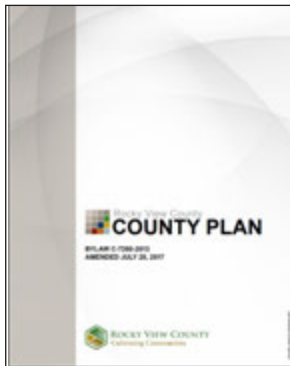


Figure 3: Plan development process



# BACKGROUND DOCUMENT REVIEW

The background document review was conducted to identify and summarize relevant policies, plans, and other strategic documents in and around Rocky View County related to the development of the ATPSC. Key documents are discussed here and the entire document review is found in Appendix B.



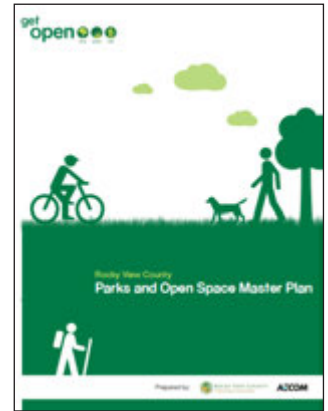
*The Rocky View County Plan* (2013) identified nodes for growth and development. The plan states that hamlets should be pedestrian- and cyclist-friendly; safe, accessible, and attractive; and be connected by pathways and sidewalks. It establishes the direction to work with adjacent

municipalities and stakeholders to build connected active transportation facilities. The County plan also includes the policy direction to acquire land for trails and pathways and apply design standards to achieve consistency of pathways and trails.

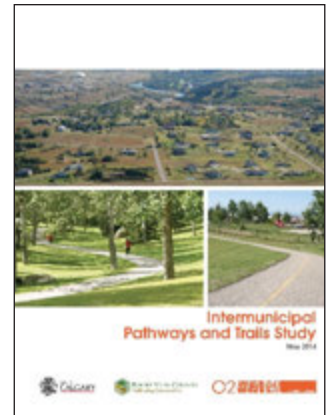


*The 2015-2018 Strategic Plan: The Road Forward*, references active transportation under the Plan's strategic pillars of Sustainable Communities, and Service Excellence. The strategy provides direction for municipal actions, and performance indicators such as the total kilometres of pathways and trails.

*The Parks and Open Space Master Plan* (2013) is a resource to the community to assist in planning parks and open space that meet the needs of the community and build a foundation for the future generations. Key outcomes of the plan include a breakdown of the County into five distinct regions with corresponding parks and open space development concepts. The plan also includes recommendations, actions, and implementation tools that focus on the incremental development of a comprehensive parks and open space system.



*Calgary/Rocky View County Intermunicipal Pathway and Trail Study* (2014) identifies linkages and connections that interface between the County and Calgary. It defines five pathway and trail facility types: regional trails, local pathways/trails, natural trails, on-street bikeways, and sidewalks.



## TECHNICAL REVIEW COMMITTEE



Figure 4: Locations of case studies

In order to understand the wide range of perspective and voices involved in active transportation within Rocky View, the County established a Technical Review Committee (TRC) to provide input throughout the duration of the project.

The TRC was comprised of Rocky View County staff and representation from various community groups including:

- Greater Bragg Creek Trails Association (GBCTA)
- Springbank Trails and Pathways Association (STAPA)
- Langdon Community Association

- Rocky View County Engineering Services
- Rocky View County Capital Projects
- Rocky View County Planning
- Rocky View County Recreation and Community Services
- Rocky View County Municipal Lands
- Rocky View County Road Operations

The TRC met six times throughout the project, providing input to the plan development and recommendations.

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*In the development of this plan, the project team researched six active transportation plans from municipalities similar to Rocky View in geography, population, climate, and land uses. The review provides examples of strategies and methods used in other jurisdictions that may be appropriate for Rocky View County's ATPSC. "Experience Elsewhere" examples illustrate how the experience in other jurisdictions helped to guide Rocky View County's ATPSC. The complete memo can be found in Appendix C. The locations of these case studies are shown in Figure 4.*

# PUBLIC AND STAKEHOLDER ENGAGEMENT

During the planning process, the County was committed to listening and understanding the needs of residents and then providing opportunities to comment on potential solutions. A variety of methods were used to reach the greatest number of people possible during engagement. The following list summarizes and outcomes of each communication or engagement activity.

## Web Page

A web page was established on the Rocky View County website to provide updates on the Plan development and information about how residents could provide input to the Plan.

## Issues Scoping Workshop

A workshop was held for the TRC, stakeholders, and other agencies and to identify issues, challenges, and opportunities associated with the Plan development. The input was used to help establish the guiding principles and initial active transportation network connections as well as to focus elements of the baseline analysis.

## Online Questionnaire

An online questionnaire, targeted at stakeholders and interest groups, allowed open-ended responses to obtain in-depth insights into active transportation activity and gauge acceptance of various types of active transportation facilities. Although targeted at key groups, the questionnaire was open to all. A total of 42 individuals completed the questionnaire.



Rocky View Active Transportation Plan - South County website

## Community Workshops

Two community workshops were held - one in Langdon and one in Springbank. Workshop participants were invited to provide input through two mapping exercises – one that focused on main streets and connections within communities and a second to identify important connections between communities. The Springbank workshop also included a review of connections across the Elbow River in the Springbank/Elbow Valley area. A total of 56 community members participated in the workshops: 16 in the Elbow River Ranch Lands, and 39 from the Bow River Plains Region.

## Youth Workshops

A workshop was held with 27 grade nine students at Langdon School to gain input from students who rely on active transportation as their only means of independent travel. The workshop focused on routes to school, methods of travel, and other desired destinations. Students also participated in a Centre Street redesign activity, which reimagined the corridor with various types of active transportation facilities and urban design elements such as lighting, street furniture, street trees, and parklets. While the workshop was focused on Langdon, the input received from the students is transferrable to other areas and provides insight into the types of features that make walking and cycling attractive to Rocky View residents.

Post-it notes were used for people to share their ideas at the community workshops. Image: HDR



## Online Mapping

The draft active transportation network was posted online, allowing for public input on the network. The online mapping tool provided the ability to zoom in and out as desired and provided the capability for the public to leave comments related to specific locations or about the network in general. There were 19 comments received on the draft network.

## Meetings with Adjacent Municipalities

Meetings were held with the City of Calgary and Town of Cochrane to coordinate active transportation planning efforts between municipalities. Although no formal meeting with the City of Chestermere was held, Chestermere staff participated in the Issue Scoping Workshop and input received at this meeting was considered during network planning activities.

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*Public engagement was used in the development of many of the plans that were reviewed. Engaging the community on the plans allowed for local ownership, helped identify challenges and opportunities, and identified community priority. Using public workshops was one technique that gave residents the opportunity to pick up markers and draw different ideas with members of the project team.*

## CHAPTER 2

# Baseline Analysis

The following chapter provides a review of baseline information used to develop the Plan recommendations:

- Land Use Types
- Facility Inventory
- Facility Types

## LAND USE TYPES

The US Federal Highway Administration's *Small Town and Rural Multimodal Networks* guide identifies three general land use areas found in rural communities. These generalized land uses have been applied for Rocky View County to help develop the plan. While active transportation trips are associated with all land uses, the purpose (e.g. travel to school vs. recreation trip), and the appropriate active transportation facilities (e.g. a roadway shoulder vs. protected bike lanes) vary across land uses. These areas are shown on Map 1.

### Developed Rural Core

The developed rural core (Figure 5) land use type is generally comprised of towns, hamlets, or regional centres with concentrations of residents, businesses, and community destinations. Developed rural core areas are focal points within Rocky View County. In the Plan area this would include the hamlets of Langdon and Bragg Creek. As Conrich and Harmony continue to grow, they would also be included in this category. The developed area along Range Road 33 north of Springbank Road could also be considered a developed rural core because of the land uses and activities along the corridor, especially between Highway 1 and Springbank Road.



Figure 5: The community of Bragg Creek is an example of developed rural core

### Outer Developed Rural

The outer developed rural (Figure 6) includes communities from which many people travel to the rural core for work, shopping, services, or school. These types of communities are sometime referred to as ex-urban, and include the acreage-style residential developments. In Rocky View County, this characterizes the communities of Conrich, Springbank, Elbow Valley, and Indus.



Figure 6: Indus has a school and community centre that serves residents from the surrounding area

# MAP 1. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## LAND USE TYPES

### LAND USE

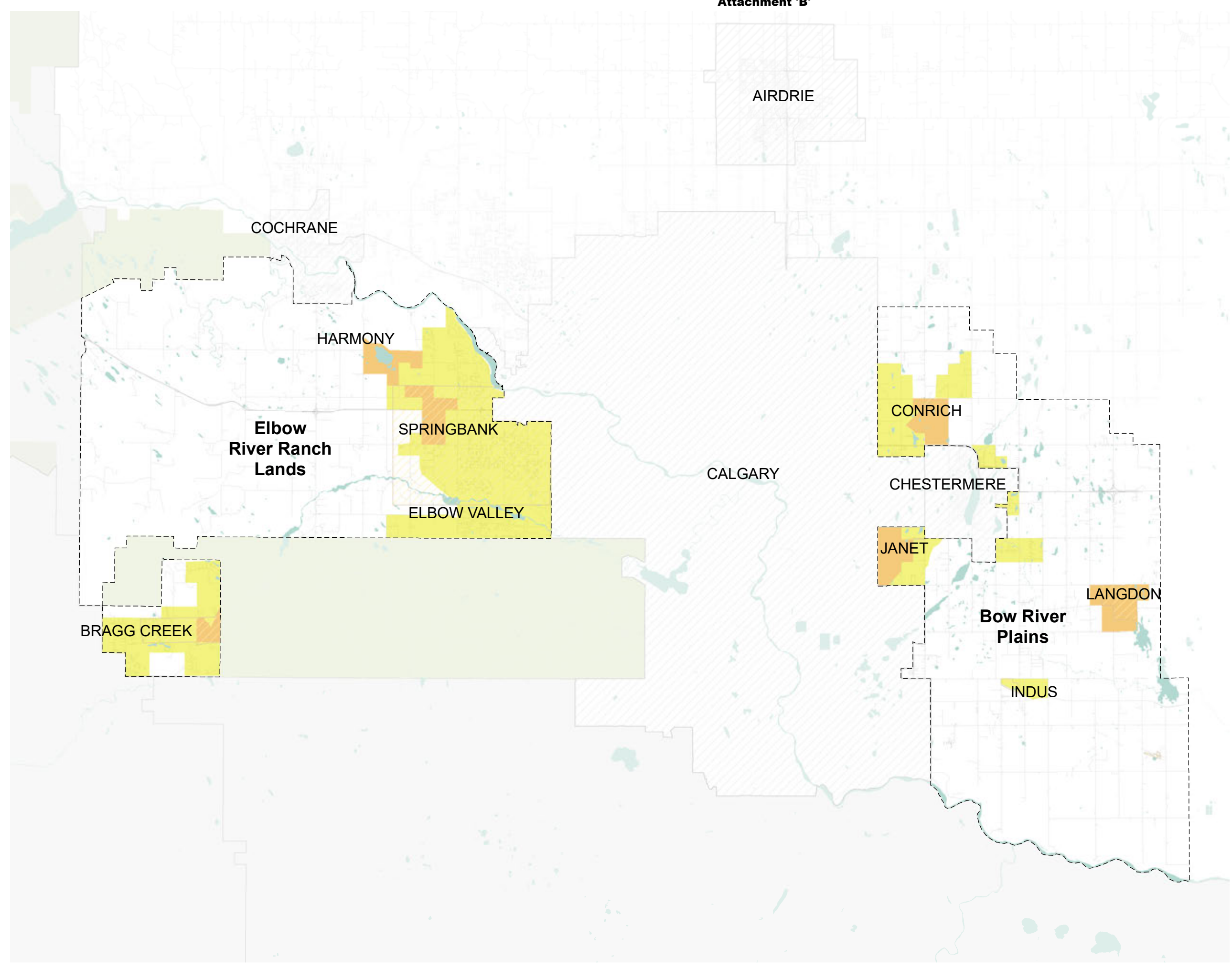
- Developed Rural Core
- Outer Developed Rural
- Basic Rural

### ROAD NETWORK

- Primary Hwy
- Secondary Hwy
- Major
- Collector

### OTHER FEATURES

- Plan Area
- Hamlets
- Water Feature
- Municipal Boundaries



N  
  
 The plan areas shown are consistent with the Parks and Open Space Master Plan regions



1 centimetre = 2,500 metres



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.





### Basic Rural

Basic rural areas (Figure 7) are communities that are relatively underdeveloped and have very low-intensity land uses. The majority of the land area in Rocky View County is agricultural in nature and fits into this category. Full active transportation coverage of a basic rural area is typically not practical, but active transportation facilities will pass through basic rural areas to create connections between developed areas, provide access to natural and community amenities, and to connect to adjacent municipalities.



Figure 7: Basic rural land use characterizes the majority of the plan area

## FACILITY TYPES

The existing active transportation network is comprised of a combination of recreational trails and pathways, sidewalks in limited areas, some shared-use pathways, and the existing street network. Overall, dedicated active transportation facilities are limited, discontinuous, and found mostly within the hamlets of Bragg Creek, Springbank, and Langdon.

### Shared-Use Pathways

Shared-use pathways are physically separated from roadways, either in their own right-of-way or parallel to a roadway. They are generally 2.5 meters to four meters wide and their use is restricted to active transportation modes and motorized mobility devices. They are generally paved with asphalt or concrete or have a hard packed aggregate surface. Shared-use pathways are intended for everyday transportation and recreational trips. In Rocky View, shared-use pathways are categorized as local pathways and regional pathways. The WID Canal Pathway is an example of an existing shared-use pathway.



The WID Canal pathway is a shared-use pathway facility. Image: HDR

**Sidewalks**

Sidewalks are intended for use by pedestrians for walking along a roadway. There are only a few areas that have sidewalks in Rocky View, most often in subdivisions or hamlets. Sidewalks may be located on one side or on both sides of the roadway. The sidewalks have generally been constructed with the development of a new subdivision and may end suddenly without connection to other pedestrian facilities. The widths of the existing sidewalks vary, but are generally 1.5 meters wide. The sidewalks are typically constructed directly next to the curb, and do not include a planted buffer strip.



An example of a sidewalk in Langdon. Image: HDR

**Trails**

Rocky View has many existing natural trails. These trails are generally on aggregate surfaces, throughout the countryside, and are built to varying standards. They are primarily intended for recreational uses.



Some pathways are in their own right-of-way, while others are adjacent to roadways, like the West Bragg Creek Trail. Image: HDR

**Paved Shoulders**

The presence of shoulders in Rocky View County is highly variable. Shoulders are a paved area adjacent to the right general use lane, visually separated with a white painted edgeline. Paved shoulders are used for a variety of purposes including active transportation users, snow storage, and can be used as parking or as a breakdown lane for motor vehicles. In Rocky View, shoulder widths vary from 0.5 m to over 2 m on both sides of a roadway, where they exist.



Paved shoulders are present on some roads in Rocky View County, like this example in Springbank. Image: HDR

**Pedestrian Crossings**

There are some locations in Rocky View County where improvements have been implemented to enhance crossing visibility and provide safer crossing opportunities. Examples include striped high visibility crosswalks and overhead pedestrian crossing signals.



Crosswalks allow pedestrians to cross busy roadways such as this crossing of Centre Street in Langdon. Image: HDR

# EXISTING ACTIVE TRANSPORTATION CONDITIONS INVENTORY

Currently in Rocky View County, the active transportation network is comprised primarily of trails and pathways. These have both a facility type (e.g. shared use or multi use) and a facility status (e.g. existing, adopted or proposed). Pathways and trails with the adopted status are included in plans adopted by the Country and are found in area structure plans, conceptual schemes, and master plans. Pathways and trails with the proposed status have been identified through another planning process.

While a small number of the adopted or proposed pathways and trails are intended to create regional connections, the majority are intended for local use and circulation.

Table 1 summarizes the total distance for facilities in Bow River Plains and Elbow River Ranch Lands in Rocky View County.

Table 1: Length of existing transportation facilities in Bow River Plains and Elbow River Lands

Facility Type	Total Facility Distance (km)
Total length of roadways within plan area	1,100 (approximately)*
County pathways and trails – existing	115
County pathways and trails – adopted	126
County pathways and trails – proposed	251
County sidewalks	34
Paved shoulder (one side of street)	2
Paved shoulder (both sides of street)	237

When viewed at a county-wide scale, the lack of active transportation connections between and within the Plan area is evident. The inventories for the Bow River Plains and Elbow River Ranch Lands are shown on Maps 2 and 3, respectively.

In the Elbow River Ranch Lands, adopted pathways and trails will connect Springbank to Calgary in the east but do not reach Cochrane to the north. Bragg Creek, in the south, is not connected to the active transportation network. In Bow River Plains,

the adopted pathways and trails link Langdon to Boulder Creek. Proposed pathways, particularly in the west, help address gaps in the adopted network. Key connections will include Township Road 244 and Springbank Road.

People crossing the road in Langdon. Image: Alta Planning + Design



# MAP 2. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## ELBOW RIVER RANCH LANDS INVENTORY MAP

### ACTIVE TRANSPORTATION INFRASTRUCTURE

- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed
- Chestermere Paths

### ROAD NETWORK

- Primary Hwy
- Other Roads

### OTHER FEATURES

- Plan Area
- School
- Community Facility
- Church
- Hamlets
- Municipal Boundaries
- Railway
- Rocky View County
- Water Feature

N  
The plan areas shown are consistent with the Parks and Open Space Master Plan regions

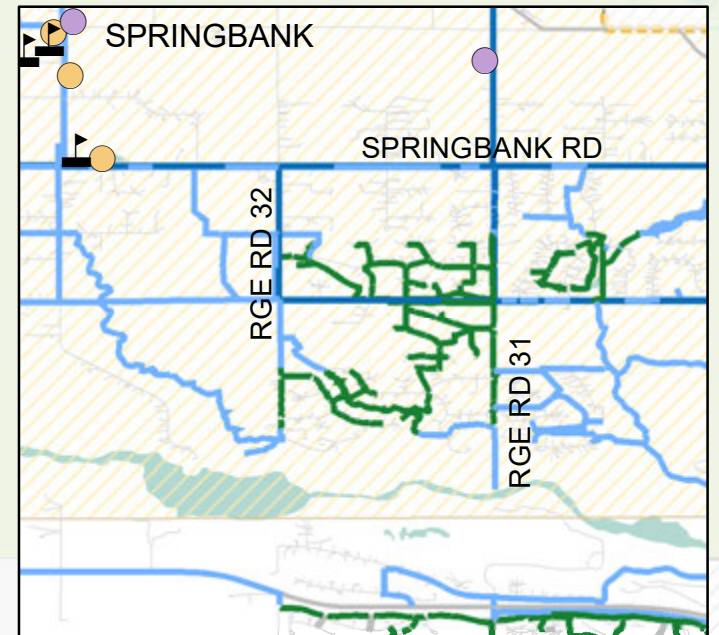
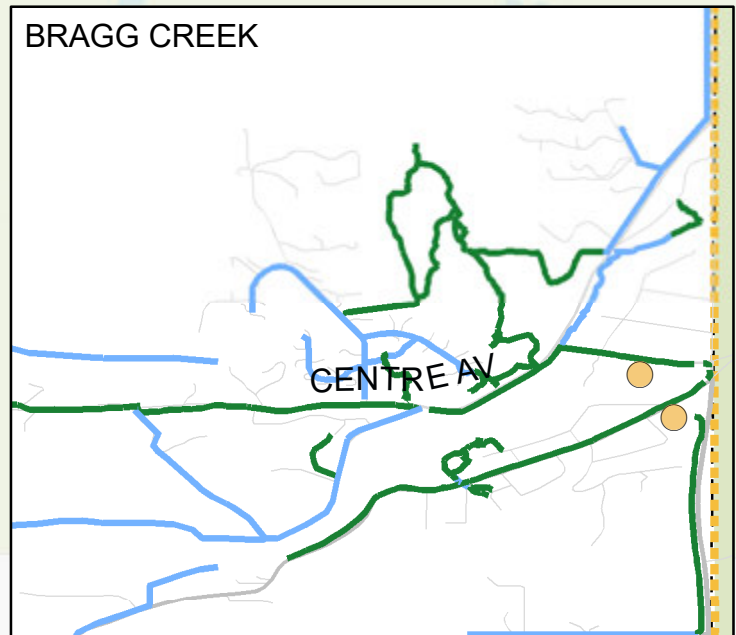
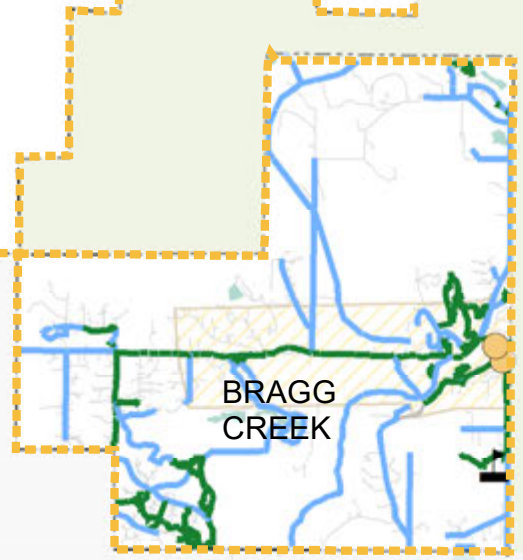
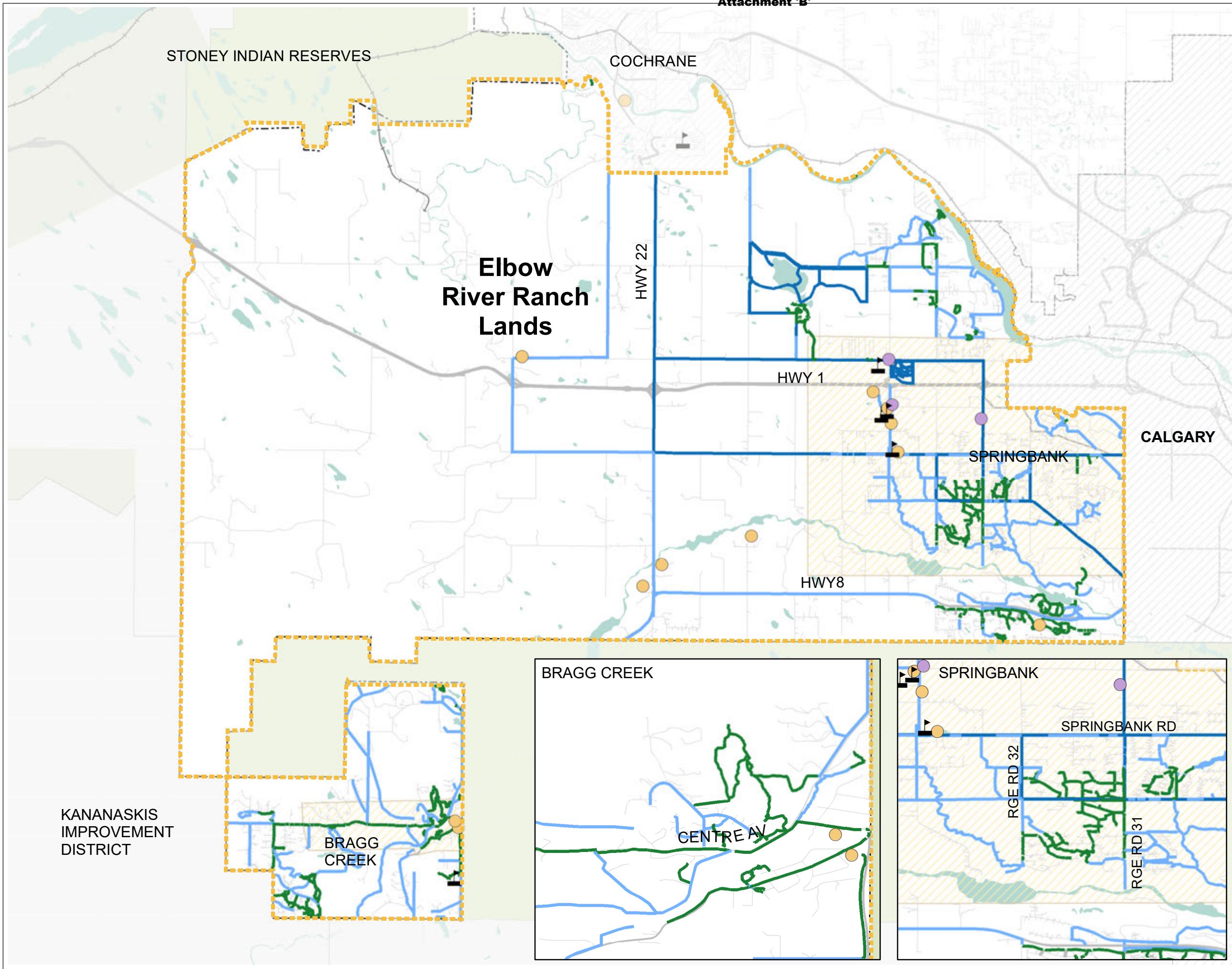


0 2 4 KILOMETRES

1 centimetre = 1,250 metres



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.





# MAP 3. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## BOW RIVER PLAINS INVENTORY MAP

### ACTIVE TRANSPORTATION INFRASTRUCTURE

- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed
- Chestermere Paths

### ROAD NETWORK

- Primary Hwy
- Other Roads

### OTHER FEATURES

- Plan Area
- School
- Community Facility
- Church
- Hamlets
- Municipal Boundaries
- Railway
- Rocky View County
- Water Feature

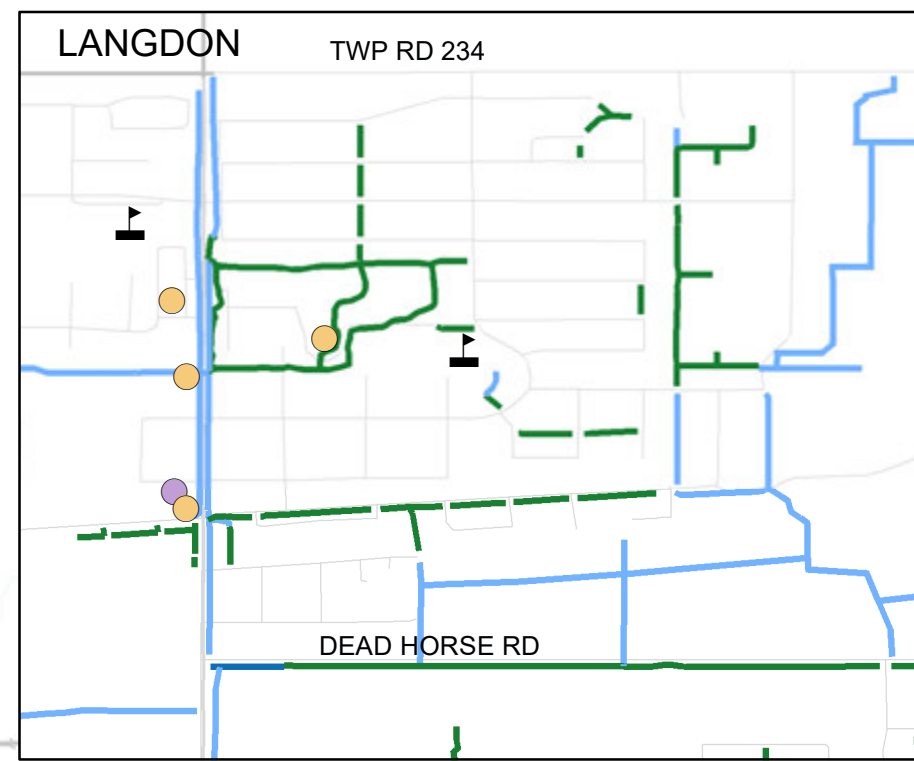
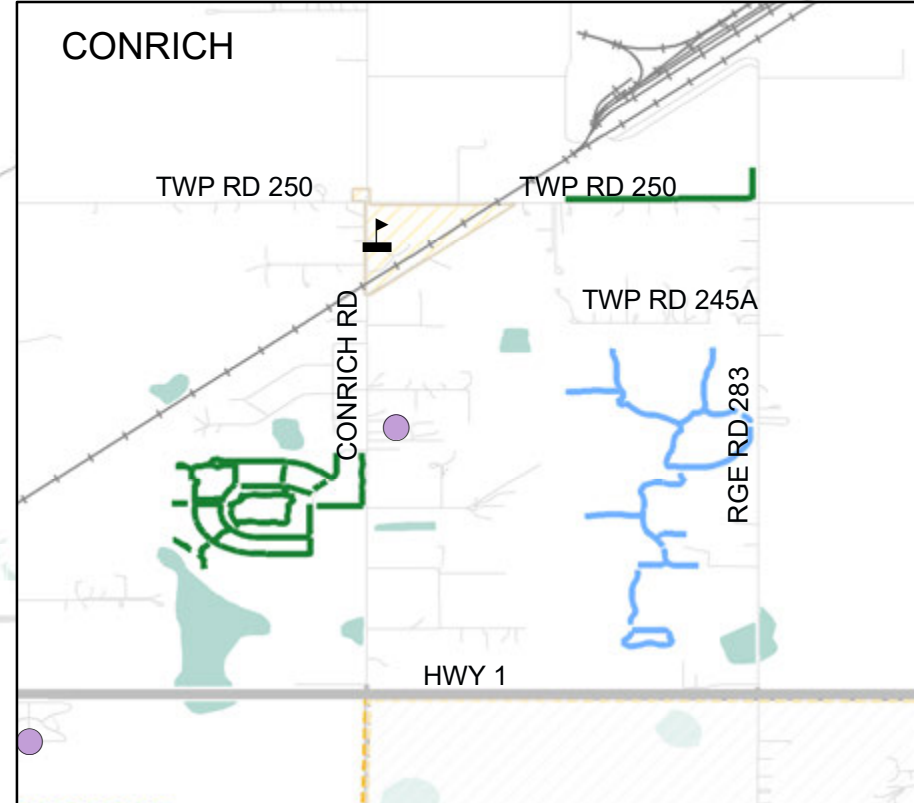
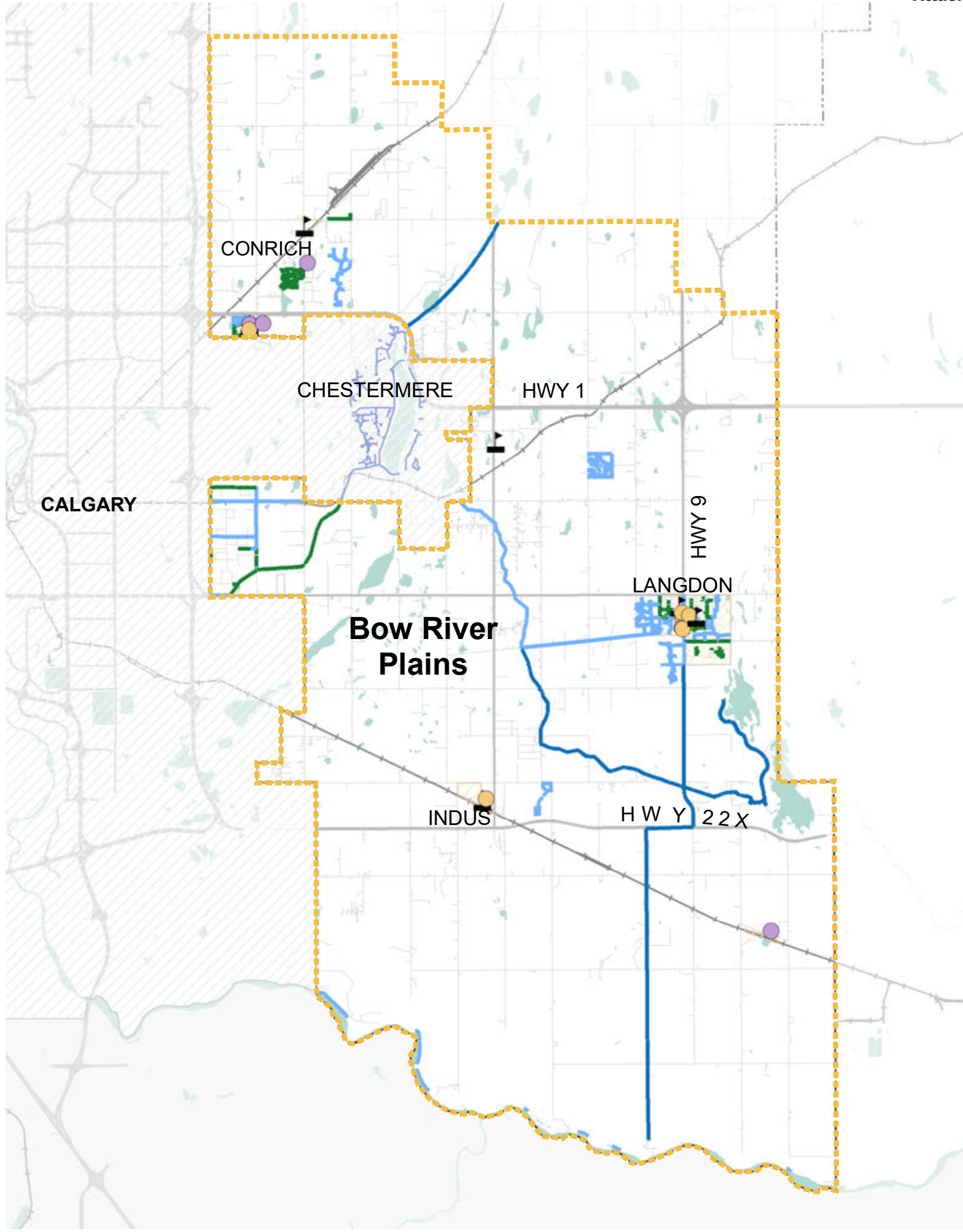
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The plan areas shown are consistent with the Parks and Open Space Master Plan regions



0 2.5 5 KILOMETRES  
1 centimetre = 1,600 metres



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.







**CHAPTER 3**

# Active Transportation Needs Analysis

This section provides an evaluation of active transportation needs and identifies the framework for a near-term active transportation network. This assessment of needs covers the following topics:

- Travel Trends
- Level of Traffic Stress Analysis
- Collision Analysis
- Equity Analysis
- Identified Needs



A youth walks home on Range Road 33 in Springbank. Image: Alta Planning + Design

## TRAVEL TRENDS

Data on travel by active transportation within Rocky View County is limited. There are pockets of concentrated land use where walking, cycling, and other modes are common, but in general, activity is dispersed and most travel occurs by private motor vehicles. Although there is little data specifically related walking and cycling, there are sources of information about travel patterns in Rocky View County that provide insights into existing active transportation activity.

### REGIONAL TRAVEL

The City of Calgary and other communities in the region have a significant influence on travel within Rocky View County. The City of Calgary conducts a comprehensive household travel survey approximately every ten years called the Calgary Household Travel Survey (CHTS). The survey includes the City of Calgary and the surrounding region, including all of Rocky View County, and assesses all modes of travel. The 2011

survey identified several changes in travel patterns since the previous survey, conducted in 2001.

Some of the key findings for the region include:

- More people are travelling to Calgary for work, but discretionary trips generally remain outside Calgary. This suggests there are more trips for purposes like shopping and dining provided locally, in closer proximity to where people live.
- There was a slight mode shift toward high occupancy vehicles and cycling between 2001 and 2011.
- Walk mode share has decreased from about ten percent in 2001 to just under five percent in 2011. This is likely related to a shift away from employment in downtown Calgary, meaning walks over lunch work breaks occur with less frequency and destinations are not as walkable.

## COMMUTING

As part of the 2016 Census of Canada, the Journey to Work data series provides insights into commuting trends and patterns. Similar to the results of the CHTS, the large majority of residents in Rocky View County commute alone by motor vehicle.

Specifically, only two percent of people walk or cycle compared with seven percent nationally and 5.7 percent in Alberta. Surprisingly, even in the absence of a formal public transit system, four percent of Rocky View County residents report transit as their main mode of travel to work.

Residents of Rocky View County spend almost 31 minutes per day commuting to work, compared to just over 26 minutes nationally and 25 minutes for Alberta.

Walking and cycling accounts for a small proportion (about two percent) of all commutes, in part due to the long distances. However, for commutes within Rocky View County (people that live and work in Rocky View County), 13 percent of people commute by active modes, which is considerably higher than the provincial and national averages, indicating that when distances are short, walking and cycling are popular.

## OTHER TRAVEL

Commuting represents an important component of overall travel. But, it is not the only reason people travel. The CHTS showed that households outside the City of Calgary average 3.6 trips a day per household.

Less than 12 percent of commuting trips have an origin and destination within Rocky View County. But, about two-thirds of discretionary

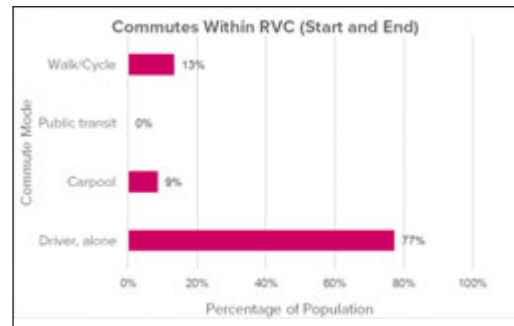
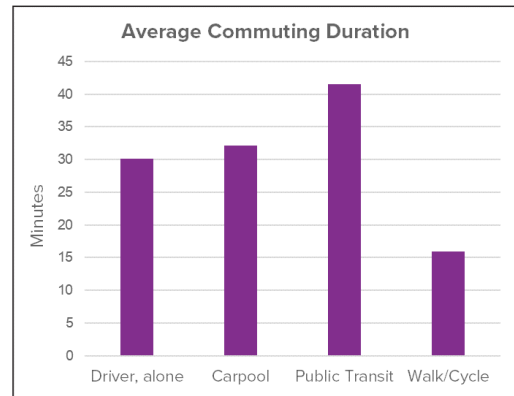
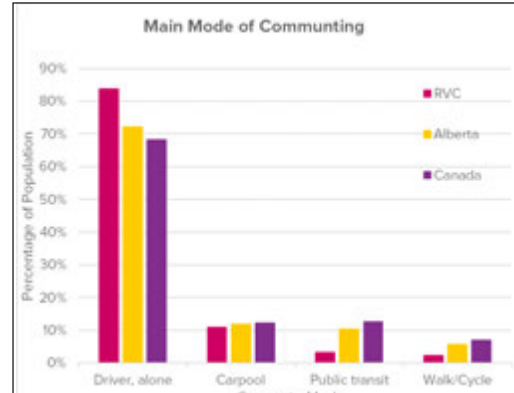


Figure 9: Graphs summarizing the data from the 2016 Census of Canada Journey to Work data

trips such as those for shopping or recreation have an origin and destination within Rocky View County. As identified in the Journey to Work data, there is a higher potential for trips that start and end in Rocky View County to be made by active transportation modes.

### CYCLING TRAVEL PATTERNS

Rocky View County does not currently collect information on cyclist or pedestrian travel such as volumes on particular routes. There are sources of information that can demonstrate the relative popularity of routes such as the Strava Global Heatmaps. Strava information focuses on recreational and fitness training trips, but provides useful information about what corridors are most popular, particularly where multiple routes are available. Figure 10 illustrates the Strava Global Heatmaps showing activity up to May 2018 for cycling in the Elbow River Ranch Lands and Bow River Plains, respectively. The highest use corridors are red, with medium use in dark blue and lowest use in light blue.

A review of the heatmaps shows:

- The highest use east-west corridors are Township Road 250, Springbank Road, and Highway 8 and are well-used relative to other corridors in the region.
- There are several roads within Springbank that also have high use such as Range Road 33, Range Road 32, Banff Coach Trail, Lower Springbank Road, and Highway 22.
- The relative cycling activity in the Bow River Plains is considerably lower than in the Elbow River Ranch Lands with the only corridors showing high use being the WID Canal Pathway and Highway 22X.

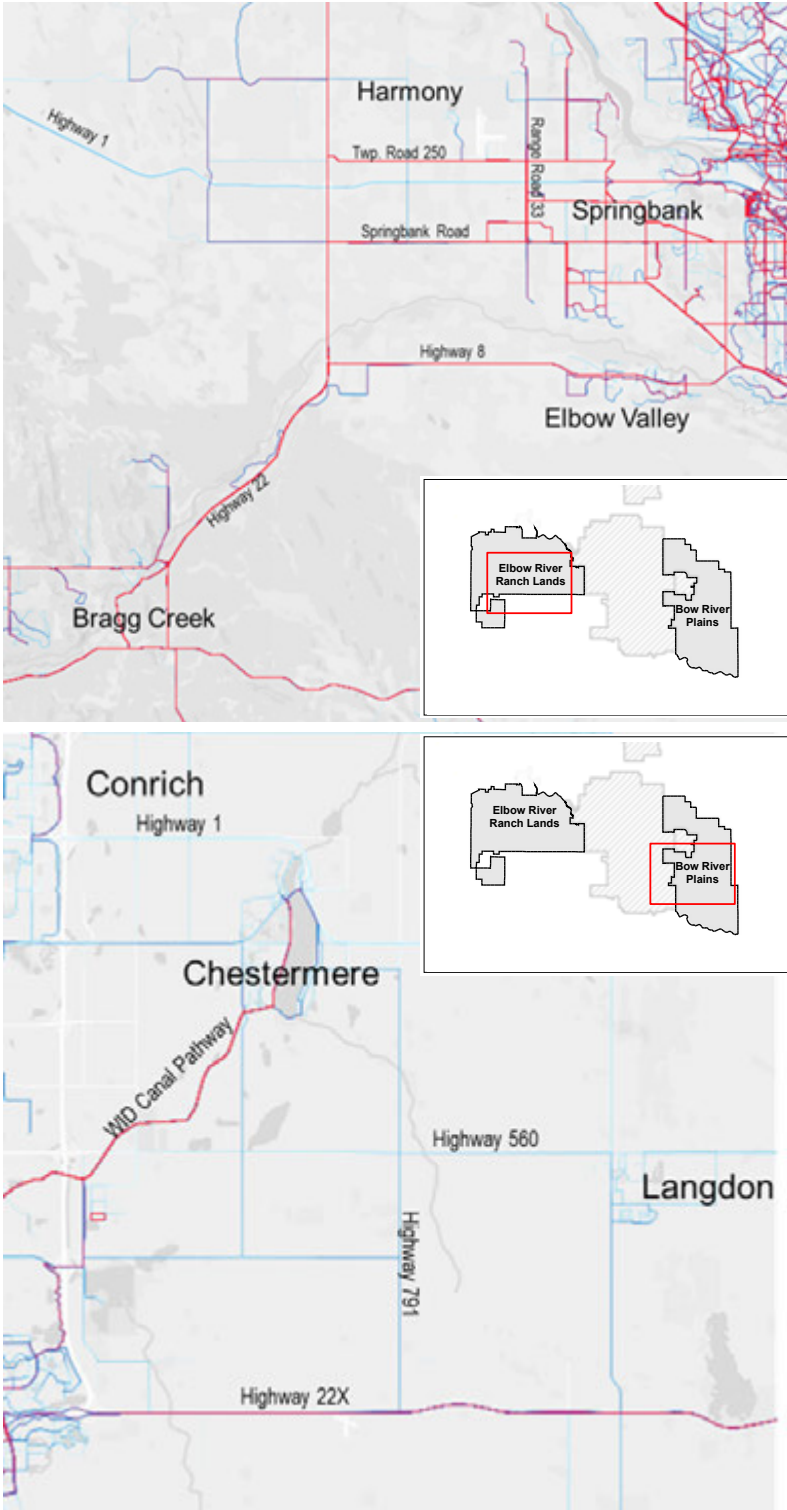


Figure 10: Strava Global Heatmap – Elbow River Ranch Lands (top) and Bow River Plains (bottom) (Retrieved May 2018)

# LEVEL OF TRAFFIC STRESS ANALYSIS

The Level of Traffic Stress (LTS) Analysis identifies a perceived level of comfort for potential cyclists determined based on factors such as posted speed limit, roadway width or number of traffic lanes, and the presence and character of bicycle lanes or other bicycle infrastructure. The combination of this criteria separates the bicycle network into one of four scores as shown in Figure 11.

While this analysis is focused primarily on cyclists, the speed and volume factors have been shown to provide a reasonable understanding of the typical pedestrian experience and can be used as a proxy to draw high level conclusions about the comfort of current walking conditions in Rocky View County.

The majority of roads in the County have an LTS score of 3 or 4 due to their high posted speeds. Many local roads in subdivisions and in Langdon received low LTS scores due to their lower posted speeds. The results of this analysis are shown on Maps 4 and 5. Additional details of the analysis and methods are included in Appendix D.

LTS 1



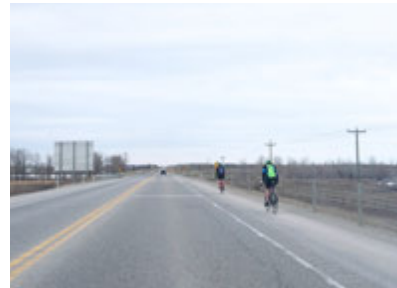
Appropriate for all ages and abilities

LTS 2



Appropriate for most adults

LTS 3



Appropriate for some adults

LTS 4



Appropriate for confident adults

Figure 11: The four LTS scores with examples to illustrate a roadway/facility in Rocky View County with the corresponding LTS score

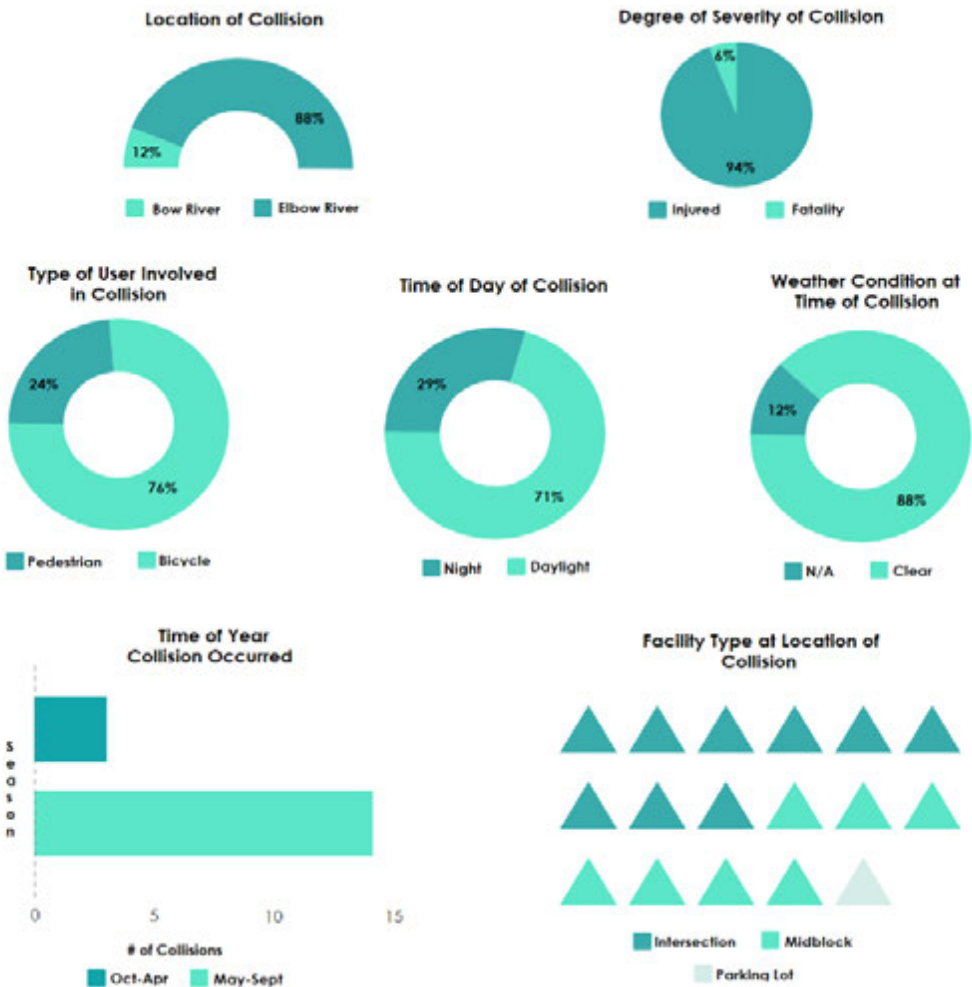
# SAFETY ANALYSIS

A detailed review of pedestrian- and cyclist-involved collisions between 2011 and 2015 was conducted, which is the most-recent five-year period of data available for the Plan area. The data was derived from police reports where location of the collision was described in text form and often included estimated distances.

In addition to collision frequency, the review included consideration of roadway

characteristics including: location (midblock or at intersection), speed limit, number of lanes, and presence of an existing active transportation facility such as a paved shoulder, pathway, trail, or sidewalk. The safety analysis results are summarized graphically below. The locations of collisions involving pedestrians or cyclists in the 2011 to 2015 period are shown on Map 6. Additional details are included in Appendix D

Figure 12: Infographic illustrating the results of the safety analysis



# MAP 4. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## ELBOW RIVER RANCH LANDS LEVEL OF STRESS ANALYSIS

### Level of Traffic Stress Analysis

- 1- Low Stress Roadway
- 2
- 3
- 4- High Stress Roadway

- Existing Regional Pathway
- Existing Local Pathway or Trail
- Existing Sidewalk

### OTHER FEATURES

- Plan Area
- Hamlets
- Municipal Boundaries
- Rocky View County
- Water Feature



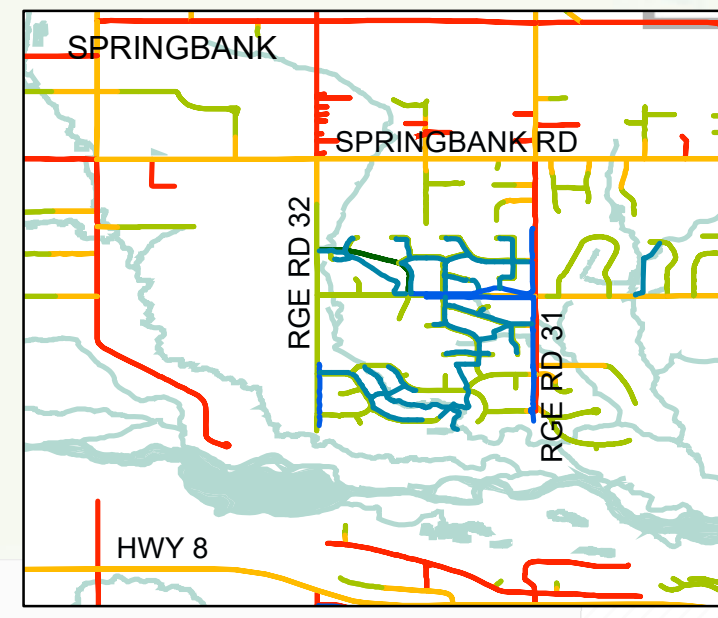
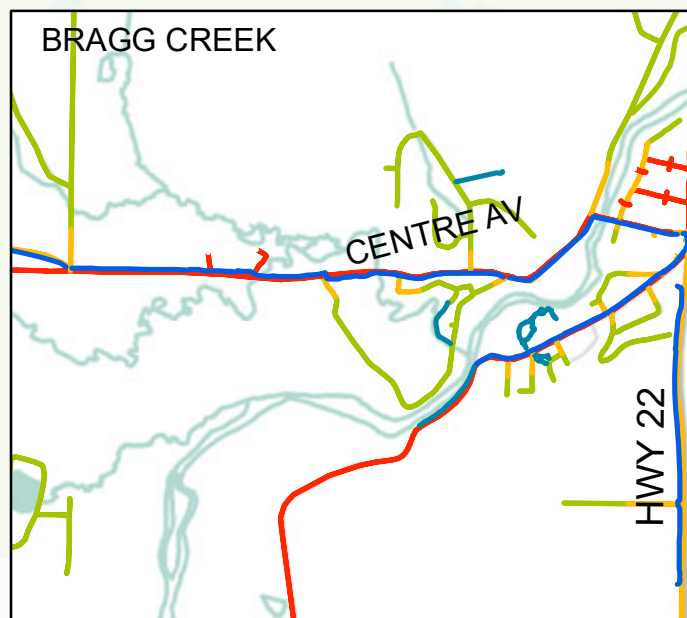
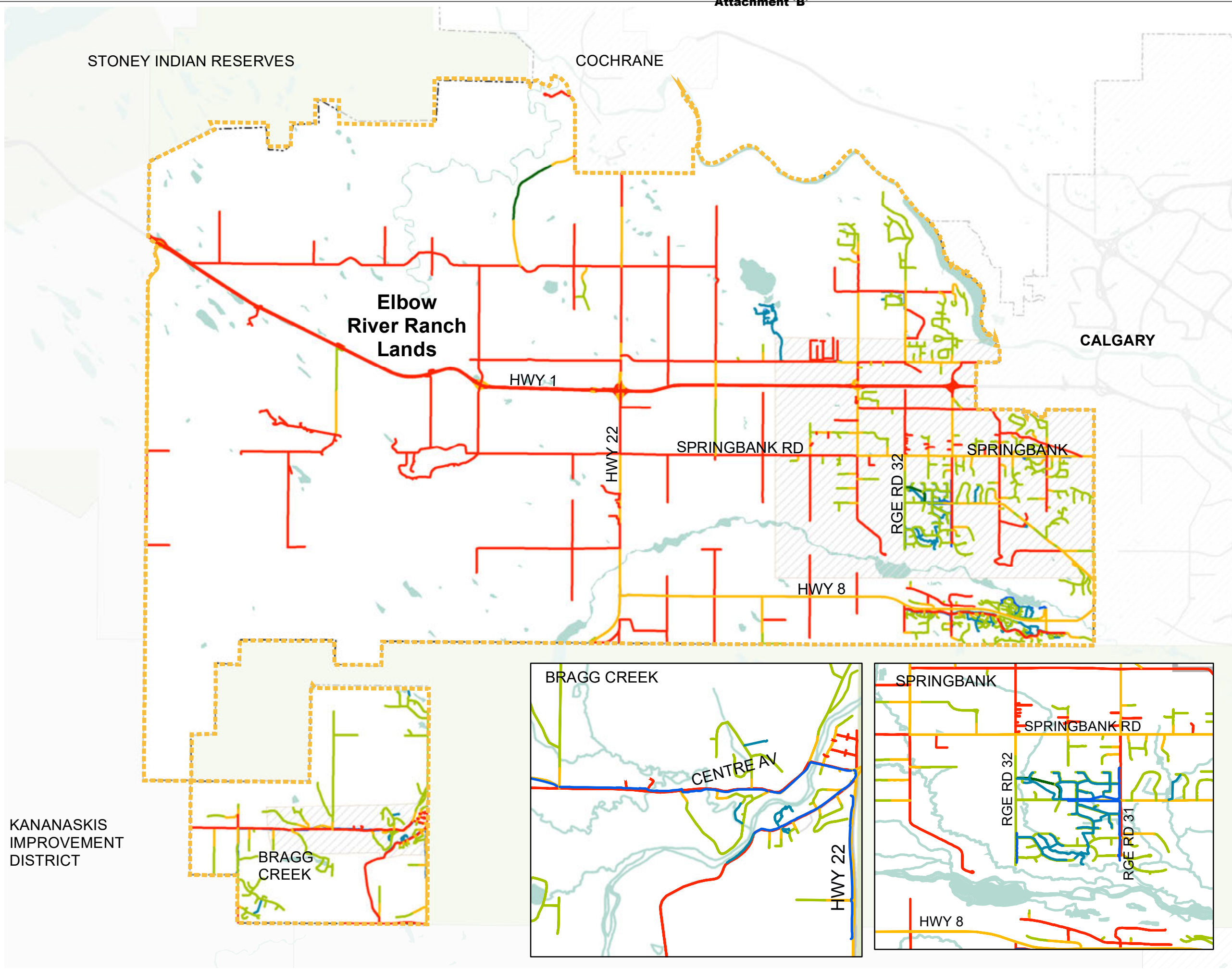
The plan areas shown are consistent with the Parks and Open Space Master Plan regions



0 2 4 KILOMETRES  
1 centimetre = 1,250 meters



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.







# MAP 5. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## BOW RIVER PLAINS LEVEL OF STRESS ANALYSIS


Level of Traffic Stress Analysis

- 1- Low Stress Roadway
- 2
- 3
- 4- High Stress Roadway

- Existing Regional Pathway
- Existing Local Pathway or Trail
- Existing Sidewalk

### OTHER FEATURES

- Plan Area
- Hamlets
- Municipal Boundaries
- Rocky View County
- Water Feature

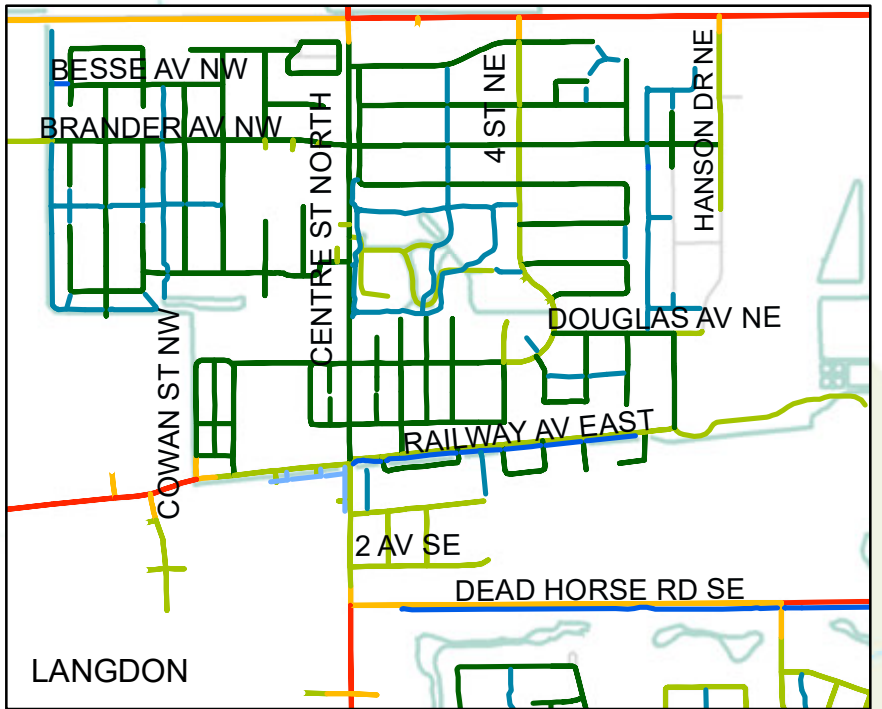
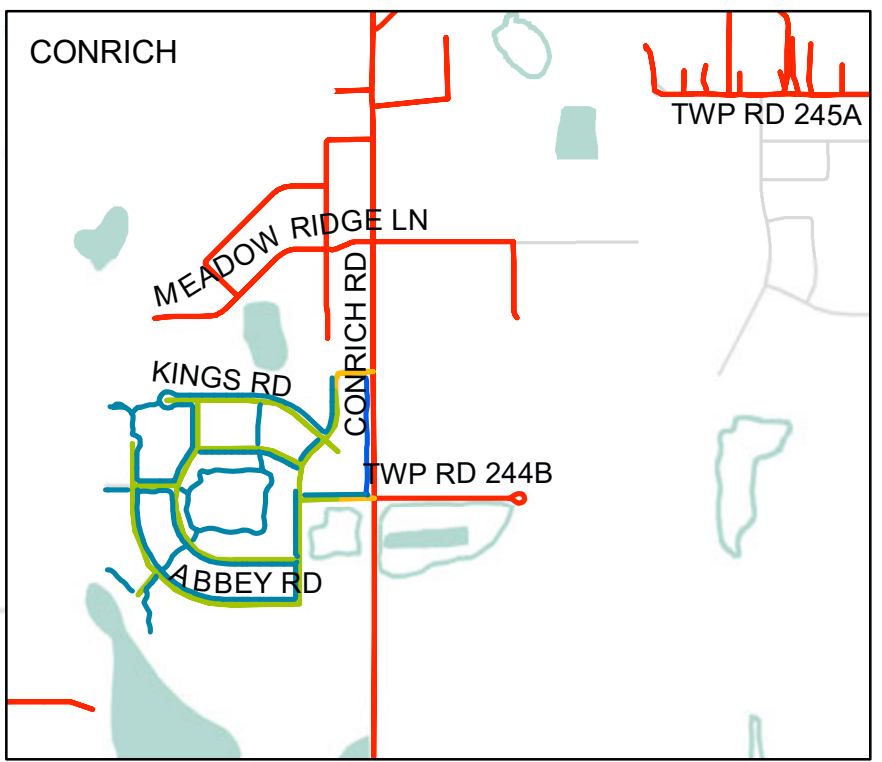
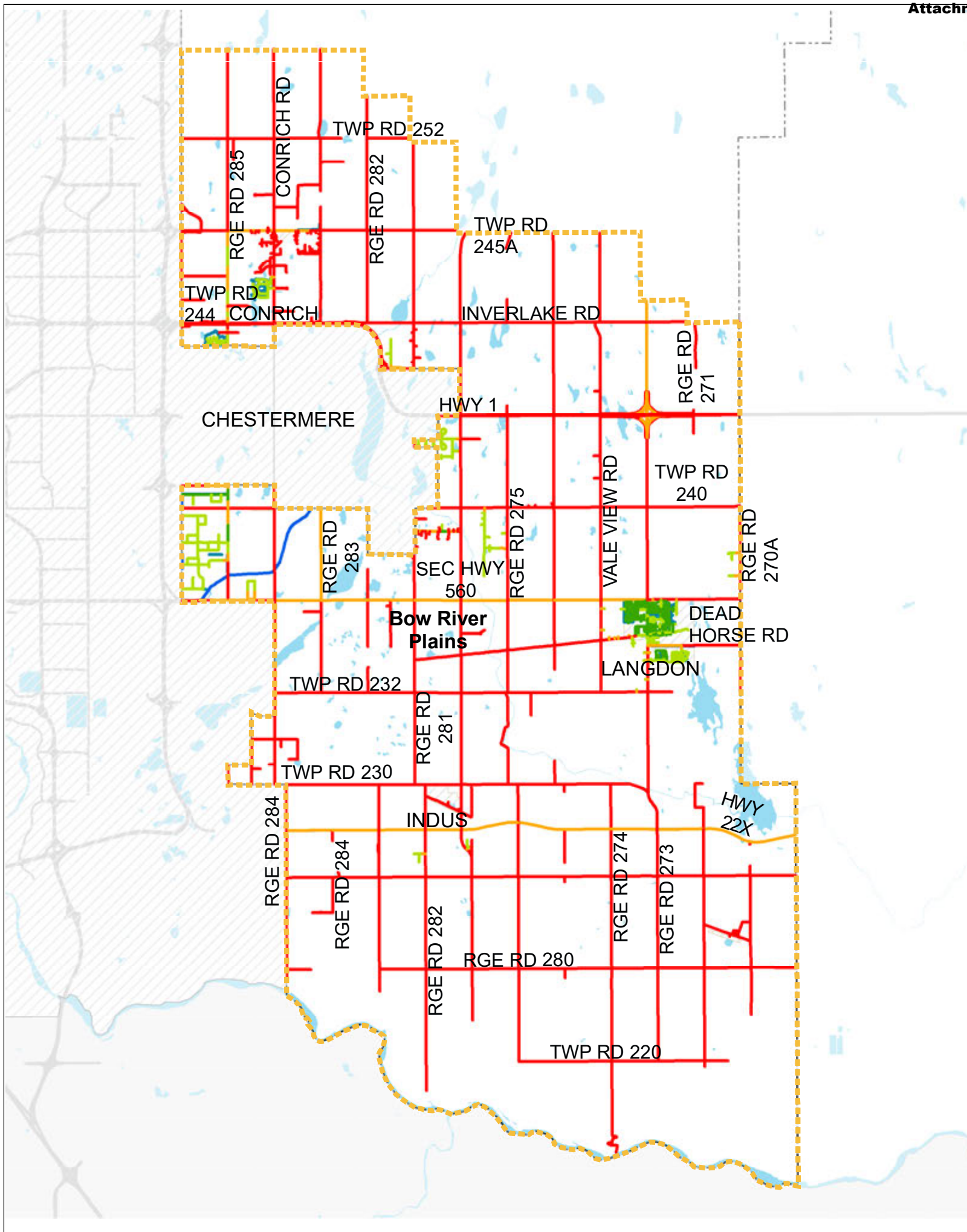
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 The plan areas shown are consistent with the Parks and Open Space Master Plan regions



0 2.5 5 KILOMETRES  
 1 centimetre = 1,600 metres



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.





# MAP 6. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## COLLISIONS

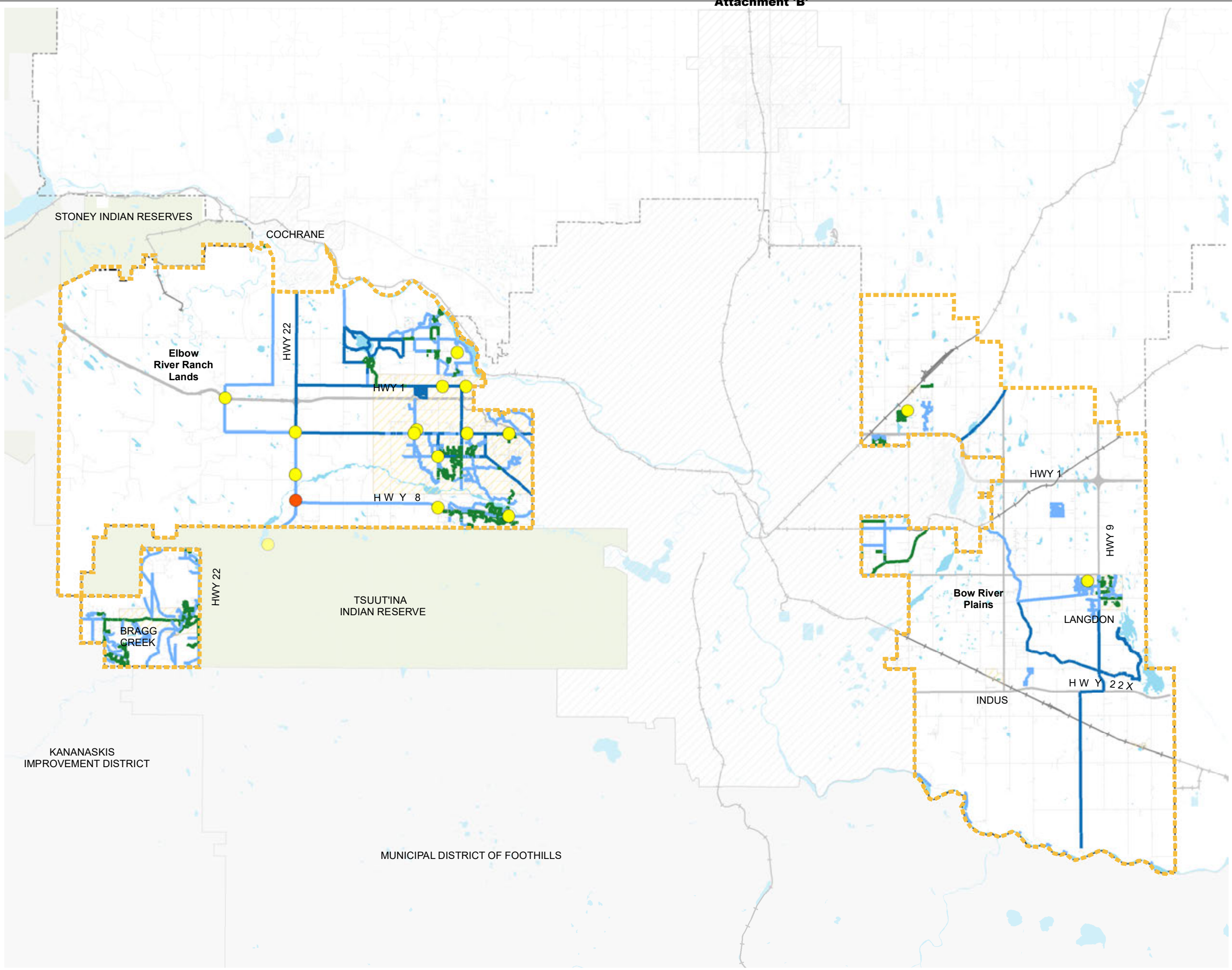
- Collisions with Fatality
- Collisions with Injury

## ACTIVE TRANSPORTATION INFRASTRUCTURE

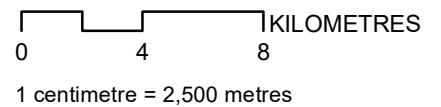
- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed

## OTHER FEATURES

- Railway
- ▭ Plan Area
- ▭ Hamlets
- ▭ Municipal Boundaries
- ▭ Rocky View County
- ▭ Water Feature



N  
 The plan areas shown are consistent with the Parks and Open Space Master Plan regions



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.



# EQUITY ANALYSIS

Equity mapping is the process of visually depicting the general areas where historically vulnerable or disadvantaged groups of people reside according to the Census. In the simplest of terms, equity recognizes that different people experience different barriers when travelling in Rocky View County.

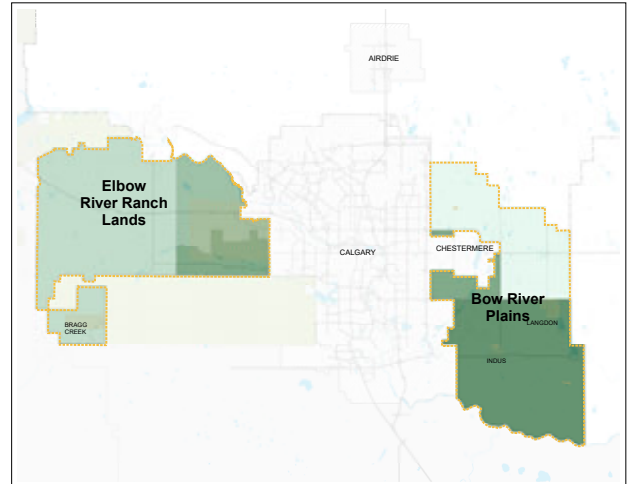
The 2016 Census of Canada data was retrieved for census tracts in Rocky View County that fall predominantly within the Plan area. For purposes of analysis, the following socio-economic indicators define the potential underserved populations, as shown as thumbnail maps on Figure 13.

- Percentage of population aged 19 or younger
- Percentage of population aged 65 or older
- Prevalence of low income population
- Percentage of population with recognized Aboriginal identity
- Percentage of population without a high school diploma (or equivalency)

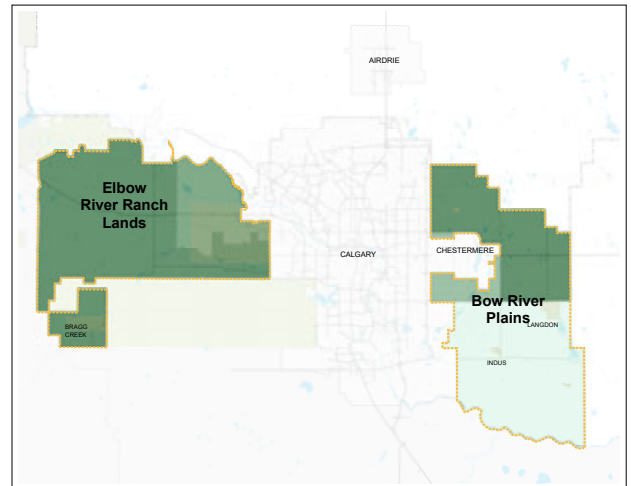
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### *Kern County ATP*

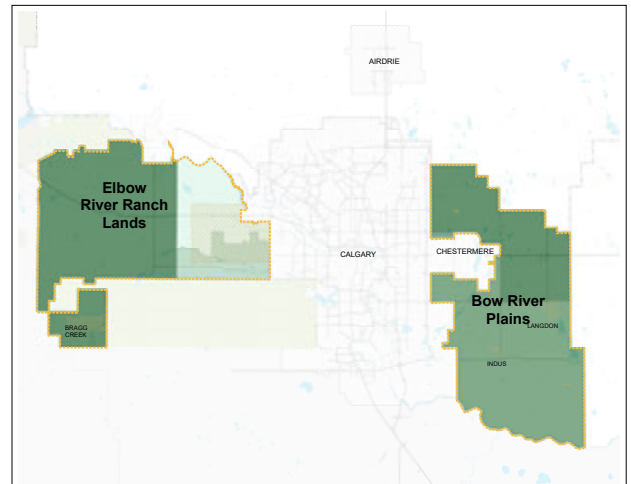
*Kern County uses focus areas to provide attention to urban and suburban areas as well as specific user groups such as lower income communities. The focus area strategy has been used for the Rocky View County ATPSC with focus areas being Bragg Creek, Springbank, and Langdon.*



Youth (19 or younger)



Seniors

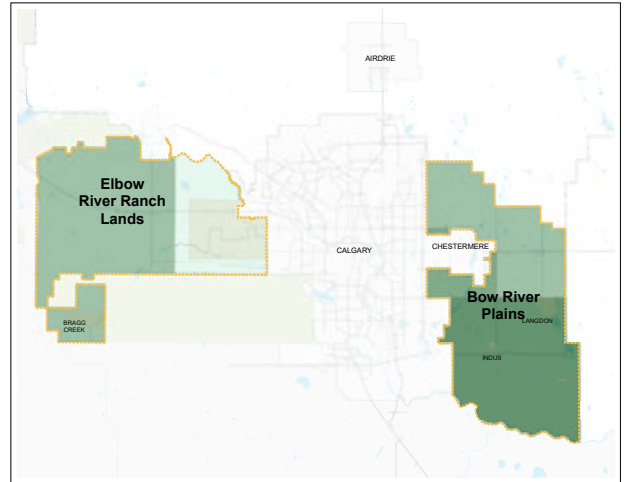


Low Income

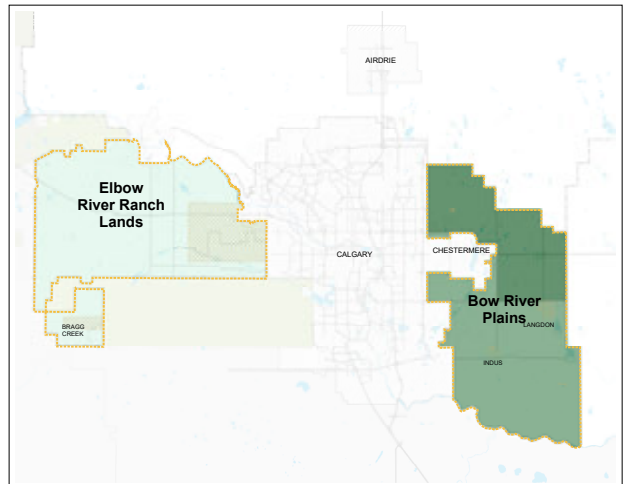
The analysis used the Alberta mean average as a threshold for each of the above indicators, so that census tracts that had a greater value than the Alberta statistical mean were given a score of one. For example, if a census tract had an above average number of adults aged 65 and older, and above average population without a high-school diploma, then the census tract was given a score of two. The highest equity score possible was a five and the lowest possible score was zero. This total equity score combined with further review of the component parts were used to identify areas where making active transportation investment could make the most impact.

### EQUITY ANALYSIS RESULTS

Areas of inequality are spread throughout the South County and were summarized at a level that allows only general conclusions to be drawn. Generally, the Plan area has above average populations of children and seniors, and population without a high-school diploma. Based on this information, there are needs for seniors and children who can benefit from enhanced connections to schools and community facilities such as libraries, community centres, and recreation centres.



Aboriginal Identity



Education Attainment



People currently walk along an unpaved shoulder to access stores in Langdon. Image: HDR

## IDENTIFIED NEEDS

The demand for travel is generated by land use and associated activities – whether employment sites, schools, retail centres, or residential areas. Not all trips can be made by active modes, but there is an opportunity to better align land use and needs to make active transportation a more attractive option for current and potential users. The land use types identified in the Chapter 2 help to frame the Needs Identification process.

Connections between communities within Rocky View County to active transportation networks in neighbouring municipalities and safe routes to parks and natural amenity areas have been identified as priorities for the ATPSC. Currently, gaps exist that make walking and cycling between existing destinations challenging. The introduction of active transportation facilities can “fill in the gaps” for many of these connections. Beyond addressing gaps in the network, the identified needs analysis builds on the LTS, safety, and equity analyses.

A portion of the ATPSC focused on specific identified issues in these following three communities.

- Langdon – connections to schools and Centre Street
- Bragg Creek – improving Balsam Avenue, White Avenue, and Highway 22
- Springbank – continuous connectivity through Springbank

Identified Needs are described on Maps 7 and 8. Details of the corridors are found in Appendix E.

Table 2 summarizes the priority connections and needs in the Elbow River Ranch Lands.

The connections between Rocky View communities will generally take advantage of existing north-south and east-west routes, with a focus on the currently popular routes. Given the distances, most of these connections will be suited to more experienced and confident cyclists and pedestrians and will be less of a priority for facility investment than those connections suited to all ages and abilities.

The Transportation and Utility Corridor (TUC) creates a barrier for connections to Calgary. Therefore, the connections the east of the TUC into Calgary will need to take advantage

Table 2: Elbow River Ranch Lands priority connections

<b>Elbow River Ranch Lands Communities</b>	<ul style="list-style-type: none"> <li>• Bragg Creek</li> <li>• Range Road 33 (Township Road 250 to Springbank Road)</li> <li>• Harmony</li> <li>• Other Developed Rural areas (Remainder of Springbank, Elbow Valley, areas around Bragg Creek)</li> </ul>
<b>Neighbouring Communities</b>	<ul style="list-style-type: none"> <li>• Cochrane</li> <li>• Calgary</li> </ul>
<b>Amenity Areas</b>	<ul style="list-style-type: none"> <li>• West Bragg Recreation Area</li> <li>• Glenbow Ranch Provincial Park</li> <li>• Elbow River</li> <li>• Weaselhead (in the City of Calgary)</li> </ul>

of appropriate crossings of the TUC. The proposed West Ring Road interchanges at 17 Avenue SW / Township Road 242 and Old Banff / Springbank Road have ramps on the south side of West Ring Road crossings. This will allow a shared-use pathway or other active transportation facility to be developed on the north side of the road with no interaction with freeway ramps. By contrast, the Bow Trail crossing of the West Ring Road will include ramps in all four quadrants, some at high speed, which are uncomfortable for cyclists and pedestrians to cross.

Highway 22 is the primary north-south route within the County. With a lower level of traffic stress than many of the other roads, it has higher cycling use than many other roadways. It is the only continuous north-south route, and is the only crossing of the Elbow River. It provides access to Cochrane to the north and is the connection between Bragg Creek and the rest of the County. Range Road 33 provides a north-south connection in Springbank and, most importantly, between the schools and recreation centre.

The priority corridors and needs for the Bow

River Plains are summarized in Table 3.

There are several destinations within and adjacent to the Bow River Plains that can be accessed with longer-distance active transportation links. Chestermere includes a range of services and amenities that are not available in the County. Similarly, Calgary supports a high proportion of employment in the region. Within the Bow River Plains, there is a potential cycling desire line between Langdon and the Indus Recreation Centre. Janet is an emerging employment centre within the region and could be a commuting destination, particularly for those commuting from Langdon.

There other destinations and corridors that are opportunities to create pleasant active transportation corridors. There is an opportunity to extend the existing Western Irrigation District Canal southward to provide access to Langdon (along with Township Road 233), to Indus (with connections along Range Road 275, Township Road 230 and Highway 791). Weed Lake east of Langdon may be a popular destination and an appropriately-placed shared-use pathway could create a loop that is easily accessed from Langdon. Although distant from Langdon and other populated areas, McKinnon Flats is a popular destination and could be a destination for day trips by bike. Chestermere Lake is approximately 11 kilometres from Langdon and seven kilometres from Conrich (straight-line distance) and could be accessed by bicycle from these communities with the appropriate facilities.

Table 3: Bow River Plains priority connections

<b>Bow River Plains Communities</b>	<ul style="list-style-type: none"> <li>• Langdon</li> <li>• Conrich</li> <li>• Janet</li> <li>• Indus</li> </ul>
<b>Neighbouring Communities</b>	<ul style="list-style-type: none"> <li>• Chestermere</li> <li>• Calgary</li> </ul>
<b>Amenity Areas</b>	<ul style="list-style-type: none"> <li>• Chestermere Lake</li> <li>• Weed Lake Complex</li> <li>• McKinnon Flats</li> </ul>



# MAP 7. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN ELBOW RIVER RANCH LANDS IDENTIFIED NEEDS

## ACTIVE TRANSPORTATION INFRASTRUCTURE

- Identified Gap
- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed

## LAND USE

- Developed Rural Core
- Outer Developed Rural
- Basic Rural

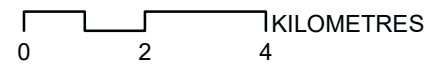
## ROAD NETWORK

- Primary Hwy
- Secondary Hwy
- Major
- Collector

## OTHER FEATURES

- Plan Area
- Municipal Boundaries
- Rocky View County
- Water Feature

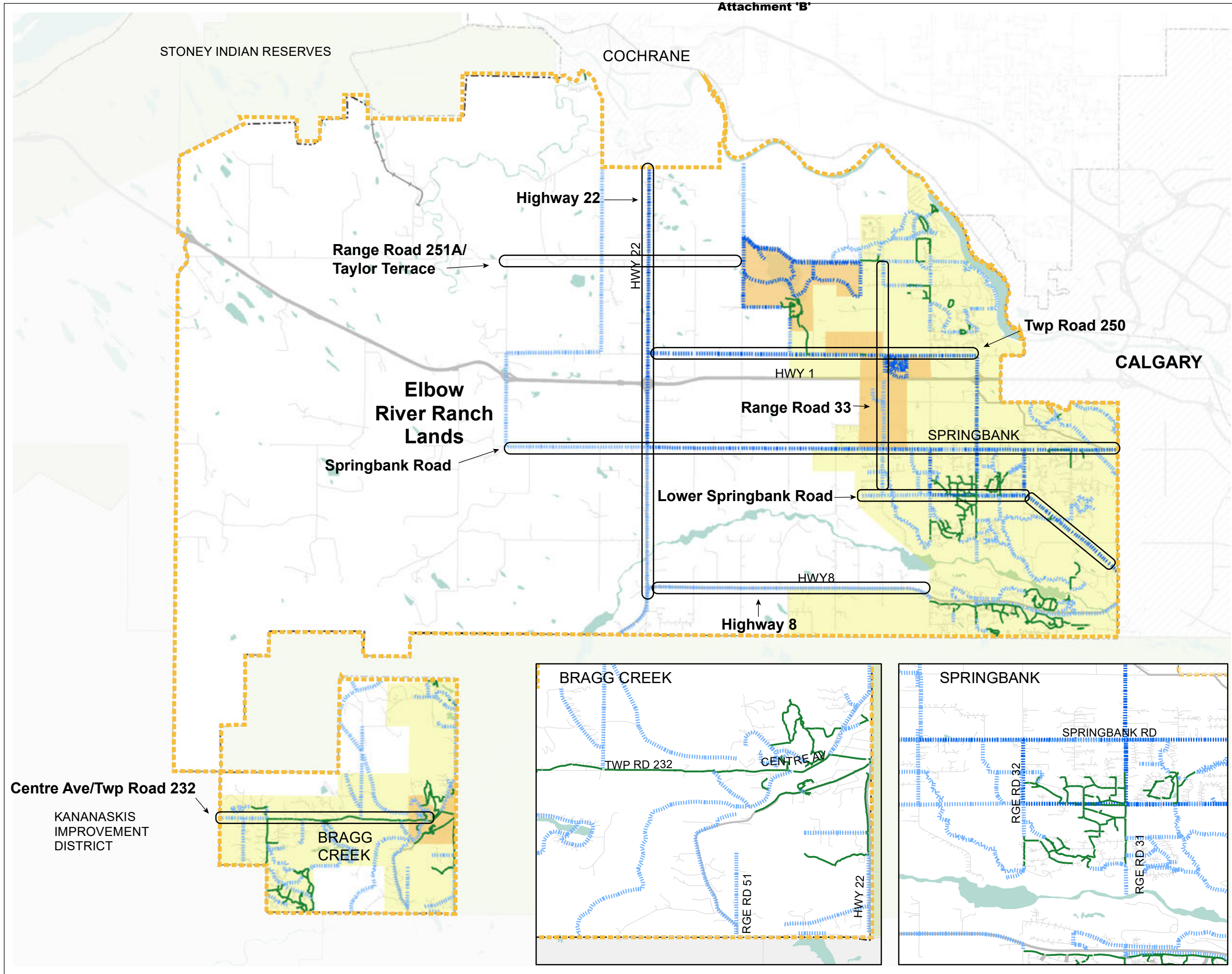
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The plan areas shown are consistent with the Parks and Open Space Master Plan regions



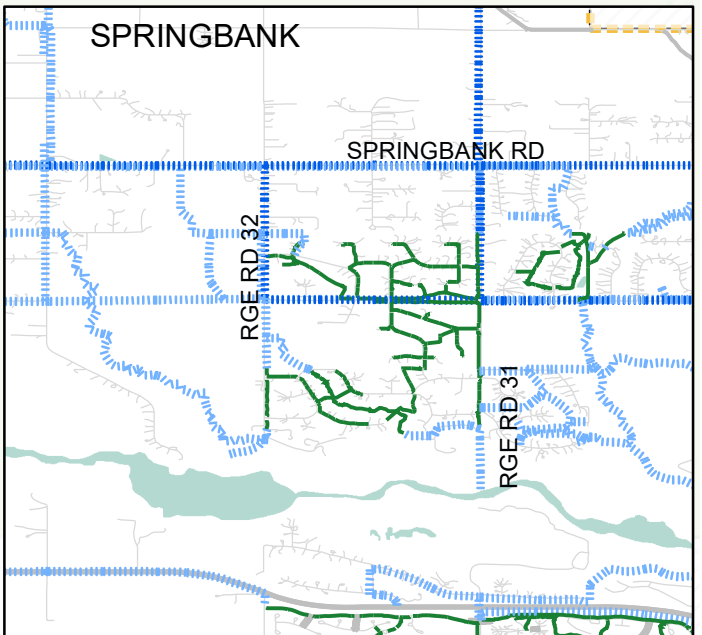
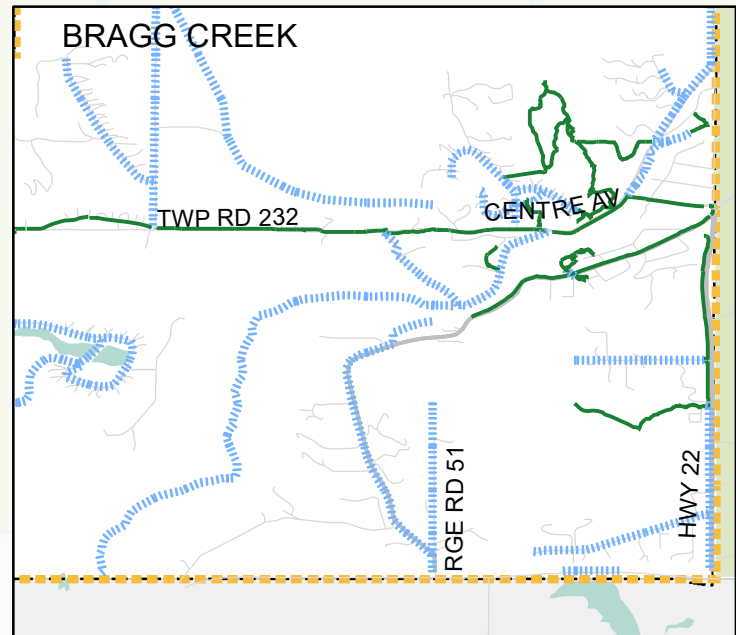
1 centimetre = 1,250 metres



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.



Centre Ave/Twp Road 232  
KANANASKIS IMPROVEMENT DISTRICT





# MAP 8. ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## BOW RIVER PLAINS IDENTIFIED NEEDS

### ACTIVE TRANSPORTATION INFRASTRUCTURE

- Identified Gap
- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed

### LAND USE

- Developed Rural Core
- Outer Developed Rural
- Basic Rural

### ROAD NETWORK

- Primary Hwy
- Secondary Hwy
- Major
- Collector

### OTHER FEATURES

- Plan Area
- Municipal Boundaries
- Rocky View County
- Water Feature

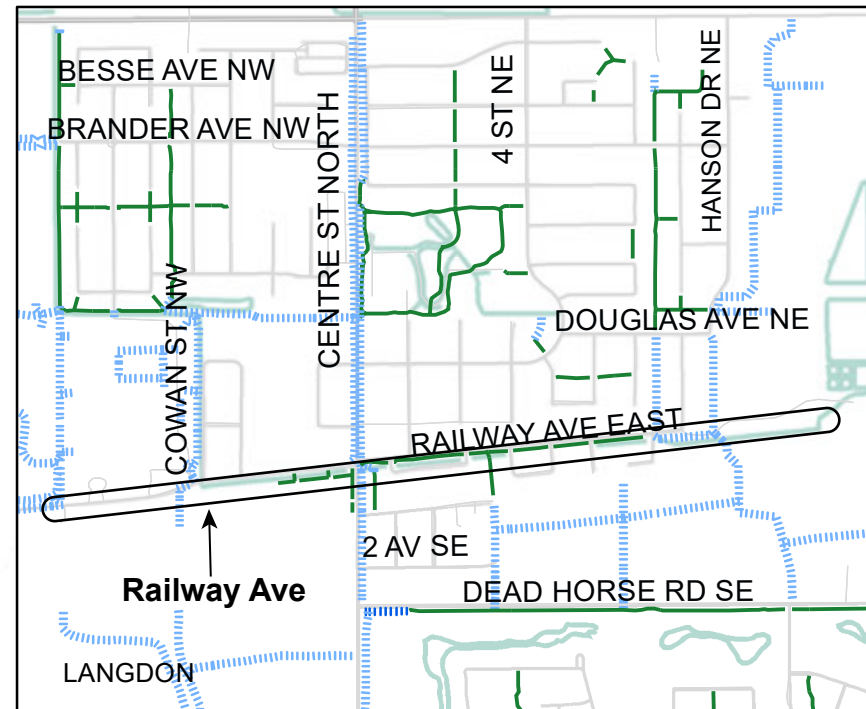
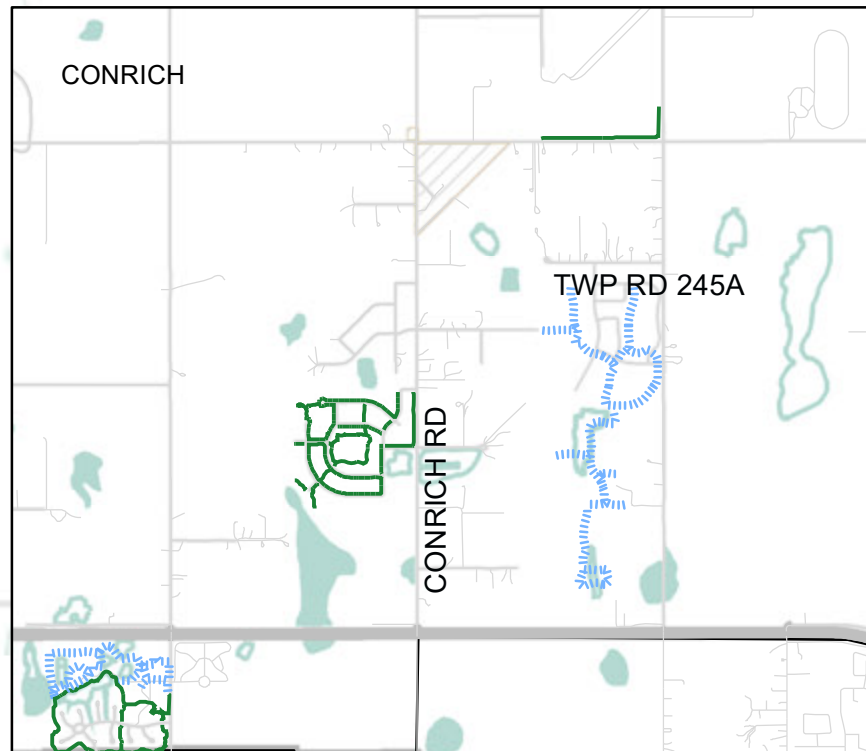
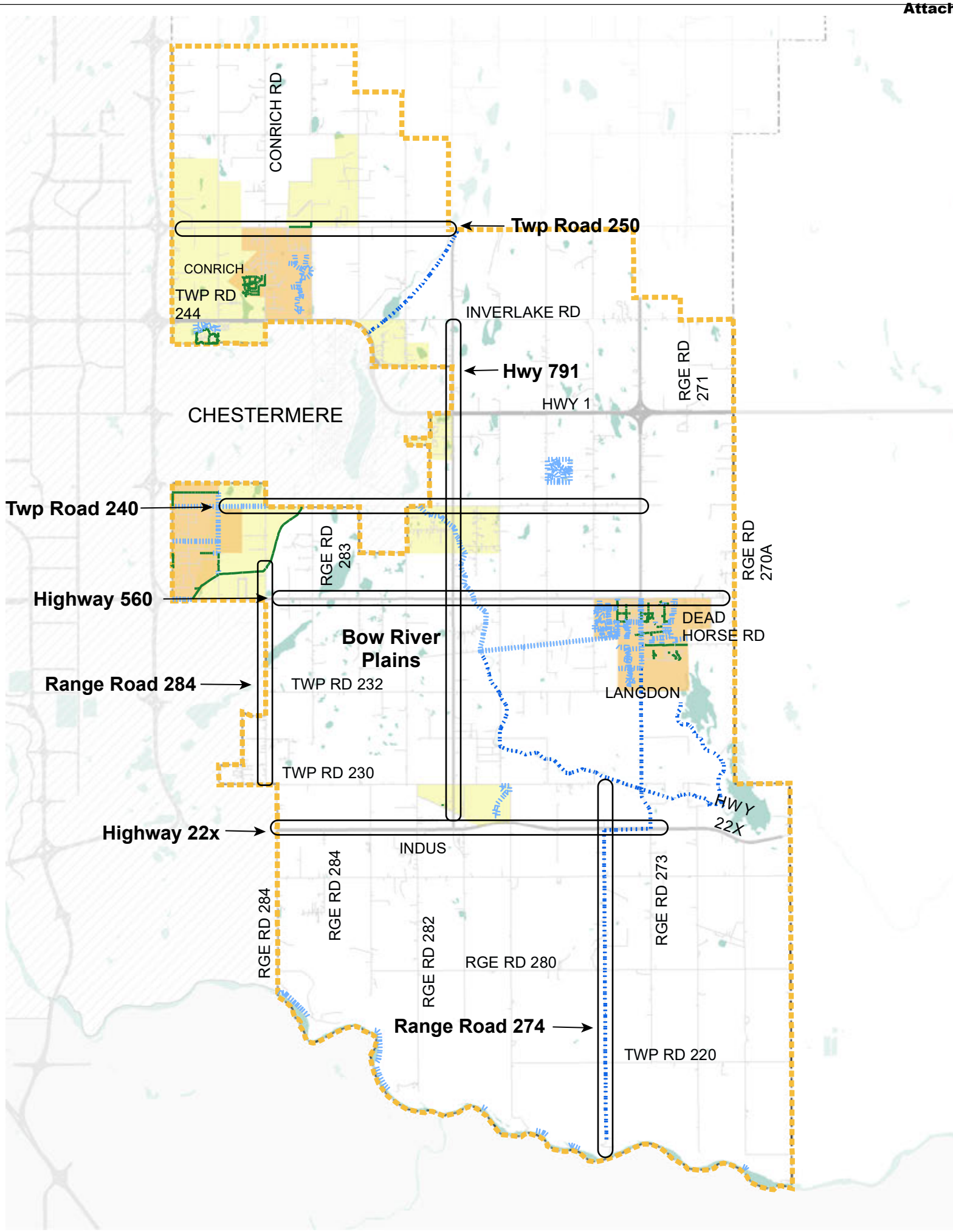
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The plan areas shown are consistent with the Parks and Open Space Master Plan regions



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1 centimetre = 1,600 metres



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.





**CHAPTER 4**

# Active Transportation Network

This chapter provides the following information:

- Facility Design Toolbox
- Facility Selection Matrix
- Network Recommendations (5-year timeframe)
- Area Specific Plan Recommendations (5-year timeframe)

The recommended active transportation network for Rocky View County is comprised of on-street bicycle facilities, shared-use pathways and dedicated pedestrian facilities. The recommendations are context sensitive to land use, probable user types, and local conditions. In Chapter 2, the land use in Rocky View County was organized into three types based on those described in the FHWA Small Town and Rural Multimodal Networks Guide. Connections in Developed Rural areas are described in area plans, while Outer Developed Rural and Basic Rural recommendations are included in the generalized network recommendations.

## FACILITY DESIGN TOOLBOX

The Rocky View County Plan (2013) policies 12.7 and 12.11 identify a need for guidelines on facility types and treatments in Rocky View and guidance on where types of facilities should be implemented. The facility design toolbox provides a summary of the facilities that are recommended as part of this plan and into the future for Rocky View. The facility design guidelines (Appendix A) builds on the toolbox with more detail about design considerations and various treatment options. The toolbox and design guidelines were developed for Rocky View using existing best practice guidelines such as the FHWA Small Town and Rural Multimodal Networks Guide

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### Design Guidelines

*As part of the Kern Region Active Transportation Plan (2017, California), Arapahoe County Bicycle and Pedestrian Master Plan (2017, Colorado), and Halton Region Active Transportation Plan (2015, Ontario) design guidelines were developed to provide technical guidance on the types of infrastructure recommended as part of the plan. Design guidelines are helpful for establishing standards based on context.*

(2016) and the Transportation Association of Canada Geometric Design Guide for Canadian Roads (2017) while also considering the context of Rocky View County.

The toolbox organizes facilities by the context they should be applied in, shown in Table 4.

Table 4: Facilities in the toolbox by context

Rural	Urban
<ul style="list-style-type: none"> <li>• Paved Shoulder</li> <li>• Bicycle Lane</li> <li>• Advisory Lane</li> <li>• Pedestrian Lane</li> <li>• Shared-Use Pathway</li> </ul>	<ul style="list-style-type: none"> <li>• Bicycle Lane</li> <li>• Buffered Bike Lane</li> <li>• Protected Bike Lane</li> <li>• Bicycle Boulevard</li> <li>• Advisory Lane</li> <li>• Yield Roadway</li> <li>• Sidewalk</li> <li>• Shared-Use Pathway</li> </ul>

The toolbox includes a description of the facility and the recommended roadway characteristics for the application of the facility. The roadway characteristics that are used for the application are motor vehicle volumes, posted speed limit, and are complimented by other application considerations.

Detailed guidance on the construction recommendations for these facility types is found in the Design Guidance, in Appendix A.

## Rural Areas



Rocky View County, AB. Image: Alta Planning + Design

### PAVED SHOULDER

A shoulder is a paved area outside the general-purpose travel lanes delineated by a continuous white line. Located on rural roadways, shoulders suitable for active transportation should be at least 1.5 metres wide and may include bicycle and/or pedestrian-oriented signing and striping. If the shoulder also serves as a breakdown lane for motor vehicles, there should be an additional unpaved portion of approximately 2.4 metres so that disabled vehicles would not block people from walking or bicycling in the shoulder. Parking for motor vehicles in the shoulder should be discouraged. Shoulders may include buffers or rumble strips to discourage motor vehicles from straying into the shoulder.

### RECOMMENDED APPLICATION

- Appropriate on rural roads with low to medium volumes, and medium to high speeds
- Posted speed 50 to 80 km/hr
- ADT >1,000 veh/day to <4,000 veh/day (or the road is part of a known cycling route)
- Rural areas (TAC sets density of <400 persons/km<sup>2</sup>)

## Rural Areas



Bow Valley Trail, Canmore, AB. Image: HDR

### BICYCLE LANE

Bicycle lanes designate an exclusive space for cyclists distinct from motor vehicle lanes. Bicycle lanes are marked with a solid white line between the vehicle lane and the bicycle lane and include a bicycle stencil, diamond, and are marked with dedicated signs. Located directly adjacent to motor vehicle travel lanes, bicycle lanes follow the same direction of travel. Bicycle lanes can accommodate cycling on roads without a curb and gutter. Where space is available, painted buffer areas can further distance the bike from the adjacent motor vehicle travel lanes.

### RECOMMENDED APPLICATION

- Used in rural or urban areas with low to medium average daily traffic (ADT) and high bicycle volumes
- Posted speed is <50 km/hr
- ADT <4,000 veh/day



## Rural Areas



Hanover, NH, USA. Image: FHWA Small Town and Rural Multimodal Networks Design Guide pg 2-23

### ADVISORY LANE

Advisory lanes include a single bi-directional travel lane for motor vehicles bordered by shoulders. The shoulders are separated from the vehicle travel lanes by dashed white lane lines. When vehicles traveling in opposite directions meet, motorists enter the advisory shoulder to pass. This facility type better accommodates active transportation users within a constrained roadway width.

### RECOMMENDED APPLICATION

- Most appropriate on streets with low to moderate motor vehicle volumes and speeds
- Posted speed <50 km/hr
- ADT <2,500 veh/day
- Narrow roadways  $\leq 11.1$  m

## Rural Areas



Detroit, OR, USA. Image: Alta Planning + Design

### PEDESTRIAN LANE

Pedestrian lanes provide interim or temporary pedestrian accommodations on roadways lacking sidewalks. Pedestrian lanes are not intended as an alternative to sidewalks and are often used to fill short gaps between higher quality facilities. Use a PED ONLY pavement marking to indicate exclusive pedestrian use.

### RECOMMENDED APPLICATION

- May be appropriate on rural roads with low to moderate speeds and volumes
- Appropriate for interim or temporary pedestrian accommodation in areas without sidewalks
- Posted speed is <40 km/hr
- ADT <2,500 veh/day

## Rural Areas



Winnipeg, AB. Image: Alta Planning + Design

### SHARED-USE PATHWAYS

Shared-use pathways are physically separated from motor vehicles by a large buffer or barrier and provide sufficient width and supporting facilities to be used by cyclists, pedestrians, and other non-motorized users. Shared-use pathways are generally paved and ideally at least three meters wide. Pathways should also have 0.6 meters of clear shoulders to offer adequate maneuvering space and visibility and to reduce potential for user conflicts at crossing or curves.

### RECOMMENDED APPLICATION

- Provision may be based on opportunity/connectivity rather than roadway context
- Posted speed >40 km/hr
- ADT > 4,000 veh/day
- Areas with high pedestrian or bicycle volumes

## Urban Areas



Coquitlam, BC. Image: HDR

### BICYCLE LANE

Bicycle lanes designate an exclusive space for cyclists distinct from motor vehicle lanes. Bicycle lanes are marked with a solid white line between the vehicle lane and the bicycle lane and include a bicycle stencil, diamond, and are marked with dedicated signs. Located directly adjacent to motor vehicle travel lanes, bicycle lanes follow the same direction of travel. Bicycle lanes can be retrofitted onto roadways by road diets, which reduce the number of travel lanes and/or reallocate space to better accommodate active transportation.

### RECOMMENDED APPLICATION

- Used in rural or urban areas with low to medium average daily traffic (ADT) and high bicycle volumes
- Posted speed is <50 km/hr
- ADT <4,000 veh/day

## Urban Areas



Burlington, ON. Image: Alta Planning + Design

### **BUFFERED BIKE LANE**

Buffered bicycle lanes are an exclusive space for cyclists separated from motor vehicle lanes by solid white lane lines with gored pavement markings. Buffered bike lanes are indicated with a bicycle stencil and a diamond and are marked with dedicated signs. Located directly adjacent to motor vehicle travel lanes, buffered bike lanes follow the same direction of travel. The painted buffer areas distance the bicyclist from the adjacent motor vehicle travel lane.

### **RECOMMENDED APPLICATION**

- Used in urban areas with low to medium average daily traffic (ADT) and high bicycle volumes
- Posted speed is <60 km/hr
- ADT <7,000 veh/day

## Urban Areas



Kelowna, BC. Image: Alta Planning + Design

### PROTECTED BICYCLE LANE

A protected bicycle lane, also called a cycle track, is a bicycle exclusive facility located within or directly adjacent to the roadway. Cycle tracks increase the distance between vehicle traffic and cyclists by including a physical separation from vehicle traffic with a vertical element. This physical separation can include a raised curb or median, flexible delineators and painted buffer, or painted buffer and parked vehicles.

### RECOMMENDED APPLICATION

- Used in urban areas with high vehicle volumes and speeds, and high bicycle volumes
- Transition areas into towns with higher speeds
- Posted speed is >40 km/hr and <80 km/hr, since a protected facility is not required for speeds lower than 40 km/hr

## Urban Areas



Vancouver, BC. Image: Alta Planning + Design

### BICYCLE BOULEVARD

Neighbourhood greenways include a range of traffic calming treatments to improve conditions for cyclists and pedestrians on local streets. This typically includes signage and pavement markings and varying degrees of vehicle speed and volume management. Potential traffic calming infrastructure includes speed humps, cushions or tables, traffic circles, lateral shifts (chicanes), or diverter median islands.

### RECOMMENDED APPLICATION

- Appropriate on local streets with low volumes and low speeds. Speed and volumes may be managed to create desired operating conditions
- Posted speed <40 km/hr
- ADT <2,500 veh/day. Ideal volumes are around ≤1000 veh/day

## Urban Areas



Gibsons, BC. Image: Alta Planning + Design

### ADVISORY LANE

Advisory lanes include a single bi-directional travel lane for motor vehicles bordered by shoulders. The shoulders are separated from the vehicle travel lanes by dashed white lane lines. When vehicles traveling in opposite directions meet, motorists enter the advisory shoulder to pass. This facility type better accommodates active transportation users within a constrained roadway width. On-street parking is not allowed within advisory bike lanes. However, if an adjacent parking lane is provided, it should be delineated by a solid white stripe and optionally marked with signage.

### RECOMMENDED APPLICATION

- Most appropriate on streets with low to moderate volumes and moderate speed motor vehicles
- Posted speed of <50 km/hr
- ADT <4,000 veh/day, ideally <2,500 veh/day
- Narrow roadways  $\leq 11.1$  m



## Urban Areas



Manaznita, OR, USA. Image: FHWA Small Town and Rural Multimodal Network Design Guide, pg. 2-7

### YIELD ROADWAY

Yield roadways are designed to serve pedestrians, cyclists, and motor vehicle traffic in the same low-speed travel area. They serve bidirectional motor vehicle traffic without roadway markings in the travel area. Roadway lanes should be narrow to encourage slow travel speeds and require courtesy yielding when vehicles traveling opposite directions meet.

### RECOMMENDED APPLICATION

- Most appropriate on streets with low to moderate volumes and moderate speed motor vehicles
- Posted speed of <40 km/hr
- ADT <2,5000

## Urban Areas



Langdon, AB. Image: Alta Planning + Design

### SIDEWALK

Sidewalks provide dedicated space intended for use by pedestrians that is safe, comfortable, and accessible. They are physically separated from the roadway by a curb or unpaved buffer space.

### RECOMMENDED APPLICATION

- Recommended on all but the most low-speed and low-volume roadways

## Urban Areas



### SHARED-USE PATHWAYS

Shared-use pathways are physically separated from motor vehicles by a large buffer or barrier and provide sufficient width and supporting facilities to be used by cyclists, pedestrians and other non-motorized users. Shared-use pathways are generally paved and ideally at least three meters wide. Pathways should also have 0.6 meters of clear shoulders to offer adequate maneuvering space and visibility, and to reduce potential for user conflicts at crossing or curves.

### RECOMMENDED APPLICATION

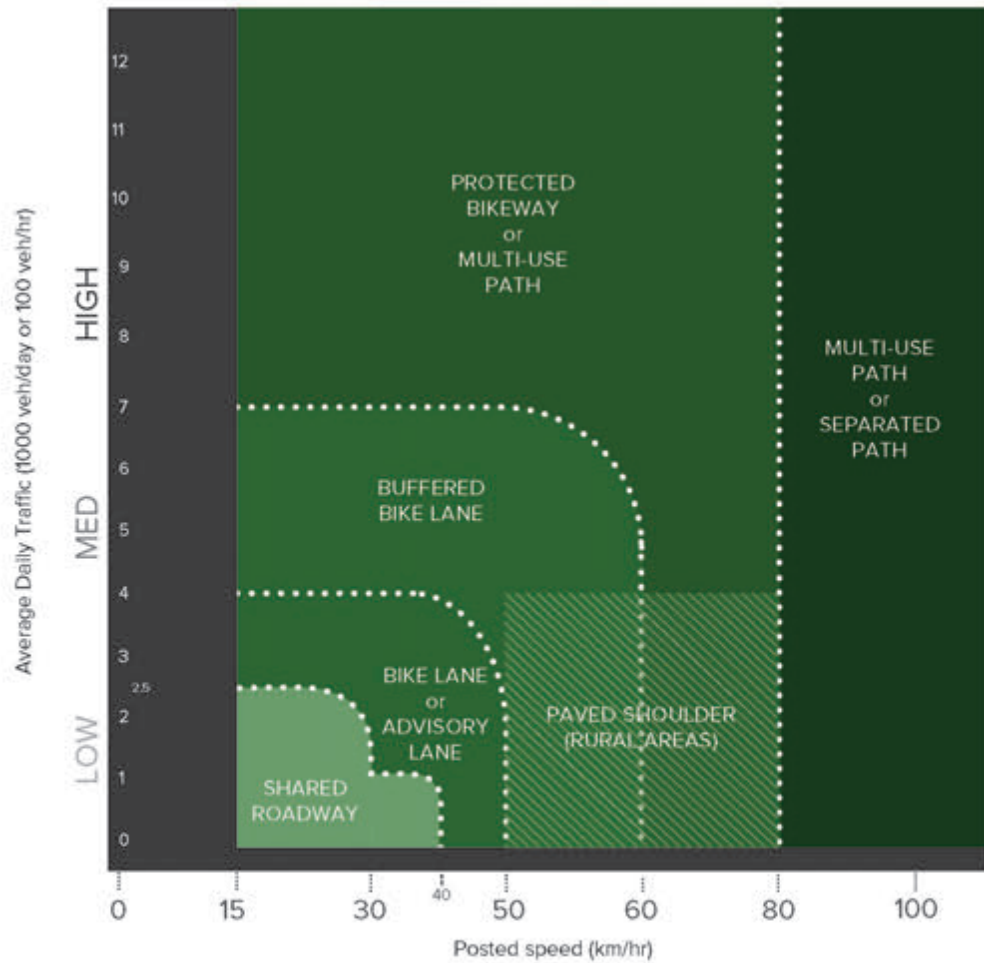
- Provision may be based on opportunity/ connectivity rather than roadway context
- Posted speed >40 km/hr
- ADT > 4,000 veh/day
- Areas with high pedestrian or bicycle volumes

# FACILITY SELECTION

Figure 11 shows the relationship between roadway type and recommended bicycle facility types. As a general rule of thumb, increasing roadway speeds and motor vehicle volume require increasing modal separation to achieve comfortable active

transportation travel conditions for most potential users. This figure provides a reference for planners, designers, and the public for determine what type(s) of active transportation facilities can be used within a given corridor.

Figure 14: Rocky View County bicycle facility selection tool Note: Shared roadways includes: Bike boulevard, yield roadway, pedestrian lane, and advisory lane



SHARED ROADWAY includes:  
BIKE BOULEVARD, YIELD ROADWAY,  
PEDESTRIAN LANE, AND ADVISORY LANE

# NETWORK RECOMMENDATIONS

The following section describes the active transportation network for the Elbow River Ranch Lands and the Bow River Plains. More detailed networks in Bragg Creek, Springbank, and Langdon areas reflect the short distance, day-to-day needs within these communities. The longer-distance networks show connections between communities and other destinations.

These networks represent the backbone corridors of the broader network, which also includes local trails and pathways (existing, proposed, and approved). Most recommended network elements have been included in previous plans or are among those trails and pathways previously identified as adopted or proposed. However, the adopted and proposed trails and pathways include facilities that extend beyond primary active transportation needs and, in many cases, represent local facilities that will connect to the backbone corridors. Exclusion of a particular corridor or facility, particularly those that have previously been proposed or approved from these networks does not imply that they should not be

developed or are not important, only that they do not represent the near-term priorities set forth as part of the backbone for the active transportation network.

The corridors are designated as on-street or off-street, and in some cases a combination of both. On-street corridors are either on low volume/low speed roadways or are priorities for development as connections for confident cyclists. Corridors designated as off street should include separation of pedestrians and/or cyclists from vehicle traffic. Where a corridor includes both on- and off-street facilities, there is a desire to separate high speed cyclists from pedestrian and less confident cyclists.

Highway 791 in Bow River Plains. Image: Alta Planning + Design



# AREA-SPECIFIC PLAN RECOMMENDATIONS

## BRAGG CREEK

The proposed network is shown on Figure 15 and includes:

- Cycling and pedestrian improvements along and across Balsam Avenue and Whyte Avenue
- Designation of active transportation space on Two-Pines Drive
- Upgrades to the West Bragg Creek Trail from the Centre Avenue Bridge over the Elbow River to the bridge at Bragg Creek west of Echlin Drive so that it can accommodate a full range of pedestrians and cyclists

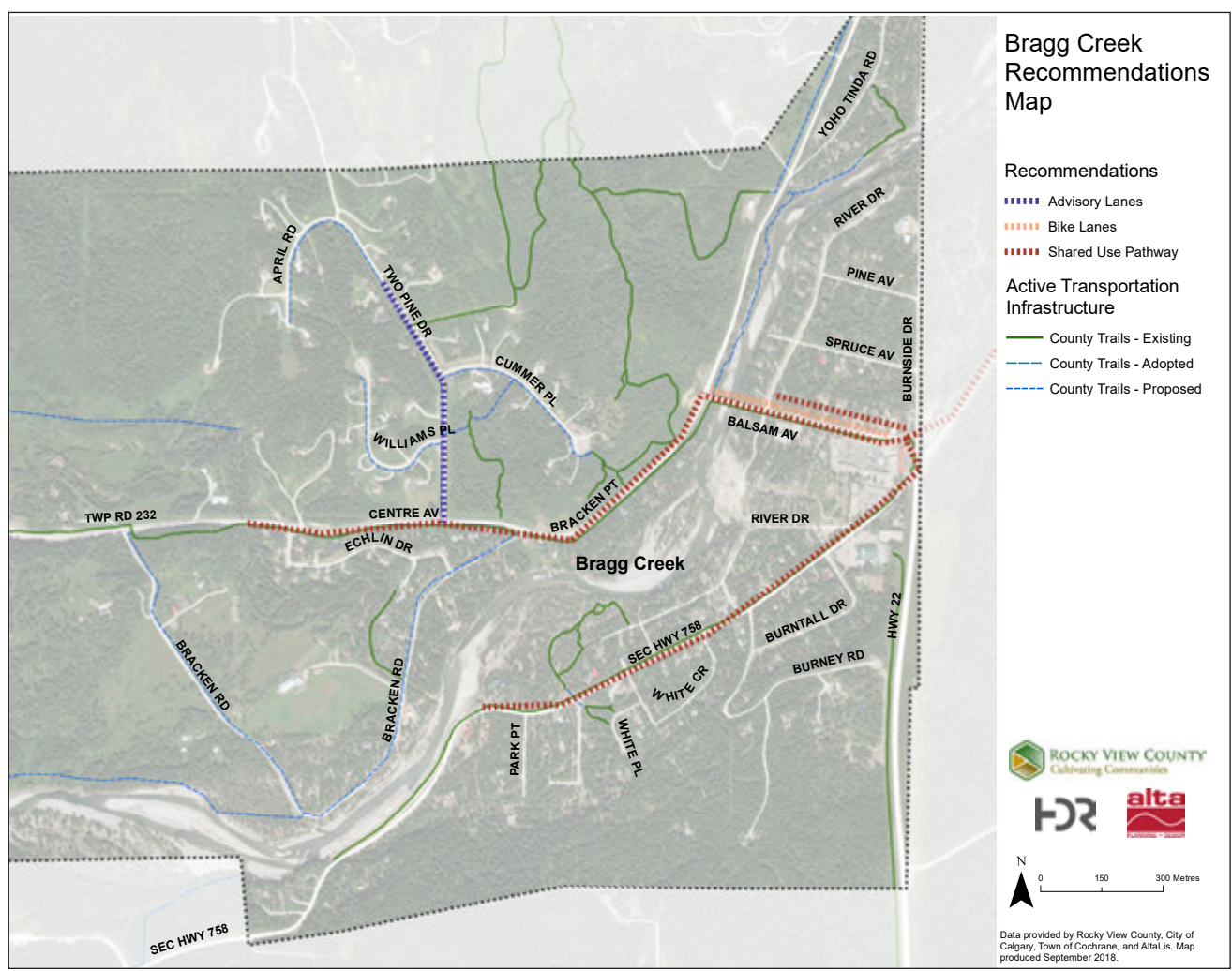


Figure 15: Bragg Creek proposed network

**SPRINGBANK**

The Springbank area has several local pathways and trails that provide connections within individual neighbourhoods. The initial active transportation network, shown in Figure 16, provides a continuous route through Springbank for various user types and allows some of the existing local pathways and trails to connect to a broader network. The Springbank Trails and Pathways Association (STAPA) have proposed a much broader network of facilities in Springbank. The network shown in Figure 13 is considerably less comprehensive than the STAPA proposals as it is focused on the initial active transportation network only. It does not conflict, nor preclude future development of other

network components as proposed by STAPA. Key features of the Springbank active transportation network in this area include:

- Recognition of the popularity of the area for experienced, high-speed cyclists with on-street corridors identified on Springbank Road, Twp Road 250, Range Road 33, Old Banff Coach Road, Range Road 31, and Lower Springbank Road
- Off-street corridors on Range Road 252, connecting to a corridor along the Bow River to the south to the City Calgary future pathway
- Off-street corridors through Harmony, along Twp Road 252 from Harmony to Calling Horse Drive, north to a new active transportation

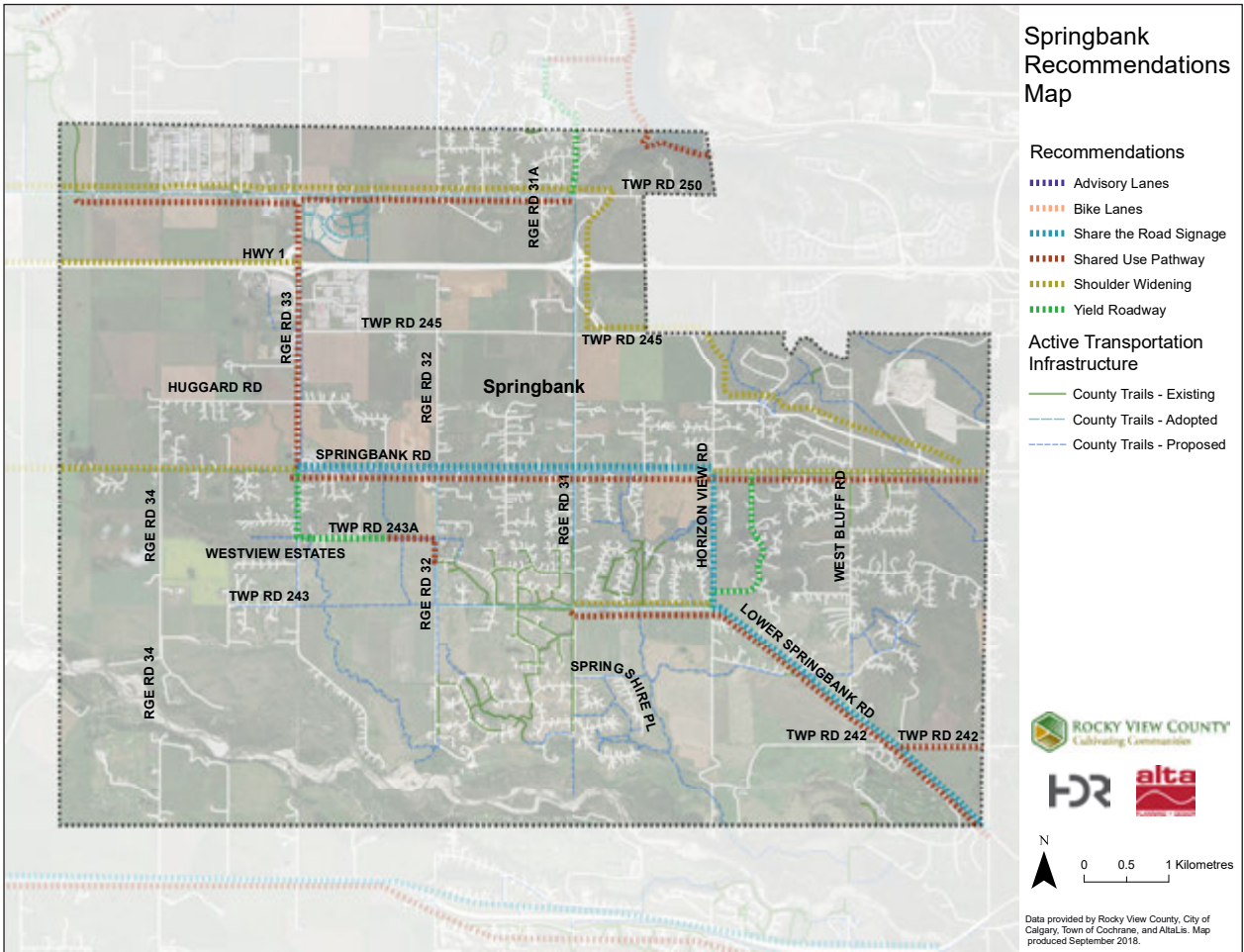


Figure 16: Springbank proposed network

corridor south of the golf course extending east to the corridor along the Bow River to create an off-street loop for recreational use and a comfortable connection between neighbourhoods

- Off-street corridor along Range Road 33, extending south to Twp Road 243A, then continuing east with a combination of new active transportation corridors and corridors along existing streets to Horizon View Road where it splits north to Springbank Road and south along Lower Springbank Road, providing a continuous connection between Harmony and Springbank through to the Calgary network

### LANGDON

The Langdon network is the most complex of the communities in the plan area and has the most diverse group of pedestrians and cyclists, representing a range of ages, abilities, and trip purposes. The network is shown on Figure 17 and includes the following new corridors to support the existing pathways and sidewalks:

- Combined on-street and off-street corridor along the full length of Centre Street in Langdon, including provisions for crossing Centre Street and creating a more comfortable environment

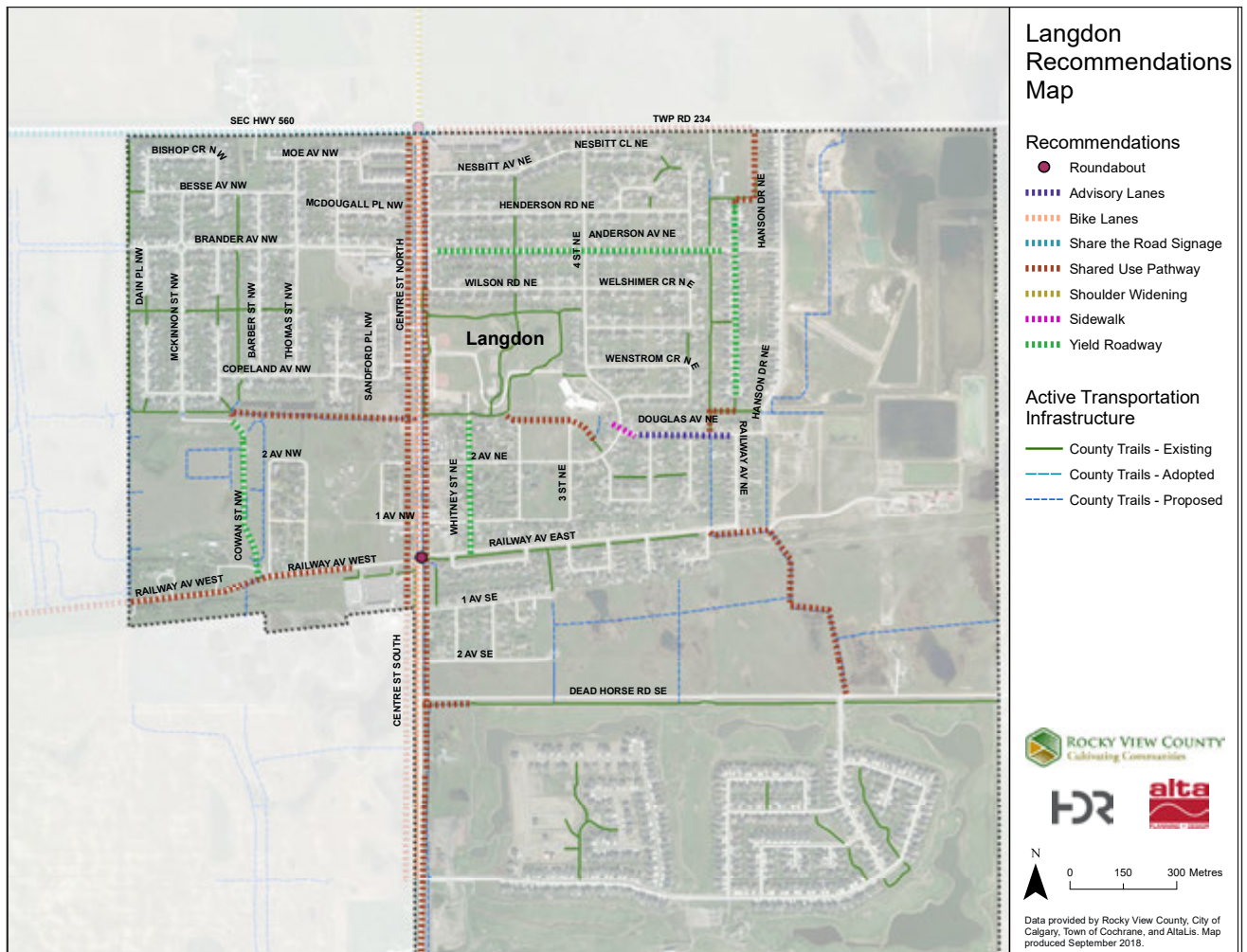


Figure 17: Langdon proposed network



## ACTIVE TRANSPORTATION PLAN SOUTH COUNTY | OCTOBER 2018

- New on-street treatments to better support pedestrians and cyclists, with a focus on connections to school on 4 Street, Anderson Avenue, Douglas Avenue, Railway Avenue, Whitney Street, and Cowan Street
- Off-street corridors connecting Boulder Creek to Railway Avenue and in other key locations to make missing connections

**FULL NETWORK**

The South County Network is shown on Maps 9 and 10. Within the Elbow River Ranch Lands, it includes components of the future Trans Canada Trail (TCT) along Highway 8 and Highway 22 south of Highway 8, as well as into Cochrane north of Township Road 252. The section between Highway 8 and Township Road 252 has not been included in the initial network as an off-street corridor because it does not represent a priority connection for Rocky View County. However, it remains an important TCT connection and

A view of the mountains from Range Road 33. Image: Alta Planning + Design



should funding become available through TCT sources, an off-street connection along the full length of the TCT should be considered.

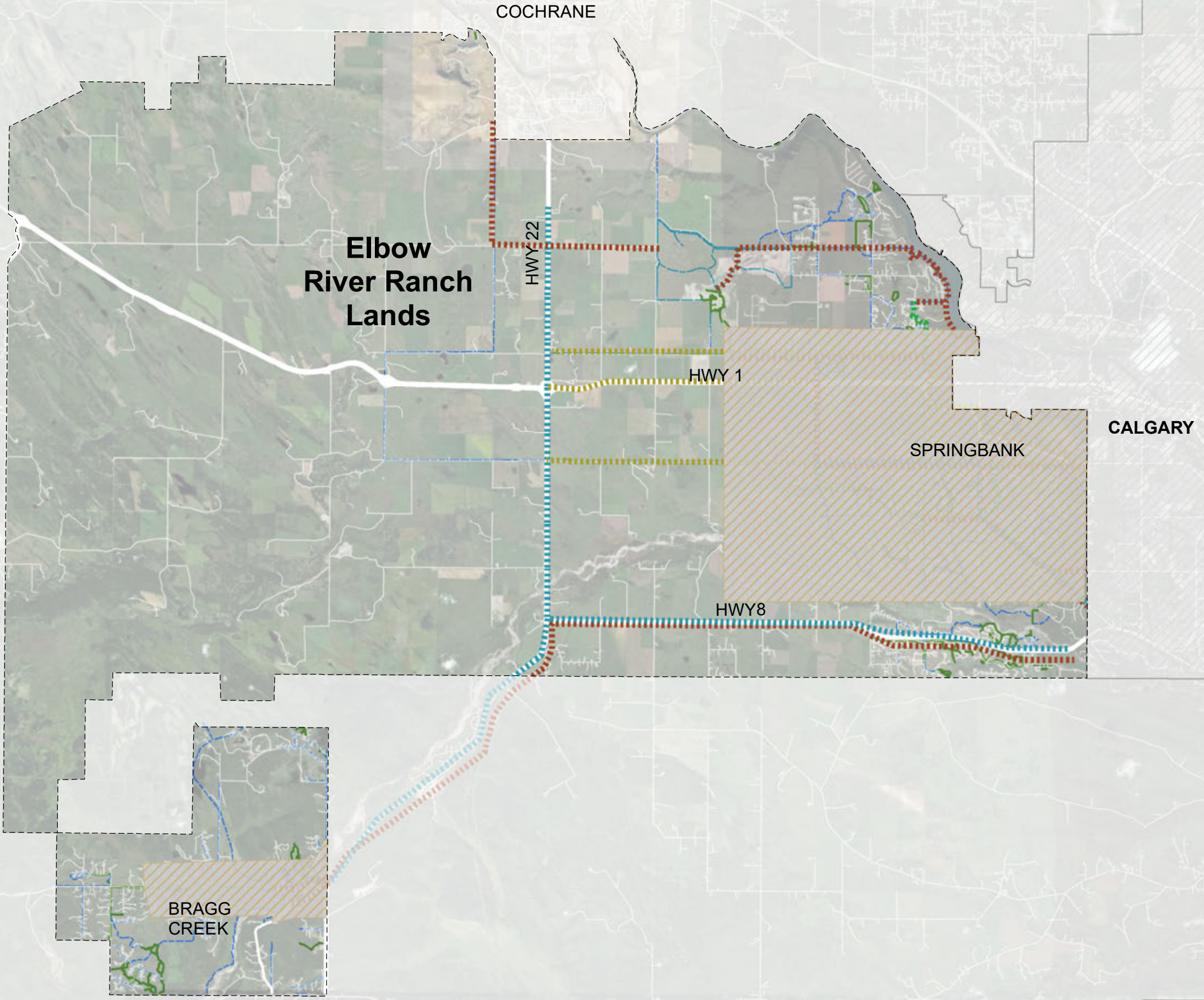
In the Bow River Plains, new off-street corridors along the WID Canal and into the Weed Lake complex and south to Indus, combined with an off-street corridor along Railway Avenue will provide comfortable off-street connections between Langdon and most other destinations. The cities of Calgary and Chestermere are undertaking plans for Conrich Road (Range Road 284)

that are expected to include off-street active transportation facilities. Corridors that make use of Conrich Road will provide north-south connectivity.








McKinnon Flats in the Bow River Plains Area. Image: Alta Planning + Design






# MAP 9. ELBOW RIVER RANCH LANDS RECOMMENDATIONS MAP

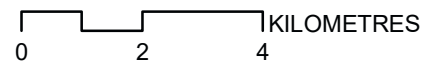


### Recommendations

-  Crossing Study
-  Advisory Lanes
-  Bike Lanes
-  Share the Road Signage
-  Shared Use Pathway
-  Shoulder Widening
-  Yield Roadway

### Active Transportation Infrastructure

-  County Trails - Existing
-  County Trails - Adopted
-  County Trails - Proposed



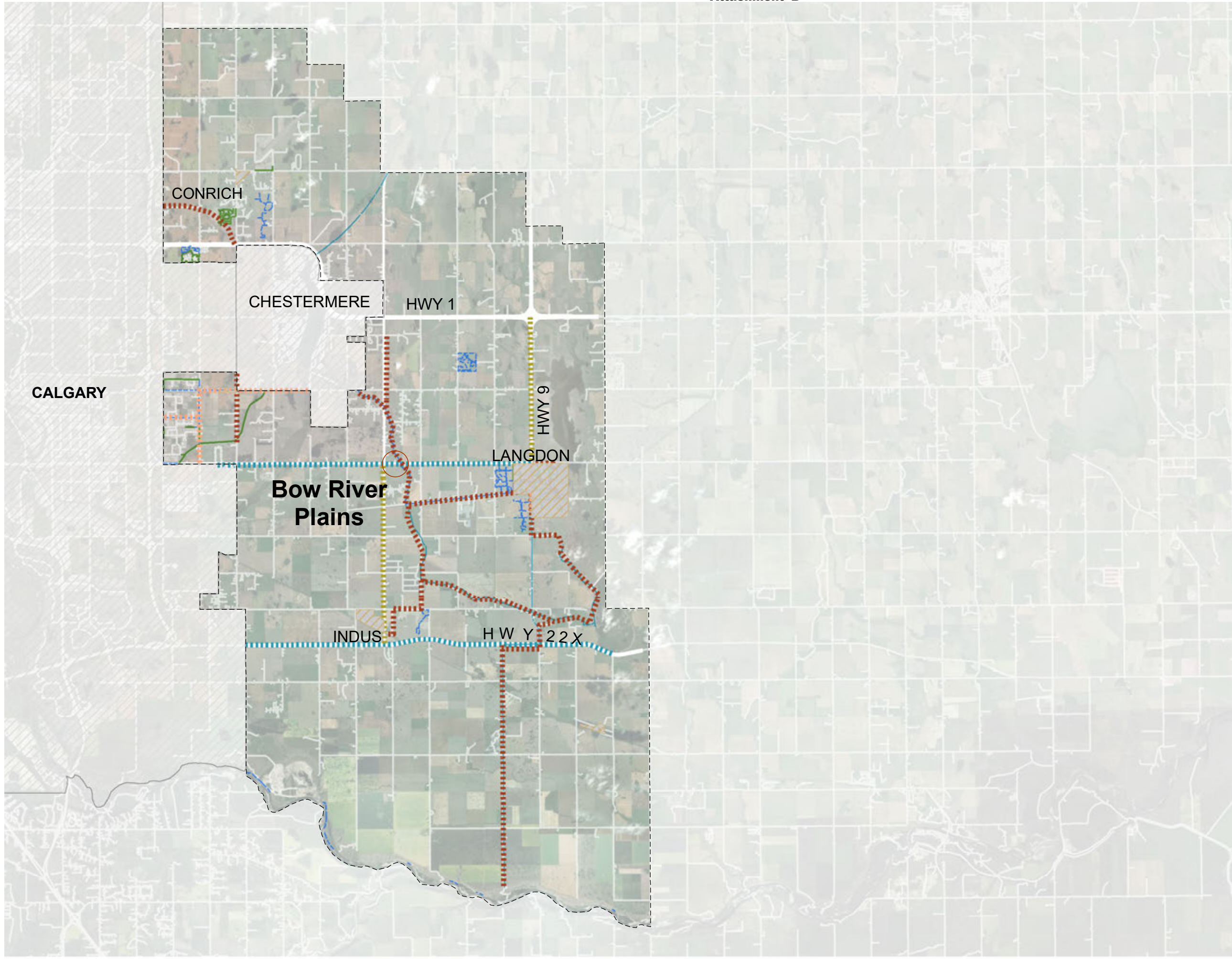
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Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.






# MAP 10. BOW RIVER PLAINS RECOMMENDATIONS MAP




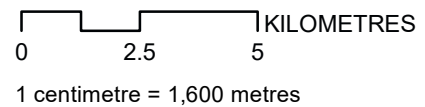
### Recommendations

-  Crossing Study
-  Advisory Lanes
-  Bike Lanes
-  Share the Road Signage
-  Shared Use Pathway
-  Shoulder Widening
-  Yield Roadway

### Active Transportation Infrastructure

-  County Trails - Existing
-  County Trails - Adopted
-  County Trails - Proposed

N  
  
 The plan areas shown are consistent with the Parks and Open Space Master Plan regions



Data provided by Rocky View County, City of Calgary, Town of Cochrane, AltaLis, and Statistics Canada. Map produced July 2018.



CHAPTER 5

# Implementation

With the development of the ATPSC, Rocky View County is at a turning point. Previous plans and trail construction efforts have resulted in a discontinuous active transportation network. The recommendations set forth in this plan build upon previous planning efforts and identify a complete and continuous network along with accompanying program and policy recommendations. The recommendations are divided into a set of steps to be undertaken in the near term and then a longer list of recommendations that can be accomplished opportunistically. This section concludes with a high level discussion of funding sources that can be used to implement recommendations.

## NEAR-TERM PRIORITY

The steps included here build upon each other and are intended to build Rocky View County’s capacity to build, educate, and encourage visitors and residents to walk and cycle. The recommendations outlined in the seven steps below include a balance of infrastructure, programs and policies that provide a balanced, holistic approach to plan implementation. A general description of each step is identified in the table and details are provided in the accompanying text. Each of these steps require further budget development and council approval prior to expenditure of funds.

### STEP 1 – DEVELOP ENABLING POLICIES AND PROCEDURES

Before making significant investments, Rocky View County needs to clarify organizational responsibilities and update current policies and procedures to facilitate ATPSC implementation.

#### 1a. Active Transportation “Champion”

Currently, there is no clear staff position responsible for active transportation. An internal staff “champion” can continue to make active transportation a priority and support the various RVC departments that will be involved with the plan implementation. At this time, it is unlikely that the County

Table 5: Step 1 tasks/actions

Task / Action	Type	RVC Responsibility
1a – Identify RVC Active Transportation “Champion”	Policy	RVC Senior Management
1b – Pathway Development Guideline and County Servicing Standards	Policy	RVC Engineering
1c – Classify Active Transportation Facilities as Transportation Infrastructure	Policy	RVC Council / Management
1d - 30 km/h zones on identified active transportation streets	Policy	RVC Operations
1e - Update maintenance policies and procedures to support active transportation	Policy	RVC Operations



can support a full-time active transportation position and therefore this would need to be a new responsibility for an existing position. This individual could come from one of many departments but understanding of project development implementation will be important. A reporting structure and responsibilities description will depend on the responsible department.

### **1b. Pathway Development Guideline and County Servicing Standards**

All upgrades to active transportation facilities or development of new active transportation facilities, whether undertaken by the County, private entity, or community group, will refer to the Rocky View County Active Transportation Facility Development Guidelines. The County Servicing Standards will need to be updated and expanded active transportation facilities and reference the Active Transportation Facility Design Guidelines. The Pathway Development Guidelines included in Appendix F provides guidance for planning and engineering requirements associated with pathway development and upgrading to minimize feasibility, cost and constructability risks.

### **1c. Classify Active Transportation Facilities as Transportation Infrastructure**

Designate active transportation facilities as identified in the ATPSC as capital infrastructure projects. Alignment with the ATPSC should be a consideration in the prioritization scoring. Similar to the Storm Drainage Improvements Policy, develop a process to identify improvements to support active transportation with a value under \$400,000. Update the Annual Road Program policy to reference active transportation

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### *Policy Recommendations*

*The Arapahoe County Bicycle and Pedestrian Master Plan (2017, Colorado) in addition to defining an active transportation network, recommended policies to support and develop active transportation. Some recommended policies were accompanied by strategies to help direct the implementation of the policy. Examples of policies included publishing a bike and walking map or implementing a wayfinding signage program, both of which support active transportation.*

facilities and include on-street active transportation improvements referenced in the active transportation network in the Annual Road Program.

### **1d. 30 km/h Zones on Identified Active Transportation Streets**

The County Servicing Standards indicate that any school or playground zone must have a posted speed of 30 km/h, but there are currently no provisions for accommodating 30 km/h zones on other streets. It is recommended that the 30 km/h requirement for school and playground zones be expanded to shared space streets that have been designated in the ATPSC as Yield Roadways, Advisory Lanes, or Bike Boulevards. This would not apply to streets not identified as part of the ATPSC network or to other active transportation facility types such as bike lanes.

**1e. Maintenance Policies and Procedures**

The following updates should be considered to support active transportation:

- Identify key active transportation facilities or off-street pathways as “all-season” and designate as priorities for snow and ice control through and update to the Snow and Ice Control policy
- Include budget items to allow for the monitoring, development, and maintenance of active transportation facilities as part of the County’s transportation system
- Prioritize street sweeping in the spring on urban roads and rural subdivision roads that are included in the active transportation network and expand the sweeping of rural non-residential roads to include annual spring sweeping of those included in the active transportation network. An operating budget should be developed for this activity
- Expand the Sidewalk Maintenance policy to include pathways and other facilities designated as part of the active transportation network within hamlets
- Develop an app or other method to allow users to report maintenance issues on active transportation facilities

**STEP 2 – IMPLEMENT CONNECTIONS AND PROGRAMS TO SUPPORT ACTIVE SCHOOL TRAVEL IN LANGDON**

Walking and cycling are already important modes for travel to and from schools in Langdon. But, the lack of sidewalks and cycling facilities in many areas of the hamlet may discourage some walking or cycling trips. The workshop with grade nine students at Langdon School as well as other analysis and feedback, revealed a number of potential priority issues for active school travel in Langdon.

**2a. Yield Roadways on Anderson Avenue and Whitney Street**

Yield Roadways involve minimal investment, limited primarily to signage and education (see the final task for Step 2). Both streets are important routes to school and in many ways already function as yield roadways at the end of the school day. Designation as Yield Roadways will include reducing the posted speed to 30 km/h and adding share the road signage and shared lane pavement markings.



Figure 20: Map showing near-term priority step 2a

Table 6: Step 2 tasks/actions

Task / Action	Type	RVC Responsibility
2a – Yield Roadways on Anderson Ave and Whitney St	Project	RVC Engineering / Operations
2b – Advisory Lanes on Douglas Ave	Project	RVC Engineering / Operations
2c – Extend 2nd Ave sidewalk from school to Douglas Ave	Project	RVC Engineering / Capital Projects
2d – Shared-Use Pathway from Boulder Creek Dr to south end of Railway Ave	Project	RVC Capital Projects (potentially partnered with developer)
2e – Langdon Active and Safe Routes to School	Program	RVC Planning / Rocky View Schools

**2b. Advisory Lanes on Douglas Avenue**

With relatively few driveways on Douglas Avenue and low on-street parking demand, removal of on-street parking to allow advisory lanes will have minimal disruption. Implementation will include reducing the posted speed to 30 km/h, adding signage and pavement markings, and providing education on how advisory lanes work.

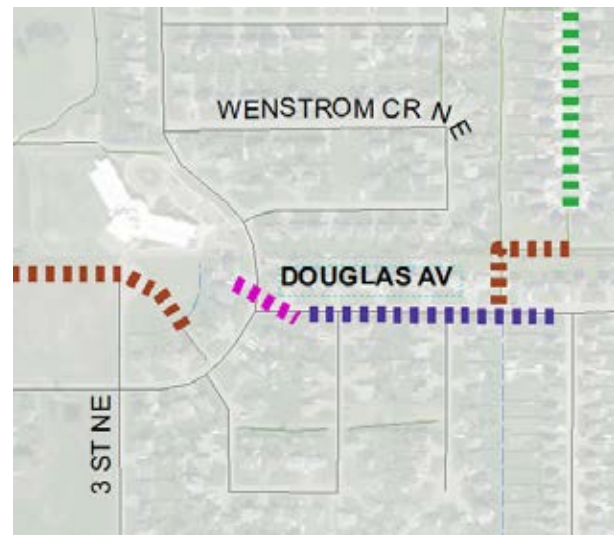
**2c. Extend 2nd Avenue Sidewalk from School to Douglas Avenue**

The sidewalk along 4 Street NE currently ends at the Sarah Thompson Elementary School property line. This project will extend the sidewalk approximately 60 meters south to Douglas Avenue and may require drainage infrastructure as the sidewalk would encroach into the ditch. The sidewalk should match the design of the existing sidewalk. A marked crosswalk across 4 Street NE should also be included at Douglas Avenue.

**2d. Shared-Use Pathway from Boulder Creek Drive to south end of Railway Avenue**

This pathway will provide a connection between Boulder Creek and the east side of Langdon. A future adopted pathway is indicated, but is tied to future development

Figure 21: Map showing near-term priority step 2b and 2c



of the parcel. This project advances the pathway connection, recognizing that it may need to be partially replaced once the land is developed.

**2e. Langdon Active and Safe Routes to School**

An Active and Safe Routes to School program targets improvements to school traffic safety and increases active transportation among families and children. Active and Safe Routes to School (ASRTS) refers to a variety of multi-disciplinary programs aimed at promoting healthy alternatives to driving alone in the family car and improving traffic safety around school areas through education, fun

## ACTIVE TRANSPORTATION PLAN SOUTH COUNTY | OCTOBER 2018

Figure 22: Map showing near-term priority step 2d



events, enforcement and safety reminders, and engineering measures. Walking and biking to school are healthy alternatives to being driven and can provide a sense of independence for children. Riding the bus and carpooling similarly reduce traffic and improve safety for the school community and can be part of promoting healthy lifestyles.

ASRTS programs typically involve partnerships among municipalities, school districts, community organizations, parent volunteers, and law enforcement agencies. Among the goals of ASRTS programs are improved safety for children, establishing good health and fitness habits in families, and decreased traffic and air pollution from private automobiles. ASRTS programs help integrate physical activity into families everyday routine and can help to address parents' safety concerns by encouraging greater compliance with traffic laws and implementation safer streets near schools.

There are many ways to begin an ASRTS effort, including convening a Task Force to define goals and problems and begin developing an action plan to focus efforts on addressing identified problems. ASRTS plans may include individual school plans that identify needed safety improvements around schools and/or regional strategic ASRTS plans that may focus on funding, staffing or communications needs. An ASRTS Task Force may include representatives from the county, the school district, the RCMP, school administration, parents, and interested community members.

Schools and partners can implement a wide variety of encouragement activities, depending on volunteer and school staff capacity and interest. Popular events across Canada include:

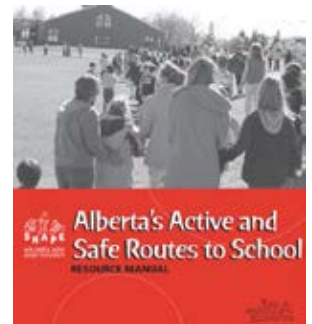
- International Walk to School Day every October
- Winter Walk Day in February
- Bike to School Week at the end of May
- Walking School Bus and/or Bike Train programs
- Walk-a-Thons and theme days
- Other fun and educational activities

Partners can track the progress of their ASRTS efforts by collecting data on school travel behaviours and parents' concerns through the student hand tally and parent surveys.

There are several potential partners, including Rocky View Schools, Rocky View County, City of Calgary, City of Chestermere, and RCMP.

The following sample programs and resources can be used to support implementation:

- Safe Healthy Active People Everywhere (SHAPE) Active and Safe Routes to School Resource Manual
- Ontario Active School Travel
- Healthy Schools BC Programs and Support
- U.S. National Center for Safe Routes to School, Safe Routes Guide



The ASRTS should be combined with an overall education program associated with Yield Roadways and Advisory Lanes, targeted at both drivers and students.

**STEP 3 - BRAGG CREEK, BALSAM AVENUE AND BURNSIDE AVENUE**

Bragg Creek is a popular destination in the region. The concentration of restaurants and shops in the centre of the hamlet generates considerable walking trips between businesses and for local residents, and is popular with cyclists. In the summer, the section along Balsam Avenue is very walkable, but issues with drainage and snow make walking challenging in winter and spring.

**3a. Upgrade Existing Balsam Avenue Pathway on South Side and New North Side Pathway**

The existing pathway on the south side of Balsam Avenue is narrow and suffers from drainage issues. Upgrading the pathway will provide a better pedestrian environment and will improve accessibility for all users. Providing a pathway on the north side of the street will close an existing network gap, reduce the need for pedestrians to walk in the roadway, and enhance connections to several destinations. These projects could be implemented in conjunction with a broader “main street” program for Bragg Creek.

**3b. New Burnside Drive Pathway to Connect Balsam Avenue and White Avenue**

This project would provide a continuous pathway connection along Balsam Avenue to White Avenue, via Burnside Drive. Like the Balsam Avenue pathways, this could be considered as part of a “main street” program in Bragg Creek.



Burnside Drive in Bragg Creek. Image: HDR

Table 7: Step 3 tasks/actions

Task / Action	Type	RVC Responsibility
3a – Upgrade existing Balsam Ave pathway on south side and new north side pathway	Project	RVC Capital Projects
3b – New Burnside Dr pathway to connect Balsam Ave and White Ave	Project	RVC Capital Projects



Existing pathway on Balsam Drive in Bragg Creek. Image: HDR

**STEP 4 – WALK FRIENDLY COMMUNITIES DESIGNATION**

The Walk Friendly Communities program is not yet a national program and formal designation is not available in Alberta. However, even if designation cannot be secured, there is value in promoting and celebrating walkability in Langdon and Bragg Creek through compliance with the program.

**4a. Langdon and Bragg Creek Walk Friendly Communities**

Participation in the Canada Walks program can help create a source of pride associated with being a walkable community. Canada Walks is a department of Green Communities Canada and is a leader in Canada’s walking movement. Canada Walks has a well-established WALK Friendly Communities program that encourages municipalities to create and improve walk conditions by awarding four tiers of recognition: Bronze, Silver, Gold, or Platinum. The program helps to give walking a prominent profile in the community while encouraging municipal governments to set targets for ongoing improvements.

Canada Walks also offers online resources and services to support and encourage communities in promoting walking and active transportation. Resources include a guide to safer streets near schools, walk survey results, and information on the benefits and obstacles to walking. Canada Walks also offers customized fee-for-service workshops and on-site services including walkability audits, community engagement, and facilitated breakout groups.



Rocky View County can work with residents in Langdon and Bragg Creek to assess their readiness to apply for WALK Friendly Community status and encourage them to apply or simply to promote their compliance with the program. At this time, WALK Friendly Community designation is only available in Ontario. Rocky View County could take a leadership role to encourage Green Communities Canada the Province of Alberta to develop a similar program in Alberta – and possibly with a rural and small town focus.

Potential program partners include seniors clubs, community associations, and Alberta Health Services.

Table 8: Step 4 tasks/actions

Task / Action	Type	RVC Responsibility
4a - Langdon Walk Friendly Communities Designation	Program	RVC Planning
4b - Bragg Creek Walk Friendly Communities Designation	Program	RVC Planning

**STEP 5 – CONTINUOUS PATHWAY ON CENTRE STREET (EAST SIDE), LANGDON**

Currently, there sections of pathway, but no continuous off-street walking or cycling facilities along Centre Street in Langdon. This means that pedestrians and cyclists utilize motor vehicle travel lanes along Centre Street. A continuous pathway along one side of Centre Street will significantly improve active transportation along the street and reduce potential conflicts with motor vehicles. The pathway should be placed at the outside of the existing right-of-way to maximize separation between the roadway and the pathway. The pathway could be implemented in conjunction with a “main street” improvement for Centre Street.

**5a. Highway 560 to Wilson Road**

This section of pathway would close a gap between existing pathway at the project’s southern end and provides access to the businesses fronting on Township Road 234. The pathway should be located as close to the frontage road as possible to maintain options for future Centre Street widening and to avoid the existing ditch. This project requires consideration of drainage issues at the northern end.

Figure 23: Map showing near-term priority step 5a, 5b, and 5c, and 5d



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Table 9: Step 5 tasks/actions

Task / Action	Type	RVC Responsibility
5a - Centre St East Side Shared-Use Pathway - Hwy 560 to Wilson Rd	Project	RVC Capital Projects
5b - Centre St East Side Shared-Use Pathway - 3 Ave to Railway Ave	Project	RVC Capital Projects
5c - Centre St East Side Shared-Use Pathway - Railway Ave to Boulder Creek Dr	Project	RVC Capital Projects
5d - Dead Horse Rd South Side Shared-Use Pathway - Centre St to existing pathway	Project	RVC Capital Projects

**5b. 3 Avenue to Railway Avenue**

This section of pathway is the most challenging along the Centre Street corridor. The right-of-way is narrow (only 20 meters) and property access is not well defined. Project considerations include:

- potential property acquisition
- development of enclosed drainage
- access management
- order of magnitude costs do not include property acquisition

Further refinement and understanding of plans for Centre Street will be necessary to develop more reliable costs for this section.

Design for this section should be undertaken along with design for Centre Street upgrading. For this short section, it may be appropriate to consider more “urban” facility types such as a raised cycle track due to the space constraints.

**5c. Railway Avenue to Boulder Creek Drive**

This section completes the pathway to Boulder Creek Drive and provides a walking and cycling connection between Boulder Creek, Langdon Centre Shopping Centre, and the rest of Langdon. There are no significant uncertainties or issues on this section. Like the other sections, maximizing the setback from Centre Street will reduce the potential for drainage issues.

**5d. Dead Horse Road South Side**

This short section of pathway (approximately 125 meters) connects the existing pathway on the south side of Dead Horse Road to the future Centre Street pathway.



## STEP 6 – ACTIVE TRANSPORTATION POLICY GUIDANCE FOR NEW DEVELOPMENTS

Basic active transportation infrastructure such as sidewalks and pathways are already requirement for new developments. However, current policies, guidelines, and standards are focused on infrastructure requirements. New guidelines should provide direction on connectivity within new developments and to the overall active transportation network. Although an active transportation network has only been developed for the plan area, it is recommended that this policy guidance be applicable to all new developments and that the ATPSC be recognized in the next update of the County Plan.

### 6a. Formalize Active Transportation Policies for New Developments

The following can used as guidance to help develop policies and guidelines for active transportation new developments:

- Area Structure Plans (ASP), conceptual schemes, master plans, and subdivision applications should be reviewed against the ATPSC as part of the development application and approval process

- In new communities, developers should demonstrate that the active transportation network maximizes ten-minute walking access to commercial areas, schools, major parks, village centres, and other key attractions
- New urban and suburban developments should require a low stress cycling network with approximately 80 meters spacing allowing for connections to day-to-day destinations, to the regional active transportation network, and to networks in neighbouring communities
- New communities should include high-quality dedicated pedestrian crossings with a minimum spacing of 400 meters on arterial roadways, connecting to walking and cycling destinations along arterial and major collector streets within urban and suburban areas
- Pedestrian connections into neighbourhoods should be provided at a minimum spacing of approximately 200 meters along collector and arterial streets within urban and suburban areas
- New residential developments over ten lots and within two kilometers of the active transportation network should include off-site upgrades consistent with the Active Transportation Facility

Table 10: Step 6 tasks/actions

Task / Action	Type	RVC Responsibility
6a – Formalize active transportation policies for new development approval applications	Policy	RVC Planning
6b – Update traffic impact assessment guidelines to include active transportation and trip reduction provisions	Policy	RVC Engineering
6c – Add bike parking requirements to the Land Use Bylaw	Study / Policy	RVC Planning
6d – Include off-site active transportation facilities in the Off-Site Levy Bylaw	Study / Policy	RVC Planning / Engineering

Guidelines to allow safe and comfortable connections to the active transportation network

- New neighbourhoods should have a minimum active modes connectivity index of 1.6 (see Appendix G for a description of connectivity index)
- Encourage the dedication of linear corridors that connect to the active transportation network as municipal and environmental reserves

**6b. Update Traffic Impact Assessment Guidelines to Include Active Transportation Provisions**

The traffic impact assessment guidelines should be expanded to include consideration of active transportation. Specifically, they should require reviews of the adequacy of pedestrian and cycling facilities within a proposed development and connecting to the proposed development. Connections within 800 meters of the proposed development should be considered for pedestrian facilities and within two kilometers for cycling facilities. Modifications to traffic impact analysis guidelines should encourage assessments of methods to reduce vehicle trip generation through active transportation infrastructure and services. Additionally, the assessment

guidelines should include the following:

- **Bike Parking Requirements** - Consider adding bike parking requirements to the Land Use Bylaw, particularly for industrial, institutional and commercial land uses
- **Off-Site Levy Bylaw** - Include off-site active transportation facilities in the Off-Site Levy Bylaw, by treating active transportation facilities as part of the overall transportation network

**STEP 7 – RANGE ROAD 33 SHARED-USE PATHWAY**

A pathway along Range Road 33 will connect Springbank Community High School and Springbank Park for All Seasons with Elbow Valley Elementary School and Springbank Middle School. This pathway will create the initial stage to an ultimate network between Cochrane and Calgary through Harmony and Springbank. A gravel pathway currently exists along the west side of the Range Road 33 that provides an initial alignment for the pathway. The pathway should be extended into the high school / Park For All Seasons site.

Table 11: Step 7 tasks/actions

Task / Action	Type	RVC Responsibility
7a – Range Rd 33 West Side – Springbank Middle School to Springbank Rd	Project	RVC Capital Projects

## LONGER-TERM PROJECTS AND PROGRAMS

The following infrastructure projects and programs can be implemented opportunistically to align with other projects or as funds become available.

### PROJECTS

The following are the remaining projects to complete the active transportation network. Timing for these projects will be reliant on other initiatives such as road rehabilitation projects, new development and other improvement projects. The remaining projects include:

- Range Road 33 / Twp Road 250 Shared Use-Pathway (School to Harmony)
- Langdon – Indus Connection (via WID Canal)
- Bragg Creek Encouragement / Awareness Programs and Balsam Avenue Bike Lanes
- Janet Industrial Area Bike Lanes
- Bragg Creek Network Expansion
- Springbank – Share the Road and Pathway Connections South of Springbank Road
- WID Canal Langdon to Chestermere
- Calling Horse Drive (Share the Road) / Twp Road 250 (Shared-Use Pathway) to Range Road 33
- Harmony to Highway 22
- Complete Langdon Network
- Springbank Road Shared-Use Pathway – Range Road 33 to Calgary
- Hwy 8/22 Wayfinding and Share the Road Signage
- Lower Springbank Road / Twp Road 242 Shared-Use Pathway Connection to Calgary
- Langdon Centre Street West Side Pathway and Bike Lanes
- Springbank Road and Lower Springbank Road On-Street – Range Road 33 to Calgary
- WID Canal / Weed Lake
- Harmony – Bow River Connection (via Twp Road 252 Shared-Use Pathway)
- Highway 8 / Highway 22 Pathway
- Highway 22 – Harmony/Cochrane Pathway
- Highway 791 Shoulder Widening and Signage
- Highway 560, Highway 22X Share the Road Signage
- Springbank Road to Highway 22 Shoulder Widening
- Janet – Conrich Shared-Use Pathway
- Old Banff Coach Road / Twp Road 250 Shoulder Widening and Signage
- Centre Street Roundabouts / Traffic Control
- Springbank – Upgrade Share the Road Routes to Shared-Use Pathways
- McKinnon Flats Shared-Use Pathway Connection
- Highway 9 Shoulder Widening / Signage
- Highway 8 Bridge – Elbow River
- Shoulder Widening, Twp Road 250 – Harmony to Hwy 22

## PROGRAMS AND POLICY GUIDANCE

In addition to the education and encouragement programs identified in Langdon and Bragg Creek, there are other potential programs that may be considered, particularly as longer-term projects are implemented. Similarly, the implementation of longer-term projects will generate the need for additional policy development. The following are longer-term programs and policy guidance for consideration:

- Active transportation use monitoring before and after infrastructure and/or program implementation, potentially involving volunteers from various community groups to collect and organize data
- Updated signage on roadways with active transportation facilities consistent with provincial guidelines currently being developed
- Development of a wayfinding plan for active transportation routes
- Media campaign designed to increase respectful behavior between cyclists, pedestrians and motorists, targeted in the Springbank area
- Bicycle tourism program to attract and better support mountain and road cyclists in the Elbow River Ranch Lands focused on the Bragg Creek area
- Bike to Work Day or Bike Month to encourage and promote cycling during Bike Month and Bike to Work Week
- Bicycle Education Workshops to enhance understanding, confidence and independence related to cycling for transportation, with a focus in Springbank
- Bicycle-Friendly Business Program target at employers in Bragg Creek to recognize employers for their efforts to encourage, support, and promote bicycling in their workplace

Further details on the longer-term education and encouragement programs are provided in Appendix H.

Cyclists riding on a paved shoulder in Elbow River Ranch Lands. Image: Alta Planning + Design



## ACTIVE TRANSPORTATION PLAN SOUTH COUNTY | OCTOBER 2018

Table 12: Summary of Grant Opportunities

Funding Source Name	Funder	Funding Match Required	Infrastructure	Operations or Maintenance	Plans, Studies, or Programs	Available Funding
Alberta Blue Cross Healthy Communities Grant Program	Alberta Blue Cross	N				\$50,000
Municipal Sustainability Initiative Capital Funding	Government of Alberta – Municipal Affairs	N				Not specified
Basic Municipal Transportation Grant	Government of Alberta – Alberta Transportation	Unclear				Per capita
Strategic Transportation Infrastructure Program (STIP)	Government of Alberta – Alberta Transportation	Y				\$100 million over 3 years
Alberta Municipal Infrastructure Program (AMIP)	Government of Alberta – Alberta Transportation	N				\$500,000 over 5 years
TD Friends of the Environment Foundation Grant	TD Canada Trust	N				Typically up to \$8,000
Alberta Healthy School Community Wellness Fund	Alberta Health and University of Alberta	N				Not specified
Green Municipal Fund	Federation of Canadian Municipalities	Y				Loan maximum of \$50 million
Alberta Traffic Safety Fund	Government of Alberta – Alberta Transportation	N				Up to \$25,000
10 Year Capital Road Plan / Annual Road Plan and Annual Operating and Capital Budget	Rocky View County	N/A				Based on prioritization of projects

## FUNDING

### ROCKY VIEW COUNTY

The County's project prioritization and budgeting processes are critical to implementing active transportation infrastructure and supporting active transportation education, encouragement, monitoring, and reporting programs in the South County. The ATPSC identifies active transportation facilities that largely fall within the County's rights-of-way, and so the prioritization, funding, and construction should largely be incorporated into the short- and long-term planning and budgeting for roadway facilities. Prioritized active transportation projects should be included in the shorter-term annual road Capital and Operations Budget as well as longer-term the ten-year Strategic Road Development Plan. A process similar to the Storm Drainage Improvements Policy (Policy #459) should also be considered to support active transportation projects with a value less than \$400,000.

Additional administrative resources should also be accounted for from supporting departments, including: Capital Projects, Municipal Lands, Engineering, Planning, Recreation and Community Services, and Communications. County staff may be involved in the implementation of active transportation projects, design reviews, engagement activities, monitoring and reporting programs, and/or education and outreach campaigns.

### GAS TAX FUND

The Alberta Government flows the Gas Tax Fund (GTF) to the County to support local infrastructure priorities. Many municipalities solely use GTFs for roadway projects; however, under Schedule B, GTF can be applied to the construction, renewal, or enhancement of active transportation infrastructure including, but not limited to: bicycle lanes, shared-use pathways, and sidewalks.

### GRANT OPPORTUNITIES

Grant opportunities are broken down by Federal, Provincial, Regional/Local, and Private sources. Most funding sources are currently available; however, their future is uncertain and may change with successive governments or economic cycles. A summary of potential funding sources directly accessible by the County is shown in Table 12. There are additional funding sources available to community / non-profit organizations within Rocky View County such as the Government of Alberta's Community Initiatives Program (CIP).

## FEDERAL

### Green Municipal Fund (Federation of Canadian Municipalities)

This program funds studies that focus on reducing or avoiding fossil fuel use and reducing pollution by improving transportation networks or encouraging people to switch to less-polluting commuting options. Funding is available for a study grant for transportation networks and commuting options, pilot projects, and capital project loan and grant.

## PROVINCIAL

The sections below present a summary of federal active transportation funding sources currently available. At the time of writing this plan, future federal levels of investment and the future of some programs is uncertain.

### Municipal Sustainability Initiative (MSI) Capital Funding (Government of Alberta)

MSI capital funding is limited to projects that involve the purchase, construction, development, betterment, rehabilitation, or non-routine maintenance of a capital asset, owned by a municipality.

### Healthy Communities Grant Program (Alberta Blue Cross)

Every year, five \$50,000 grants are awarded to support community amenities and facilities that promote active living including active transportation infrastructure. For this fund, 'political organizations' are ineligible, so the County may need to partner with a local organization to apply and receive the funds.

### Basic Municipal Transportation Grant (BMTG) (Government of Alberta)

BMTG supports municipalities in developing and maintaining their capital transportation infrastructure requirements, promote economic growth, and improve the quality and transportation safety of community life. This program provides annual allocation-based funding for capital construction and rehabilitation of local transportation infrastructure including ancillary roadway facilities such as sidewalks, commuter bikeways, lighting, traffic control signals, and pedestrian signals.

### Strategic Transportation Infrastructure Program (STIP) (Government of Alberta)

The Strategic Transportation Infrastructure Program (STIP) provides financial assistance to rural and smaller urban municipalities for developing and maintaining key transportation infrastructure. STIP supports municipalities as they complete projects that improve accessibility and the movement of goods to market, increase opportunities for economic growth, and enhance safety and efficiency while extending the life of key transportation infrastructure. Eligible projects include local municipal initiatives as well as activities to improve existing bridges – engineering, maintenance, rehabilitation, or replacement.

### Alberta Municipal Infrastructure Program (AMIP) (Government of Alberta)

The goal of the Alberta Municipal Infrastructure Program (AMIP) is to support the development of infrastructure to maintain or enhance economic, social, and cultural opportunity and wellbeing while protecting and improving the quality of our environment upon which people and economies of

Alberta depend. Eligible projects include ancillary works such as sidewalks, commuter bikeways, lighting, traffic control signals, pedestrian signals, storm drainage and utility relocations; construction or implementation of traffic management projects such as major intersection improvements, major traffic signal coordination; and municipality-wide or regional transportation planning studies and major systems planning reviews.

#### **Alberta Healthy School Community Wellness Fund (Alberta Health and University of Alberta)**

The Alberta Healthy School Community Wellness Fund promotes sustainable and healthy school communities across the province by providing facilitated support and grants to schools and jurisdictions. This is an opportunity for the County to partner with Rocky View School District to develop and active and safe route to school program.

#### **Alberta Traffic Safety Fund (ATSF) (Government of Alberta)**

The purpose of the ATSF is to encourage and enable communities and other traffic safety stakeholders to develop and implement community-based collaborative traffic safety projects. The ATSF aims to build community capacity to identify and address local traffic safety priorities. The project must focus on one or more of the following traffic safety improvements: Vision Zero, speed, driver error, intersections, aggressive drivers, new and young drivers, pedestrians, cyclists, and motorcyclists.

#### **PRIVATE**

##### **TD Friends of the Environment Foundation Grant (TD Canada Trust)**

Eligible projects include trail buildings and restoration and in-class/indoor and outdoor environmental education programming.





## **APPENDIX**

The appendix is available as a standalone document, available on the Rocky View County website.

APPENDIX A

# Facility Guidelines

Rocky View County

# ACTIVE TRANSPORTATION FACILITY GUIDELINES



October 2018





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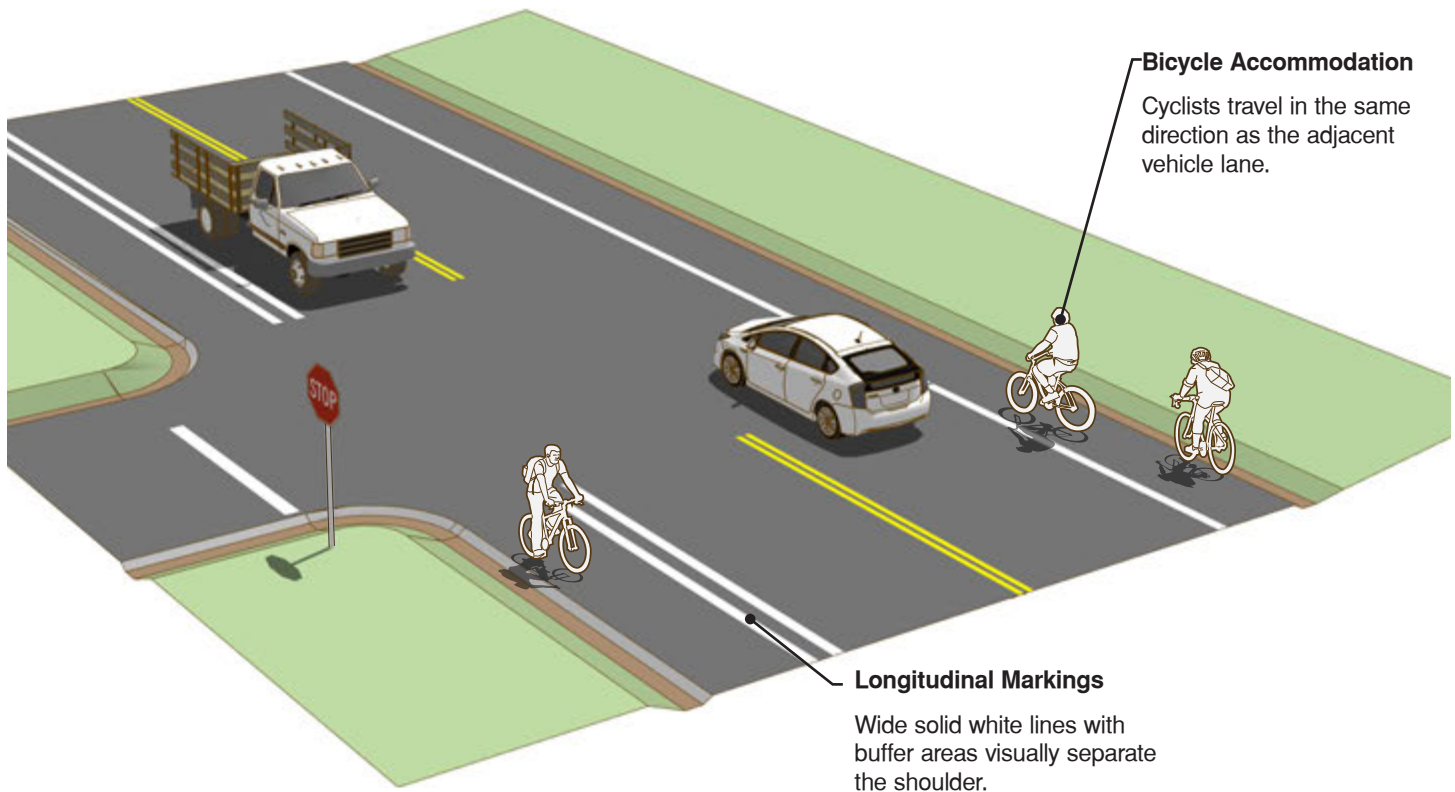


Figure 1: Wide paved shoulders can be bicycle accessible. Image adapted from FHWA Small Town and Rural Multimodal Networks Design Guide.

# Paved Shoulder

Paved shoulders are areas between the right of a painted travel lane and the roadway edge which can function as a space for cyclists and pedestrians to travel in the absence of other facilities with more separation.

## APPLICATION CRITERIA

- Rural areas (TAC sets density of <400 persons/km<sup>2</sup>).
- Posted speed is 50 to 80 km/hr.
- ADT >1000 veh/day to <4000 veh/day (or the road is part of a known cycling route).
- Include or upgrade shoulders during roadway resurfacing, rehabilitation, and reconstruction in new construction projects.

## BENEFITS

- Improves cyclist experiences on roadways with higher speeds or traffic volumes.
- Provides a stable surface off the roadway for pedestrians to use where sidewalks are not provided.
- Can reduce pedestrian "walking along roadway" crashes.
- Can reduce cyclist "struck from behind" crashes, which represent a significant portion of rural road crashes.
- Provides advantages for all roadway users, by providing space for cyclists, pedestrians, and motor vehicles.

## CONSIDERATIONS

- Is not an exclusive bicycle facility.
- Requires a wider roadway to provide an accessible shoulder space.

## FACILITY DESIGN

### CLEAR PAVED SHOULDER AREA

Paved shoulders should be wide enough to accommodate the horizontal operating envelope of pedestrians and cyclists. Though shoulders are not designed exclusively for cycling as are bicycle lanes, they may be accessible for bicycle use if there is adequate width, a painted separation between the travel lane, and a surface clear of snow or debris.

- Provide a minimum width of 1.5 m adjacent to a road edge or curb, exclusive of any buffer or rumble strip. In higher speed environments or areas of heavy truck traffic, wider shoulders are preferable.
- 1.5 m can be used for low speed roadways, and wider in higher speed environments or areas with heavy truck traffic.
- Where possible, provide greater width for added comfort and user passing.
- A desirable width is 2.0 m to allow for 1.5 m through width and a 0.5 m buffer.
- A maximum practical width is 3.0 m so that motor vehicle drivers are discouraged from using the shoulder as a travel lane.
- Buffers are used where width is available for greater cyclist comfort and delineate wide shoulders so that they are not used as a parking lane.
- Where shoulders are intended for use by pedestrians, they must meet accessibility guidelines.

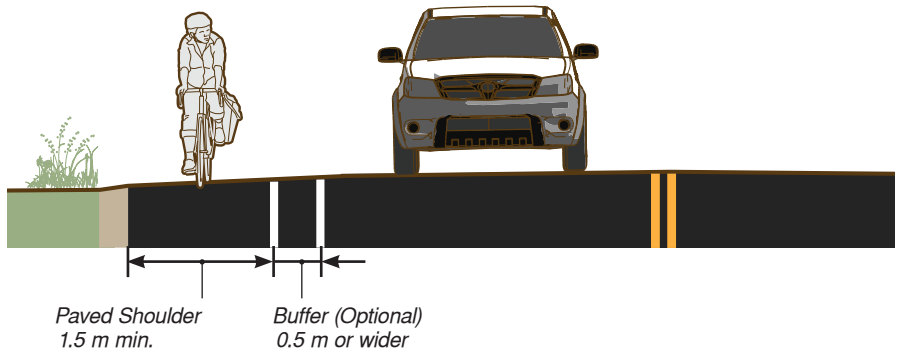


Figure 2: Preferred minimum widths for a paved shoulder. Image adapted from FHWA Small Town and Rural Multimodal Network Design Guide.

	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Width (m)	1.5	1.8	3	3

Table 1: Width of paved shoulders adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.9 Design Domain: Width of Bicycle Accessible Shoulders. Widths are measured from the centre of the edgeline to the outer edge of the paved shoulder.

### PAVEMENT MARKINGS

The travel lane edge of paved shoulders should be clearly delineated to discourage unnecessary encroachment by motor vehicles. A 100 mm solid white line is the standard lane marking. Options to visually enhance the space between the travel lane and the shoulder include:

- A wide 200 mm white line.
- A narrow buffer space two 100 mm solid white lines separated by and 0.45 m or greater space.
- A wide buffer space two 100 mm solid white lines, separated by a 1.2 m or greater space and optional crosshatch markings.

At intersections and major driveways, the solid edge line should be discontinued to indicate motor vehicles may cross into the shoulder space.

### RUMBLE STRIPS

Shoulder rumble strips are a raised or grooved pattern in the pavement surface and are a proven cost-effective safety measure for reducing vehicle roadway departure crashes. However, rumble strips negatively impact bicycle travel as cyclists riding over rumble strips may experience discomfort or lose control. Where possible, rumble strips should be avoided on bicycle routes. If rumble strips must be included on a bicycle route, refer to the TAC Geometric Design Guide and Alberta Ministry of Transportation Technical Standards for bicycle tolerable rumble strip design details.

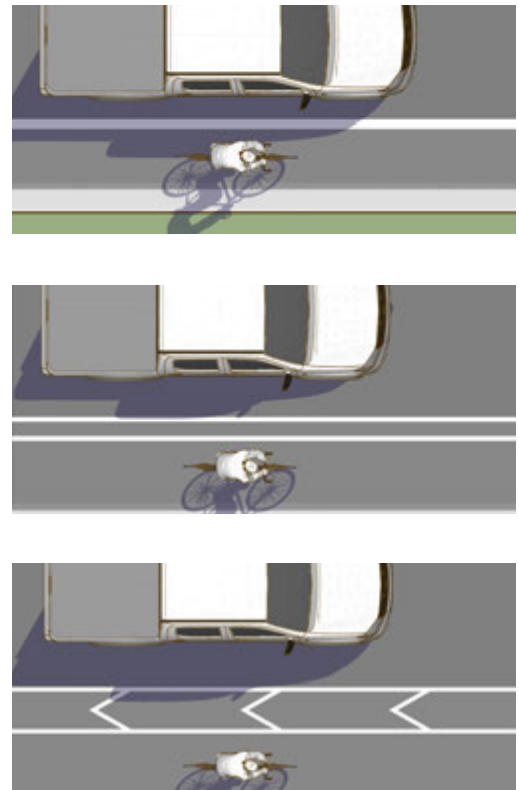


Figure 3: Longitudinal marking options along paved shoulders. Image adapted from the FHWA Small Town and Rural Multimodal Networks Design Guide.



## PAVEMENT CONTRAST AND COLOUR

Contrasting or coloured pavement materials may be used to further differentiate the shoulder from the adjacent travel lanes. Coloured pavement in a paved shoulder is an aesthetic treatment to enhance awareness and is not intended to communicate a regulatory, warning, or guidance message to road users.

The colours that can be used are shades of grey through a seal coating or asphalt versus concrete or different concrete aggregates/shades. Green is commonly used to denote bicycle lanes.

Adding pigments or integral colour to asphalt or concrete can create a coloured mixture than goes over the existing asphalt on the road. Some North American cities use a top dressing of coloured aggregate as a seasonal chipseal, which would need regular upkeep. Others use integral colour concrete, however colour continuity from batch to batch can be a concern.

## SIGNS

Signs are not required on paved shoulders, but may be used to identify a road as a bicycle route.

- The bike route sign (IB-23) indicates a roadway is part of bicycle route system and alerts motor vehicles to the presence of cyclists.
- The share the road sign (WC-19 or WC-19S) can be installed as a cautionary signs to warn the driver that they should be aware of any cyclists on the road, and give them adequate space.
- While Rocky View County has developed a custom regulatory sign MOTORISTS AND CYCLISTS SHARE THE ROAD, it is recommended to replace these signs with standard TAC Share the Road (WC-19 and WC-19S) signage for system wide consistency and enhanced visibility to drivers.

## SIGN PLACEMENT

Frequency of bike route signs for periodic confirmation for cyclists should be reduced to 2-3 km in absence of other bicycle signage or pavement markings.

As bike route signs are guide signs, they are of lower priority than regulatory and warning signs. Regulatory and warning sign types will take location precedence over guide signage.

To avoid obstruction of sign visibility between signs, use an 85th percentile speed to determine longitudinal sign spacing. Refer to the MUTCDC Table A1-4 for the amount of time required to read a sign based on its complexity.



Figure 7: Share the Road Sign (WC-19 and WC-19S) can be used as cautionary signs. WC-19S is a supplementary tab sign used to convey the meaning of WC-19. Images from TAC Bikeway Traffic Control Guidelines for Canada 2nd Edition (4.6.7).



Figure 8: Bicycle Route Marker Sign IB-23. Image from TAC Bikeway Traffic Control Guidelines for Canada.

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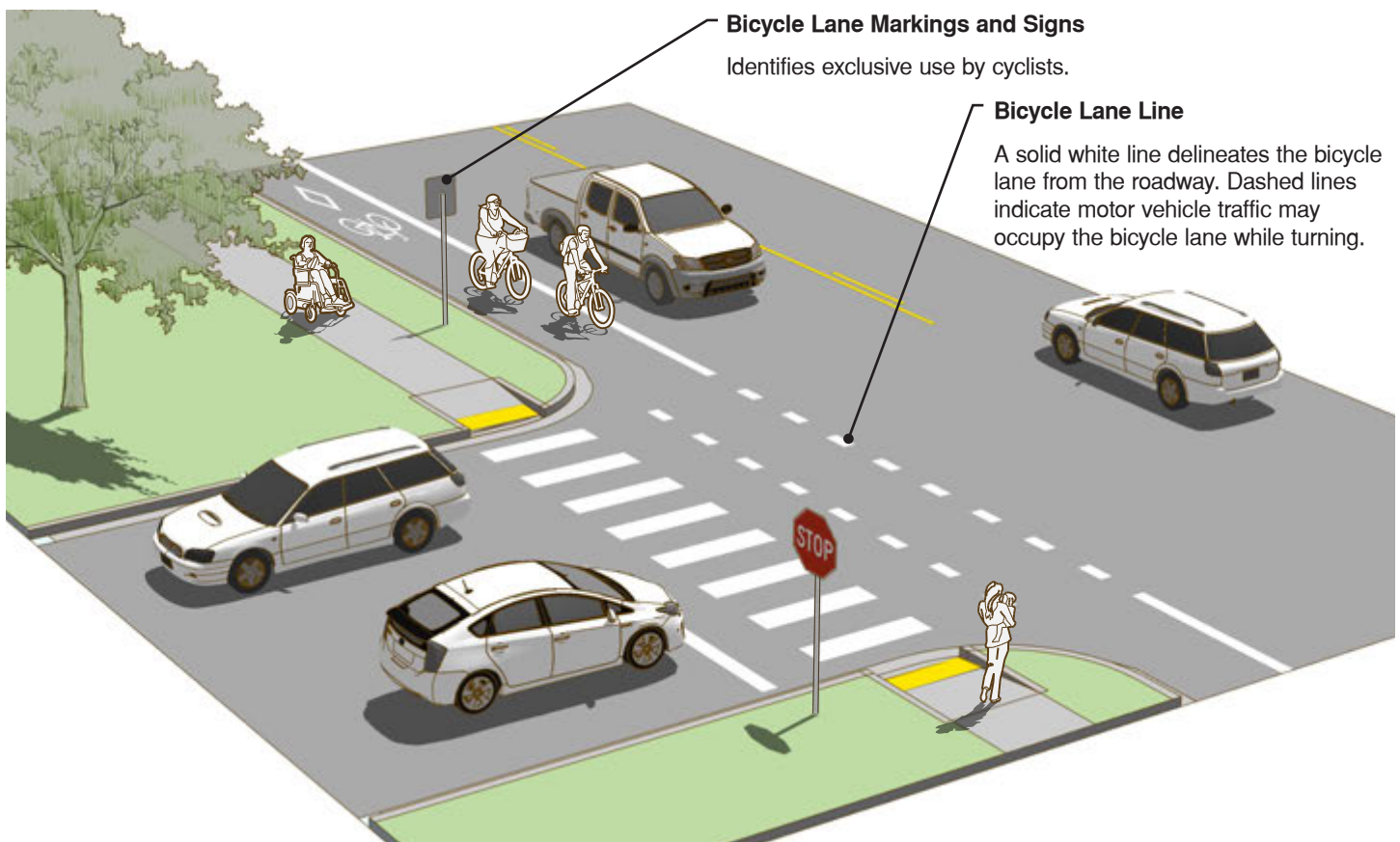


Figure 9: Bicycle lanes are an exclusive facility for cyclists and can be applied in an urban or in a rural setting. Image adapted from FHWA Small Town and Rural Multimodal Networks Design Guide.

# Bicycle Lane

Bicycle lanes designate an exclusive space for cyclists through the use of pavement markings and signs. A bicycle lane is located adjacent to a curb or parking lane, and generally follows the same direction as motor vehicle traffic.

## APPLICATION CRITERIA

- Used in rural or urban areas with low to medium average daily traffic (ADT) and high bicycle volumes.
- Posted speed is <50 km/hr
- ADT <4,000 veh/day

## BENEFITS

- Provides an exclusive, designated space on the roadway for cyclists.
- Provides additional separation distance between the sidewalk (if present) and the motor vehicle lanes.
- Provides visual cues to drivers to anticipate cyclists on the roadway.

## CONSIDERATIONS

- Reflects a more urban visual atmosphere.
- Requires a wider roadway to provide adequate space for bicycle lanes.
- It is important to have clear visual separation of bicycle lanes through either extending a solid white line marking, or a double white line with buffer space in between. Refer to Buffered Bike Lanes.
- May require additional width adjacent to on-street parallel vehicle parking.
- Requires a separate pedestrian sidewalk or pathway. Where multi-modal use is needed, consider a shared use pathway, or in low volume rural areas, a paved shoulder.

## FACILITY DESIGN

### DIRECTION

Bicycle lanes travel in the same direction as adjacent motor vehicle lanes.

### BICYCLE LANE WIDTH

- The preferred minimum width of a bicycle lane is 1.8 m to allow single-file bicycle traffic and basic passing movements.
- The practical lower limit width of a bicycle lane is 1.5 m and should only be used in constrained conditions for short distances.
- Adjacent to on-street parallel parking lanes, bicycle lanes should be a minimum of 2.1 m wide to allow a 0.6 m buffer for opening vehicle doors.
- Where bicycle volumes are high (>1500 bicycles/day) the bicycle lane should be wider (up to 2.1 m) to make passing movements easier.
- The minimum bicycle lane width is 1.5 m. Where a gutter is wider than 375 mm, the minimum bicycle lane width is 1.8 m to prevent the lip of the gutter from interfering with the bicycle wheel.
- Widths 2.1 m or greater may encourage motor vehicle use of bicycle lane for parking or driving. If extra width is available or desired, configure the bicycle lane with a buffer zone to delineate space, or consider a protected bicycle lane.

### BUFFER WIDTH

Bicycle lanes may be enhanced with a longitudinal marked buffer for greater vehicle separation distance. This treatment is appropriate for bicycle lanes on roadways with high motor vehicle traffic volumes and speed, adjacent to parking lanes, or a high volume of truck or oversized vehicle traffic. Refer to Buffered Bike Lanes for further detail.

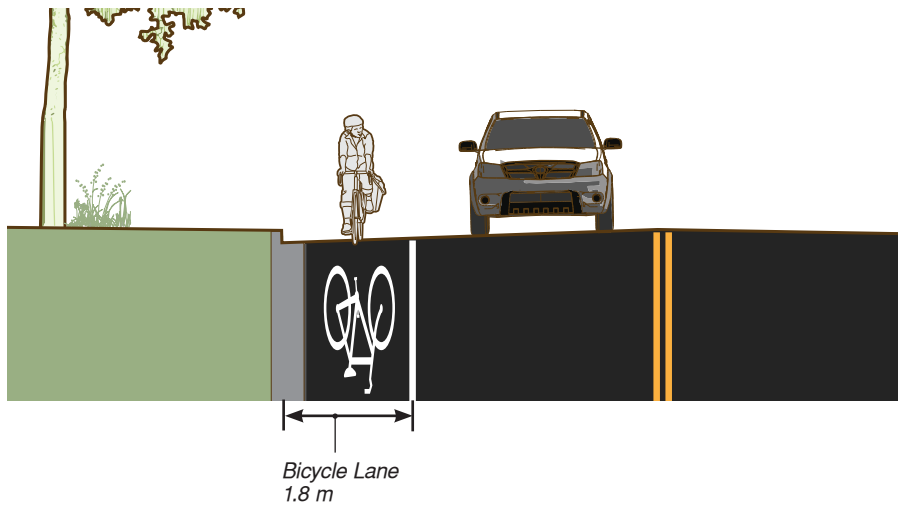


Figure 10: Preferred minimum widths for bicycle lanes. Image adapted from FHWA Small Town and Rural Multimodal Networks Design Guide.

Width (m)	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Bicycle lane	1.5	1.8	2.1	2.1

Table 2: Width of bicycle lanes adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.1 Design Domain: Width of Unbuffered Bike Lane.



Figure 11: Depending on roadway context, bicycle lanes can be applicable in urban, small town, and rural areas. This example is in Canmore, AB.

**PAVEMENT MARKINGS**

- Bicycle lanes are delineated by a 100 mm solid white line and bicycle pavement markings. Detailed standards and guidance for applying these elements can be found in the TAC Bikeway Traffic Control Guidelines for Canada.
- Bicycle lanes that are positioned away from the curb in the roadway, such as lanes adjacent to on street parking, should have an additional solid white line on the curb side to delineate both sides of the bicycle lane.
- Bicycle lanes are defined as a type of reserved lane in the MUTCDC. A diamond pavement marking indicates a reserved lane.
- Dashed lane lines are used where vehicles are permitted to cross the bicycle lane to perform a turning movement. A 15 m minimum broken line is used, with a 1.0 m segment and 1.0 m gap.
- Green can be used as a colour treatment to increase the conspicuity of bicycle lanes at conflict areas such as driveways and intersections.

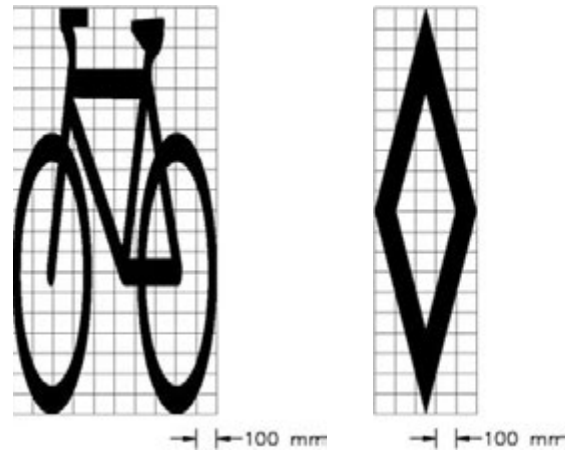


Figure 12: TAC Bikeway Traffic Control Guidelines for Canada 7.4.1 Bicycle Symbols and 7.4.2 Diamond Symbols

**SIGNS**

A reserved bicycle lane sign (RB-90, RB-91) and a reserved bicycle lane ends sign (RB-92) accompanies the diamond and bicycle pavement markings to indicate a reserved bicycle lane. Dimensions and guidance for these signs can be found in section A2.9.9 of the MUTCDC as well as the TAC Bikeway Traffic Control Guidelines for Canada.

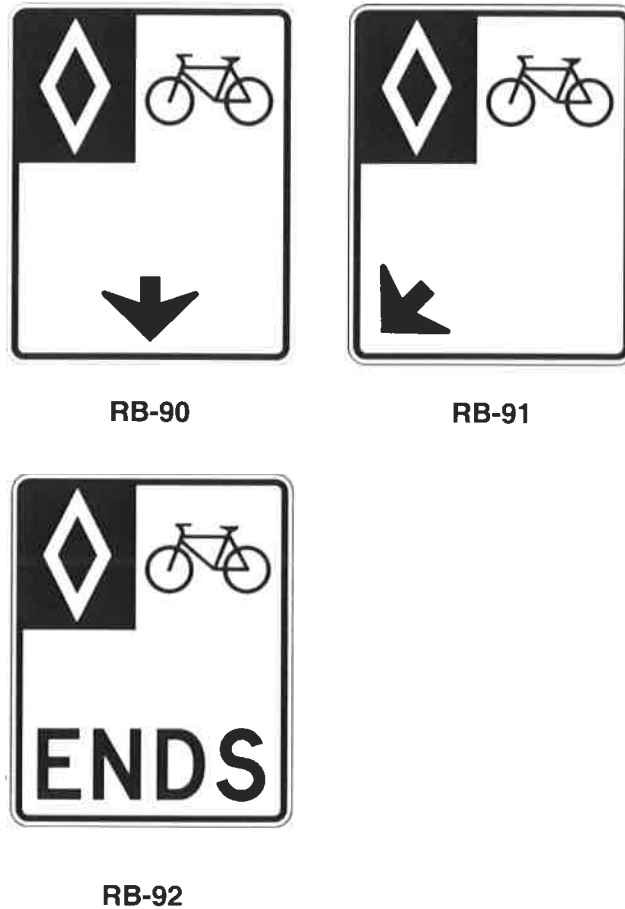


Figure 13: RB-90, 91, and 92 signs are positioned directly above or adjacent to reserved bicycle lanes. Images TAC Bikeway Traffic Control Guidelines for Canada Reserved Bicycle Lane Signs.

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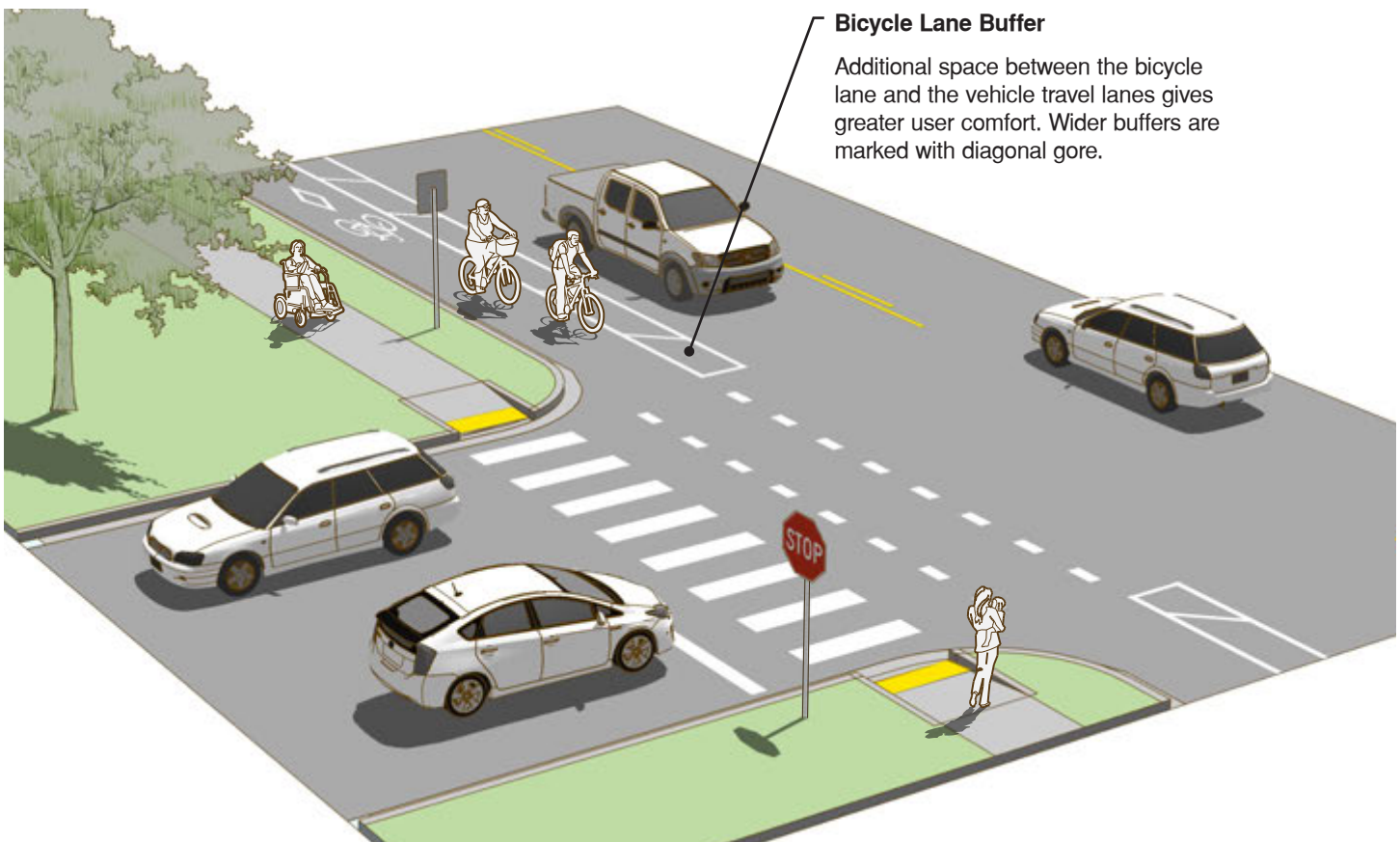


Figure 14: Buffered bicycle lanes add separation between the bicycle lane and motor vehicle lanes. Image adapted from FHWA Small Town and Rural Multimodal Networks Design Guide.

# Buffered Bicycle Lane

Buffered bicycle lanes add a designated buffer space between a bicycle lane and adjacent motor vehicle travel lanes and/or parking lanes for greater user comfort.

## APPLICATION CRITERIA

- Used in areas with low to medium average daily traffic (ADT) and high bicycle volumes.
- Posted speed is <60 km/hr
- ADT <7000 veh/day

## BENEFITS

- Provides a designated space on the roadway for cyclists.
- Provides additional separation distance between the sidewalk (if present) and the motor vehicle lanes.
- Provides visual cues to drivers to anticipate cyclists on the roadway.
- Provides clear visual separation of the lanes through a double white line with buffer space in between.

## CONSIDERATIONS

- Reflects a more urban visual atmosphere.
- Requires a wider roadway to provide adequate space.
- Requires a separate pedestrian sidewalk or pathway. Where multi-modal use is needed, consider a shared use pathway.
- May require additional width adjacent to on-street parallel vehicle parking.

**BUFFER ZONE**

Bike lanes may be enhanced with a longitudinal marked buffer area for more separation distance. This treatment is appropriate for bike lanes on roadways with high motor vehicle traffic volumes and speed, adjacent to parking lanes, or a high volume of truck or oversized vehicle traffic. Buffers may be placed between the bike lane and the motor vehicle lane, and between the bike lane and the parking lane, if present.

**PAVEMENT MARKINGS**

- A minimum width buffer of 0.3 m is delineated by two 100 mm solid lines without interior markings.
- If the buffer is 0.5 m or wider, mark the interior with diagonal or chevron hatching to clearly demarcate the space.
- Adjacent to a parking lane, the buffer must be a minimum of 0.6 m to allow space for opening motor vehicle doors.
- Dashed lane lines are used where vehicles are permitted to cross the bicycle lane to perform a turning movement. A 15 m minimum broken line is used, with a 1.0 m segment and 1.0 m gap.
- Green can be used as a colour treatment to increase the conspicuity of bicycle lanes at conflict areas such as driveways and intersections.

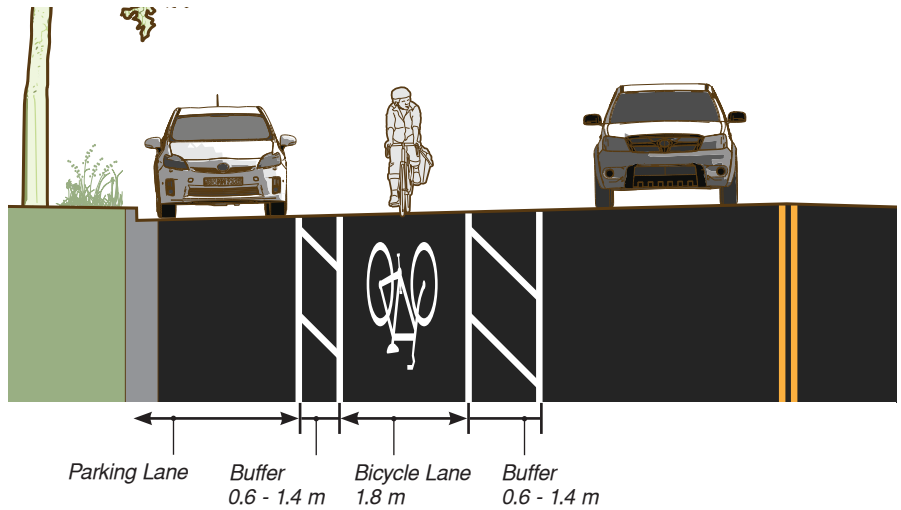


Figure 15: Preferred minimum widths for bicycle lanes and buffers configured adjacent to motor vehicle parking lanes.

Width (m)	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Bicycle lane	1.5	1.8	2.1	2.1
Buffer	0.3*	0.3*	0.9	1.4
Total Width Buffered Bicycle Lane	1.8	2.1	3.0	3.5

\*NOTE: A minimum buffer width of 0.6 m is required when bicycle lanes are adjacent to motor vehicle parking.

Table 3: Width of bicycle lanes adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.2 Design Domain: Width of Buffered Bicycle lane.

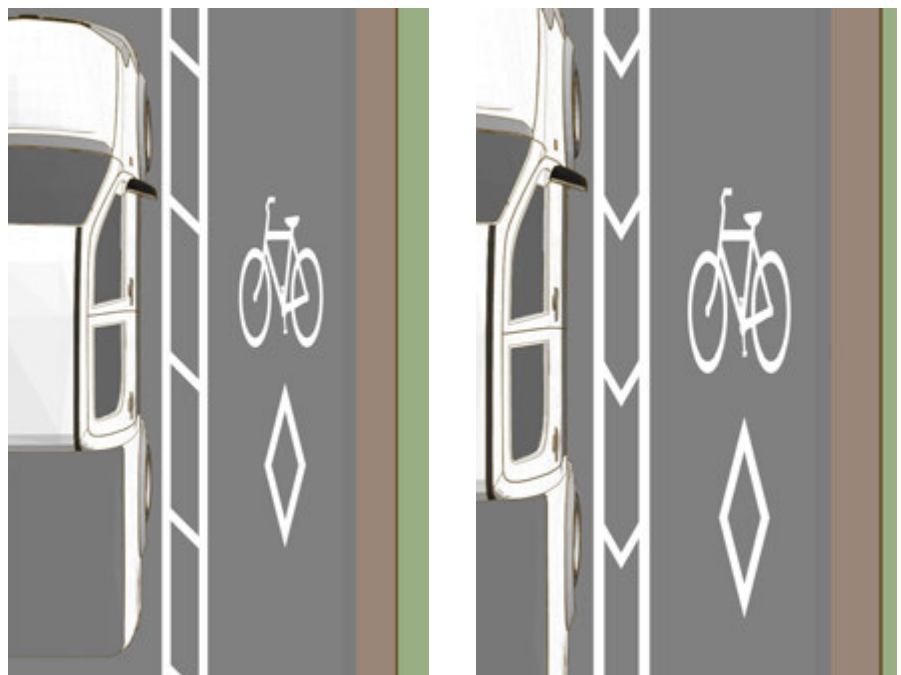


Figure 16: Bicycle pavement marking with a reserved lane diamond symbol and buffer with diagonal or chevron interior markings.



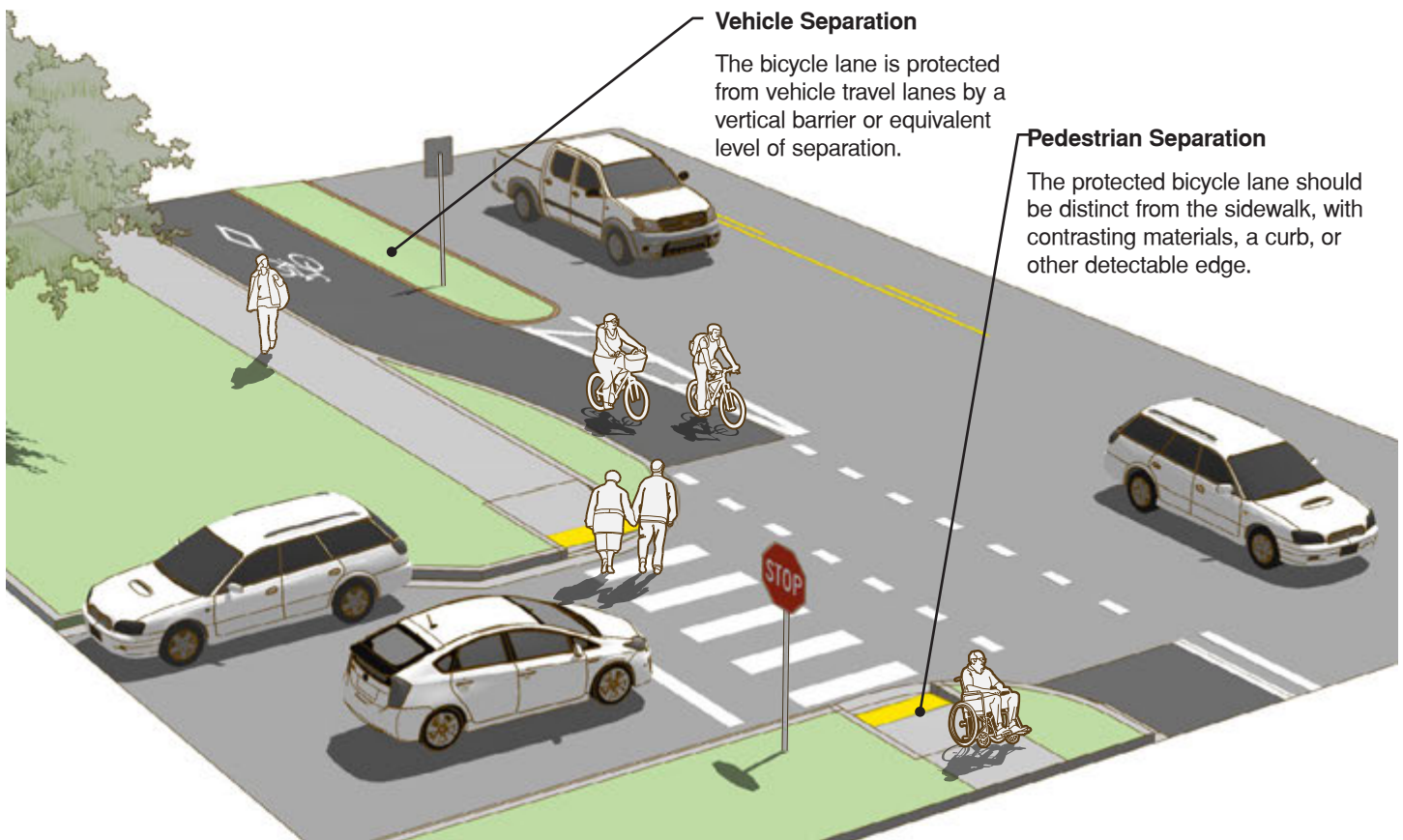


Figure 17: Protected bicycle lanes in a rural setting. Image: FHWA Small Town and Rural Multimodal Networks Design Guide.

# Protected Bicycle Lane

A protected bicycle lane, also called a cycle track, is an exclusive facility for cycling that is located on or directly adjacent to the roadway and is protected from motor vehicle traffic by a vertical barrier or equivalent physical separation.

## APPLICATION CRITERIA

- Used in urban areas with high average daily traffic (ADT) and high bicycle volumes.
- Posted speed is >40 km/hr and <80 km/hr, since a protected facility is not required for speeds lower than 40 km/hr.
- Transition areas into towns with higher speeds.
- Can be achieved in road retrofits as well as resurfacing or full road reconstruction.

## BENEFITS

- Provides a more comfortable experience than paved shoulders, bicycle lanes, or buffered bicycle lanes.
- Can reduce the incidence of sidewalk riding and potential user conflicts.
- Protected bike lanes offer cyclists a similar riding experience to multi-use paths but with fewer operational and safety concerns, particularly in areas with high-volumes of pedestrians.

## CONSIDERATIONS

- Reflects a more urban visual atmosphere. Use of a wide landscaped buffer may lessen visual impact concerns.
- Requires a wide roadside environment to provide for protection, bicycle lane, and sidewalk areas.
- May require specialized equipment for sweeping and snow clearing.
- May require extra signage and pavement treatments at driveways and intersections.

## FACILITY DESIGN

### DIRECTION

Protected bicycle lanes may be unidirectional on both sides of the roadway, or bidirectional on one side of the roadway.

### ELEVATION

Protected bicycle lanes may be at roadway grade, sidewalk grade, or an elevation in between.

- Elevation differences between the bicycle lane and the roadway are possible with a mountable or vertical curb face.
- Wide barrier widths of 1 m or greater are recommended to accommodate grade differences between the bikeway and the roadway.

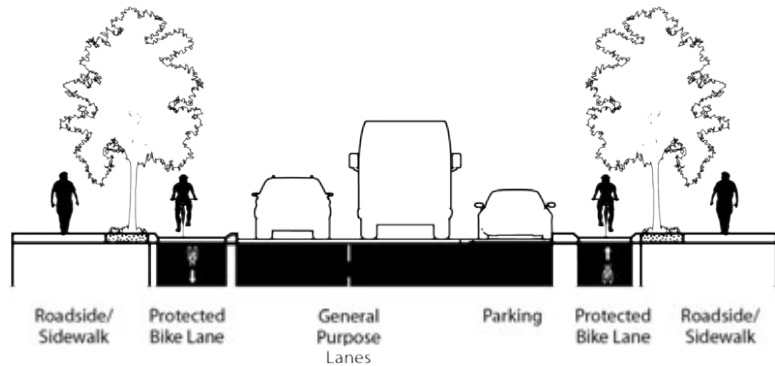
### PROTECTED BICYCLE LANE WIDTH

The protected bicycle lane should be a comfortable width for a clear operating area for cyclist travel.

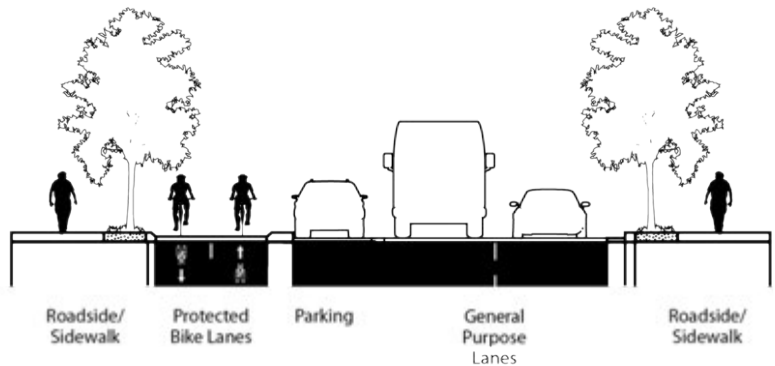
- Preferred minimum width of a one-way protected bicycle lane is 2.1 m, excluding the buffer. This width allows for side-by-side riding or passing.
- Preferred minimum width for a two-way protected bicycle lane is 3.0 m, excluding the buffer. This width accommodates the operational envelope of two cyclists in each direction with horizontal clearances for passing movements.
- The absolute minimum one-way protected bicycle lane width is 1.5 m, excluding the buffer. At this width, cyclists will not be able to pass slower users until there is a break in the facility and an opportunity to overtake.
- A wide through area of 3.0 m is beneficial to allow access for snow plows and street sweeping equipment.

### BARRIER WIDTH

- The width of the buffer ranges from 0.3 - 1.0 m, depending on the type of barrier selected, see following page for barrier types.
- Adjacent to parked vehicles, there must be a minimum of 0.6 m to accommodate opening vehicle doors.



(A) Unidirectional Protected Bike Lane



(B) Bidirectional Protected Bike Lane

Figure 18: Unidirectional and bidirectional protected bicycle lanes. Image: TAC Geometric Design Guide for Canadian Roads Figure 5.3.3 Protected Bike Lanes.

Width (m)	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Unidirectional protected bicycle lane, including barrier	1.8	2.7	3.5	5
Bicycle lane	1.5	2.1	2.5	3
Barrier**	0.3*	0.6*	1.5	2
Bidirectional protected bicycle lane, including barrier	2.7	3.3	4.6	6
Bicycle lanes	2.4	3	3.6	4
Barrier	0.3*	0.3*	1	2

\*NOTE: A minimum barrier width of 0.6 m is required when bicycle lanes are adjacent to motor vehicle parking.

\*\*NOTE: A low curb barrier is suggested as 0.5 m required for shy distance to vertical obstruction.

Table 4: Width of protected bicycle lanes adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.3 Design Domain: Width of Protected Bike Lane.

**ROADWAY PROTECTION**

There are a number of barrier options to protect a bicycle lane from the motor vehicle lanes. The TAC Geometric Design Guide lists a number of criteria for selecting a vertical barrier type including:

- Presence or absence of a parking lane.
- Roadway speed.
- Available width.
- Sight lines.
- Drainage.
- Maintenance requirements.
- Streetscape design.

New barrier types are being developed and refined, however the FHWA Separated Bicycle lane Planning and Design Guide provides an overview of the general types of barriers:

- Delineator posts.
- Bollards.
- Concrete barrier.
- Raised median.
- Raised lane.
- Planters.
- Parking stops.
- Parked cars.
- Combination thereof.

Table 5 outlines the appropriate context applications of the various barrier types.

**WINTER MAINTENANCE**

A protected bicycle lane buffer of at least 1.0 m is required for snow storage. It is expected that a clear path of at least 1.5 m will be maintained within protected bicycle lanes and that snow will be cleared into the buffer zone following a snowfall. This policy may differ for built up areas as opposed to more rural settings.

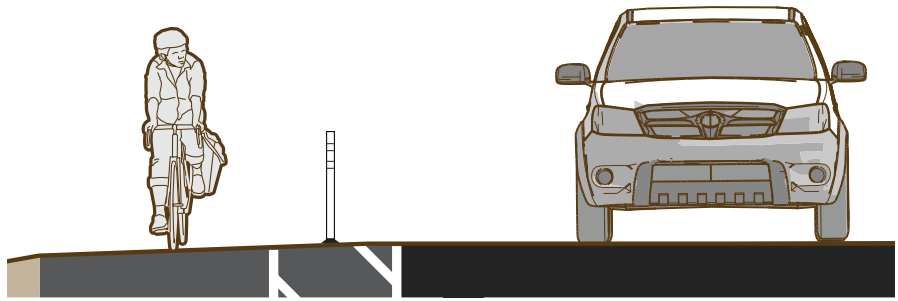


Figure 19: Protected bicycle lanes may be separated by a paved roadway separation, and a vertical element. Note this type of treatment is only appropriate for roadways with vehicle speeds up to 60 km/hr, refer to Table 5 below. Image adapted from FHWA Small Town and Rural Multimodal Networks.

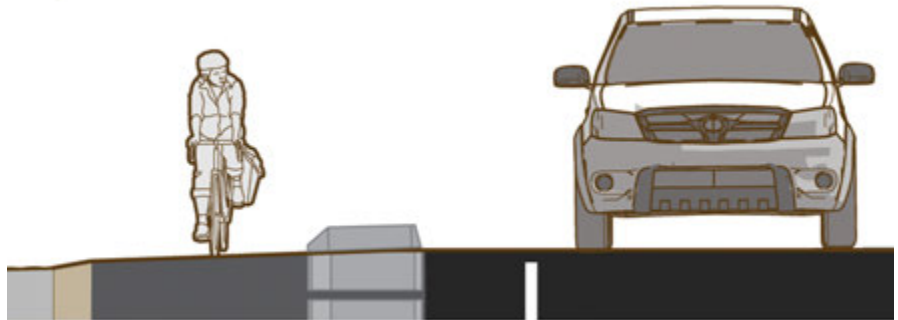


Figure 20: Protected bicycle lanes may be configured on an existing roadway surface by using a physical barrier such as a curb or median to separate the bikeway from the roadway. Image from the FHWA Small Town and Rural Multimodal Networks Design Guide pg 4-28.

Adjacent Lane	Examples of Suitable Delineators
Parking lane	A raised median, with a width of at least 0.6 m to avoid collisions with potential opening passenger-side car doors  Other delineators at least 0.6 m wide  If flexible bollards and buffered pavement markings are used, bollards should be longitudinally spaced at a maximum of 5.0 m  If used, parking stops should be spaced with longitudinal gaps of 2.0 m or less
General purpose lane 50 km/hr or less	75 mm raised median, minimum 0.3 m wide  Parking stops with longitudinal gaps of 2.0 m or less  Flexible bollards longitudinally spaced at a maximum of 5.0 m, centred laterally on 300 mm buffered bicycle lane pavement markings
General purpose lane 50-60 km/hr	150 mm raised median, minimum 0.3 m wide  Semi-rigid barrier (such as a guardrail)  Flexible bollards longitudinally spaced at a maximum of 2.0 m, centred laterally on 0.5 m buffered bicycle lane pavement markings  Precast curb (i.e.: parking stops), longitudinally continuous, 200 mm high  Planter boxes
General purpose lane 60-80 km/hr	Rigid barrier (such as a concrete safety shape or masonry wall)

Table 5: Application of various barrier options for protected bicycle lanes adapted from TAC Geometric Design Guide for Canadian Roads Table 5.7.1 Delineators Based on Type and Speed of Adjacent Lane.

**PEDESTRIAN SEPARATION**

Separation from pedestrians is particularly important in areas with high volumes of pedestrian traffic, and where a protected bicycle lane is configured at the same elevation as a sidewalk. Protected bicycle lanes can be clearly distinguished from the sidewalk by:

- Buffer space.
- Different pavement or surface treatments.
- Detectable tactile guidance strips. These are detectable by colour contrast, width, height differential and texture.



Figure 21: Separation from the sidewalk is valuable for reducing pedestrian use of the bicycle lane. The use of physical separation with vertical elements is one configuration. Image from the FHWA Small Town and Rural Multimodal Networks Design Guide.

**PAVEMENT MARKINGS**

Protected bicycle lanes use markings to clarify intended users and travel direction.

- Standard bicycle and diamond symbol markings clarify that the lanes are for the exclusive use of cyclists.

**SIGNS**

A Reserved Bicycle Lane (RB-90) sign must be used to supplement the bicycle lane pavement markings. Standards and guidance can be found in the TAC Bikeway Traffic Control Guidelines for Canada.



Figure 22: Visual delineation can also be supplemented . Image from the FHWA Small Town and Rural Multimodal Networks Design Guide.



Figure 23: Bicycle pavement marking, textured delineation, tree buffer space, and varying materials distinguish the protected bicycle lane from the sidewalk in Vancouver, BC.

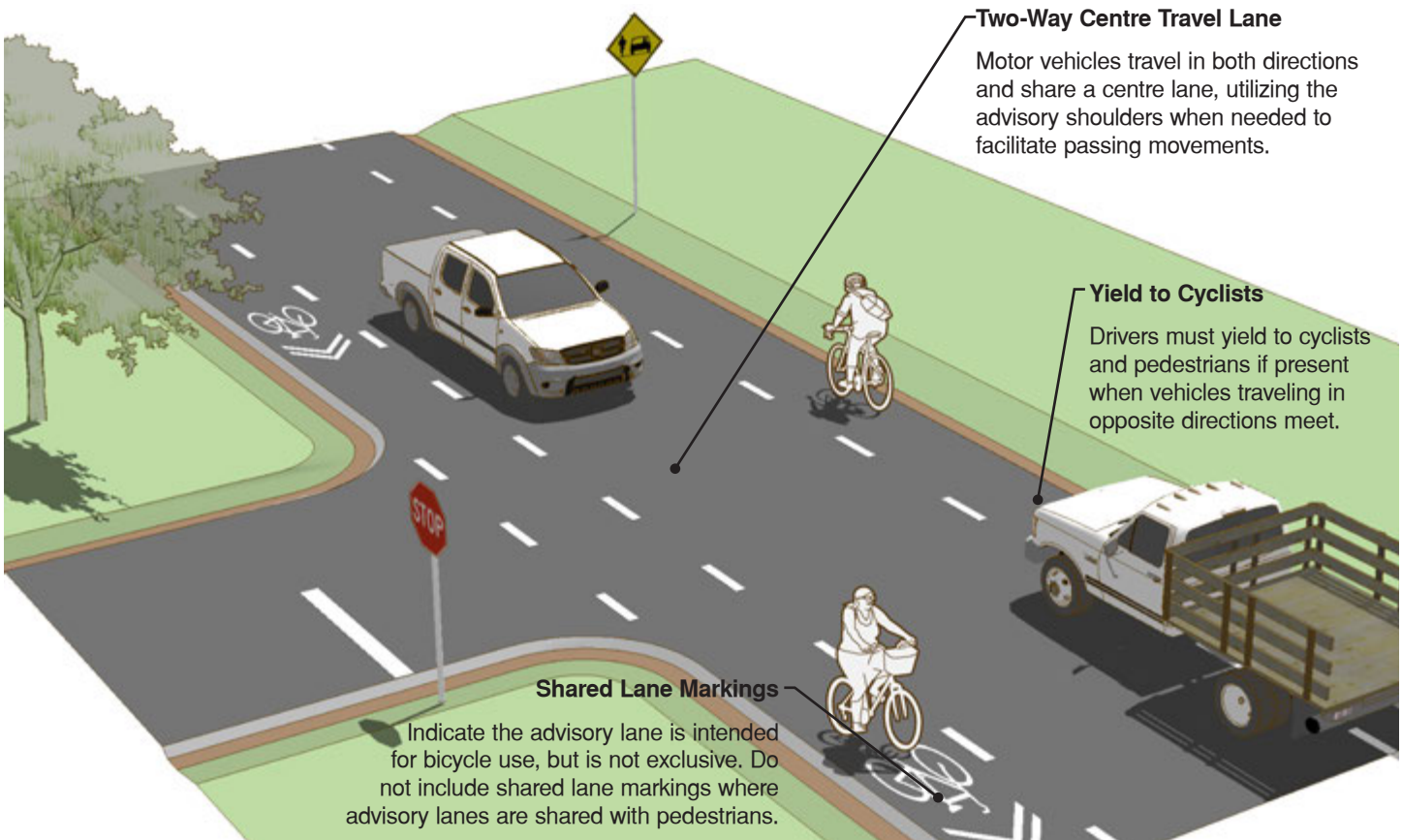


Figure 24: Advisory bicycle lanes in a rural setting. Image adapted from the FHWA Small Town and Rural Multimodal Networks Design Guide.

# Advisory Lane

An advisory lane is a dashed line on narrow roadways which delineates space for walking or cycling, but allows motor vehicles to enter the space in order to yield to oncoming traffic or complete passing maneuvers.

## APPLICATION CRITERIA

- Posted speed is <50 km/hr.
- Low volume areas, <4000 veh/day, ideally <2500 veh/day.
- Narrow roadways ≤11.1 m.
- Roadway segments without frequent stop or signal controlled intersections.
- Functions well within a rural and small town traffic and land use context.

## BENEFITS

- Increases predictability by clarifying lateral positioning space for people driving and people walking or cycling on a narrow roadway.
- Reduces motor vehicle speeds.
- Provides delineated space for cycling where ROW is limited.
- May be an appropriate interim measure to future road widening.

## CONSIDERATIONS

- Is not an exclusive bicycle facility.
- Relies on motor vehicle drivers to yield to bicycle traffic.
- May be shared with pedestrians in rural areas with no sidewalks.

## FACILITY DESIGN

### DIRECTION

Advisory bicycle lanes are part of the traveled way and prioritize roadway space to allow vulnerable road users to travel in the same direction as adjacent motor vehicles. It is expected that motor vehicles will encounter meeting or passing situations in the centre travel lane, and will enter the advisory lane where necessary. Motor vehicles are expected to yield to pedestrians or bicycles prior to entering the advisory lane to complete a passing maneuver.

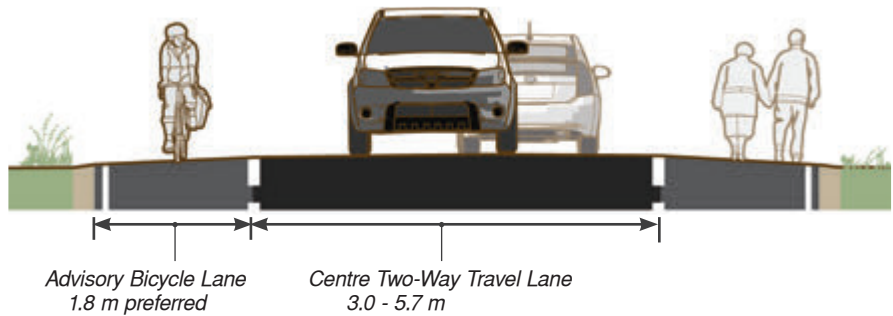


Figure 25: Advisory lanes clarify positioning and yield priority on roads too narrow to provide exclusive travel space. When pedestrians or cyclists are present, motorists may need to yield to users present in the advisory shoulder before passing. Image from the FHWA Small Town and Rural Multimodal Networks Design Guide.

### PEDESTRIAN USE

Where advisory shoulders are intended for use by pedestrians, they must meet accessibility standards.

### ADVISORY BICYCLE LANE WIDTH

- The preferred minimum width of an advisory bicycle lane is 1.8 m to allow single-file bicycle traffic and basic passing movements.
- The minimum advisory bicycle lane width is 1.5 m.

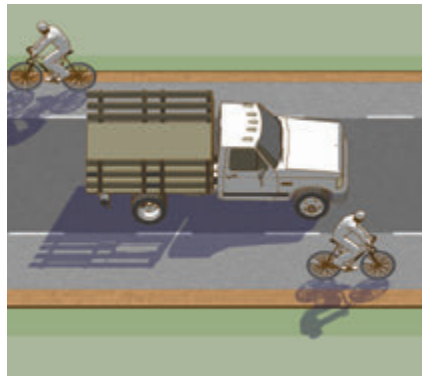
### CENTRE TRAVEL LANE WIDTH

The maximum width of the centre travel lane should be 5.7 m so that it does not appear to be the full width of a two-way two lane roadway. At this width, two passenger vehicles may be able to pass one another without encroaching into the advisory lane, though at lower speeds. Ideally, the centre travel lane is narrower than 5.7 m to require bidirectional traffic to share the centre lane and use the advisory lane space to allow oncoming traffic to pass. This configuration reduces vehicle speeds and encourages yielding to cyclists or pedestrians in the advisory lanes.

Width (m)	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Roadway with advisory bicycle lanes	6	6.6	9.9	11.1
Advisory bicycle lane	1.5	1.8	2.1	2.9
Two-way centre travel lane	3	3	5.7	5.7

Table 6: Width of protected bicycle lanes adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.8 Design Domain: Width of Advisory Bike Lanes.

3.0 m Centre Travel Lane



5.7 m Centre Travel Lane

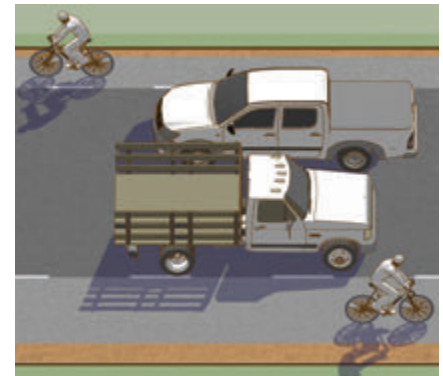


Figure 26: Total roadway width affects the number of road users that can meet and pass simultaneously. Wider roadways allow for more simultaneous interactions and can support higher volumes of motor vehicles. Images from the FHWA Small Town and Rural Multimodal Networks Design Guide.

**PAVEMENT MARKINGS**

- Advisory bicycle lanes are delineated with dashed 100 mm wide white lines to indicate motor vehicles may enter the space. The dashed pattern consists of a 1 m long stripe followed by a 1 m gap per TAC Bikeway Traffic Control Guidelines for Canada.
- The dashed lane line markings should continue through intersections and across major driveways.
- As this is not an exclusive bicycle facility, shared lane markings may be placed in the centre of the advisory lane to indicate roadway positioning to people riding bicycles. Shared lane markings should not be used where the advisory lane will be used by pedestrians.
- In general, do not mark a centre line on the roadway. Short sections may be marked with yellow centre line pavement markings to separate opposing traffic flows at specific locations, such as around curves, over hills, on approaches to at-grade crossings, and at bridges. At these locations, widen the paved roadway surface to provide space for paved bicycle-accessible shoulders and conventional width travel lanes. Refer to MUTCDC section C2.2 for no passing zone application and sight distance requirements.



Figure 27: Advisory lane with shared lane markings in Gibsons, BC.

**PAVEMENT CONTRAST AND COLOUR**

Contrasting or coloured pavement materials may be used to further differentiate the advisory bicycle lane from the adjacent travel lanes. Coloured pavement in an advisory lane is an aesthetic treatment to enhance awareness and is not intended to communicate a regulatory, warning, or guidance message to road users.

If a contrasting or coloured pavement material is used, it should also be applied through driveway crossings and minor intersections to visually maintain the advisory lane.

**SIGNS**

Potential signs for use with advisory shoulders include:

- A modified Two-Way Traffic Ahead warning sign (WB-3) to clarify two-way operation of the roadway. The sign is modified to remove the centre line, since roadways with advisory bicycle lanes do not have a centre line.
- A Share the Road warning sign (WC-19, supplementary tab WC-19S) to indicate to vehicle drivers the presence of cyclists on the roadway.
- Parking Prohibited (RB-51) signs to discourage parking within the advisory shoulder.
- Temporary educational signage.



Figure 28: Share the Road Sign WC-19 and supplementary tab sign WC-19S can be used as cautionary signs with advisory lanes. Images from TAC Bikeway Traffic Control Guidelines for Canada 2nd Edition (4.6.7).



Figure 29: The modified WB-3 Two-Way Traffic Ahead warning sign can clarify undivided two-way operation of the advisory shoulder configuration. Image modified from MUTCDC (A3.6.3).



**RB-51**  
**300 mm x 300 mm**

Figure 30: The RB-51 Parking Prohibited sign. Image from MUTCDC (A2.8.3).

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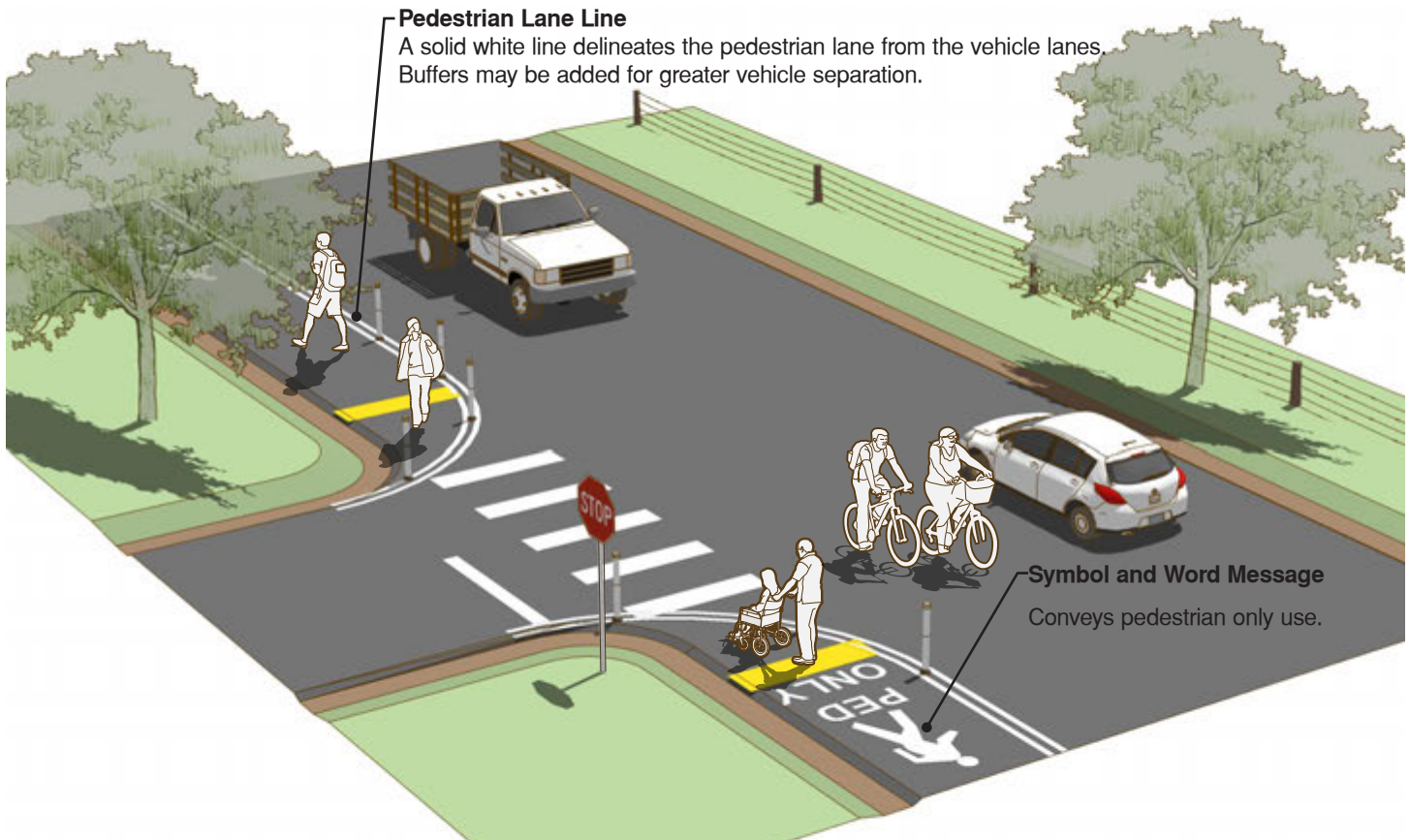


Figure 31: Pedestrian lanes are an interim low-cost measure in areas with low vehicle volumes and low pedestrian volumes.

# Pedestrian Lane

A pedestrian lane is a temporary or interim measure which designates space on the roadway for the exclusive use of pedestrians. The lane may be on one or both sides of the roadway and can fill gaps between important destinations.

## APPLICATION CRITERIA

- Posted speed is  $\leq 40$  km/hr.
- Volumes of  $\leq 2500$  veh/day.
- Rural areas (TAC sets density of  $<400$  persons/km<sup>2</sup>).

## BENEFITS

- May fill in gaps between sidewalks.
- Low cost measure.

## CONSIDERATIONS

- Detectability by people with vision disabilities.
- Undesired use by cyclists.
- Accessible cross-slope requirements.
- Maintenance strategies, such as sweeping and snow removal.
- Lighting for night-time visibility.

**FACILITY DESIGN**

Pedestrian lanes function similarly to paved shoulders, however they are marked for pedestrian only use. Pedestrian lanes provide interim or temporary pedestrian accommodation on roadways lacking sidewalks. They are not intended to be an alternative to sidewalks.

- 2.4 m width is preferred for side by side walking.
- 1.2 m width is the minimum operational width of a single pedestrian.
- Because of the lack of physical separation between people walking and vehicle travel lanes, additional buffer width beyond the pedestrian lane should be included where possible for added comfort. Buffers may include flexible delineators as a vertical element. Buffers and delineators should be a priority at intersections, if they are being added.
- Buffers may range from 0.3 m - 1.2 m wide.
- Pedestrian lanes are intended for use by pedestrians and must meet accessibility guidelines. The cross slope shall be 2 percent maximum, and the surface of shall be firm, stable, and slip resistant.

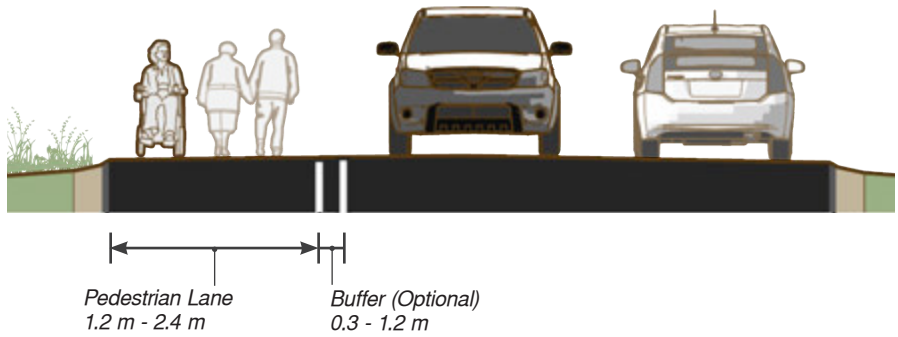


Figure 32: Pedestrian lane widths. Image adapted from the FHWA Small Town and Rural Multimodal Networks Design Guide.

**PAVEMENT MARKINGS**

- Use a 100 mm solid white line for longitudinal delineation of the lane.
- The lane may also be delineated by a buffer space of two 100 mm solid white lines separated by 0.1 m - 1.2 m space. For buffers 0.5 m or wider, mark the interior with diagonal or chevron hatching to clearly demarcate the space.
- Use a PED ONLY word pavement message to designate exclusive pedestrian use of the lane. A pedestrian pavement marking symbol can add conspicuity to the lane and communicate exclusive pedestrian use.



Figure 33: Pavement markings for pedestrian lanes. Image from the FHWA Small Town and Rural Multimodal Networks Design Guide pg 5-7.

**SIGNS**

- A modified WC-2R may be considered to indicate to drivers the presence of pedestrians on the roadway.



Figure 34: A modified WC-2R Sign. Image modified from MUTCDC (A6.4.3).

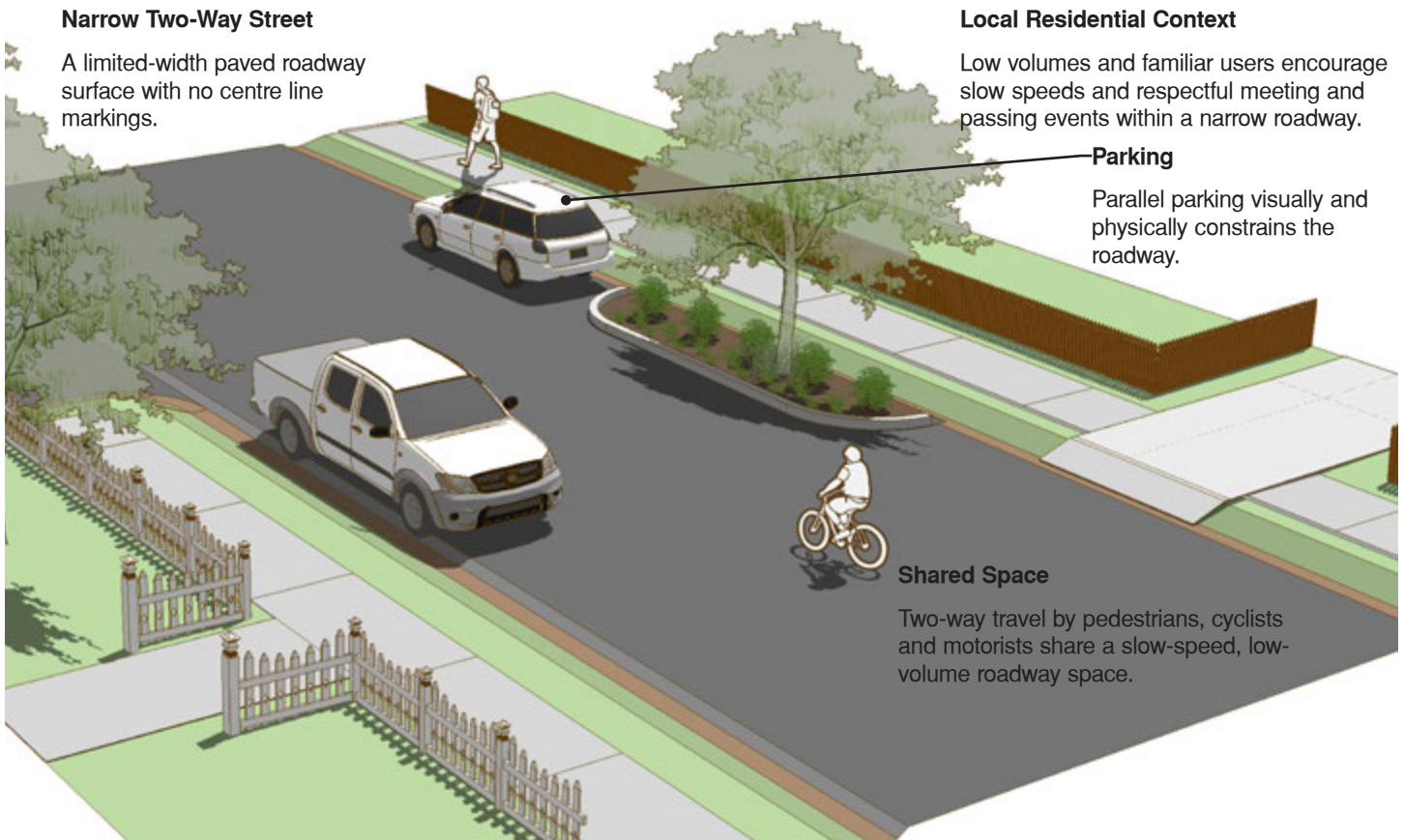


Figure 35: Shared roadway treatments along a residential street.

# Yield Roadway

A yield roadway, also called a shared roadway, is a narrow, slow-speed roadway in which pedestrians, cyclists, and two-way vehicle traffic share the same space. Typically parking is allowed on both sides of the roadway, limiting overall width.

## APPLICATION CRITERIA

- Posted speed is ≤ 40 km/hr.
- Volumes of ≤ 2500 veh/day. Ideal volumes are ≤ 1000 veh/day.
- Residential areas or local streets that connect to other active transportation facilities

## BENEFITS

- Connects local residential areas to destinations on the network.
- Encourages slow travel speeds when narrower than 6.0 m.
- Supports on-street or shoulder parking for property access.
- Low maintenance needs over time.
- Maintains rural or small town aesthetic.

## CONSIDERATIONS

- Yield roadways are not designated bicycle facilities, but can be low volume, low speed connectors for the bicycle network.
- Low speed, low volume shared roadways are ideal candidates for bicycle boulevard traffic calming treatments.
- Emergency vehicle access.

## FACILITY DESIGN

### ROADWAY WIDTH

- Yield roadways are narrow roadways that allow bidirectional travel on a shared centre travel lane. Bidirectional movements are accomplished by one direction yielding to the other by waiting in the parallel parking lane.
- Parking may be on one or both sides of the shared travel lane. Depending on this configuration, yield roadways are ideally 5.5 m to 8.0 m wide. Where the roadway is wide enough to provide a travel lane in either direction, the roadway should be configured with shared lane markings in the centre of each lane, or as advisory bicycle lanes.
- Parking may be prohibited in advance of intersections for clear sight lines and turning movements of larger vehicles.
- Trees may be planted within the parking lane area at regular intervals to visually and physically narrow the roadway and encourage slow speeds.

### PEDESTRIAN USE

If pedestrians will be accommodated on the roadway rather than on sidewalks, the roadway must meet accessibility standards for exterior paths of travel including surface and slope requirements.

### PAVEMENT MARKINGS

- Shared roadways may be marked or unmarked with shared lane markings. Sharrows are applied according to the TAC Bikeway Traffic Control Guidelines for Canada.
- Centrelines are not marked on yield roadways.

### SIGNS

Potential signs for use with on yield roadways include:

- A modified Two-Way Traffic Ahead warning sign (WB-3) to clarify two-way operation of the roadway. The sign is modified to remove the centre line.
- A Share the Road warning sign (WC-19, supplementary tab WC-19S) to indicate to vehicle drivers the presence of cyclists on the roadway.

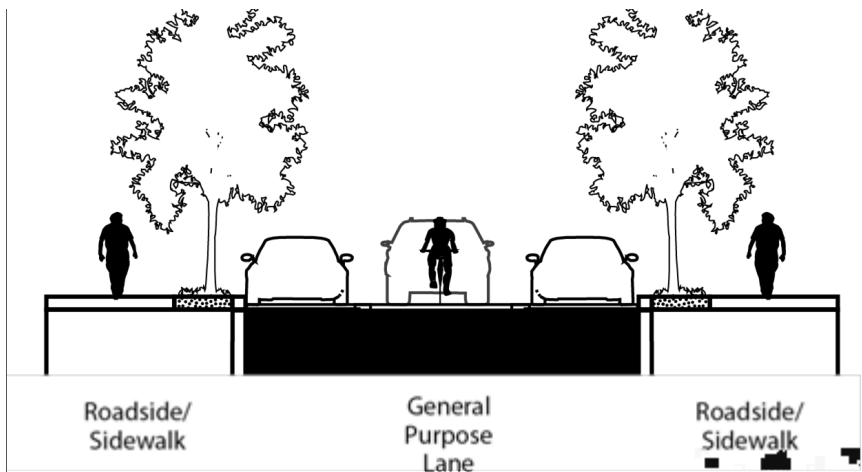


Figure 36: Configuration of shared roadways. Image from TAC Geometric Design Guide for Canadian Roads.

Width (m)	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Shared roadway with parking both sides and two-way traffic flow, typically one direction at a time	8	8	9	10
Shared roadway with parking on one side and two-way traffic flow, typically one direction at a time	5.5	5.5	7	7.5

Table 7: Width of shared roadways adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.6 Design Domain: Width of Shared Roadways.



Figure 37: Share the Road Sign WC-19 and supplementary tab sign WC-19S can be used as cautionary signs. Images from TAC Bikeway Traffic Control Guidelines for Canada 2nd Edition (4.6.7).



Figure 38: The modified WB-3 Two-Way Traffic Ahead warning sign can clarify undivided two-way operation of the roadway. Image modified from MUTCDC (A3.6.3).

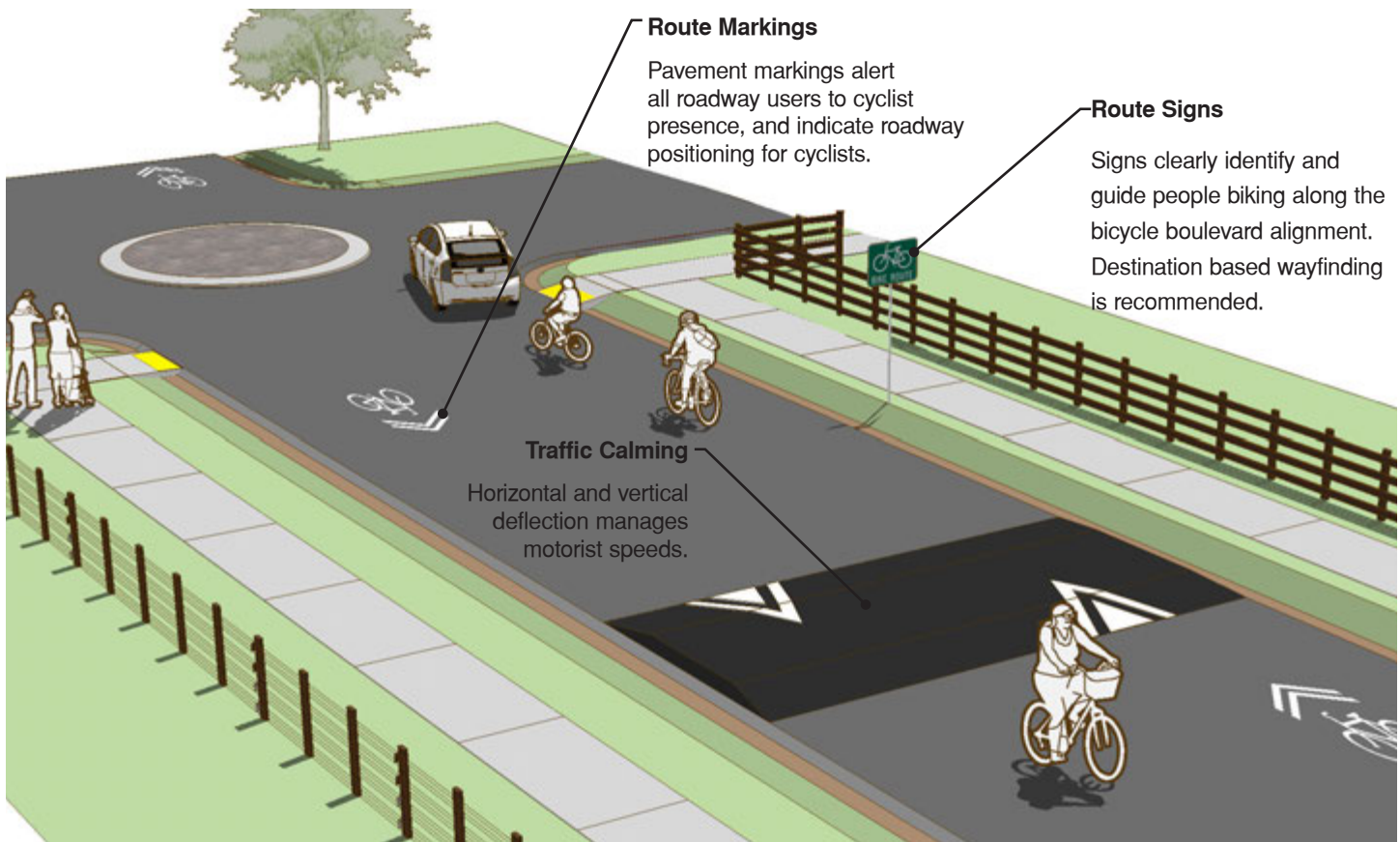


Figure 39: Bicycle boulevard treatments along a residential street.

# Bicycle Boulevard

A bicycle boulevard, also known as a local street bikeway, is a low-stress bike route shared with motor vehicle traffic that is designed to prioritize bicycle traffic by reducing motor vehicle volumes and speeds.

## APPLICATION CRITERIA

- Posted speed is  $\leq 40$  km/hr.
- Volumes of  $\leq 2500$  veh/day. Ideal volumes are around  $\leq 1000$  veh/day.
- Residential areas or local streets.

## BENEFITS

- Increases comfort for people cycling by reducing motor vehicle operating speeds and volumes.
- Improves the quality of life for residents through calmer traffic streets and safer crossings.
- Visually less impacting than separated facilities.

## CONSIDERATIONS

- May be an indirect route for cyclists.
- May divert vehicle traffic to other roadways or require route planning for vehicle traffic.
- Rural roadway networks may not have through connections for bicycle connectivity.
- May require additional paved surface to provide sidewalk space for pedestrians.

## FACILITY DESIGN

### TRAFFIC CALMING

Where speeds and volumes do not meet preferred values for a bicycle boulevard, traffic-calming techniques may be used.

- Intersections may have diagonal diverters, median diverters, and neighbourhood traffic circles to restrict vehicle speeds, but allow the through movement by bicycle.
- Between intersections, chicanes and speeds humps or speed cushions can reduce speeds.
- Stop signs are oriented to control cross routes intersecting the bicycle boulevard to prioritize the through movement of the bicycle boulevard and reduce motor vehicle speeds prior to intersecting with the bicycle boulevard.
- It is recommended to reduce the posted speed limit if it is higher than 40 km/hr.

### WIDTH

To further calm vehicle speeds, bicycle boulevards can be designed as shared roadways, which have parking on one or both sides of a narrow centre travel lane for bidirectional traffic. This restricts motor vehicle traffic to flow in one direction at a time while the opposing vehicles wait in the parking lane.

### PEDESTRIAN USE

On bicycle boulevards, the appropriate pedestrian facility is a sidewalk. If pedestrian travel must be facilitated within the roadway, it must meet accessibility standards for exterior paths of travel including surface and slope requirements.

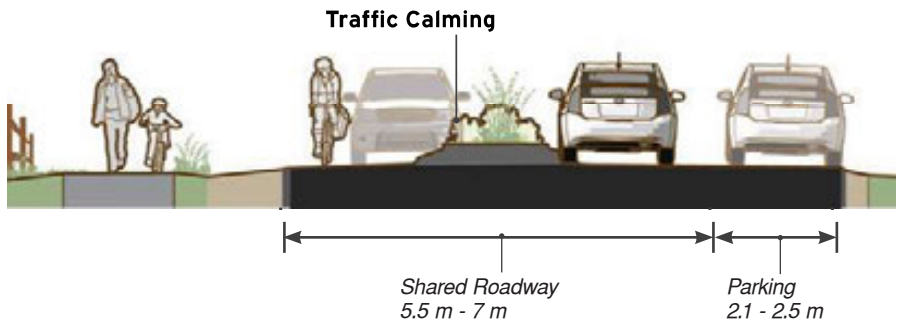


Figure 40: Typical shared roadway widths. At intersections, neighbourhood traffic circles can also narrow roadway width and reduce vehicle speeds. Image from the FHWA Small Town and Rural Multimodal Networks Design Guide.

Width (m)	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Shared roadway with parking both sides and two-way traffic flow, typically one direction at a time	8	8	9	10
Shared roadway with parking on one side and two-way traffic flow, typically one direction at a time	5.5	5.5	7	7.5

Table 8: Width of shared roadways adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.6 Design Domain: Width of Shared Roadways.

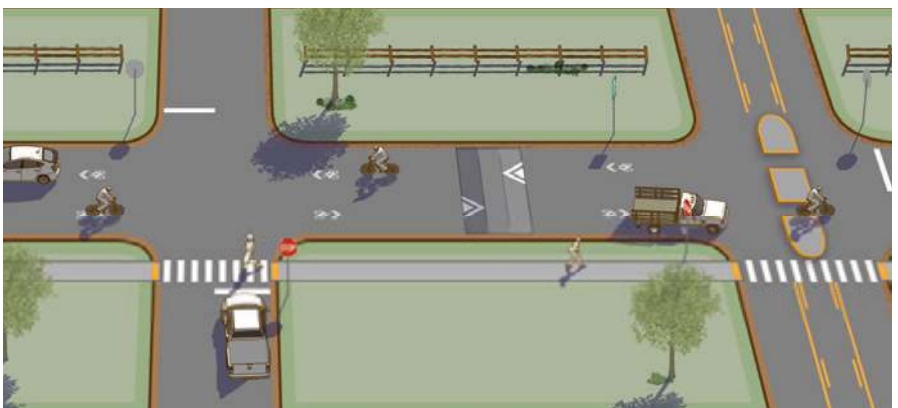


Figure 41: Bicycle boulevards combine road markings, traffic-calming measures, and crossing improvements to enhance the comfort and priority of cyclists traveling along the route. Image from the FHWA Small Town and Rural Multimodal Networks Design Guide.

**PAVEMENT MARKINGS**

Pavement markings can identify a route as a bicycle boulevard and alert motor vehicle drivers to the presence of people cycling.

- Shared lane markings or "sharrows" are the standard marking for indicating shared roadway bicycle operations (Figure 42).
- Sharrows indicate cyclist positioning on the roadway. Place sharrows in the centre of the travel lane to minimize vehicle tire wear and to encourage cycling a safe distance from potentially opening vehicle doors of parked cars. The markings should be centred at least 3.4 m from the face of the curb to avoid the door zone of parked cars.
- Refer to section 7.4.3 of the TAC Bikeway Traffic Control Guidelines for Canada for longitudinal placement.
- Centrelines are not marked on bicycle boulevards unless it is a short channelization at intersections.



Figure 42: Shared use lane symbol from TAC Bikeway Traffic Control Guidelines for Canada.

**SIGNS**

Signs can also be used to identify a bicycle boulevard. It is recommended that all bicycle routes include route and wayfinding signage. This is particularly important for non linear bicycle boulevard routes. There are three functional types of bicycle wayfinding signs:

- Decision Signs. Decision signs, such as IA-3 scaled to bicycle traffic, can mark the junction of two or more bikeways and provide cyclists directional guidance to access key destinations. Decision signs are typically placed 15-60 m in advance of a bikeway junction to allow a bicyclist enough time to slow, change lanes, and prepare for any turns that may be necessary.
- Confirmation Signs. Bicycle Route Marker (IB-23) (Figure 43) signs indicate to cyclists that they are on a designated bikeway and make motorists aware of the bicycle route. Confirmation signs are placed 15-30 m after a turn movement or intersection. These signs need not occur after every intersection, but should be prioritized at locations where a designated route is not linear, as well as after complex intersections such as those having more than four approaches at greater or less than 90 degrees and roundabouts.
- Turn Signs. Turn signs indicate where a bike route turns from one street onto another street. Turn signs may include a destination name, or may simply be an arrow plaque, such as the IS-5 through IS-9 (Figure 45) tab signs. Turn signs are placed 15-60 m in advance of turns to give cyclists adequate time to slow down or, if necessary, change lanes to prepare for a turn.



Figure 43: Bicycle Route Marker Sign IB-23. Image from TAC Bikeway Traffic Control Guidelines for Canada.



IS-7



IS-8R



IS-9R



Figure 44. MUTCDC guide signs IA-2 or IA-3 can be appropriately scaled for active transportation users (refer to MUTCDC Table A1-5), and assembled with the IB-23 for bicycle navigation. Image from the MUTCDC A4.2.1.

Figure 45: Directional arrow tab signs which can be assembled with an IB-23 sign for bicycle turn navigation. Image from TAC Bikeway Traffic Control Guidelines for Canada.

**GENERAL SIGN PLACEMENT**

Where two or more bikeways intersect, the general approach is to place a decision sign prior to the decision point followed by a confirmation sign or pavement marking after the intersection to confirm intended direction. While this approach provides redundant information, it ensures continuity in the wayfinding system, even if a particular sign is damaged. See Figure 46 for typical sign placement at intersections.

The TAC Bikeway Traffic Control Guidelines for Canada specifies the lateral and vertical clearances for bicycle sign placement. Figure 47 illustrates the minimum sign clearances for signs on bicycle paths, and Figure 48 illustrates sign clearances in urban areas where pedestrian movements are expected.

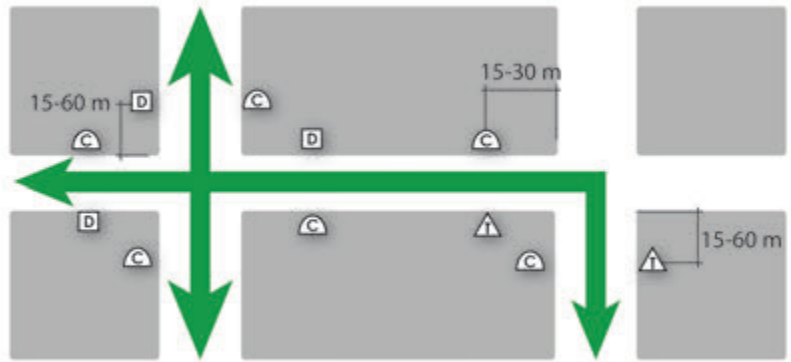


Figure 46: Typical bikeway Decision (D), Confirmation (C), and Turn (T) sign placement.

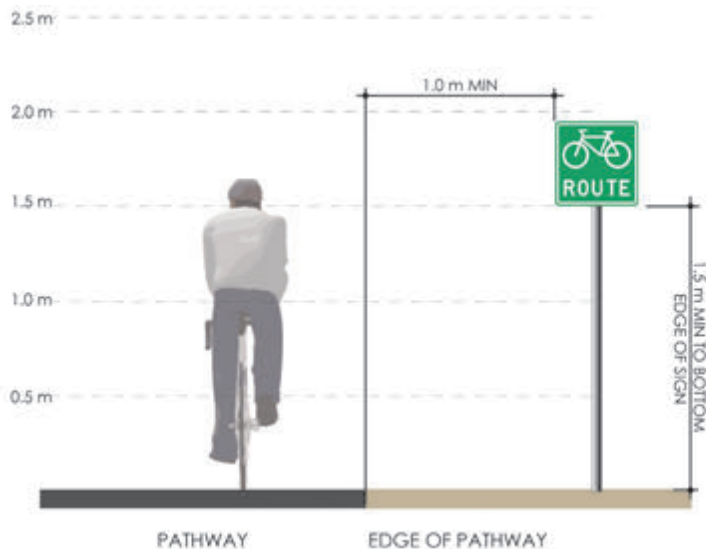


Figure 47: Sign clearances for a bicycle path.

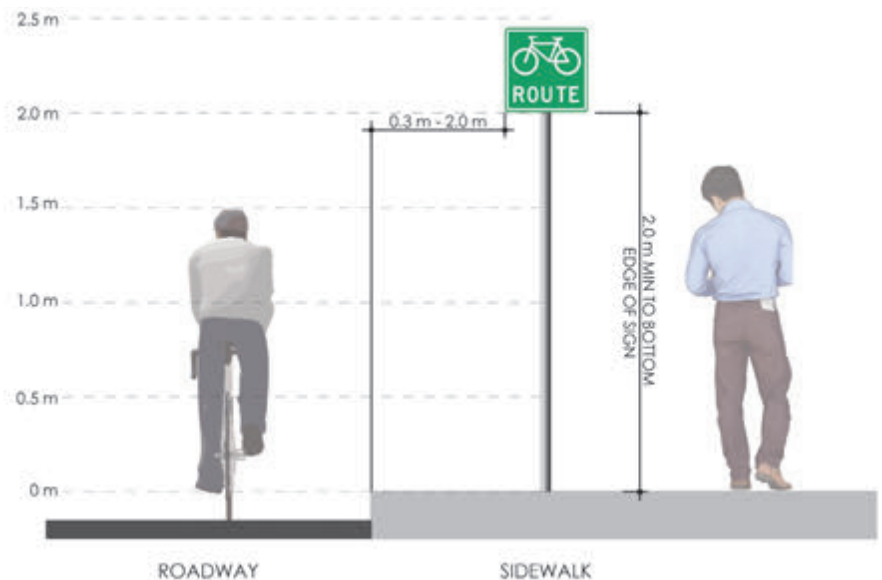


Figure 48: Sign clearances in urban areas with pedestrian traffic.



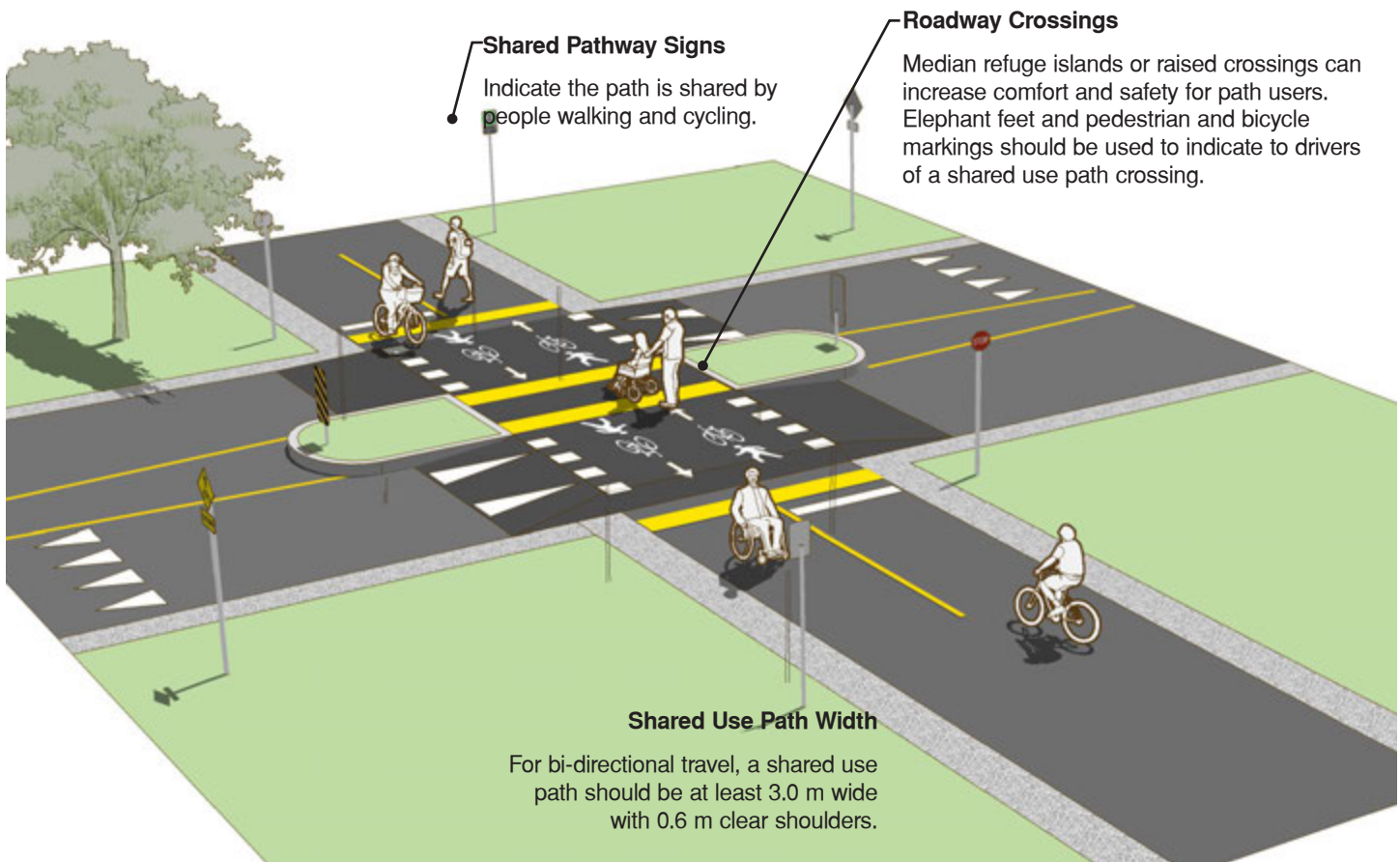


Figure 49: Shared use path at a roadway crossing. Image adapted from the FHWA Small Town and Rural Multimodal Networks Design Guide.

# Shared Use Path

A shared use path, also called a multi-use path, provides a travel area separate from motorized traffic for a low-stress experience for a variety of users including cyclists, pedestrians, skaters, joggers, people that use mobility aids, and others.

## APPLICATION CRITERIA

- Posted speed is >40 km/hr.
- Volumes of >4000 veh/day.
- Areas with heavy truck volumes.
- Areas with high pedestrian or bicycle volumes.

## BENEFITS

- Provides a dedicated facility for users of all ages and abilities.
- May be used for transportation or recreation.
- Paths can have a distinctly rural character.
- Can connect neighbourhood, community, and regional parks through urban and rural areas.

## CONSIDERATIONS

- Operational concerns between users.
- Winter maintenance.
- Requires independent ROW, or wide roadway ROW when configured adjacent to a roadway.
- Added design considerations for driveways and side streets when configured adjacent to a roadway.

## FACILITY DESIGN

### PATH WIDTH

The width of a shared use path will vary depending on if the path is intended for local or regional use, and the amount of user volumes.

- 3.0 m width is recommended in most situations for bi-directional travel and moderate trail use.
- 2.4 m is absolute minimum for two-way bicycle travel. Considering shared use paths would also be used by pedestrians walking abreast, 2.7 m is a more practical minimum. Narrow paths would only be recommended for low trail volumes or for short lengths.
- The Trails in Alberta Highway Rights-of-Way Policies, Guidelines, and Standards recommends wider 3.4 - 4.3 m trails in areas with a high percentage of pedestrians (30% or more), a high usage by users requiring additional operating width such as inline skaters, or where there is high user volumes >300 users at peak times.
- Wider paths are also useful to accommodate maintenance vehicles; on steep grades to allow for comfortable passing and meeting; and through curves to provide more operating space and clear sightlines.

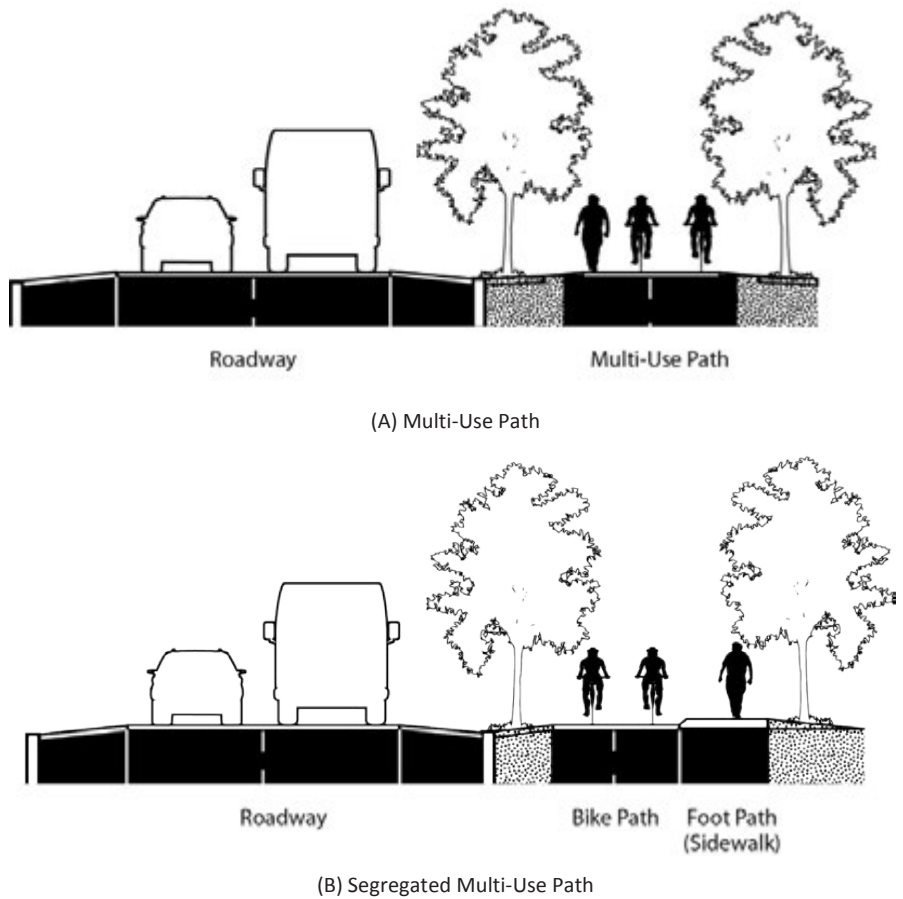


Figure 50: Multi-use paths configured as (A) shared use path and (B) modally separated paths. Image TAC Geometric Design Guide for Canadian Roads Figure 5.3.5 Multi-Use Paths.

### USER CONFIGURATION

- In areas with particularly high pedestrian volumes, it may be prudent to modally separate the path users. This would operate similar to a protected bike lane with adjacent pedestrian sidewalk. See Figure 50.
- TAC recommends modal separation for paths with 20% or more pedestrians with user volumes >33 persons/hours/metre, or regardless of pedestrian percentages, paths with total user volumes >50 persons/hour/metre.

### SHOULDER WIDTH

- 0.6 m minimum shoulders should be provided on each side of the path, kept clear of vertical elements or obstructions.
- A 1.0 m minimum shoulder is recommended in Rocky View County Parks and Pathways Planning, Development, and Operational Guidelines.
- In areas with significant side slopes, the shoulder may need to be increased to 1.5 m wide or include a vertical barrier. Refer to Table 10 and figures on the following page.

Width (m)	Practical Lower Limit	Recommended Lower Limit	Recommended Upper Limit	Practical Upper Limit
Shared multi-use path	2.7	3.0	6.0	6.0
Bike path, bi-directional	2.4	3.0	3.6	4.0
Pedestrian path <400 ped/15 min	1.5	1.8	2.0	Roadside width
Pedestrian path >400 ped/15 min	2.0	2.25 - 3.0 or based on crowd capacity and maneuvering space		Roadside width

Table 9: Width of shared multi-use paths and modally separated paths adapted from TAC Geometric Design Guide for Canadian Roads Table 5.3.5 Design Domain: Width of Multi-Use Paths, Table 5.3.4 Design Domain: Width of Bike Path, and Table 6.3.1 Design Domain: Pedestrian Through Zone.

**SIDE SLOPES**

Where shared use paths are located in areas with significant side slopes, safety rails to prevent path users from falling into a slope may be needed if there is not an adequate setback or recovery distance from the paved edge of the path to the slope.

- Ideally paths are set back 1.5 m from the top of the slope. Shoulders can be included in the 1.5 m measurement.
- For locations without a 1.5 m setback from slopes, guardrails should be provided in the conditions outlined in Table 10.
- Safety rails should have a minimum vertical height of 1.05 m with a preferred vertical height of 1.2 m.
- As safety rails may impact the horizontal operating width of a bicycle handlebars, safety rails should be set back a minimum of 0.5 m from the paved edge of the path.

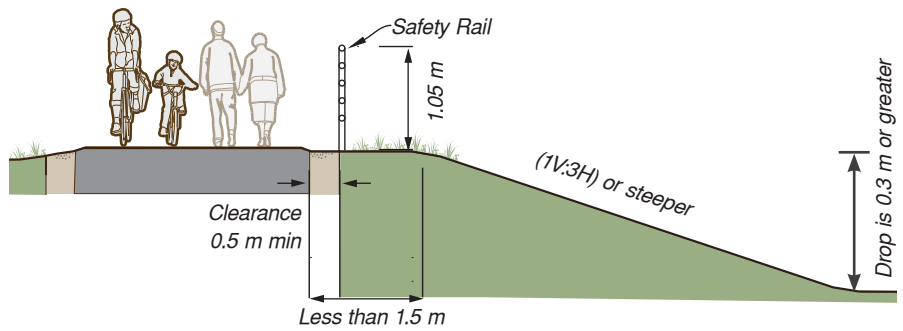


Figure 51: Shared use path clearance dimensions adjacent to significant side slopes. Safety rails should be provided per side slope conditions listed in table below. Image adapted from FHWA Small Town and Rural Multimodal Networks Design Guide.

Side Slope	Vertical Drop
1:1 or steeper	≥ 0.3m
2:1 or steeper	≥ 1.2m
3:1 or steeper	≥ 1.8m or adjacent to water bodies or other hazards

Table 10: side slopes requiring safety rails where 1.5 m path setbacks are not feasible. Dimensions from Trails in Alberta Highway Rights-of-Way Policies, Guidelines, and Standards.

**CLEAR ZONE**

In rural areas or along highway ROW, shared use paths should be located outside of the roadway clear zone. Refer to the Geometric Design Guide Table 7.3.1 and/or Alberta Transportation's Highway Geometric Design Guide Table C.5.2a for clear zone distances.

- For low volume roadways with <1000 veh/day, clear zones may not be practical or necessary.
- In locations where the only possible location for the path is within the clear zone, a physical barrier should be provided to protect the path from errant vehicles. The barrier will not impact the usable width of the path.
- Paths within the clear zone are ideally no closer to than 2 m from the edge of the roadway shoulder. In very constrained conditions such as retrofitting shared use paths onto existing bridges, this distance may be reduced to 1.5 m with a rigid barrier.
- For paths located within a highway clear zone, warning signs may be added to the roadway to alert motorists of the nearby path.
- Refer to Alberta Transportation Roadside Design Guide for further information on acceptable barrier types.

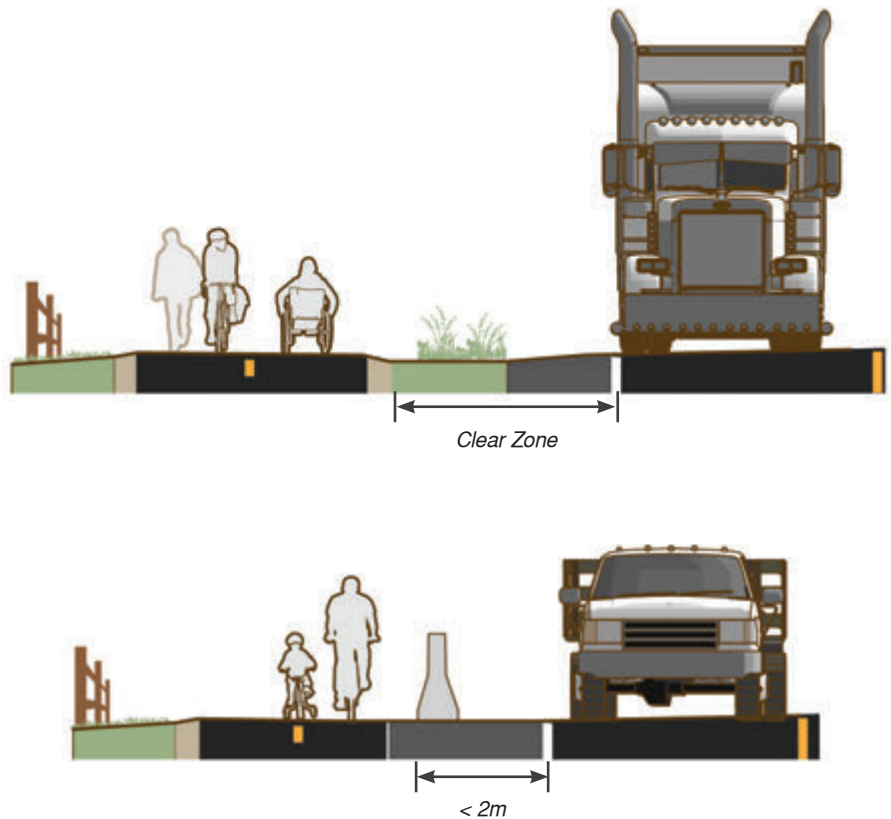


Figure 52: Adjacent to highways, shared use paths are ideally located outside of the vehicle clear zone. Where path locations are constrained, a path may be placed within the vehicle clear zone if a physical barrier is provided between the path and the roadway. Image from the FHWA Small Town and Rural Multimodal Networks Design Guide.

## LANDSCAPING

Trees and landscaping can improve the experience of using a path, especially as a buffer between a path and a roadway. Plantings also help to absorb stormwater runoff from the path or adjacent roadway.

- Provide a 1.0 m horizontal clearance between trees and the path to minimize pavement cracking and heaving of the paved surface.
- When trees are desired within the roadway separation area, consider planting small caliper trees with a maximum diameter of 100 mm to alleviate concerns about fixed objects or visual obstructions between the roadway and the path.
- Paths must have a 3.0 m vertical clearance clear from overhanging branches.
- Refer to Rocky View County Parks and Pathways Planning, Development, and Operational Guidelines for plant selections along paths. Consult a local arborist in the selection and placement of trees.

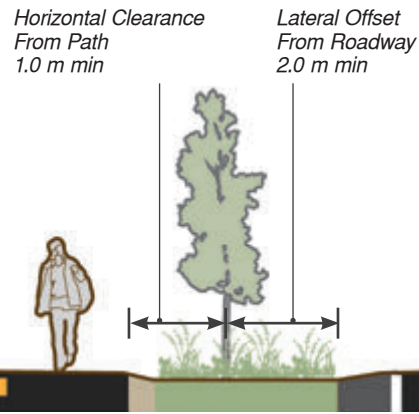


Figure 53: Even small trees can provide an additional feeling of separation between a path and a roadway. Image from FHWA Small Town and Rural Multimodal Networks Design Guide.

## PATH SURFACE

- Materials appropriate for a shared use path surface are asphalt and concrete. Asphalt has a lower construction cost with an estimated life span of 10-15 years. Concrete can last up to 25 years but has a much higher construction cost.
- Install paths with a proper foundation of a geotextile fabric and base course to increase the longevity of the path surface and prevent cracks and potholes. Refer to Rocky View County Servicing Standards Figure 400.27 for standard pathway construction details.
- Depending on soil characteristics of the subgrade, the thicknesses of the base course and surfacing may need to increase. Refer to Trails in Alberta Highway Rights-of-Way Policies, Guidelines, and Standards Table 4.12.

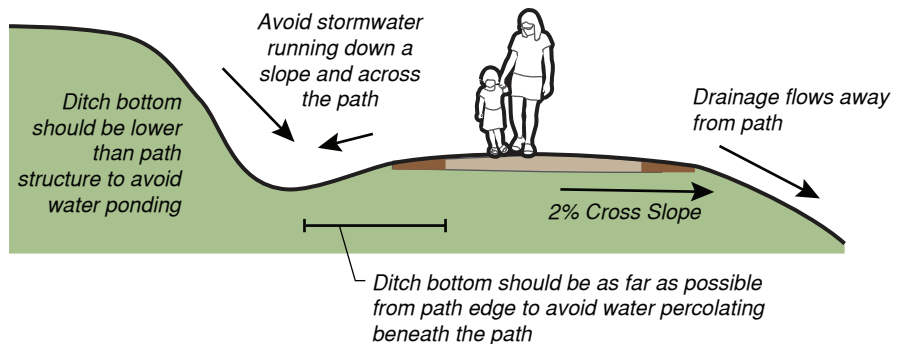


Figure 54: Water should drain away from the trail. Refer to Figure 4.13 Trail Drainage Characteristics in Trails in Alberta Highway Rights-of-Way Policies, Guidelines, and Standards for greater detail.

## PATH DRAINAGE

Paths need adequate drainage to avoid ponding, or in the winter, ice patches.

- Paths should have a 2% cross slope from the crown of the path outward in both directions for positive drainage.
- A cross slope should be no greater than 5% for accessibility, and should only be for very limited distances.
- Path shoulders should also have a cross slope of 2%, a maximum cross slope of 6:1.
- Ditches, culverts, and swales may be needed to divert run off onto and from the trail.

**BOLLARDS**

Bollards are physical barriers intended to restrict motor vehicle access to shared use paths. The routine use of bollards and other similar barriers to restrict motor vehicle traffic is not recommended (AASHTO Bike Guide p. 5-46). Bollards are often ineffective at preventing undesired motor vehicle access to shared use paths, and create obstacles to legitimate path users.

- Although Rocky View County Parks and Pathways Planning, Development, and Operational Guidelines recommend bollards or gates to control path access points, these types of barriers may pose a safety hazard or operational obstacle for path users.
- Alternative design strategies use signage, landscaping and curb cut design to reduce the likelihood of motor vehicle access.
- Landscape medians can be configured to allow emergency vehicle access by straddling the median.



Figure 55: At path access points, the path can be configured as two 1.5 m (min) paths separated by a landscaped median. This example is in Detroit, MI, USA.

**PAVEMENT MARKINGS**

Although a centreline is included for regional pathways in Rocky View County servicing standards, under most conditions, centreline markings are not necessary, and path users will naturally keep right except to pass.

On shared use paths with heavy peak hour and/or seasonal volumes, the use of a centre line stripe may help organize pathway traffic.

- When striping is required, use a 100 mm broken yellow centreline stripe.
- Solid centrelines can be provided to discourage user passing on tight or blind corners, and on approaches to roadway crossings.
- On paths expecting evening use, path edges can be marked with 100 mm solid white lines.

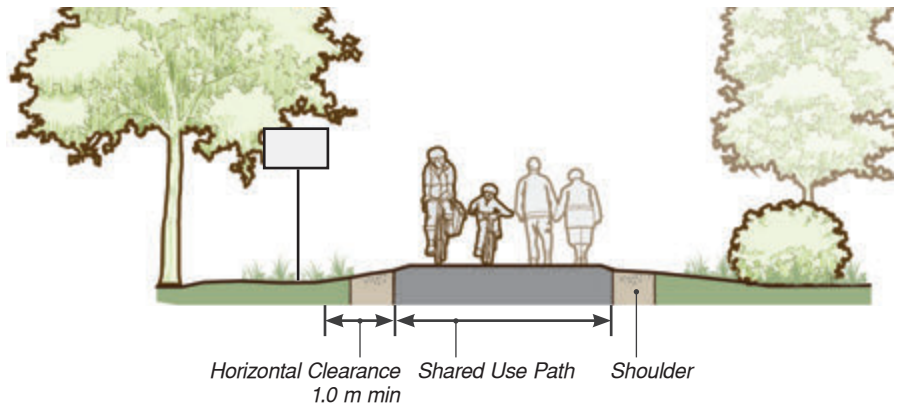


Figure 56: Shared use path horizontal clearances. Image from FHWA Small Town and Rural Multimodal Networks Design Guide pg 4-5.

**SIGNS**

- Shared Pathway (RB-93) signs may be used at the entrances of path segments.
- Signs should be laterally placed so that the near edge of the sign is located 1.0 m away from the paved edge of a path per TAC Bikeway Traffic Control Guidelines for Canada.



Figure 57: Shared Pathway Sign RB-93. Image from TAC Bikeway Traffic Control Guidelines for Canada.

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APPENDIX B

# Background Document Review

HR

# Tech Memo 01: Background Document Review

Rocky View County

Active Transportation Plan – South County

October 26, 2017





## Table of Contents

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# 1 Purpose

The purpose of this document is to provide a summary of relevant policies, plans and other strategic documents in and around Rocky View County related to the development of the Active Transportation Plan – South County. The review will help the project team develop a better understanding of the strategic planning context, the direction for the County as a whole, as well as the direction for specific areas in the County's southwest (Elbow River Ranch Lands) and southeast (Bow River Plains) regions.

# 2 Document Types

The review includes a mix of Rocky View County plans, policies, guidelines, standards, and studies. The documents have been grouped by their coverage – some plans extents are County-wide, while others are specific to the Elbow Valley Ranch Lands (County southwest) or Bow River Plains (County southeast) regions. The review also includes documents from adjacent municipalities, such as the City of Calgary or the City of Chestermere. These documents have been included since they identify areas of intermunicipal collaboration, mutual active transportation benefit, and key linkages to/from Rocky View County.

# 3 Document Summary

Reviewed plans are summarized by applicability to the entire County (County-wide), or are grouped into their southwest (Elbow River Ranch Lands) or southeast region (Bow River Plains). Adjacent municipality plans are included in a separate section.

Document Name	Page #	Area			Use		Direction		
		Urban/Suburban	Exurban	Commercial/ Industrial	Recreational	Mobility/Access	Policy	Network	Guidelines
<b>County-Wide</b>									
County Plan (2013)	6	✓	✓	✓	✓	✓	✓	-	-
2015-2018 Strategic Plan: The Road Forward	7	✓	✓	✓	✓	✓	✓	-	-
Calgary / Rocky View County Intermunicipal Development Plan (2012)	7	✓	✓	✓	✓	✓	✓	-	-
Parks and Open Space Master Plan (2013)	8	✓	✓	✓	✓	✓	✓	✓	-
Calgary / Rocky View County Intermunicipal Pathway and Trail Study (2014)	10	✓	✓	✓	✓	✓	-	✓	✓
Recreation and Culture Master Plan (2014 Draft)	14	✓	✓	-	✓	✓	-	-	-
Parks and Pathways Planning, Development and Operational Guidelines (2013)	14	✓	✓	✓	-	-	-	-	✓
Commercial, Office, and Industrial Design Guidelines (2010)	14	-	-	✓	-	-	-	-	✓
Agricultural Boundary Design Guidelines (2014)	14	✓	✓	✓	-	-	-	-	✓
Servicing Standards (2013)	14	✓	✓	✓	-	-	-	-	✓
Policy 304: Roadway Links in New Subdivisions	15	✓	✓	✓	-	✓	✓	-	-



Document Name	Page #	Area			Use		Direction		
		Urban/Suburban	Exurban	Commercial/ Industrial	Recreational	Mobility/Access	Policy	Network	Guidelines
Policy 318: Pathway and Trails	15	✓	✓	✓	-	✓	✓	-	-
Policy 319: Inspection and Maintenance of Pathway and Trails within County Lands	15	✓	✓	✓	-	-	✓	-	-
Policy 320: Inspection and Maintenance of County Lands	15	✓	✓	✓	-	-	✓	-	-
<b>Elbow River Ranch Lands Region (Southwest)</b>									
Cochrane / Rocky View County Intermunicipal Development Plan (2013)	16	✓	✓	✓	✓	✓	✓	-	-
Springbank North Area Structure Plan (1999)	16	✓	✓	✓	-	-	-	-	-
Springbank Central Area Structure Plan (2001)	16	✓	✓	✓	-	-	✓	-	-
Springbank Master Drainage Plan (2016)	16	✓	✓	✓	-	-	-	-	✓
Harmony Conceptual Scheme (2007) / Harmony Stage 1 Neighbourhood Plan (2008) & Harmony Stage 2 and 3 Neighbourhood Plans (2017)	16	✓	-	✓	✓	✓	✓	✓	✓
North Springbank Gate Conceptual Scheme (2014)	18	-	-	✓	✓	✓	✓	✓	-
Lariat Loop Conceptual Scheme (2005)	18	-	✓	-	✓	✓	✓	-	-
Bingham Crossing Conceptual Scheme (2012) / Bingham Crossing Master Site Development Plan (2013)	18	✓	-	-	✓	✓	✓	✓	-
Morgans Rise Conceptual Scheme (2005)	18	-	✓	-	✓	✓	✓	✓	-
Robinson Road Conceptual Scheme (2007)	18	-	✓	-	-	-	-	-	-
Partridge View Conceptual Scheme (20)	18	-	✓	-	✓	✓	✓	✓	-
North Escarpment Drive Conceptual Scheme (2017)	18	-	✓	-	-	-	-	-	-
Grand View Estates Conceptual Plan (2005)	18	-	✓	-	✓	✓	✓	✓	-
Montebello Conceptual Scheme (2005)	19	-	✓	-	✓	✓	✓	✓	-
Wilson Conceptual Scheme (2007)	19	-	✓	-	-	-	-	-	-
Proposed Highway 8 Regional Pathway Report (2012)	19	✓	✓	-	✓	✓	-	✓	✓
Elbow Valley Area Structure Plan (1997)	19	✓	✓	-	✓	✓	✓	✓	-
Stonepine Conceptual Plan (2001)	20	-	✓	-	✓	✓	✓	✓	-
Elbow Valley West Conceptual Scheme (2004)	20	✓	✓	-	✓	✓	✓	✓	-
Greater Bragg Creek Area Structure Plan (2007)	20	✓	✓	✓	✓	✓	✓	✓	✓
Bragg Creek Revitalization Plan (2015)	21	✓	✓	✓	✓	✓	✓	✓	✓
Bragg Creek Design Standards	22	✓	✓	✓	✓	✓	-	✓	✓
Moose Mountain Trails Conceptual Scheme (2014)	22	-	✓	-	-	-	-	-	-
Wintergreen Forest Estates Conceptual Scheme (2015)	22	-	✓	-	✓	✓	✓	✓	-
Wintergreen Redevelopment Conceptual Scheme (2017)	23	✓	✓	✓	✓	✓	✓	✓	✓
<b>Bow River Plains Region (Southeast)</b>									
Chestermere / Rocky View County Intermunicipal Development Plan (2001)	24	✓	✓	✓	✓	✓	✓	-	-
Conrich Area Structure Plan (2015)	24	✓	✓	✓	✓	✓	✓	✓	-
Shepard Area Structure Plan (2001)	24	✓	✓	✓	✓	✓	✓	✓	-
Janet Area Structure Plan (2014)	25	-	✓	✓	✓	✓	✓	✓	-
CN Logistics Park Master Site Development Plan (2011)	25	-	-	✓	✓	-	✓	✓	✓
Emcor Business Park Conceptual Scheme (2013)	25	-	-	✓	✓	-	✓	✓	-
Frontier Industrial Park Phase I Conceptual Scheme (2007)	25	-	-	✓	✓	-	✓	✓	-
Frontier Industrial Park Phase II Conceptual Scheme (2009)	26	-	-	✓	✓	-	✓	✓	-
Kleysen Transport Conceptual Scheme (2001)	26	-	-	✓	-	-	-	-	-
Boychuk Industrial Conceptual Scheme (2004)	26	-	-	✓	✓	-	✓	✓	-



Document Name	Page #	Area			Use		Direction		
		Urban/Suburban	Exurban	Commercial/ Industrial	Recreational	Mobility/Access	Policy	Network	Guidelines
Conrich Station Conceptual Scheme (2014)	26	-	-	✓	✓	-	✓	✓	-
South Conrich Conceptual Scheme (2006)	26	✓	✓	✓	✓	✓	✓	✓	-
Patton Conceptual Scheme (2001)	26	-	-	✓	-	-	-	-	-
Buffalo Hills Conceptual Scheme (2006)	26	✓	✓	✓	✓	✓	✓	✓	✓
Meadowlands Country Estates Conceptual Scheme (2007)	26	-	✓	-	✓	✓	✓	✓	✓
Northglen Estates Conceptual Scheme (1999)	26	-	✓	-	-	-	-	-	-
Transport Industrial Park Conceptual Scheme (2003)	27	-	-	✓	✓	-	✓	✓	✓
Wesview Industrial (2008)	27	-	-	✓	-	-	-	-	-
Langdon Area Structure Plan (2016)	27	✓	-	✓	✓	✓	✓	✓	-
Boulder Creek Conceptual Scheme (2006)	28	✓	-	-	✓	✓	✓	✓	✓
Mornington Conceptual Scheme (2011)	28	✓	-	-	✓	✓	✓	✓	-
Bridges of Langdon Conceptual Scheme (2016)	28	✓	-	✓	✓	✓	✓	✓	✓
Langdon Crossing West Conceptual Scheme (2005)	28	✓	-	-	✓	✓	✓	✓	✓
Langdon East Conceptual Scheme (2005)	28	✓	-	✓	✓	✓	✓	✓	✓
Langdon Meadows Conceptual Scheme (2006)	28	✓	-	✓	✓	✓	✓	✓	-
Langdon Station Conceptual Scheme (2002)	28	✓	-	-	✓	✓	✓	✓	-
Leland Business Park Conceptual Scheme (2006)	28	-	-	✓	✓	✓	✓	✓	-
Indus Area Structure Plan (2016)	29	-	✓	✓	✓	✓	✓	-	-
Fulton Industrial Conceptual Scheme (2010)	29	-	-	✓	✓	✓	✓	-	-
Greenleaf Acres Conceptual Scheme (2006)	29	-	✓	-	-	-	-	-	-
<b>Adjacent Municipalities</b>									
Calgary Transportation Plan (2009)	30	✓	-	✓	✓	✓	✓	✓	-
Calgary Pathway and Bikeway Plan & Implementation Map (2000)	30	✓	-	✓	✓	✓	✓	✓	✓
Chestermere Transportation Master Plan (2017)	31	✓	✓	✓	✓	✓	✓	✓	✓



## 4 County-Wide Documents

Document Name	Direction
<p><b>County Plan (2013)</b></p> <p>To direct growth and development and plan for supporting infrastructure and services.</p>	<p>Nodes:</p> <ul style="list-style-type: none"> <li>The plan identifies regional business centres (North Springbank, Balzac East, Conrich, and Janet/Shepard).</li> <li>Highway business areas (Highway 1 at Highway 22, Highway 1 in Springbank, Peigan Trail Extension, Highway 791 at Highway 22X) and hamlet business centres (Langdon).</li> </ul> <p>Hamlets and Public Spaces:</p> <ul style="list-style-type: none"> <li>9.10 Support hamlets in providing (a) an attractive community and distinct identity and (b) a high quality built environment.</li> <li>9.13 Well-designed public gathering places in hamlets that: (a) are pedestrian and cyclist-friendly, safe, accessible and attractive; (d) address the needs to residents of all ages and abilities; and (e) are connected by pathways and sidewalks.</li> </ul> <p>Pathways and Trails:</p> <ul style="list-style-type: none"> <li>12.1 Collaboration with adjacent municipalities and stakeholders</li> <li>12.3 Promoting and providing pathway connectivity to hamlets, towns, and adjacent municipalities.</li> <li>12.4 Collaborate with adjacent municipalities on the development of the Trans Canada Trail.</li> <li>12.6 Acquire land for pathways and trails.</li> <li>12.7 Develop and apply design principles and guidelines to provide for high quality pathways, trails, and associated amenities.</li> <li>12.8 Integration and connections to escarpments, creek valleys, river valleys, and water bodies through the development and use of parks, open spaces, pathways and trails.</li> <li>12.9 Connect residential communities, institutional, commercial, and industrial areas by pathways and trails where feasible.</li> <li>12.10 Planning and design of pathways and trails in the agricultural area shall address the safety and protection of agricultural operations.</li> <li>12.11 Apply design standards to help ensure consistent development of parks, pathways and trail systems.</li> <li>12.12 Finance a system of parks, open space, pathways and trails through County and non-County funding measures, using the Parks and Open Space Master plan as guiding policy document in determining strategies and priorities.</li> <li>When approving new and redeveloped parks, open space, pathways, and trails, undertake a cost feasibility and life cycle analysis in accordance with County Policy.</li> </ul> <p>Transportation:</p> <ul style="list-style-type: none"> <li>16.1 Collaboration with the Provincial government and neighbouring municipalities to protect and improve regional transportation corridors where necessary.</li> <li>16.2 Partner and collaborate with adjacent municipalities and developers to coordinate transportation improvements and the expansion of transportation infrastructure.</li> </ul>



Document Name	Direction
	<ul style="list-style-type: none"> <li>• 16.6 The County's long-term (10 year) and current capital plan for road construction, and the medium (5 year) and current operational plan for road maintenance shall guide road development and maintenance.</li> <li>• 16.8 Roads, pathways and trails shall connect adjacent neighbourhoods within hamlets and country residential areas.</li> <li>• 16.10 Support and encourage alternative forms of transportation in hamlets and regional business centres. Opportunities should provide for: a) pathways, trails and sidewalk connections; b) cyclists; and c) public/private transportation.</li> <li>• 16.11 Support and encourage the development of pathways and trails for recreational use in country residential areas.</li> <li>• 16.12 Support the long-term development of the conceptual regional pathway and trail plan as identified in the Parks and Open Space Master Plan.</li> </ul>
<p><b>2015-18 Strategic Plan: The Road Forward</b></p> <p>To guide projects and develop performance measures.</p>	<p><b>Strategic Pillar: Sustainable Communities</b></p> <p>Guiding sustainability principle: encourage the creation of well-designed parks, open spaces, and trails to support the protection and enjoyment of the natural landscape. Related key objectives:</p> <ul style="list-style-type: none"> <li>• Develop and implement an Area Structure Plan review policy for keeping plans up to date and relevant (2016).</li> <li>• Implement and measure the effectiveness of the County Plan (ongoing).</li> <li>• Carry out the actions laid out in master plans, such as those for agriculture and recreation (ongoing).</li> </ul> <p>Sustainability Performance Indicator: total kilometres of pathways and trails.</p> <ul style="list-style-type: none"> <li>• Rationale: As outlined in the County Plan, new developments should incorporate pathways in their construction plans to help achieve the County's recreation goals. This measure helps assess if pathways and trails are created in concert with new developments.</li> </ul> <p><b>Strategic Pillar: Service Excellence</b></p> <p>Guiding service principles:</p> <ul style="list-style-type: none"> <li>• Invest and maintain a well-planned infrastructure network that is connected, safe and functional</li> <li>• Strive for partnerships with neighbouring municipalities to develop shared service and cost agreements to extend the range of Count facilities and services.</li> </ul> <p>Related key objectives:</p> <ul style="list-style-type: none"> <li>• Design facilities that enhance resident interaction with the County (2015).</li> <li>• Ensure the County's long-term road development and maintenance plans address infrastructure longevity, user needs, connectivity to our neighbours, and a methodology to service both commercial and residential districts (2018).</li> </ul>
<p><b>Calgary / Rocky View County Intermunicipal Development Plan (2012)</b></p> <p>To plan and coordinate future growth and development</p>	<p>Focus and Growth Areas:</p> <ul style="list-style-type: none"> <li>• Six areas are of particular interest to both municipalities: Section 29-24-2-5 (south of Highway 1 and west of the Transportation Utility Corridor), Highway 1 West, Highway 1 East, Peigan Trail Extension, and Highway 560/Glenmore Trail East.</li> <li>• Major residential growth areas: Highway 8 west, Springbank, and Conrich.</li> <li>• Major industrial growth areas: Balzac and Shepard/Janet.</li> </ul> <p>Connectivity:</p>



Document Name	Direction
<p>efforts around shared borders.</p>	<ul style="list-style-type: none"> <li>• Policy 9.1.1.b states that municipalities should “work together on an intermunicipal pathway inventory to identify opportunities for cross-boundary connections”</li> <li>• Policy 13.1.1 states that municipalities should coordinate the planning of major transportation links for all modes of transportation.</li> </ul>
<p><b>Parks and Open Space Master Plan (2013)</b></p> <p>To guide the development of an interconnected parks and open space system in the County.</p>	<ul style="list-style-type: none"> <li>• Policy 4.6.10 recommends collaboration with adjacent municipalities to develop signage, share best practices, and encourage the seamless connectivity of parks, open space, and pathways/trails.</li> <li>• Policy 4.6.11 encourages utility companies to allow public access, via agreement with the County, on their lands where feasible, and integrate parks, open space, and pathway/trail development to easements and right-of-ways for roads and utilities (i.e. stormwater management and irrigation canals).</li> <li>• Policy 5.1.1. Integrate pathway/trail development within escarpments, creek valleys, river valleys, irrigation canals, lakes, wetlands, and reservoirs.</li> <li>• Policy 5.1.3 Update current standards for pathway/trail development that include pathway/trail width, materials, construction methods, and associated pathway/trail amenities such as signage, benches, garbage bins, and bicycle leaning posts.</li> <li>• Policy 5.1.4 Develop pathways/trails within undeveloped road allowances and investigate a means of incorporating pathways/trails within future road right of ways in developed areas.</li> <li>• Policy 5.1.5 Implement a comprehensive pathway/trail system that connects existing pathways/trails, considers where pathways/trails are needed most, and monitor implementation to determine where additional facilities or amenities are required</li> </ul> <p>The plan illustrates several proposed conceptual pathways and trail alignments in the Elbow River Rand Land and Bow River Plains regions. These are illustrated in Figure 1 and Figure 2.</p> <p>Elbow River Ranch Lands proposed trails and pathways include:</p> <ul style="list-style-type: none"> <li>• Trans Canada Trail along Highway 8 from the City of Calgary to Bragg Creek and north to Cochrane.</li> <li>• Trans Canada Trail along the Bow River.</li> <li>• Conceptual pathways along Springbank Road, Lower Springbank Road, Township Road 250, Highway 31 and Highway 22.</li> </ul> <p>Bow River Plains proposed trails and pathways include:</p> <ul style="list-style-type: none"> <li>• Conceptual pathways:             <ul style="list-style-type: none"> <li>○ Along Highway 564 from Calgary to Delacour</li> <li>○ From Delacour to Chestermere</li> <li>○ From the City of Calgary to Conrich</li> <li>○ From the City of Calgary to Chestermere</li> <li>○ From Chestermere to Langdon</li> </ul> </li> <li>• Conceptual bicycle route connecting McKinnon Flats to Langdon</li> <li>• Trans Canada Trail along the Bow River</li> </ul>





Figure 1. Elbow River Ranch Lands Region - Proposed Pathways and Trails. Source: Parks and Open Space Master Plan.

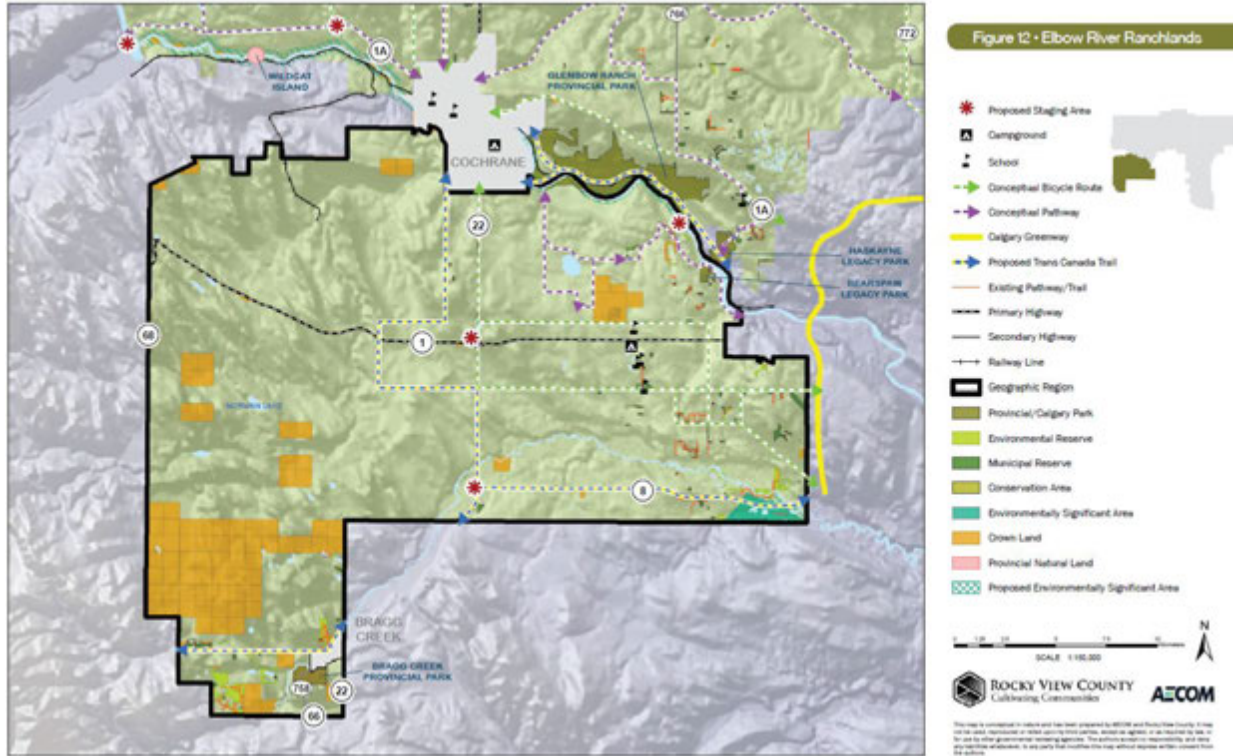
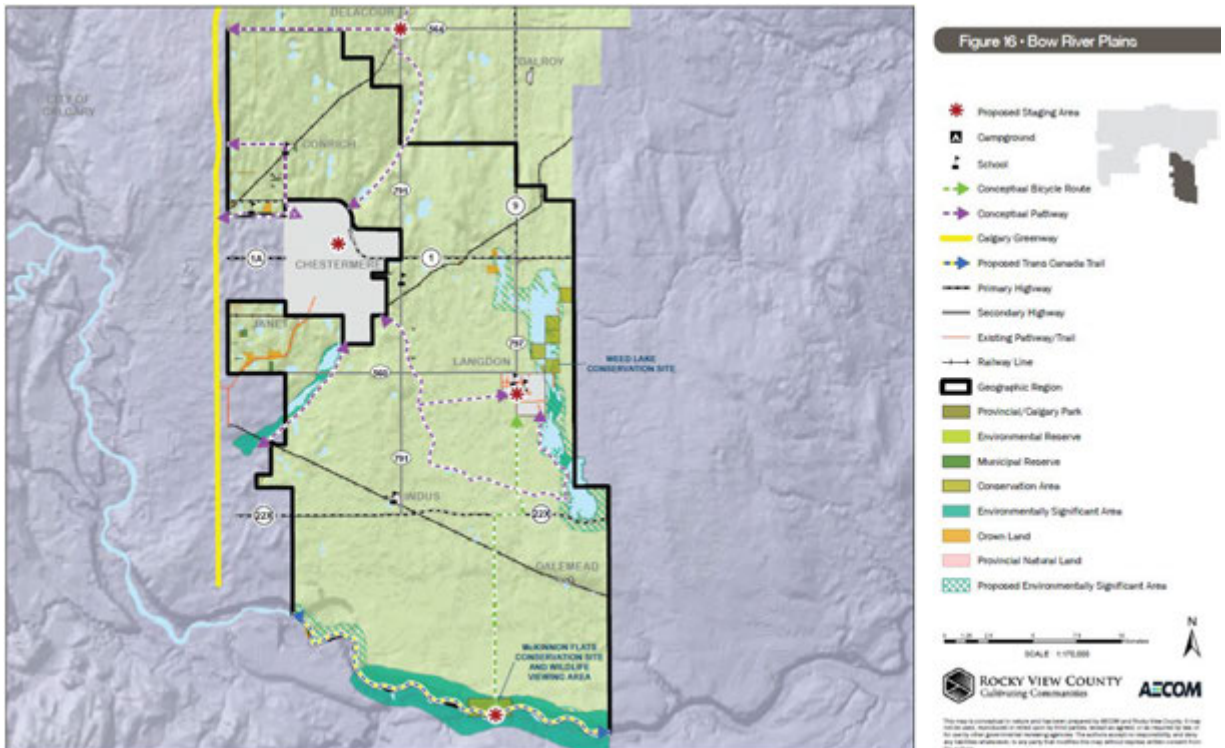


Figure 2. Bow River Plains Region - Proposed Pathways and Trails. Source: Parks and Open Space Master Plan.



Plan Name	Direction
<p><b>Calgary / Rocky View County Intermunicipal Pathway and Trail Study (2014)</b></p> <p><i>To prepare a coordinated guide to pathway and trail development.</i></p>	<p>Critical linkages:</p> <ul style="list-style-type: none"> <li>• Calgary to Bragg Creek along Highway 8 (Trans Canada Trail)</li> <li>• Calgary to Bearspaw Reservoir along Bow River</li> <li>• Calgary to Cochrane along the Bow River through Haskayne Legacy Park and Glenbow Ranch Provincial Park</li> <li>• Calgary to Conrich</li> <li>• Calgary to Chestermere and 17 Avenue SE</li> <li>• Calgary to Chestermere along WID Western Headworks Canal</li> <li>• Calgary Greenway along the Transportation Utility Corridor / Ring Road</li> <li>• Calgary (Ralph Klein Park) an Bow River along the irrigation canal</li> <li>• Connection along the Bow River east from Calgary into Rocky View County.</li> </ul> <p>Identifies and defines five pathway and trail facility types (in order of regional significance):</p> <ul style="list-style-type: none"> <li>• Regional trails,</li> <li>• Local pathways/trails,</li> <li>• Natural trails,</li> <li>• On-street bikeways, and</li> <li>• Sidewalks.</li> </ul> <p>Recommended regional trails, local trails/pathways, natural trails, on-street bikeways, and sidewalks are shown in Figure 3, Figure 4, and Figure 5.</p> <p>The study identifies priorities from short (1-5 years), medium (5-10 years), and long (over 10 years) are also identified.</p> <p>The plan also identifies pathway and trail design standards and maintenance service levels for each of the five facility types.</p>

Figure 3. Proposed Springbank area pathway and trail network. Source: Calgary / Rocky View County Pathway and Trail Study

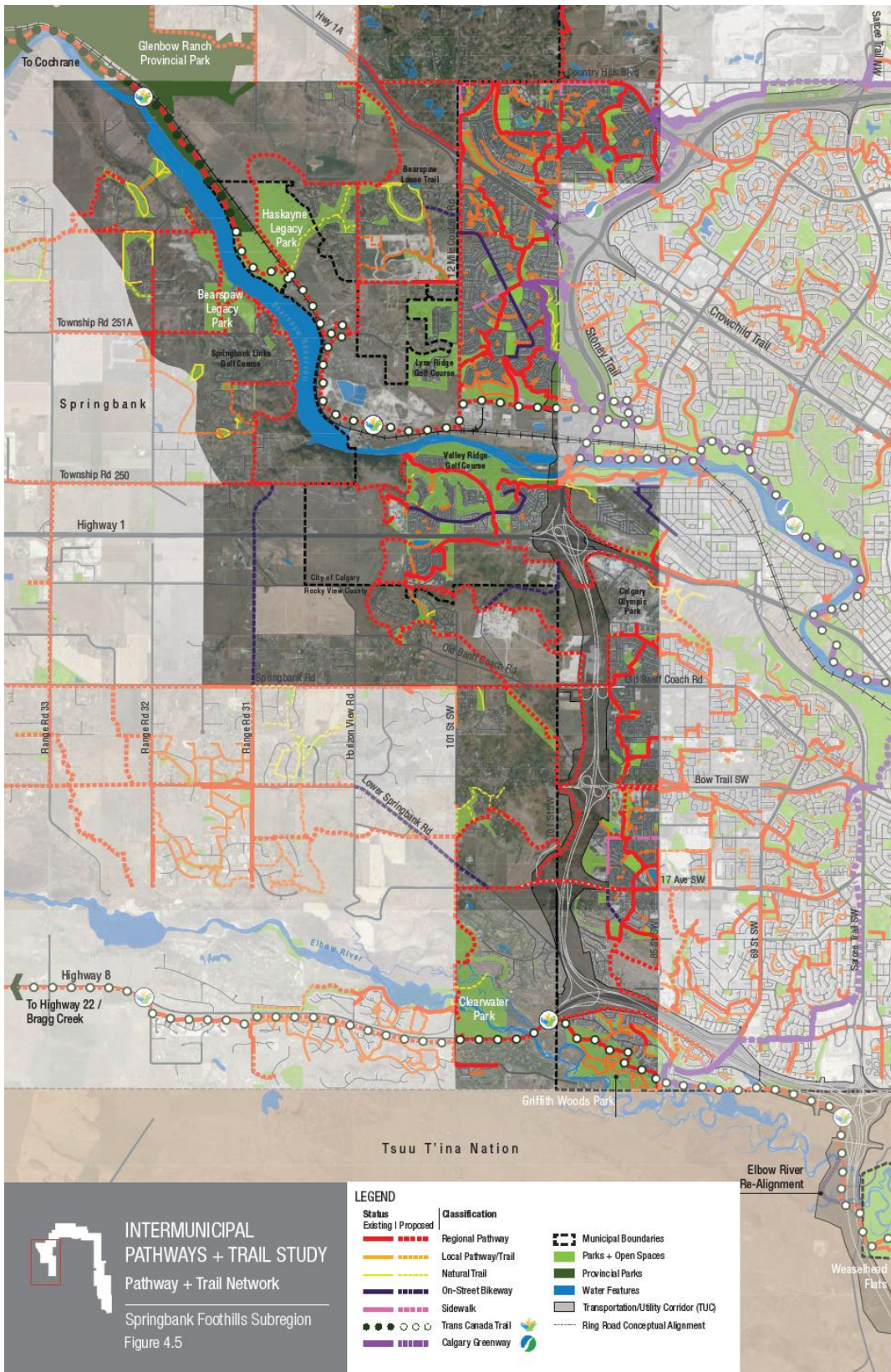




Figure 4. Proposed Calgary to Chestermere area pathway and trail network. Source: Calgary / Rocky View County Pathway and Trail Study

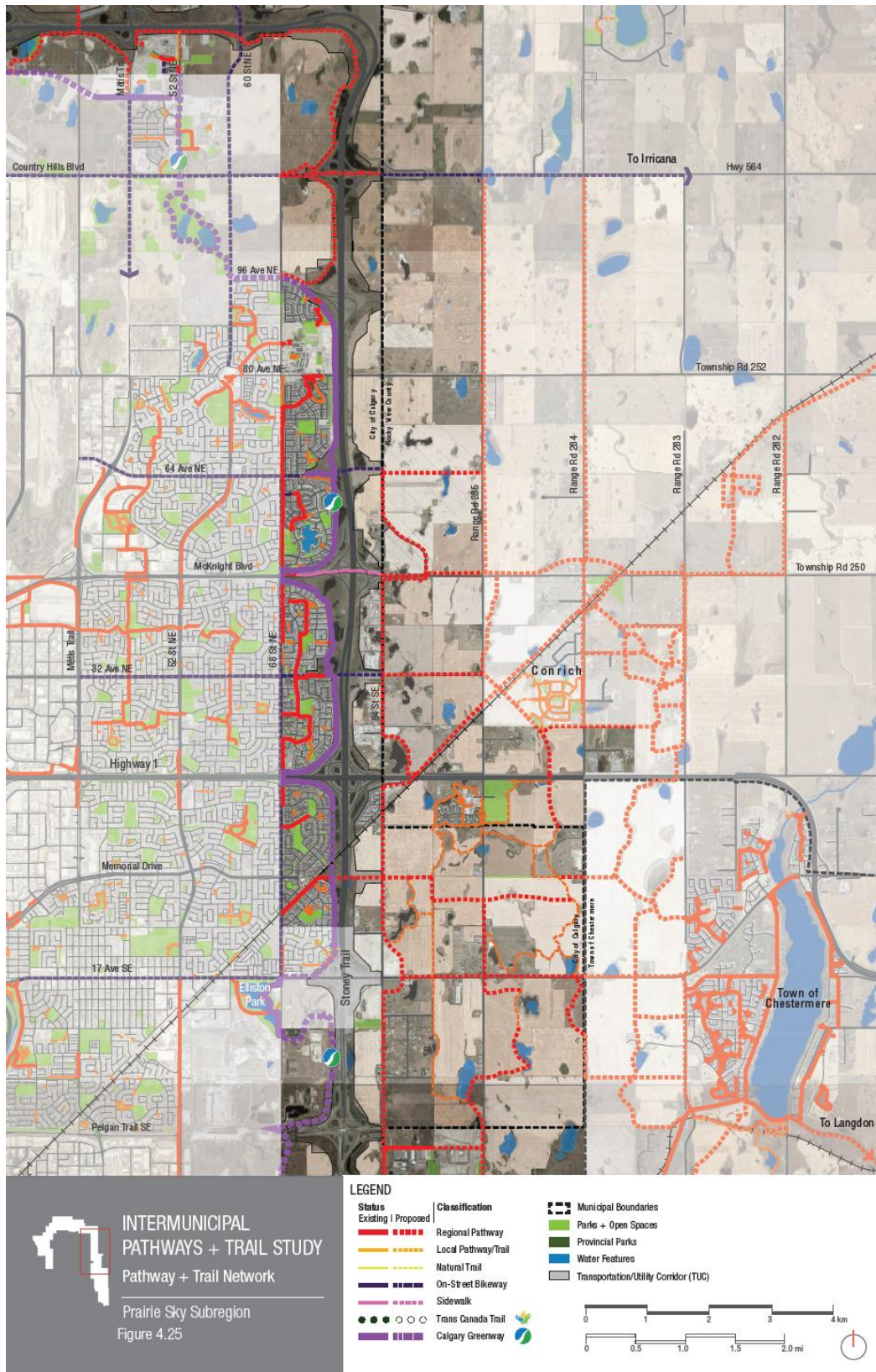
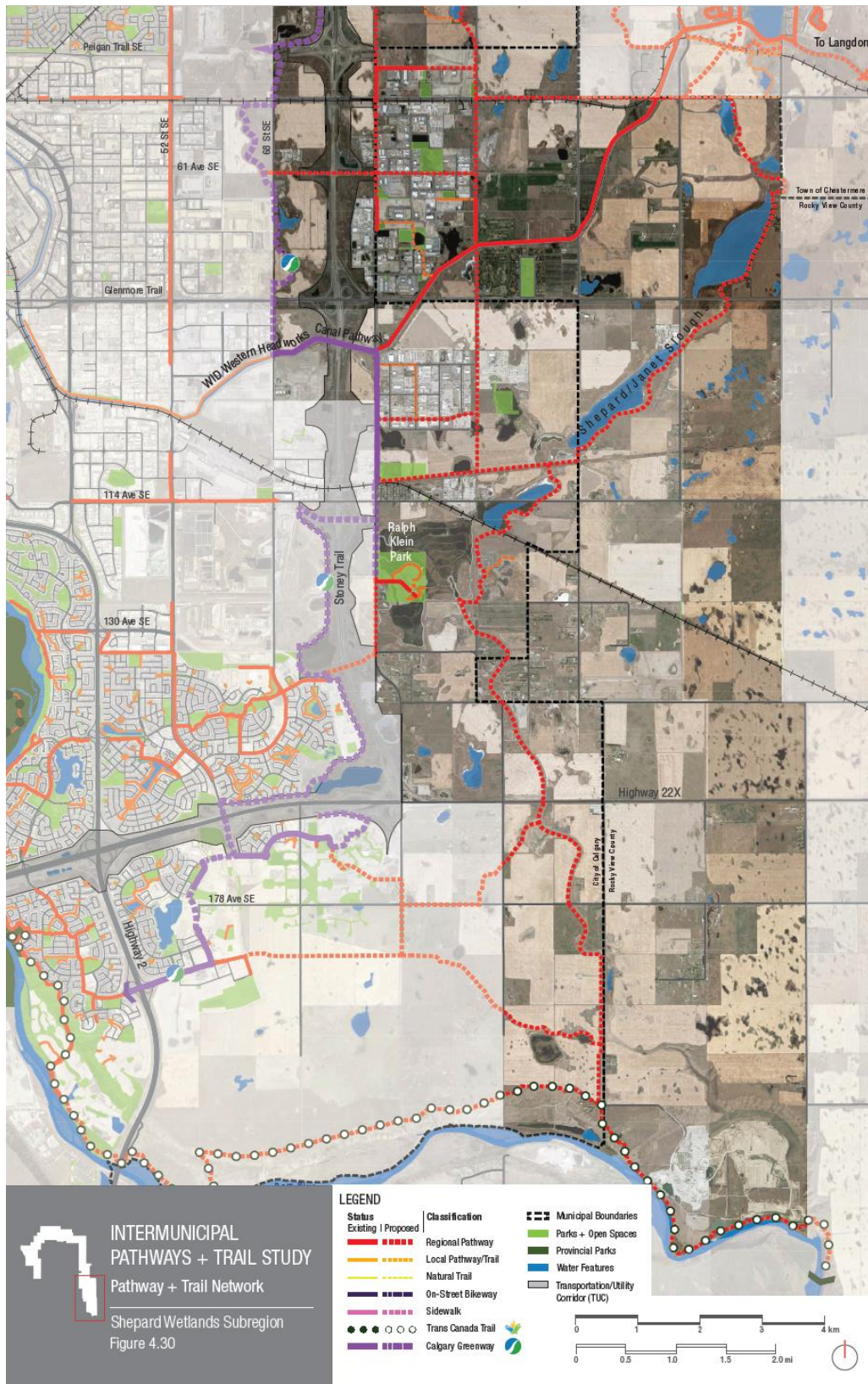




Figure 5. Proposed Shepard area pathway and trail network. Source: Calgary / Rocky View County Pathway and Trail Study



Document Name	Direction and Insight
<p><b>Recreation and Culture Master Plan (2014 Draft)</b></p> <p><i>To guide the provision of recreation and cultural services.</i></p>	<p>Questions and answers from community engagement:</p> <p>Questions: What are the basic recreational and cultural activities most important for health and community building in rural areas? What are the recreational and cultural activities most important for health and community building in the hamlets?</p> <p>Answers:</p> <ul style="list-style-type: none"> <li>• Connected trails, walking, sledding</li> <li>• Hamlet: pathways and green space.</li> <li>• Lacking/needs improvement: if the county has identified a growth node then they need to grow recreational facilities in that location. Not nearby [but] within walking distance. Partnership with corporations and developers.</li> </ul>
<p><b>Parks and Pathways Planning, Development and Operational Guidelines (2013)</b></p> <p><i>To provide direction for the acquisition and development of parks and open spaces; and the continued development and evolution of a regional and local pathway and trail system.</i></p>	<p>Identifies eight facility types:</p> <ul style="list-style-type: none"> <li>• Regional pathway</li> <li>• Local pathway or trail</li> <li>• Natural trail</li> <li>• Bike route</li> <li>• Bike Lane</li> <li>• Wetland Boardwalk</li> <li>• Equestrian trail</li> <li>• Blue way</li> </ul> <p>The plan provides design guidelines and maintenance service levels of each facility type.</p>
<p><b>Commercial, Office and Industrial Design Guidelines (2010)</b></p> <p><i>To provide design guidelines for all proposed mixed-use, commercial, office, and industrial development.</i></p>	<p>5.1 Gateways should:</p> <ul style="list-style-type: none"> <li>• Coordinate roads and pathways with adjacent municipalities.</li> </ul> <p>5.3 Local commercial areas should:</p> <ul style="list-style-type: none"> <li>• Provide improvements to the public realm such as sidewalk or pathways.</li> <li>• Provide well lit and comfortable pedestrian walkways connecting the street to rear parking areas. Share walkways with adjacent sites as much as possible.</li> </ul>
<p><b>Agricultural Boundary Design Guidelines (2014)</b></p> <p><i>To minimize land use conflicts that can occur when agricultural and nonagricultural uses are located next to one another through a design process and set of tools.</i></p>	<p>Site Analysis section asks proponent to identify nearby parks and pathways and future connections to adjacent lands.</p> <p>Within designated municipal reserve: landscaped pathways can provide buffers between agricultural and non-agricultural uses.</p> <p>Definitions:</p> <ul style="list-style-type: none"> <li>• Pathways and trails: Any recognized route that is formally engineered, constructed and hard surfaced.</li> <li>• Municipal reserve: municipally-owned land which may be used by a municipality or school board for any or all of the following purposes: public park, public recreation area, school board purposes, or to separate lands that are used for different purposes.</li> </ul>
<p><b>Servicing Standards (2013)</b></p>	<ul style="list-style-type: none"> <li>• Identifies any studies that may be required to support a development (p6)</li> <li>• Procedures for road right-of-way construction agreements (p12)</li> </ul>

Document Name	Direction and Insight
<i>To guide the design, preparation, and submission of plans and specifications for construction of new infrastructure.</i>	<ul style="list-style-type: none"> <li>• Capital asset management (p12)</li> <li>• Road design guidelines (p47)</li> <li>• Urban design guidelines (p51)</li> <li>• Roadway access and approaches guidelines (p62)</li> <li>• Road classifications, volumes, lanes, surface type, design speed...etc (p67)</li> <li>• Road cross-sections (p69 on)</li> <li>• Low impact development road classifications (p162)</li> <li>• Low impact development road cross-sections (p164)</li> </ul>
<b>Policy 304: Roadway Links in New Subdivisions</b>	To provide a policy which maximizes the efficiency of the County's road networks in order to reduce road maintenance costs, and to improve the safety and accessibility into and through country residential and other subdivided areas of the County
<b>Policy 318: Pathway and Trails</b>	<p>Purpose: to effectively administer public pathways and trails within RVC.</p> <p>Policies:</p> <ul style="list-style-type: none"> <li>• Requires public consultation prior to pathway or trail development by project proponent.</li> <li>• All pathways and trails shall be planned, classified, constructed, maintained, insured and inspected in accordance with County standards and requirements.</li> <li>• Pathway and trails may act as alternate route for maintenance and emergency access when required. In these circumstances, pathways and trails shall be constructed to a standard sufficient for maintenance for emergency service vehicles.</li> <li>• Access for emergency service vehicles shall be provided along regional pathways at regular intervals.</li> <li>• Pathway and trail development or enhancement, where feasible and when safety standards can be met, coincide with land development and infrastructure improvement projects.</li> <li>• Subdivision and development proposals shall address pathway and trail needs and connections.</li> </ul>
<b>Policy 319: Inspection and Maintenance of Pathways and Trails within County Lands</b>	<p>Purpose: to ensure that all pathways and trails within County lands are inspected regularly and adequately maintained.</p> <ul style="list-style-type: none"> <li>• Documentation of scheduled inspections.</li> <li>• Pathways and trails will be assigned a classification, these classifications dictate maintenance service level.</li> <li>• For County lands with an occupant agreement, occupants shall maintain and operate the lands in accordance to this policy.</li> </ul>
<b>Policy 320: Inspection and Maintenance of County Lands</b>	Purpose: to administer a consistent formal process when inspecting, managing, maintaining and operating County owned parks, open spaces and any and all amenities with the exception of pathways and trails located within said lands.



## 5 Elbow River Ranch Lands Documents

Policy Name	Summary of Relevant Areas
Cochrane / Rocky View County Intermunicipal Development Plan (2001)	<p>Policy 2.7.2.2 (c) - Municipalities will endeavor to identify priorities for potential connections between existing natural areas.</p> <p>Policy 2.9.2.5 Efforts will be made to connect trails and pathway systems with the Plan Area.</p>
Springbank North Area Structure Plan (1999)	<p>Apart from the road network, there is no provision for active transportation facilities in the plan.</p>
Springbank Central Area Structure Plan (2001)	<p>Active transportation considerations within the plan are informed by results from a 1999 Central Springbank Household/Landowner Survey where, "Landowners who use the roads for purposes other than driving are uncomfortable. Thought should be given to how improvements could be made to the current road infrastructure to accommodate other uses and/or development of new pathways and trails."</p> <p>The plan reiterates the household survey feedback in noting limited active transportation infrastructure in the area, "presently, pedestrians, cyclists, and equestrians intrepidly use road rights-of-way since few walkways exist to connect destination points with Central Springbank".</p> <ul style="list-style-type: none"> <li>• Section 2.6.5 outlines policies related to walkway development.</li> <li>• Section 2.0.3 outlines several strategies related to land use and pathway development: <ul style="list-style-type: none"> <li>e) Open space and pathway systems will be expanded through future development and the protection of environmentally sensitive areas.</li> <li>f) Connective open space systems including pathways, parks and open areas throughout the community will be established and developed.</li> </ul> </li> </ul>
Springbank Master Drainage Plan (2016)	<p>Recommendations include that all new development should be restricted to building within the 1:100 year floodplain such as the Bow or Elbow Rivers and their local tributaries.</p>
Harmony Conceptual Scheme (2007) / Harmony Stage 1 Neighbourhood Plan (2008) / Harmony Stage 2 and 3 Neighbourhood Plans (2017)	<p>The Conceptual Scheme proposes a connected regional and local trail network:</p> <ul style="list-style-type: none"> <li>• Regional trails are 'commuter type' paved trails that connect to external facilities used for walking, jogging, biking. Located on the periphery.</li> <li>• Local trails will provide connections between residential areas, to key community elements, and connect to regional trail network and are comprised of combination of sidewalks, hard and soft surface trails.</li> <li>• Equestrian trails will be provided along the periphery. Trailheads with parking will be provided.</li> <li>• Subsequent development approval applications will provide components of the trail network as per the principles of this scheme and through further consultation with the Rocky View Trails Association.</li> </ul> <p>Residential:</p> <ul style="list-style-type: none"> <li>• Community design is founded on the principle of locating up to 75% of residential units within the village core or a five minute walk (400m) to the village core edge (and 90% within 800m of the village core).</li> </ul> <p>The Stage 1 Neighbourhood Plan proposes an interconnected mobility network. Section 2.2.5 and Figure 6 details the Trail System, and some design elements are shown in Figure 7:</p>





Figure 6. Trail system for Phase 1.

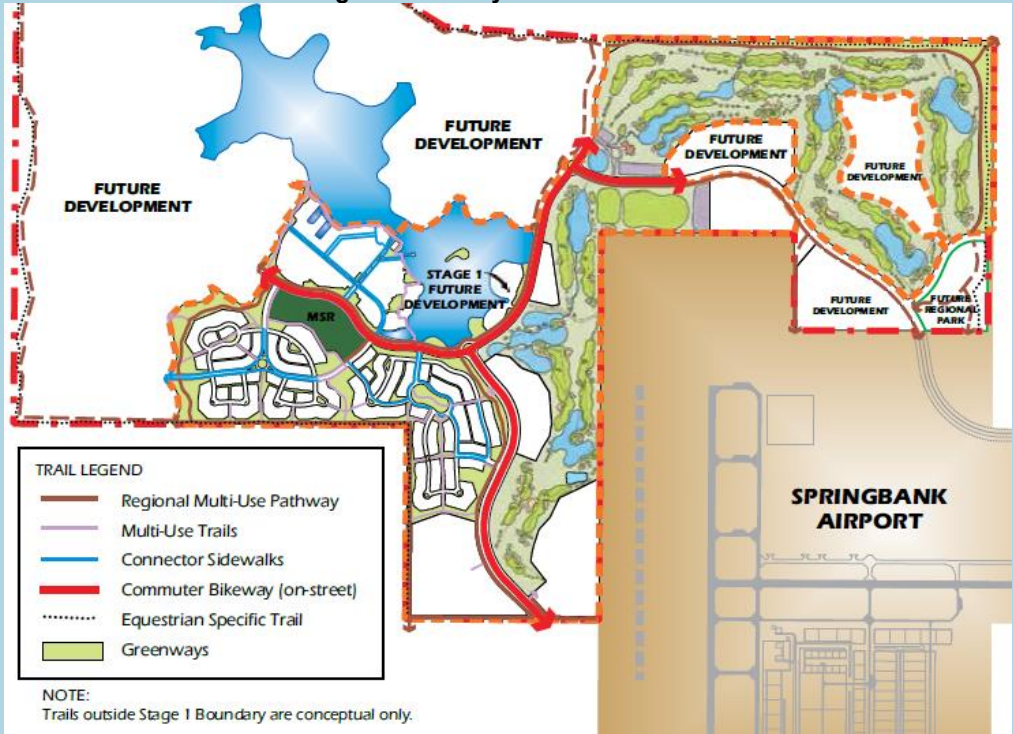
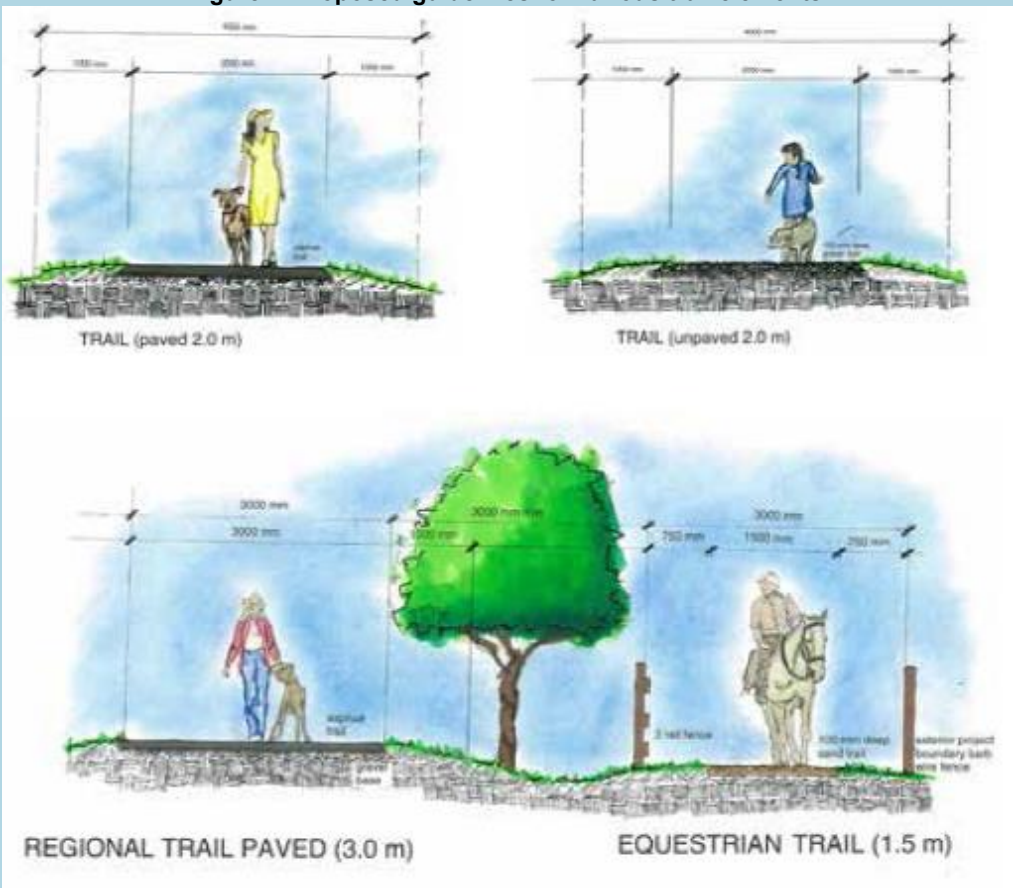
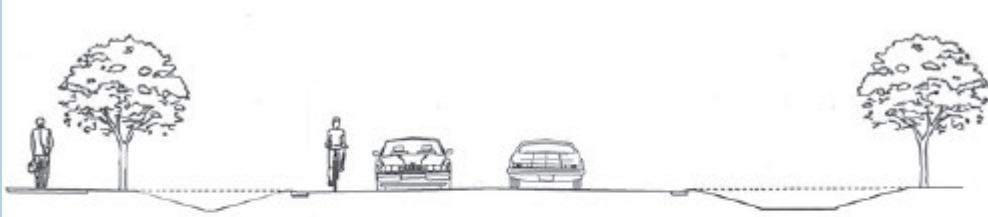




Figure 7. Proposed guidelines for various trail elements.





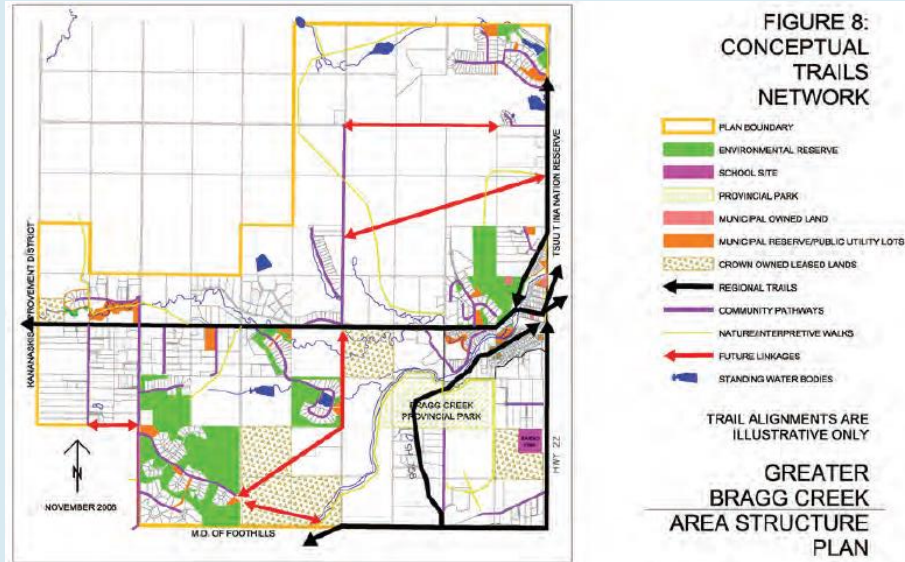
Policy Name	Summary of Relevant Areas
	<p>3.3.8.3 Road Design Standards key components include:</p> <ul style="list-style-type: none"> <li>• Reduced travel speeds;</li> <li>• Road infrastructure scaled to suit the character of the adjacent land uses;</li> <li>• Consideration for pedestrian corridors and alternate forms of transportation such as bicycles;</li> <li>• Vehicle parking requirements.</li> </ul> <div data-bbox="443 415 1425 821" style="text-align: center;">  <p data-bbox="443 709 1425 821" style="border: 2px solid black; padding: 5px; display: inline-block;">Road Standards Schematic</p> </div> <p>Stage 2 and 3 include a regional multi-use pathway with connections to other stages as well as connector sidewalks, and greenways/natural corridors. Stage 3 proposes an equestrian trail along the northern edge of the development.</p>
<p>North Springbank Gate Conceptual Scheme (2014)</p>	<p>Plan includes a conceptual bicycle route along Township Road 250 and local internal pathways, all “in accordance to Rocky View County Servicing Standards”.</p>
<p>Lariat Loop Conceptual Scheme (2005)</p>	<p>N/A - despite mentions of a looped internal pathway and linkages to the regional pathway system there is no pathway system apparent from a desktop review of the area.</p>
<p>Bingham Crossing Conceptual Scheme (2012) / Master Site Development Plan (2013)</p>	<p>The development will be connected to the adjacent areas by a network of regional and local pathways. These regional pathway will be established in the greenbelt that forms the perimeter of the project. The plan also identifies sidewalks.</p>
<p>Morgans Rise Conceptual Scheme (2005)</p>	<p>Concept proposes a regional pathway along the eastern and southern borders and an internal pathways running along local roads and open space in the southern half of the development.</p>
<p>Robinson Road Conceptual Scheme (2007)</p>	<p>Nothing to note – no active transportation links or infrastructure proposed.</p>
<p>Partridge View Conceptual Scheme (2015)</p>	<p>Active transportation provision includes a future 8m Municipal Reserve allocation along Springbank Road to allow for the County’s future pathway system.</p>
<p>North Escarpment Drive Conceptual Scheme (2017)</p>	<p>Nothing to note – no active transportation links or infrastructure proposed.</p>
<p>Grand View Estates</p>	<p>A pathway network is proposed, with a regional north-south connection along Range Road 32.</p>

Policy Name	Summary of Relevant Areas
Conceptual Plan (2005)	
Montebello Conceptual Scheme (2005)	An external pathway runs along the north and east perimeter of the development. An internal pedestrian pathway system is proposed on one side of the length of the internal road network.
Wilson Conceptual Scheme (2007)	Nothing to note – no active transportation links or infrastructure proposed.
<p data-bbox="203 474 404 625"><b>Proposed Highway 8 Regional Pathway Report (2012)</b></p> <p data-bbox="203 653 391 898"><i>To examine the feasibility of aligning a regional multi-use pathway from the City of Calgary to Highway 22.</i></p>	<p data-bbox="427 474 1365 564">The plan recommends a conceptual 5.0 metre bi-directional paved pathway alignment that connects the City of Calgary to Highway 22 along the Highway 8 corridor. An example of cross-section and alignment is provided in Figure 8.</p> <p data-bbox="427 594 865 623">Additional recommendations include:</p> <ul data-bbox="453 625 1401 751" style="list-style-type: none"> <li>• Establishing a Technical Advisory Group with members from RVC, City of Calgary, Alberta Transportation and Alberta TrailNet.</li> <li>• Establishing a committee or advisory group to build consensus and capture interests from a variety of user groups involved in the creation of the pathway.</li> </ul> <p data-bbox="480 779 1386 808"><b>Figure 8. Alignment and cross-section from City limits to Lott Creek Boulevard.</b></p> 
Elbow Valley Area Structure Plan (1997)	A plan for primarily residential development from lands south of the Elbow River and to the west of the Elbow Springs Golf Course. The lands are proposed as primarily estate and single family residential, open space and recreational (golf course). The area is now fully developed. There are no sidewalks, but a series of paved or gravel trails, as shown in Figure 9 <b>Error! Reference source not found.</b>

Policy Name	Summary of Relevant Areas
	<p align="center"><b>Figure 9. Elbow Valley trail system. Source: elbowvalley.org</b></p> 
<p><b>Stonepine Conceptual Plan (2001)</b></p>	<p>A pathway system links all four cul-de-sac neighborhoods with each other and to the open space recreation amenities offered by the Elbow River valley to the north.</p>
<p><b>Elbow Valley West Conceptual Scheme (2004)</b></p>	<p>A regional pathway will be provided along the south side of the service road as an extension of the existing regional pathway traversing the residential community to the east. A second pathway will run along the eastern boundary of the north-south public roadway that bisects the plan area and will provide residents to the south of the proposed development with access to the regional pathway and school site located in the northeast corner of the plan area.</p>
<p><b>Greater Bragg Creek Area Structure Plan (2007)</b></p>	<p>The plan discusses planning for the improvement to provincial and local roads, and discourages on-street parking. Specific to trails, the plan includes a conceptual trail network with: regional trails (typically paved), community pathways (hard and soft surface), natural / interpretive walks (soft surface), and future trail linkages. The concept is shown in Figure 10.</p>

**Policy Name**      **Summary of Relevant Areas**

**Figure 10. Bragg Creek Conceptual Trail Network. Source: Greater Bragg Creek Area Structure Plan**



**Bragg Creek Revitalization Plan (2015)**

Recommendations include:

**Hamlet-Wide:**

- Confirming looped pathway system design, routes and trailhead and embed into County plans and strategic priorities. See Figure 11 for details.

**Hamlet North:**

- Trail and pathway development with looped pathway connection to the Hamlet core.
- Thematic wayfinding.
- Riverfront access w/ lookouts.

**Hamlet Core:**

- Basalm Ave Streetscape Improvements
- Pedestrian pathway connector from Basalm Ave to Town Square
- Community Centre pedestrian link to Community Centre Park
- Trail orientation point

**Heritage Mile:**

- County owned lands have potential to become the 'Heritage Mile Park and Trailhead'
- Enhancing White Avenue to include pathway, lighting, seating, wayfinding

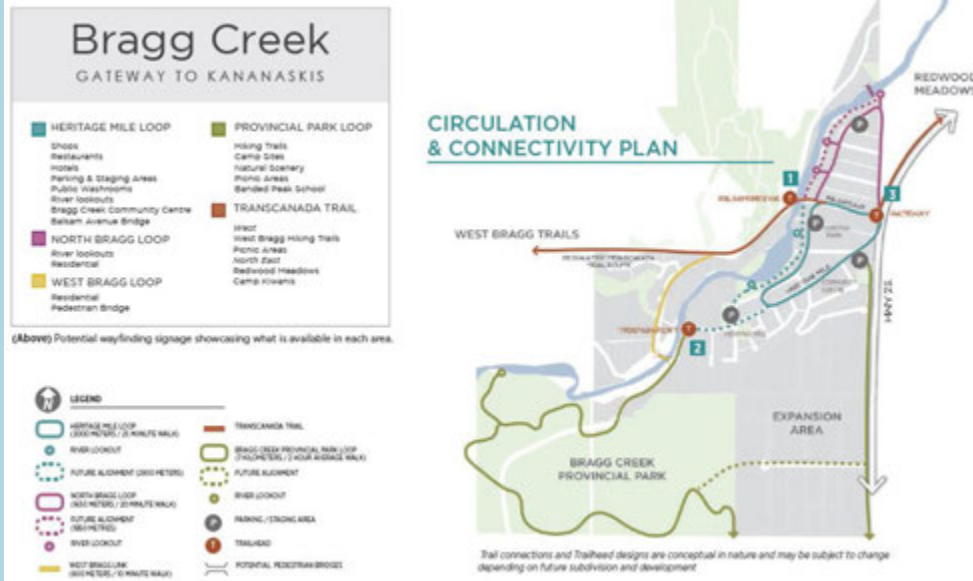
**Hamlet West:**

- Link Hamlet with lands west of Elbow River via a pedestrian bridge
- Improved pathway conditions linking Hamlet West to the Core and ER walking trails.



**Policy Name** Summary of Relevant Areas

**Figure 11. Circulation and Connectivity Plan. Source: Bragg Creek Revitalization Plan**



**Bragg Creek Design Standards (?)**

- 3.2.6: Parking and Site Access - Pedestrian Connection Standards:
- g) A direct and continuous pedestrian walkway network shall be established within and adjacent to parking lots to connect building entrances, parking spaces, public sidewalks, and other pedestrian destinations.
  - h) The pedestrian walkway network shall include upgraded pavement treatments, or pavement markings, that contrast with the drive aisle.
  - i) The walkway network should be integrated with the landscaping for the parking area.
- 4.1 Streetscapes :
- Small curb radii where pedestrian activity high
  - Future White Ave (Highway 758) and Balsam Ave should consider continuous sidewalks and other traffic calming and landscaping features
  - River Drive – pedestrian and vehicular connection between River Dr and River Dr North. Future development should consider extending Rier Dr to the west.
  - Crosswalks should be provided at key pedestrian crossings in hamlet core
- 4.3 Wayfinding and Gateway Features
- 4.4 Civic Nodes and Open Spaces
- Connections between public spaces and surrounding land uses
  - Connections w/ Elbow River
  - Universal accessibility

**Moose Mountain Trails Conceptual Scheme (2014)**

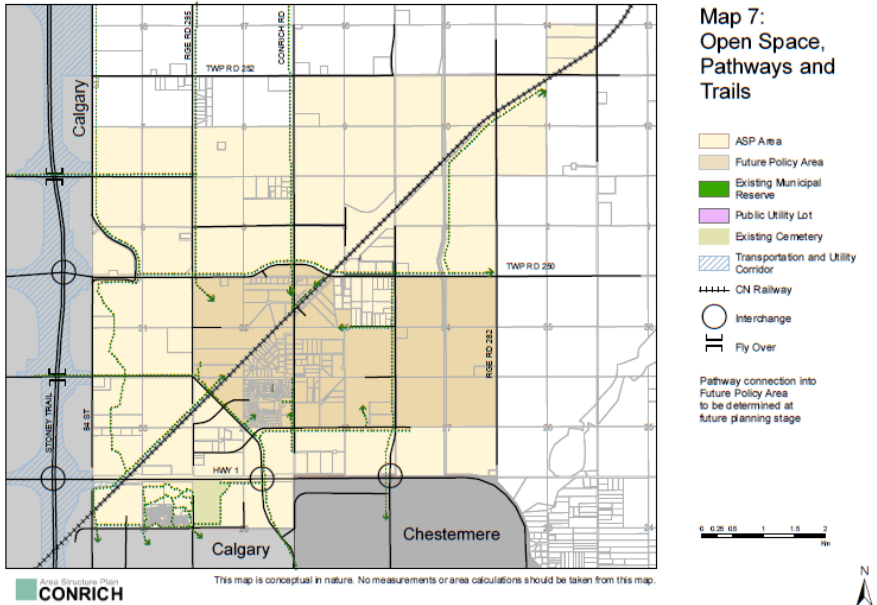
Nothing to note – very small development area with no active transportation links or infrastructure proposed.

**Wintergreen Forest Estates Conceptual Scheme (2015)**

Policy 4.2.1.1A six (6) metre wide linear Municipal Reserve shall be dedicated along the eastern boundary of the Plan Area fronting Wintergreen Road to accommodate a future County pathway.

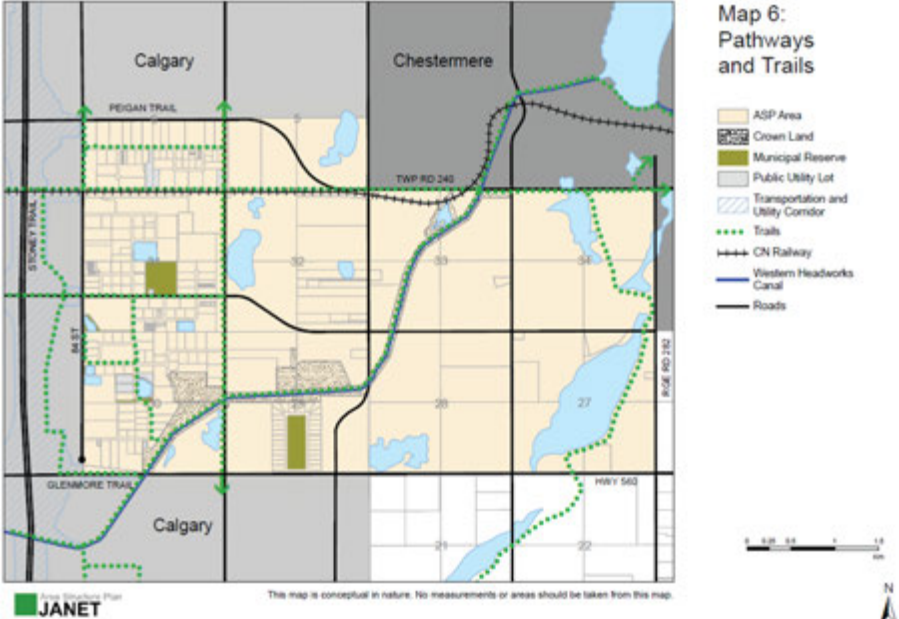


## 6 Bow River Plains Region Documents

Policy Name	Summary of Relevant Areas
(Draft)	<ul style="list-style-type: none"> <li>• Potential growth areas have been identified on the east side of Chestermere.</li> <li>• Policy 2.3.1.2 – municipalities will identify pedestrian and alternative transportation friendly connections between existing and future park sites, and between park sites and commercial and residential development.</li> <li>• Policy 2.3.1.3 – pathway connections will support alternative transportation.</li> <li>• Policy 2.3.1.4 – pathway connections across the municipal boundary will connect residents to commercial and recreational amenities with the adjacent municipality.</li> <li>• Policy 2.4.1.1 – municipalities will develop compatible design standards for roadways, local streets, and pathways.</li> <li>• Policy 2.4.3.2 – municipalities will implement infrastructure that promotes the use and efficient interconnection of alternative transportation within or near to vehicle oriented routes.</li> </ul>
<p><b>Conrich Area Structure Plan (2015)</b></p>	<p>Goals of the plan include creating “a well-designed, safe, and interconnected transportation network that addresses the needs of residents, motorists, pedestrians, and cyclists”. The plan proposes a network of pathways, trails and sidewalks with regional connections. Pathways and trails are shown in .</p> <p>Policy 18.9 calls for the provision of sidewalks within the road right-of-way in hamlets.</p> <p><b>Figure 14. Proposed pathways and trails in Conrich. Source: Conrich Area Structure Plan</b></p> 
<p><b>Shepard Area Structure Plan (2001)</b></p>	<p>The Shepard Area Structure Plan is a joint planning effort by the City of Calgary and Rocky View County. The plan has some duplicate plan coverage with the City of Calgary / Rocky View County Intermunicipal Development Plan and Janet Area Structure Plan. The plan area is bounded by the Stoney Trail Transportation Utility Corridor to the west, Peigan Trail to</p>

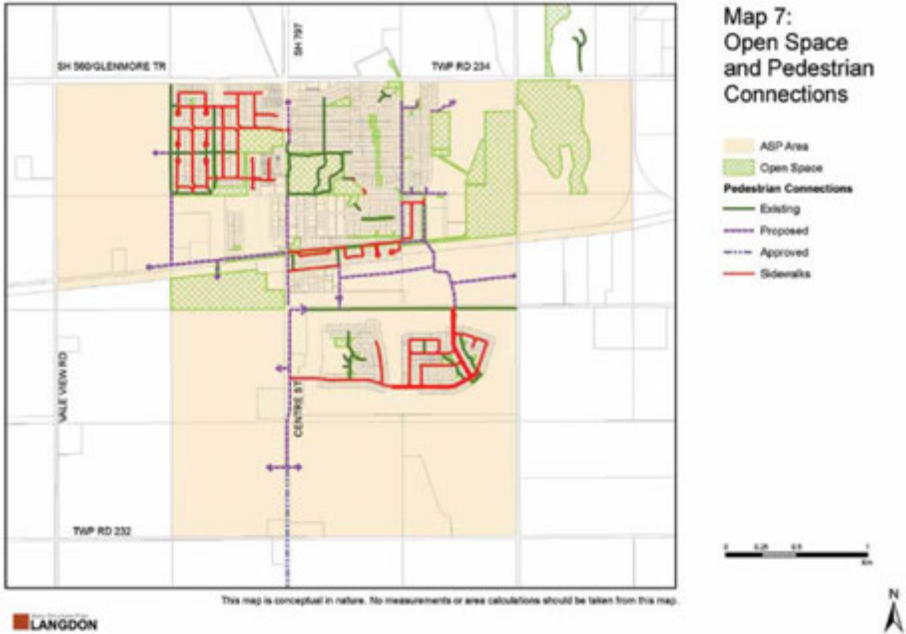




Policy Name	Summary of Relevant Areas
	<p>the north, 100 and 116 Street to the east, and 146 Avenue to the South. This area is intended to be primarily industrial.</p> <p>Active transportation policies include the provision for regional and local trails, including the development of the Western Canal Headworks Pathways (also referred to as the WID Canal Pathway) and linkages to new and existing residential areas, the Hamlet of Shepard, the Shepard Constructed Wetland Project, the City of Calgary and the Town of Chestermere.</p>
<p>Janet Area Structure Plan (2014)</p>	<p>The plan calls for regional and local sidewalk, pathway, and trail connections to promote cycling and walking. A conceptual network with shown in Figure 15.</p> <p><b>Figure 15. Janet pathway and trail network. Source: Janet Area Structure Plan</b></p> 
<p>CN Logistics Park Master Site Development Plan (2011)</p>	<p>The plan states that the developer will construct a regional pathway within the MR along the entire southern width of the MSDP area to provide a pedestrian connection between the Conrich Community and the MSDP area, and potentially with the adjoining properties situated north of Twp Rd 250. The pathway is expected to include a paved surface 2.5 m wide.</p>
<p>Emcor Business Park Conceptual Scheme (2013)</p>	<p>Policy 4.3.9: The developer shall explore the provision of a regional pathway on either the west or east side of Garden Road as a condition of the first subdivision application.</p>
<p>Frontier Industrial Park Phase I Conceptual Scheme (2007)</p>	<p>Policy 4.5.2: A potential future regional pathway shall be accommodated in one of two locations that will ultimately provide a connection to the regional pathway that parallels the Western Irrigation Canal:</p> <ol style="list-style-type: none"> <li>1. The 3.0m regional path right-of-way shall be accommodated along the north part of the CN Rail R.O.W.; or</li> <li>2. The pathway shall be accommodated in a 3m easement within the 6m setback along the north side of the proposed east/west internal road connecting 84 Street to Range Road 285 (Garden Road), and within a 3m easement within the 6 m setback along the east side of 84 Street connecting the pathway to the anticipated future pathway from the City of Calgary along 50 Avenue.</li> </ol>



Policy Name	Summary of Relevant Areas
	4.5.3: A Lot Owners Association shall be incorporated to manage and maintain the pathway system in accordance with the requirement of the MD of Rocky View.
Frontier Industrial Park Phase II Conceptual Scheme (2009)	<p>Policy 4.5.2: A potential future regional pathway will ultimately provide a connection to the regional pathway that parallels the Western Irrigation District canal. The 2.5m pathway shall be accommodated in a 6m easement within the 6m building setback along the north side of the proposed east/west internal road (Frontier Road SE) connecting 84th Street SE to Range Rd. 285 (Garden Road).</p> <p>Policy 4.5.3: A Lot Owners Association shall be incorporated to manage and maintain the pathway system in accordance with the requirements of the MD of Rocky View.</p>
Kleysen Transport Conceptual Scheme (2001)	No active transportation links or infrastructure proposed for this site.
Boychuk Industrial Conceptual Scheme (2004)	The pathway connection running north/south along 84 Street connects to the park area in the SW corner of the adjacent site and the connection running east/west along the SE corner of the site may eventually tie into the Headwaters Canal pathway system.
Conrich Station Conceptual Scheme (2014)	A regional pathway has been identified within the plan area as per the Conrich Area Structure Plan. No provision for pedestrian network due to industrial nature of the development.
South Conrich Conceptual Scheme (2006)	<p>A shale pathway system running through Cell A, is to be registered as Municipal Reserve (MR)</p> <p>The proposed pathways in Cell C are to be protected by registration as Municipal Reserve or as public sidewalks, ensuring that all public lands will be accessible not only to residents of Cell C, but also to all residents of the County. Furthermore, the pathway is designed to connect to adjacent properties, which will facilitate recreational opportunities and pedestrian linkages to surrounding lands. Public walkways will be constructed to the satisfaction of the County. Maintenance of open spaces within Cell C should be the responsibility of a Homeowners Association or Associations to be established at the time of subdivision registration.</p>
Patton Conceptual Scheme (2001)	No specific active transportation linkages or infrastructure included in the plan.
Buffalo Hills Conceptual Scheme (2006)	Includes a 2.0m paved pathway system along municipal reserve connecting open parks, schools, and stormwater ponds. Collector streets include a 1.0m sidewalk. Local streets include a 1.5m sidewalk on one side of the street.
Meadowlands Country Estates Conceptual Scheme (2007)	This open space network will incorporate a ± 13.4 km (± 8.0 mile) dual trail and pathway system. This system will run behind each acreage lot in the residential area, through a ± 15.2 m (± 50 ft) wide setback area on each side of the WID irrigation canal, through a ± 7.6 m (± 25 ft) wide setback area around storm management ponds, and around the perimeter of the plan area. It will cross the WID irrigation canal at three locations and serve to connect all parts of the community. The portion of the system running around the perimeter will be landscaped, if approved by the Municipality, to provide for additional buffering from adjacent lands. A homeowners association will maintain the MR land.
Northglen Estates Conceptual Scheme (1999)	No active transportation linkages or infrastructure included in the plan.

Policy Name	Summary of Relevant Areas
<p><b>Transport Industrial Park Conceptual Scheme (2003)</b></p>	<p>The subdivision design includes ±0.03 acres of linear open space that will be dedicated as municipal reserve. A linear open space will extend eastward from the end of the internal road to the eastern boundary of the plan area. This linear open space will measure 3.0 m in width and will provide a connection between the plan area and the Western Headworks Canal pathway that eventually links to the regional linear park system of the City of Calgary. The linear open space will be constructed by the developer and maintained by a lot owners association.</p>
<p><b>Wesview Industrial (2008)</b></p>	<p>No active transportation linkages or infrastructure included in the plan.</p>
<p><b>Langdon Area Structure Plan (2016)</b></p>	<p>The plan area includes the expansion of the full-service hamlet of Langdon at the intersection of Highway 560 and Highway 797. Specific active transportation goals include:</p> <ul style="list-style-type: none"> <li>• Increasing safety on Centre Street by directing heavy truck traffic to Vale View Road in order to focus Centre Street as a core for the hamlet where it is safe for residents to walk, bike, and drive to local shops and services.</li> <li>• Encouraging a safe and well-connected network of streets, sidewalks, and multi-use trails that connect to key locations throughout the community.</li> <li>• Encouraging safety on trails and pathways through lighting and clear signage.</li> </ul> <p>Part of the future plan vision for Centre Street is to provide a focal point for the community, Specific actions related to that part of the plan vision includes consideration of funding and design strategy for sidewalks and lighting along Centre Street. The plan vision also includes a network of sidewalks and pathways through the community. Actions related to this vision note that local plans should provide for pathway, trail and sidewalk network that aligns with the concept proposed in Figure 16.</p> <p><b>Figure 16. Pedestrian Network Concept. Source: Langdon Area Structure Plan.</b></p>  <p>This map is conceptual in nature. No measurements or area calculations should be taken from this map.</p> <p><b>LANGDON</b></p>



Policy Name	Summary of Relevant Areas
Boulder Creek Conceptual Scheme (2006)	The hamlet internal roads will have a 1.0m monowalk with rolled curb and lighting along both sides. The hamlet collector (connecting to Centre Street) will have a 1.5m monowalk on either side with rolled curb and lighting. A paved pathway will be included along northern boundary.
Mornington Conceptual Scheme (2011)	“Green infrastructure” defines the community’s eastern edge and accommodates a pedestrian pathway system linking to a developing Langdon community pathway system. With the addition of its supportive internal sidewalk system, Mornington will offer its residents a safe and walkable community with pedestrian and bikeway access to amenities in the greater Langdon community. A central “green plaza” functions as a joint use area with its dry storm water retention pond and surrounding landscaped area that is linked to the internal pathway system.
Bridges of Langdon Conceptual Scheme (2016)	Local pathway/trail system runs north-south along canal and east-west at Boulder Creek Drive and along transmission / pipeline ROW in the centre of the development. Pedestrian network includes the provision of sidewalks.
Langdon Crossing West Conceptual Scheme (2005)	Residential roads include 1.0m rolled curb monowalk on one side. Collector roads to include 1.50m rolled curb monowalk on one side.
Langdon East Conceptual Scheme (2005)	<p>Pathways:</p> <ul style="list-style-type: none"> <li>• 9.0m wide running north-south along western boundary of site;</li> <li>• 15.24m pathway running north-south through the centre linking school, residences and business park;</li> <li>• 5.0m pathway running east-west along southern boundary next to existing drainage ditch; and</li> <li>• 5.0m pathway running east-west from northeast corner to business park.</li> </ul> <p>Sidewalks</p> <ul style="list-style-type: none"> <li>• Residential collectors to include sidewalks on both sides;</li> <li>• Residential local roads to include sidewalks on one side; and</li> <li>• Business collector to include sidewalk on the western side only.</li> </ul>
Langdon Meadows Conceptual Scheme (2006)	<p>Policy 5.5.8 The system of linear parks and walkways may be integrated with stormwater management corridors or works, where appropriate.</p> <p>Policy 5.5.9 Where a pathway system runs parallel within a road right-of-way, that right-of-way may be widened to accommodate a more pleasant pathway experience by meandering it or separating it from vehicular movements.</p> <p>Policy 5.5.10 Pedestrian linkages through an extensive system of sidewalks and park pathways should be encouraged throughout the Planning Area.</p>
Langdon Station Conceptual Scheme (2002)	<p>6.03.5: A pedestrian pathway connection should be provided along the east boundary of the proposed commercial site.</p> <p>6.04.2: The linear open space along Railway Avenue is comprised of both the regional drainage ditch and the dedicated municipal reserves and serves the dual function of a community pathway and the conveyance of storm water.</p> <p>7.04: There are sidewalks along the internal street network.</p>
Leland Business Park Conceptual Scheme (2006)	<p>A pathway is proposed along the edge of the wetland.</p> <p>Numerous pathways have been designed into the concept plan to allow for easy access throughout the development and to areas outside of the</p>



Policy Name	Summary of Relevant Areas
	<p>planning area. While there is no development abutting the development in which to take advantage of a continuous pathway network at this time, the plan does provide for such linkage opportunities should the lands be developed in the future. The Developer will be responsible for the construction of all pathways throughout the plan area. Ongoing maintenance of the pathway system will be the responsibility of the Lot Owners Association that will be established upon subdivision approval.</p>
<p><b>Indus Area Structure Plan (2016)</b></p>	<p>The vision for the Area Structure Plan includes maintaining a primarily hamlet and country residential community with in an agricultural region that is supported by commercial, institutional (school) and recreational amenities. Policy 5.7.a states that sidewalks are not required for any new residential development. Additionally, streetlights are also stated as undesired, but green spaces and pathways are encouraged. Policy 5.5.b states that “paths and trails will be constructed, in conjunction with new development, to link residential areas to the school and recreation centre”.</p>
<p><b>Fulton Industrial Conceptual Scheme (2010)</b></p>	<p>Future regional pathway linkages provided to connect to the Hamlet and the land to the east. Internal public pathways are provided in the MR surrounding the storm water ponds.</p>
<p><b>Greenleaf Acres Conceptual Scheme (2006)</b></p>	<p>No active transportation links or infrastructure proposed.</p>



## 7 Adjacent Municipality Plans

Plan Name	Direction
<p><b>Calgary Transportation Plan (2009)</b></p> <p><i>To provide direction on the development and enhancement of Calgary's multi-modal transportation system.</i></p>	<p>Policies include:</p> <p>(3.2 c) The amount, directness, connectivity, accessibility, comfort, character and safety of pedestrian and bicycle routes should be increased.</p> <p>(3.2 d) The quality of pedestrian and bicycle environs should be emphasized in all transportation studies and in all future development or redevelopment plans for Activity Centres, Corridors, Transit Oriented Development sites and residential communities.</p> <p>(3.2 e) Walking and cycling must be integrated with transit services and improve intermodal opportunities at the community, city and regional scales.</p> <p>(3.2 k) The Transportation Department and Parks Business units must coordinate the design, operation and maintenance of all pathways (including snow clearing) that form part of the Primary Cycling Network to accommodate the needs of both recreational users and commuters.</p> <p>Map - Primary Cycling Network presents conceptual primary bike routes and regional multi-use pathway routes. Routes connecting to the Study Area include (clockwise from Highway 8):</p> <ul style="list-style-type: none"> <li>• Regional multi-use pathway along Highway 8</li> <li>• Primary bike route along Old Banff Coach Rd SW</li> <li>• Regional multi-use pathway on both sides of the Bow River</li> <li>• Primary bike route along Country Hills Blvd. NW</li> <li>• Primary bike route along 17 Avenue SE</li> <li>• Regional multi-use pathway along the irrigation canal to Chestermere</li> <li>• Primary bike route along 130 Avenue SE</li> <li>• Regional multi-use pathway along the Bow River</li> </ul>
<p><b>Calgary Bikeway and Pathway Plan (2000)</b></p> <p><i>To provide guiding principles relating to the planning, design, and management of Calgary's pathway and bikeway network.</i></p>	<p>The City of Calgary is currently updating this plan. Relevant guidelines from the current plan include:</p> <ol style="list-style-type: none"> <li>1. Defining user groups: <ul style="list-style-type: none"> <li>• Pathway users: Regional pathways should be designed and maintained, and retrofitted where necessary, to accommodate multi-use.</li> <li>• Sidewalk users: On roadway bridges, cyclists should be permitted to use the sidewalks where the roadway does not accommodate bikes.</li> </ul> </li> <li>2. Network development: <ul style="list-style-type: none"> <li>• Pathways should be continuous to the greatest extent possible to accommodate recreation, and therefore should avoid on-street linkages.</li> <li>• Bikeways should form a continuous on-street network wherever possible.</li> <li>• For developments with indirect and discontinuous road patterns: <ul style="list-style-type: none"> <li>○ Continuous and direct routes should be provided by a combination of bikeways, pathways and walkways.</li> <li>○ Bikeway, pathway and walkway linkages should be designed, lit and maintained to support safe operation.</li> </ul> </li> </ul> </li> <li>3. Pathways proposed routes – evaluation criteria:</li> </ol>

	<p>Functional criteria:</p> <ul style="list-style-type: none"> <li>• Recreational potential</li> <li>• Connects to other pathways (regional, local)</li> <li>• Connects parks and natural areas to each other</li> <li>• Serves destinations – connect residential areas to schools/college/university, places of employment, shopping, cultural/arts facilities, other residential areas, pathway system or parks.</li> <li>• Provides a pathway/bikeway function, when required</li> </ul> <p>Location criteria:</p> <ul style="list-style-type: none"> <li>• Prefer locating on public property: parks, utility right of ways, etc.</li> <li>• Views, aesthetic values</li> <li>• Vegetation – location of major trees</li> <li>• Can link to streets at regular intervals with safe street crossings</li> </ul> <p>Pathway route should: Be located adjacent to, but not through, environmentally sensitive areas/important habitat:</p> <ul style="list-style-type: none"> <li>• Avoid steep terrain</li> <li>• Avoid location in alleys, driveways, parking lots</li> <li>• Avoid location in boulevard of a major road with frequent intersections and driveways</li> <li>• Avoid creating a need for a pedestrian/cycle overpass or underpass</li> <li>• Avoid mid-block crossings</li> </ul> <p>The Implementation Map identifies linkages into Rocky View County. Clockwise from Highway 8:</p> <ul style="list-style-type: none"> <li>• Preferred TransCanada Trail through the Weaselhead Natural Area</li> <li>• Approved pathways and bicycle lane along 17 Avenue SW</li> <li>• Recommended bicycle lane along Old Banff Coach Road SW</li> <li>• Proposed pathway alignments through Crestview and Valley Ridge</li> <li>• Approved and proposed pathway along the Bow River west</li> <li>• Recommended bicycle lane along Country Hills Boulevard NE (Hwy 564)</li> <li>• Recommended bicycle lane along 84 Avenue NE</li> <li>• Recommended bicycle lane along 32 Avenue NE</li> <li>• Recommended bicycle lane along Memorial Drive NE</li> <li>• Approved pathway along 50 Ave SE</li> <li>• Approved pathway and proposed bicycle lane along 61 Ave SE</li> <li>• Existing pathway along Western Headworks Canal</li> <li>• Approved pathway along 114 Ave SE</li> <li>• Proposed pathway along 130 Ave SE</li> <li>• Proposed pathways connections between Hwy 22X and Bow River</li> </ul>
<p><b>Chestermere Transportation Master Plan (2017)</b></p> <p><i>To provide an integrated approach to transportation planning and multi-modal opportunities to Chestermere.</i></p>	<p>Proposed active transportation linkages from Western Headworks Canal clockwise:</p> <ul style="list-style-type: none"> <li>• A regional pathway connection along Western Headworks Canal</li> <li>• On-street connections to new developments</li> <li>• Utility corridor multi-use pathway</li> <li>• Multi-use pathway along Chestermere Boulevard</li> <li>• Multi-use pathway connection to Windermere Drive</li> <li>• Multi-use pathway connection via future interchange north along Rainbow Road</li> </ul>

- Multi-use pathway connection from Chestermere Boulevard to South of Township Road 243
- Multi-use pathway along Range Road 280
- Multi-use pathway along Merganser Drive East
- On-street connections along Lakeview Rise/Township Rd 240
- On-street connections via new local roads in developments south of Township Rd 240
- Connections along canal
- On-street connections along Range Rd 282



APPENDIX C

# Review of Experience Elsewhere

# Technical Memo #02

Date: Thursday, January 11, 2018

Project: Rocky View County - Active Transportation Plan South County

To: Greg van Soest, Rocky View County

From: Stephen Power, HDR

Subject: Review of Experience Elsewhere

## Purpose

The purpose of this review is to provide the project team with case studies and lessons learned that will help to inform future recommendations for the Rocky View County Active Transportation Plan (RVC ATP) - South County. The review is focused on plans from municipalities that share similar contexts to RVC in terms of geography, population, climate, and/or land uses.

The plans/projects reviewed include:

- Arapahoe County Bicycle and Pedestrian Master Plan (2017)
- Columbus Area Active Transportation Plan (2016)
- Halton Region Active Transportation Plan (2015)
- County of Peterborough Active Transportation Plan (2016)
- Kern Region Active Transportation Plan (2017)

## Arapahoe County Bicycle and Pedestrian Master Plan (2017)

Arapahoe County is situated along the southwestern edge of the City and County of Denver. The County is primarily urban/suburban in the west and rural in the east.

The Arapahoe Bicycle and Pedestrian Master Plan provides an explanation of methodology throughout the plan, which helps the reader understand the recommendations and decisions made during each step of the ATP development. Two areas stand out: the evaluation of existing facilities and the prioritization of projects within the proposed active transportation network.

### Existing Facility Evaluation Tools

- **Level of Traffic Stress (LTS) Tool** uses roadway characteristics, including: traffic speeds and volumes, number of thru lanes, and, if applicable, bike lane width, to calculate a grade. Grades are based on a scale of 1 to 4, and correspond to the level of comfort. The LTS was applied to the urbanized area of Arapahoe County, specifically streets classified as a Major Collector and higher in the County, regardless of whether or not a bicycle facility exists
- **Rural Road Biking Assessment Tool** considers a variety of roadway characteristics, including a few only relevant to rural roads, this tool designates the cycling conditions on each assessed road as good, moderate, or poor.

- **Pedestrian Demand Index** identifies locations in the County that are likely to have ‘high’ and ‘very high’ pedestrian demand. Inputs include: employment density, population density, zero vehicle households, urban activity centres, parks/open space/recreation centres, school zones, and transit density. The indices are summarized in a heat map.
- **Barriers to Biking and Walking** is a long list of natural or anthropogenic constraints such as: freeways, highways, arterials, railroads, major parks, and other topographic or natural features.

**Proposed Active transportation Network Project Prioritization**

After recommending an active transportation network, the plan prioritizes projects within the new network by using a scoring system based on the following criteria:

- **Demand for Biking and Walking:** job density, population density, zero vehicle households, urban activity centres, parks and open space, school zones, and transit density.
- **Access and Connectivity:** scoring considered whether a project would: 1) eliminate a major barrier (e.g., crossing of a railroad, waterway, state highway or six-lane arterial), 2) close a gap in the existing network, and/or 3) on a Regional Bike Route
- **Health and Safety:** considers the number of bicycle or pedestrian crashes within the project area.
- **Land Use Context:** Projects located in areas with a relatively high percentage of low-income and/or minority population received a higher score. For the trail projects, an additional factor of land ownership was considered. Trail projects on public property were given a higher score than those on private property because trail projects within publicly owned rights-of-way are generally easier to implement.

**Relevance to RVC ATP South County**

The clear explanation of methodologies used during various stages of the plan provides the reader with an understanding of the plan process. A similar explanation of methodology could be included in the RVC ATP South County.

Regarding the evaluation of existing facilities, the use of LTS tool for the Arapahoe County Plan demonstrates the appropriateness for the RVC context.

Regarding network prioritization, the recommended RVC active transportation network will also require prioritization, and will be developed during the Active Transportation Network and Project/Action Plan tasks. There are elements of the Arapahoe County ATP prioritization process that may be appropriate in RVC.



## Columbus Metropolitan Area Active Transportation Plan (2016)

The Columbus Metropolitan Area includes the urbanized area around the City of Columbus, as well as the outlying suburban centres and rural lands. The Columbus Area ATP was created as part of the 2016-2040 Columbus Area Metropolitan Transportation Plan. As part of the education and implementation of the ATP, the Mid-Ohio Regional Planning Commission (MORPC), responsible for plan, developed two tools: a Story Map and a Cost Estimator Tool.

### Story Map

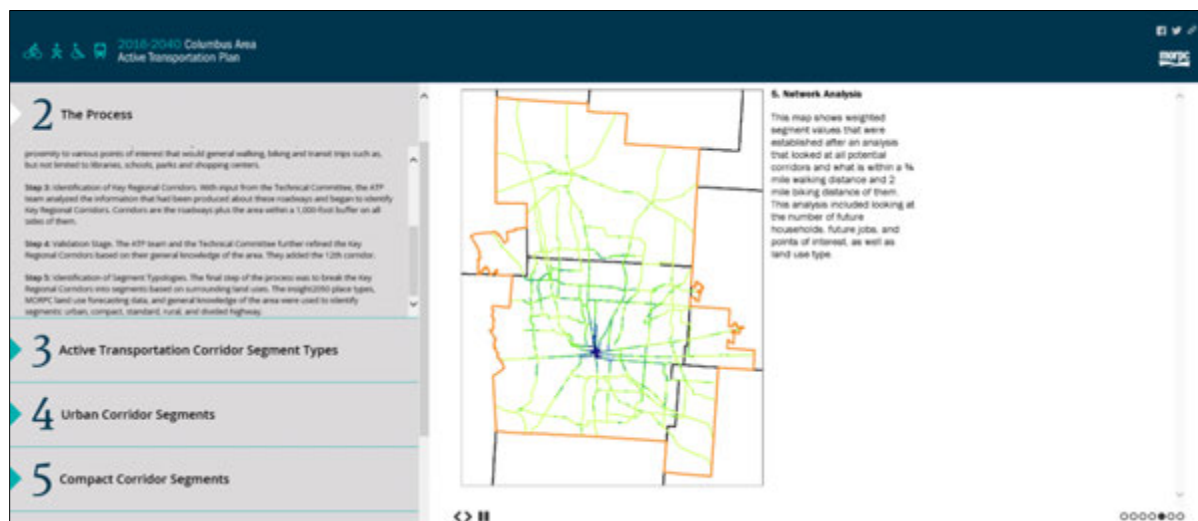
The Story Map was a request from the ATP Advisory Group who wanted a version of the ATP that they could easily access on a computer, smartphone or tablet at meetings. The project used Esri's Story Maps to deliver a web-based version of the ATP. Story Maps is an online platform that combines authoritative maps with narrative text, images, and multimedia content.

The Columbus Area ATP Story Map breaks the ATP down into eight chapters:

1. Introduction and plan purpose
2. ATP process
3. Active transportation corridor segment types.
4. Urban corridor segments and associated facilities.
5. Compact corridor segments and associated facilities.
6. Standard corridor segments and associated facilities.
7. Rural corridor segments and associated facilities.
8. Divided highway corridor segments and associated facilities.
9. An interactive map.
10. Glossary of facility types.

An excerpt of the Columbus Area Story Map is shown in **Figure 1**.

**Figure 1 Excerpt from the Columbus Area Story Map showing text and visuals associated with Chapter 2**



### Cost Estimator

The ATP Cost Estimator is a downloadable Excel file hosted on the MORPC ATP site. The MORPC found that many municipalities within the Columbus Area were quickly dismissing the inclusion of active transportation facilities in projects due to a lack of planning-level active transportation cost estimating tools.

The Cost Estimator spreadsheet includes separate sheets for estimating sidewalks, multi-use paths, and bike lanes. Unit rates are based on Ohio Department of Transportation (ODOT) past projects. An example from the Cost Estimator tool is shown in Figure 2.

Figure 2 Cost Estimator sheet for Multi-Use Paths. Unit rate assumptions included within a separate sheet in the Excel file.

# Question		Answer	Units	Notes
<b>Dimensions</b>				
1	Length of roadway with multi-use path to be constructed on <u>one</u> side of roadway.		miles	If known, exclude lengths of crosswalks, existing multi-use paths that will not be replaced, and other segments not requiring multi-use path construction.
2	Length of roadway with multi-use path to be constructed on <u>both</u> sides of roadway.		miles	If known, exclude lengths of crosswalks, existing multi-use paths that will not be replaced, and other segments not requiring multi-use path construction.
3	Total length of multi-use path to be constructed.	0	miles	This is a calculation that is shown for you to check your entries in #1 and #2.
4	Average width of multi-use path to be constructed in each direction. If the widths differ by direction, enter the average.		feet	10 ft is the minimum recommended width of a two-way multi-use path in many design resources.
5	Average width of buffer from curb/edge of road pavement to edge of multi-use path.		feet	5 ft is the minimum recommended distance between a path and the edge of paved roadway/curb. 2 ft is the minimum recommended lateral clearance from obstructions in many design resources.
<b>Clearing</b>				
6	Will multi-use path construction require additional tree removal?		Yes/No	These are trees that would not be removed for the base project.
7	If so, how many of those are <u>large</u> trees?		large trees	Trees that are 75 ft tall or taller.
8	If so, how many of those are <u>small</u> trees?		small trees	Trees less than 75 ft tall.
<b>Structures</b>				
9	Does the roadway have any culverts that will need to be modified or replaced to construct multi-use paths?		Yes/No	A culvert is a structure that allows water to flow under a road and is typically embedded so as to be surrounded by soil. It may take the form of a metal pipe or a concrete tunnel. The referenced dimension is the width or diameter of the culvert.
10	If so, how many of those are <u>small</u> (< 3 ft) culverts?		culverts	
11	If so, how many of those are <u>medium</u> (3 ft to 10 ft) culverts?		culverts	
12	If so, how many of those are <u>large</u> (> 10 ft) culverts?		culverts	
13	Does the roadway have any bridges?		Yes/No	
14	If so, how many bridges?		bridges	
15	If so, what is the total (combined) length, or span, of the bridges?		feet	For example, a 50-ft long bridge and a 100-ft long bridge would be 150 ft.
<b>Crossings &amp; Bus Stops</b>				
16	Number of 4-way intersections. Also include intersections with 5 or more legs.	0	intersections	This is a calculation that is shown for you to check your entries in #17 and #18.
17	How many 4-way intersections will be <u>signalized</u> ?		intersections	These intersections will have a traffic signal when the project is complete.
18	How many 4-way intersections will be <u>unsignalized</u> ?		intersections	These intersections will NOT have a traffic signal when the project is complete.
19	Number of 3-way or "T" intersections.	0	intersections	This is a calculation that is shown for you to check your entries in #17 and #18.
20	How many 3-way or "T" intersections will be <u>signalized</u> ?		intersections	These intersections will have a traffic signal when the project is complete.
21	How many 3-way or "T" intersections will be <u>unsignalized</u> ?		intersections	These intersections will NOT have a traffic signal when the project is complete.
22	Number of driveways the multi-use path would cross.		driveways	Include any driveways that will NOT have curb ramps or any drives intersecting the project not considered intersections for #16 and #19.
23	Number of crosswalks NOT located at intersections included above.		crosswalks	Mid-block crossings, for example.
24	Number of additional passenger pads for bus stops.		pads	In absence of a determination, enter the number of bus stops within the project. This assumes that adding multi-use paths will require the addition to the base project of a passenger pad at each bus stop. See references for more information.
<b>Other Considerations</b>				
25	Will the facility require the project to add curb and gutter that is not included in the base project?		Yes/No	
26	Will the proposed multi-use path require utilities to be moved that would otherwise not be moved?		Yes/No	
27	If so, are these utilities currently located within the public right-of-way?		Yes/No	If not, then report that there would likely be extra utility relocation costs not calculated with this tool.

**Relevance to RVC ATP South County**

The Story Map tool could be used as an end product or as a public/stakeholder engagement tool. Story Map’s functionality includes the ability to draw lines, add pins and make comments.

A cost estimation tool could be considered as an implementation item used in conjunction with facility design guidance and developer’s checklist. The tool may be used by RVC staff, local advocacy groups, or developers in determining planning-level cost estimates for the construction of active transportation facilities within the County.

**Halton Region Active Transportation Plan (2015)**

The Regional Municipality of Halton is situated directly adjacent to the City of Toronto and includes the City of Burlington and Town of Oakville. The lands along the southern part of the Region, along Lake Ontario, are largely urban, while the area to the north is more rural.

The Plan includes an Implementation Strategy that identifies the most cost-effective method for building cycling and walking facilities is to construct them as part of roadway resurfacing and construction projects. This approach is aligned with the Region’s Roads Capital Program and Roads Resurfacing Program.

The Plan identifies three strategic implementation areas: Road Capital Program, Road Resurfacing, and Active Transportation Projects. Within these three areas the Plan then identifies the total length and cost of new on-road facilities, new off-road facilities, and upgrades/replacement of off-road facilities. These proposed total length and estimate costs are shown in Figure 3 and Figure 4 respectively.



Figure 3. Active transportation facility by implementation strategy

Type of Active Transportation Facility by Implementation Strategy	Proposed Length (lane-km)	Percent of Total Length of AT Network
<b>Road Capital Program</b>	<b>705<sup>a</sup></b>	<b>80%</b>
Build new on-road cycling facility	352	
Build new off-road cycling and / or walking facility	216	
Replace off-road facility (widening) <sup>a</sup>	137	
<b>Road Resurfacing</b>	<b>137</b>	<b>15%</b>
Build new on-road cycling facility (widening)	124	
Build new off-road cycling and / or walking facility	13	
<b>Active Transportation Projects</b>	<b>46<sup>a</sup></b>	<b>5%</b>
Build new on-road cycling facility	18	
Build new off-road cycling and / or walking facility	10	
Replace off-road facility	18	
<b>Total New Active Transportation Facilities</b>	<b>733<sup>a</sup></b>	<b>100%</b>

Notes:

- a. Total length does not include 155 km of existing off-road facilities being replaced with road widening projects

The Plan then provides a breakdown of construction cost estimates, as shown in Figure 4.

Figure 4. Active transportation facility construction cost estimates (2013 dollars)

Type of Active Transportation Facility by Implementation Strategy	Approximate Construction Cost <sup>a</sup> (\$ million)	Percent of Total Cost
<b>Road Capital Program</b>	<b>\$86</b>	<b>76%</b>
Build new on-road cycling facility	\$39.6	
Build new off-road cycling and / or walking facility	\$27.7	
Replace off-road cycling and / or walking facility (widening)	\$18.6	
<b>Road Resurfacing</b>	<b>\$16</b>	<b>14%</b>
Build new on-road cycling facility (widening)	\$12.3	
Build new off-road cycling and / or walking facility	\$3.9	
<b>Active Transportation Projects</b>	<b>\$11</b>	<b>10%</b>
Build new on-road cycling facility	\$5.3	
Build new off-road cycling and / or walking facility	\$5.2	
<b>Total Cost to build AT Network</b>	<b>\$113</b>	<b>100%</b>

Notes:

- a. Approximate costs are based on benchmark costs of construction for major contract items, plus 15% contingency and 15% engineering. It does not include planning, property (if required), utility relocations (an allowance has been included for relocating some hydro poles as part of sidewalk construction in standalone active transportation capital projects), maintenance and taxes.

**Relevancy to RVC ATP**

Coordination with capital plans road resurfacing programs was demonstrated as an important principle for efficient implementation in Halton Region. A coordinated implementation strategy may also be appropriate for RVC.

**County of Peterborough Active Transportation Master Plan (2017)**

Peterborough County is east of the Halton Region in Southern Ontario and is centered on the City of Peterborough. The greater region includes a mix of townships, agricultural and recreational uses. The Peterborough County ATP focuses on cycling. The Plan includes recommendations for policies related to planning/development and infrastructure design:

1.0 Planning

1.10 Bicycle parking facilities/amenities should be considered as a requirement in new buildings, through Site Plan Control, as part of the development application process where appropriate.

2.0 Design

2.3 The County and the Townships should consider a policy for the provision of a stepped warrant for the provision of paved shoulders, where feasible and as part of rural reconstruction and resurfacing projects, to improve safety, reduce maintenance costs and support active transportation.

An example is as follows: Low volume roads (AADT < 1000): shared use of the traffic lane with a desirable 0 - 0.5 m partially paved shoulder. - Medium volume roads (1000 < AADT 5000): a desirable 2.0 m fully paved shoulder on higher speed roadways with a posted speed limit > 60 km/h.

**Relevance to RVC ATP South County**

Policies related to end-of-trip facilities and standardized paved shoulders are potential policy considerations in RVC.



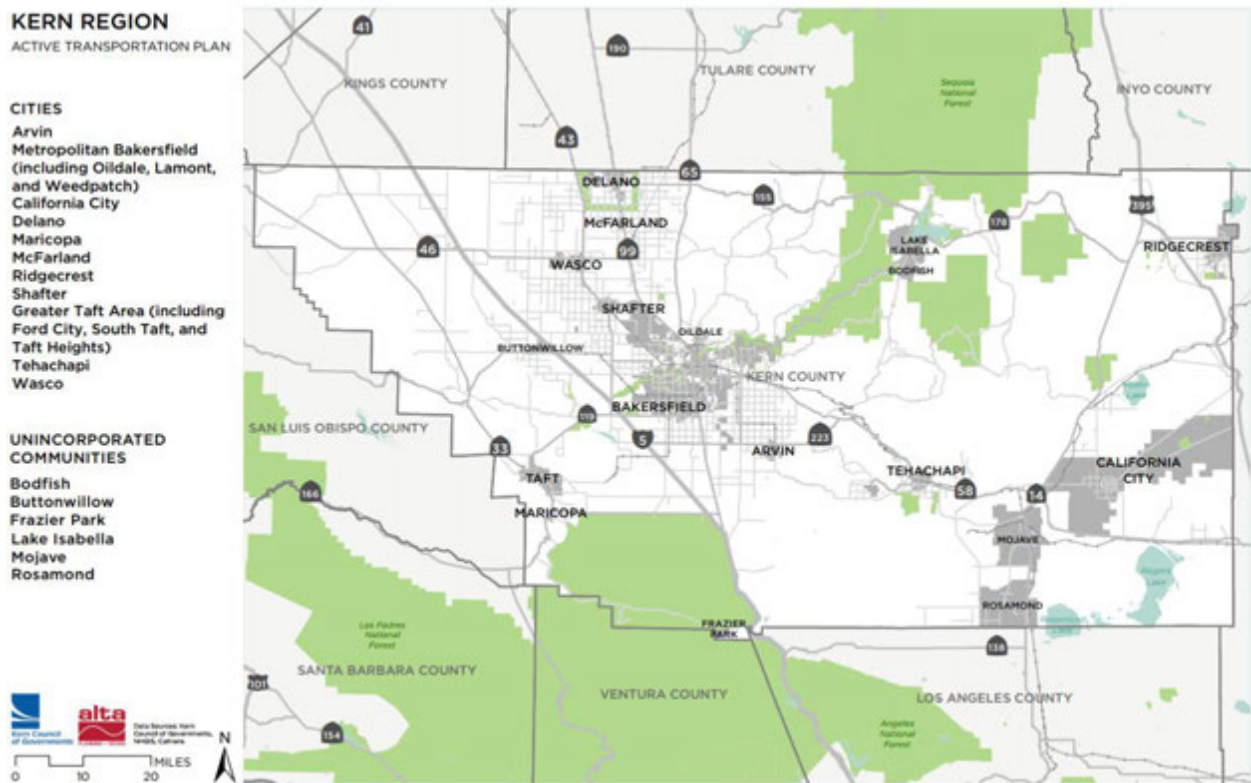


## Kern Region Active Transportation Plan (2017)

Kern County is located in the southern end of California’s Central Valley, and is known as one of the fastest growing areas in the United States. The County’s economy is closely linked to agriculture, aviation/military, and petroleum extraction. The Kern County ATP focuses on walking, bicycling and transit access.

Due to the vastness of Kern County, the Plan breaks down active transportation network recommendations into focus areas, as shown in Figure 5. Regional bicycle connections were previously completed Kern County Bicycle Master Plan in 2012.

Figure 5 Kern County ATP focus communities



### Relevance to RVC ATP South County

While the development of an active transportation network will be considered for the entirety of the South County study area (Elbow River Ranch Lands and Bow River Plains Regions), there an opportunity to apply the concept of focus areas, such as Langdon and Bragg Creek.

APPENDIX D

# Existing Conditions

# MEMORANDUM



**The Joseph Vance Building  
1402 Third Avenue, Suite 206  
Seattle, WA 98101  
(206) 735-7466**

**To:** Stephen Power, Transportation Planning Lead, HDR

**From:** Steve Durrant, Principal, Alta Planning + Design

**Date:** April 11, 2018

**Re: Rocky View Active Transportation Existing Conditions Technical Memo**

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## Existing Active Transportation Conditions Inventory

Designated active transportation facilities in the Bow River Plains and Elbow River Ranch Lands of Rocky View County are predominantly meant to serve local recreational trips. Local pathways in Elbow River Ranch Lands include those near Springbank, Bragg Creek, and Elbow River. In the Bow River Plains, there is a much smaller collection of pathways and trails, with the bulk of existing pathways and trails located in the hamlets of Conrich and Langdon.

In addition to pathways and trails, there are a number of streets that support active transportation. Many major roadways-- such as Highways 1, 22, and 8, and Springbank Road in the west, and Highways 1, 9, 22X, and 560 in the east—have paved shoulders that support cycling. For pedestrians, sidewalks are present in areas such as Langdon.

There are also a number of pathways and trails either adopted or proposed for development in the area (see maps 1A and 1B). Pathways and trails with the *adopted* status are those included in statutory documents such as Area Structure Plans, Conceptual Schemes, and Master Plans. The *proposed* status, on the other hand, indicates that the pathway or trail has been identified through non-statutory processes and are used as a planning tool.

Table 1 provides a summary of the total distance for facilities evident in Bow River Plains and Elbow River Ranch Lands in Rocky View County.

## Rocky View County Active Transportation Plan

Table 1: Length of Transportation Facilities in Bow River Plains and Elbow River Lands

Facility Type	Total Facility Distance (km)
Total length of roadways within study area	2,155 (approximately)
County Trails - Existing	114.9
County Trails - Adopted	126.2
County Trails - Proposed	250.5
County Sidewalks	34.3
Paved Shoulder (One Side of Street)	2.3
Paved Shoulder (Both Sides of Street)	236.5

The adopted pathway and trail network, when implemented, will enhance connectivity of the existing pathway and trail network to surrounding areas. For example, future connections in the west will link Springbank with Calgary to the east along Lower Springbank Rd. Springbank will also connect to Cochrane in the north along, Ridge Rd. 31, Township Rd. 250 and Highway 22. In the Bow River Plains, adopted pathways and trails will connect to Langdon to the south and Chestermere to the north.

However, the adopted pathways and trails will not provide a network of separated facilities to create continuous links through and between communities within the study area. In the Elbow River Ranch Lands, for instance, adopted pathways and trails connect Springbank to Calgary in the east but currently stop short of reaching Cochrane to the north. Bragg Creek in the south is completely separated from the rest of the active transportation (AT) network. In Bow River Plains, the issue is more pronounced. Adopted trails link Langdon to the south, but Conrich is completely self-contained.

Proposed trails, particularly in the west, help address the gaps in the adopted network; this is particularly evident along TWP RD 244/Springbank Road. When viewed at a county-wide scale, the lack of connections among communities within and adjacent to the study area is evident.

# MAP 1A INVENTORY

ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

### ACTIVE TRANSPORTATION INFRASTRUCTURE

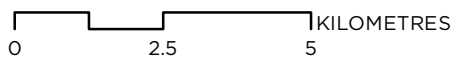
- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed
- County Sidewalks
- Calgary Bikeways
- Calgary Trails
- Redwood Meadows Trails
- Cochrane Pathways

### ROAD NETWORK

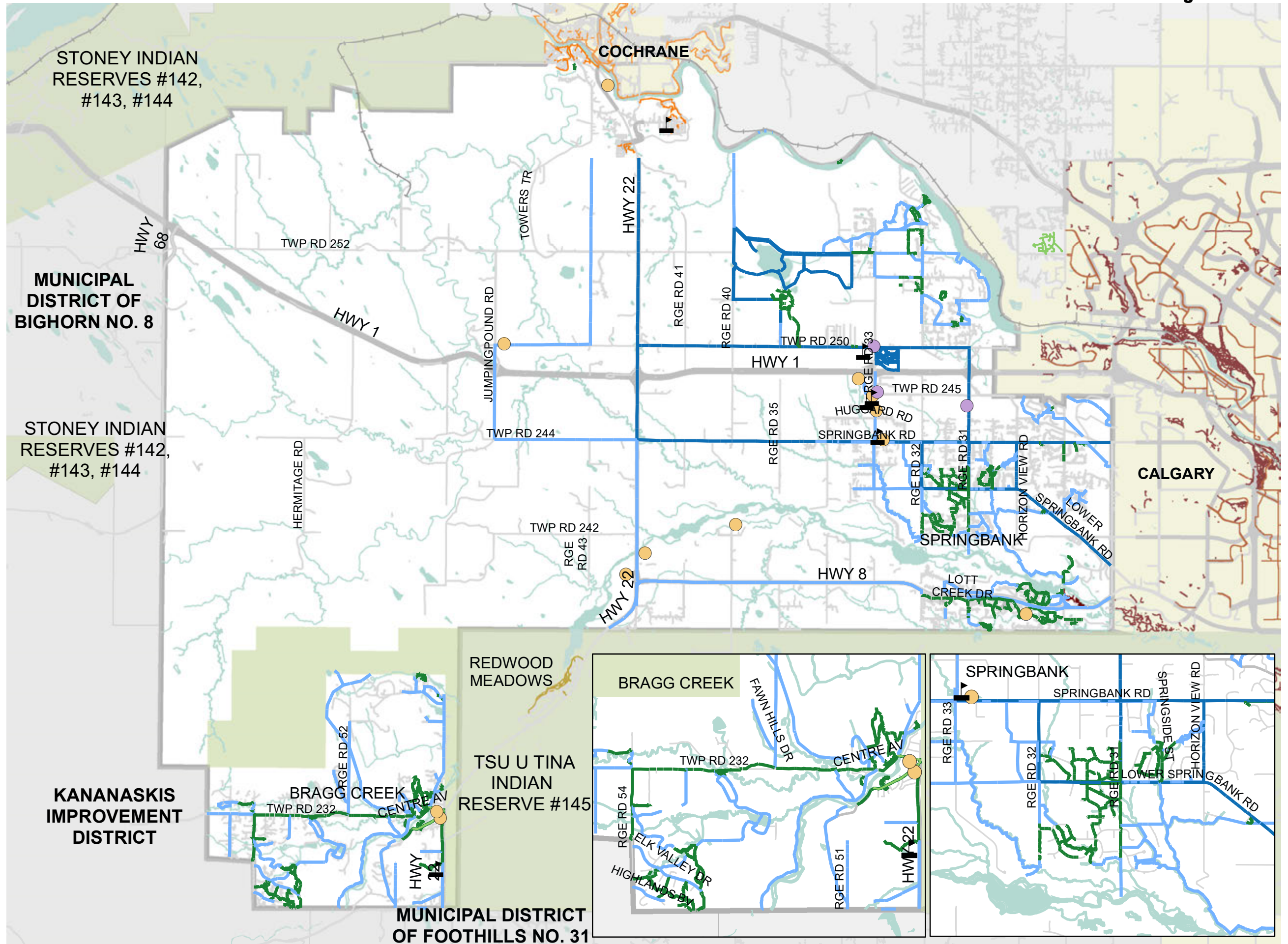
- Primary Hwy
- Secondary Hwy
- Major
- Collector
- Road
- Street

### OTHER FEATURES

- School
- Community Facility
- Church
- Railway
- Hydrology



Dr. View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced January 2018.



# MAP 1B INVENTORY

## ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN






### ACTIVE TRANSPORTATION INFRASTRUCTURE

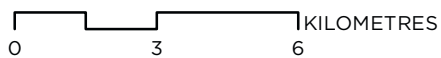
- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed
- County Sidewalks
- Calgary Bikeways
- Calgary Trails
- Chestermere Paths

### ROAD NETWORK

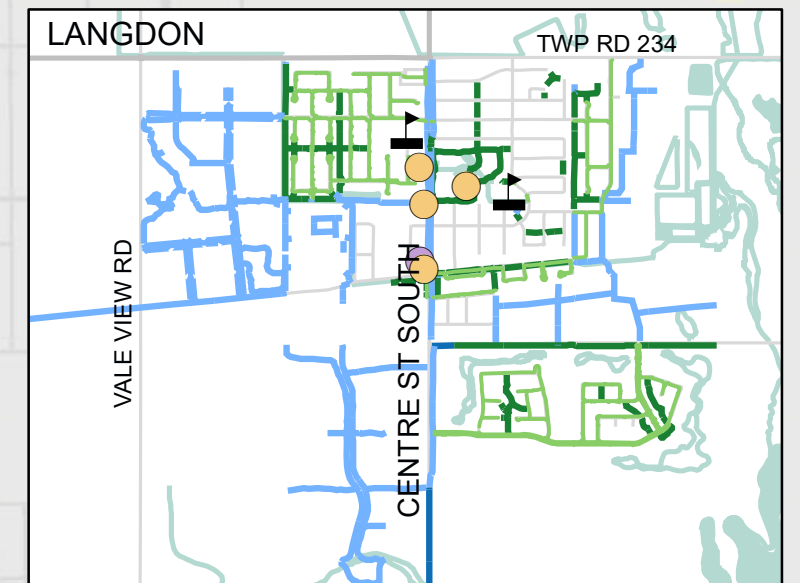
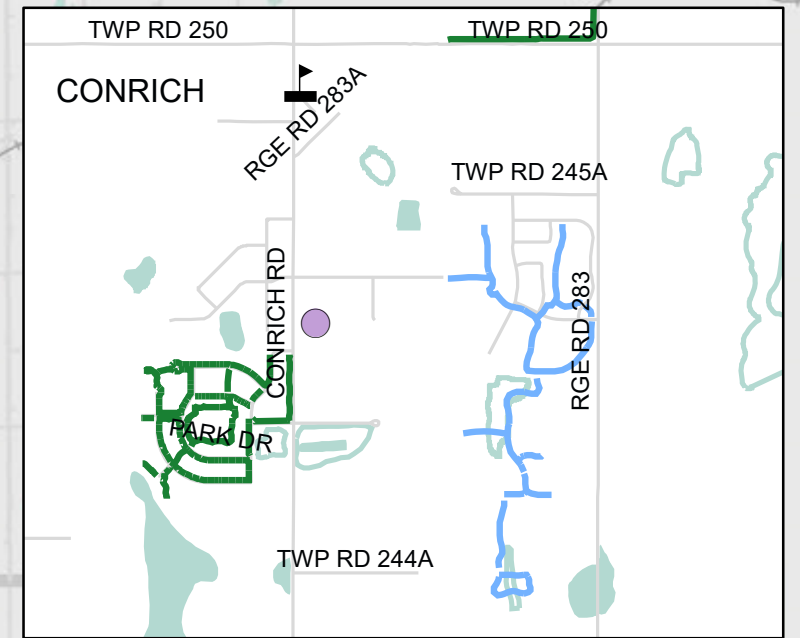
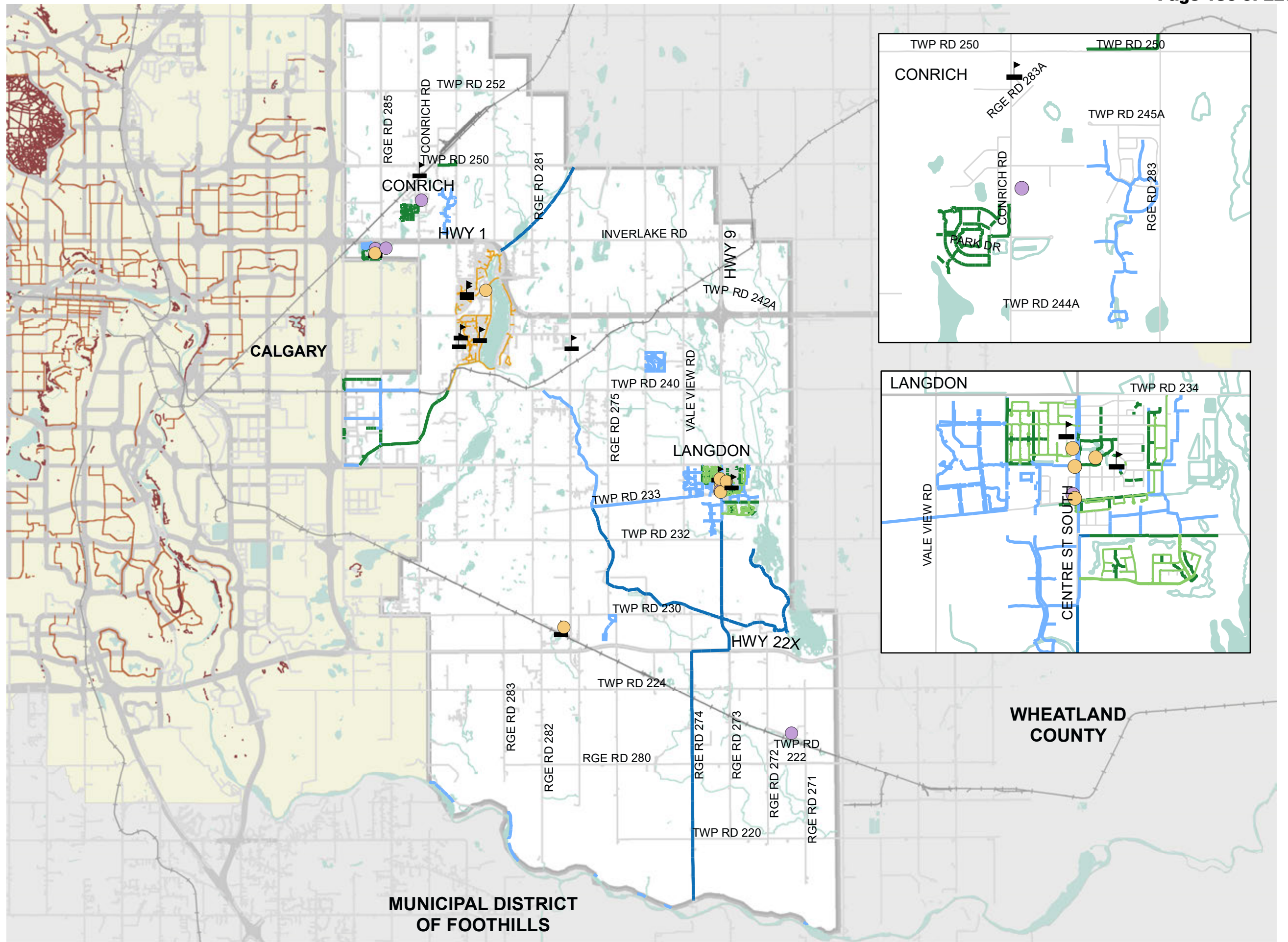
- Primary Hwy
- Secondary Hwy
- Major
- Collector
- Road
- Street

### OTHER FEATURES

-  School
-  Community Facility
-  Church
-  Railway
-  Hydrology



Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced January 2018.



MUNICIPAL DISTRICT  
OF FOOTHILLS

## Systematic Safety Analysis

A detailed review of pedestrian- and cyclist-involved collisions between 2011 and 2015 was conducted; this is the most-recent five-year period of data available for the Elbow River Ranch Lands and Bow River Plains areas. The data is derived from police reports, where location of the collision was described primarily in text form and often include estimated distances. As a result, specific locations may not be precise.

In addition to the frequency of collisions, the review included consideration of the characteristics of the roadway where the collision occurred. The roadway characteristics included as part of the analysis include: location (midblock or at intersection), speed limit, number of lanes, and presence of an existing active transportation facility, such as a paved shoulder, pathway, trail, or sidewalk.

In the data provided, 18 reported collisions within the study area involved pedestrians or cyclists. Of the 18 collisions, the location of 17 could be identified. Of those 17 collisions:

- There were 16 injuries, and one fatality
- 15 collisions occurred in the Elbow River Ranch Lands, and 2 occurred in the Bow River Plains area
- 9 collisions occurred at intersections, 7 occurred midblock, and 1 occurred in a parking lot
- More collisions occurred on roads with speeds greater than 50 km/h, which is in proportion to the total length of roads with posted speeds greater than 50 km/h
- All collisions occurred on paved roads
- 47% of collisions occurred on roads with paved shoulders.
  - Paved shoulders are present on only 18% of roads within the study area.
  - It is important to note that this analysis does not consider exposure (i.e., the number of bicyclists and pedestrians traveling on the roadway). A greater concentration of collisions along roadways with paved shoulders may reflect that more are using this roadway, increasing the potential for a collision.
  - Strava data suggests that these roadways receive greater use.

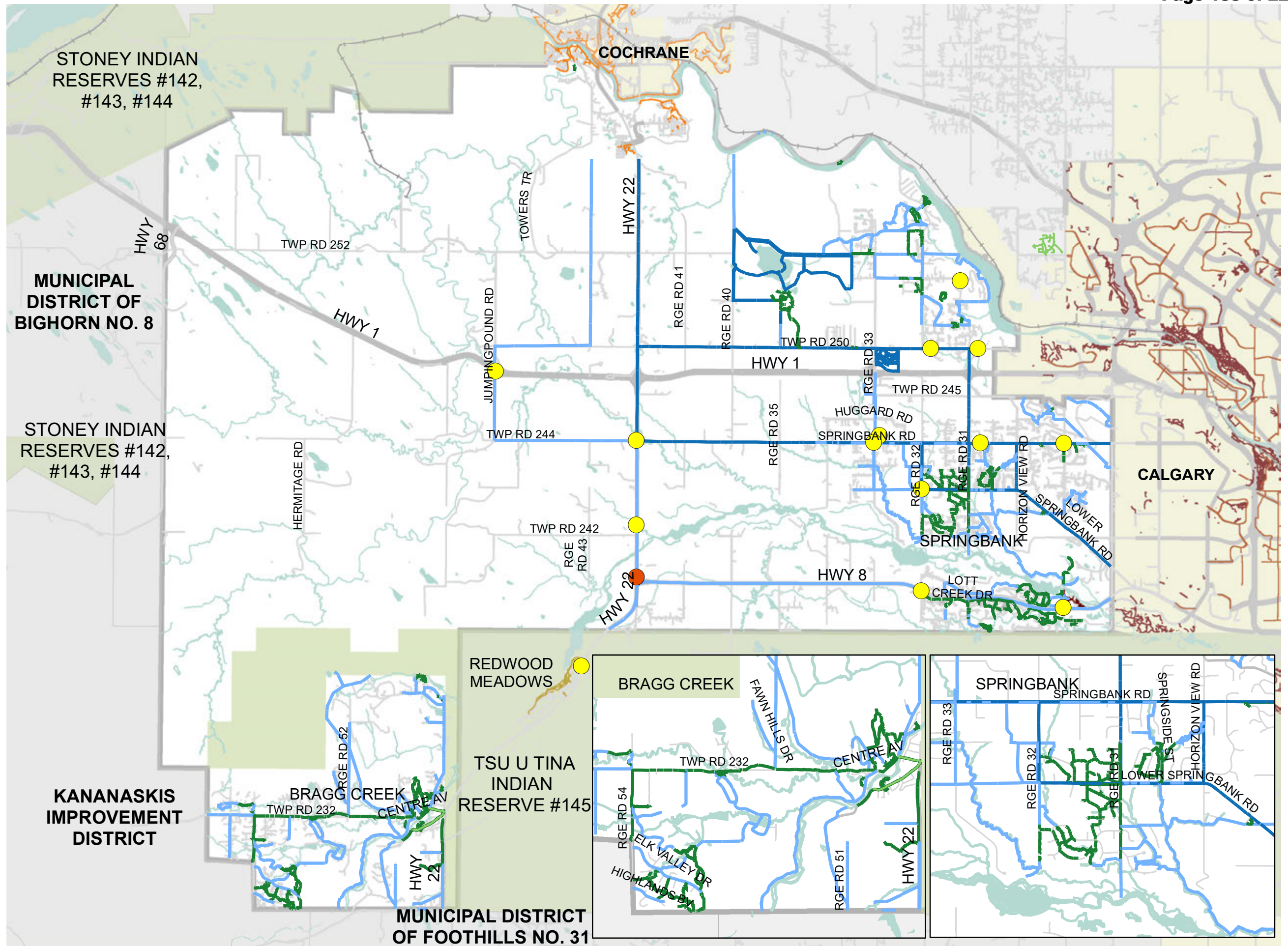
In the data provided, information was available regarding the characteristics of the collisions; based on the available data, the following is known about the 17 collisions where locations could be identified:

- 4 collisions involved pedestrians, and 13 involved people on bicycles
- 14 collisions occurred between May and September
- 12 occurred during daylight hours
- 16 occurred on dry surfaces, the surface conditions for other collisions were reported as unknown
- 15 occurred in clear conditions, with the visibility conditions for the other 2 collisions reported as unknown

Maps 1A and 1B show the reported collisions in relation to the existing, planned, and proposed pathways and trails.

MAP 2A  
COLLISION ANALYSIS  
ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

- Collision with Fatality
  - Collision with Injury
- ACTIVE TRANSPORTATION INFRASTRUCTURE
- County Trails - Existing
  - County Trails - Adopted
  - County Trails - Proposed
  - County Sidewalks
  - Calgary Bikeways
  - Calgary Trails
  - Redwood Meadows Trails
  - Cochrane Pathways
- ROAD NETWORK
- Primary Hwy
  - Secondary Hwy
  - Major
  - Collector
  - Road
  - Street
- OTHER FEATURES
- + Railway
  - Hydrology



Data provided by Rocky View County, City of Calgary, Town of Cochrane and AltaLis. Map produced February 2018.



# MAP 2B COLLISION ANALYSIS ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

- Collision with Fatality
- Collision with Injury

### ACTIVE TRANSPORTATION INFRASTRUCTURE

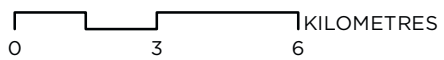
- County Trails - Existing
- County Trails - Adopted
- County Trails - Proposed
- County Sidewalks
- Calgary Bikeways
- Calgary Trails
- Chestermere Paths

### ROAD NETWORK

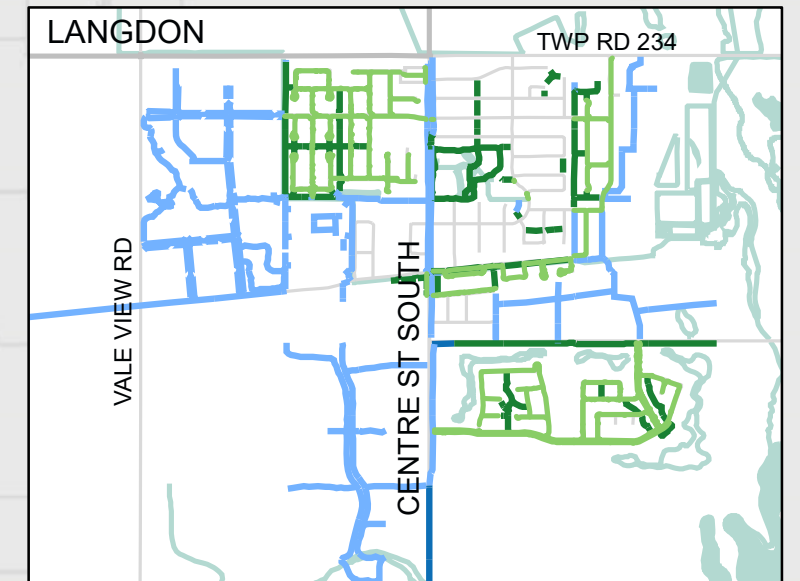
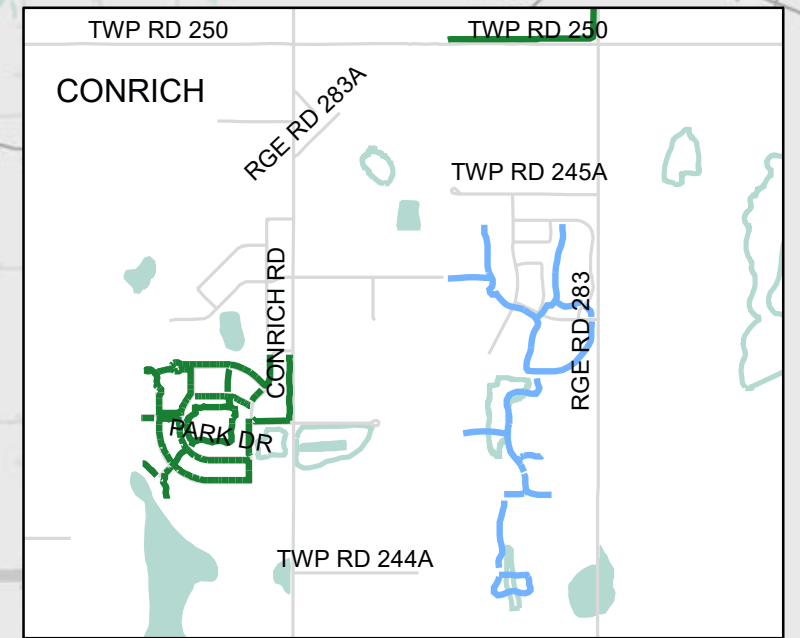
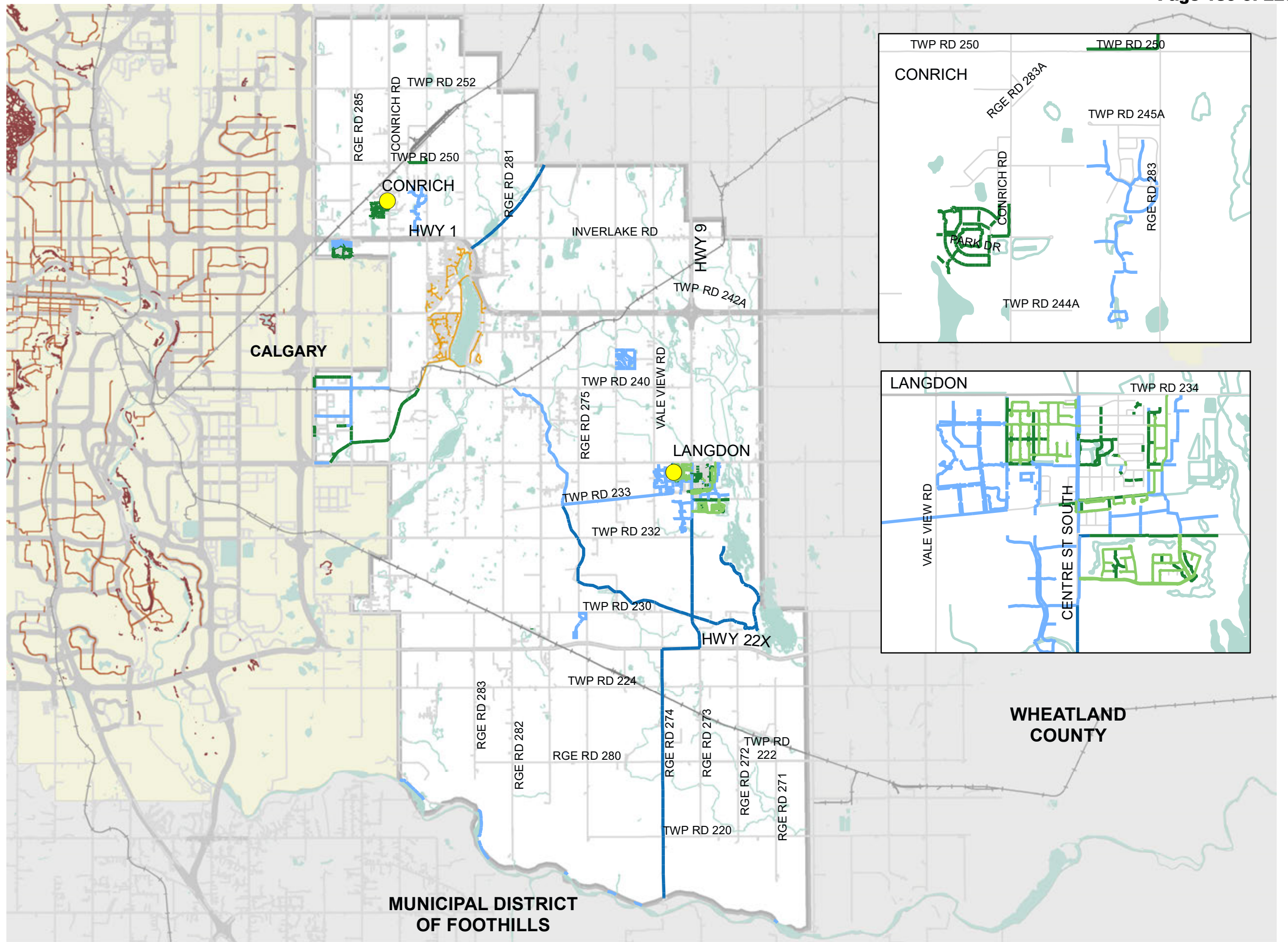
- Primary Hwy
- Secondary Hwy
- Major
- Collector
- Road
- Street

### OTHER FEATURES

- Railway
- Hydrology



Data provided by Rocky View County, City of Calgary, Town of Cochrane and AltaLis. Map produced February 2018.



MUNICIPAL DISTRICT OF FOOTHILLS

## Level of Traffic Stress (LTS) Analysis

The Level of Traffic Stress (LTS) Analysis was originally presented in the 2012 Mineta Transportation Institute Report 11-19: *Low-Stress Bicycling and Network Connectivity* and has been adapted for rural application by the Oregon Department of Transportation, and further refined by Alta Planning + Design. The results of this analysis identify a perceived level of comfort for bicyclists determined on factors, such as posted speed limit, roadway width or number of traffic lanes, and the presence and character of bicycle lanes or other bicycle infrastructure. The combination of this criteria separates the bicycle network into one of four scores:

- LTS 1: Low-stress roadway suitable for all ages and abilities
- LTS 2: Roadway comfortably ridden by the mainstream adult population
- LTS 3: Roadway ridden by “enthused and confident” cyclists
- LTS 4: Roadway ridden by the “strong and fearless” cyclists

In general, a separated bicycle facility, such as a trail or a cycle track, would qualify as a low-stress (LTS 1) bikeway, while a roadway shared with motor vehicle traffic operating at speeds greater than 40kph would receive a higher-stress score. The results of the LTS analysis helps identify existing areas where people can cycle more comfortably, as well as focus areas for improvement. LTS provides a framework to describe the benefits of bicycle infrastructure and demonstrates that some roadways need more intervention than others to provide a truly comfortable experience.

For this analysis, roadways without paved shoulders were evaluated using the original methodology presented in the 2012 Mineta Transportation Institute Report 11-19; for roadways with paved shoulders, the rural methodology defined in the Oregon Department of Transportation (ODOT) Analysis Procedures Manual was used. ODOT’s manual includes methodology for calculating LTS for roadways in a rural context, specifically considering roads with paved shoulders. The guidance assumes a speed limit of 70 km/h or greater and assigns a score based on the width of the paved shoulder and the Annual Daily Traffic (ADT). Additional details regarding the methodology are found in the following section.

### Level of Traffic Stress Methodology

The Level of Traffic Stress analysis is completed through an assessment of street segments, intersections, and approaches using spatial data and aerial imagery. Broadly, every street link (a section of roadway) receives *up to* three scores based on its characteristics (Figure 1):

- Segment – area of roadway between intersecting streets
- Approach – area of roadway segment with turn lanes, where present
- Intersection – area where one road crosses another, where present

All roadways receive a segment score. However, not all roadways receive an intersection or an approach score. For example, a midblock portion of a street link receives a segment score, but no intersection or approach score because it doesn't intersect another street, nor does it have turn lane. Figure 1 helps illustrate the three possible sections of a roadway that are scored.

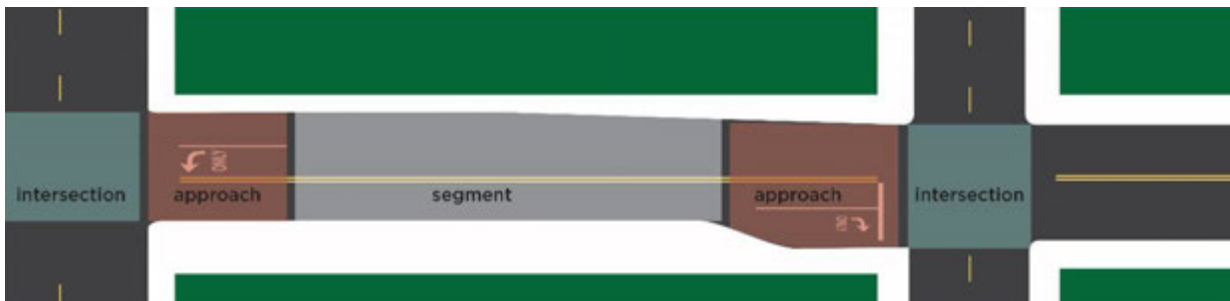


Figure 1. A street link showing the three possible scores it could receive. Because not all links have these three sections, some links may instead receive one or two scores.

The three scores assigned are based on a link's characteristics that affect a bicyclist's perception of safety and comfort. These three scores are combined to determine the overall LTS score; the weakest link principle is used, meaning that the highest stress score is used. For example, a segment may provide a low stress pathway but due to a high stress intersection, the link will receive a high stress score.

### LTS Analysis Results and Considerations

It is important to note that the LTS model is reflective of the roadway network data available. While aerial imagery was utilized to provide additional information about roadways with paved shoulders, certain characteristics—such as posted speed—are more difficult to verify using aerial imagery. For this reason, it is understood that there are limitations due to the available data; these are noted in more detail in the results below.

- Due to posted speed limits of 60 km/h or greater on many of the roadways in Rocky View County, most roads received an LTS score of 3 or 4
- Some local roads received a higher LTS score than the through-roads they branch off of due to the speed limit of the roadway
  - This could be a result of 60 km/h being used as a default speed limit in the road network data provided and may not reflect the operating speeds given the geometry of many of these roadways
- Roadways with paved shoulders were identified through a survey using satellite imagery
  - Paved shoulders found in the survey had an estimated clear width of 1.5 m or greater based on aerial imagery
- Pathways and trails were not considered as part of the LTS analysis. The analysis only considers the roadway

## Rocky View County Active Transportation Plan

- In general, physically separated facilities are considered an LTS 1, except where they intersect a higher stress road, especially if no crossing treatment currently exists
- Consideration should be given where pathways cross roadways
- In the maps included in this memo, existing pathways and trail facilities are shown on top of roadways
- Currently, many existing pathways and trails are located between lower stress roads (within subdivisions) and not along higher stress through streets

The results of this analysis are shown on Maps 3A and 3B.

# MAP 3A LTS ANALYSIS ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## LEVEL OF TRAFFIC STRESS ANALYSIS

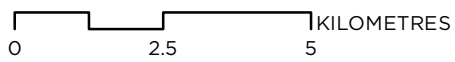
- 1 - Low Stress Roadway
- 2
- 3
- 4 - High Stress Roadway

## ACTIVE TRANSPORTATION INFRASTRUCTURE

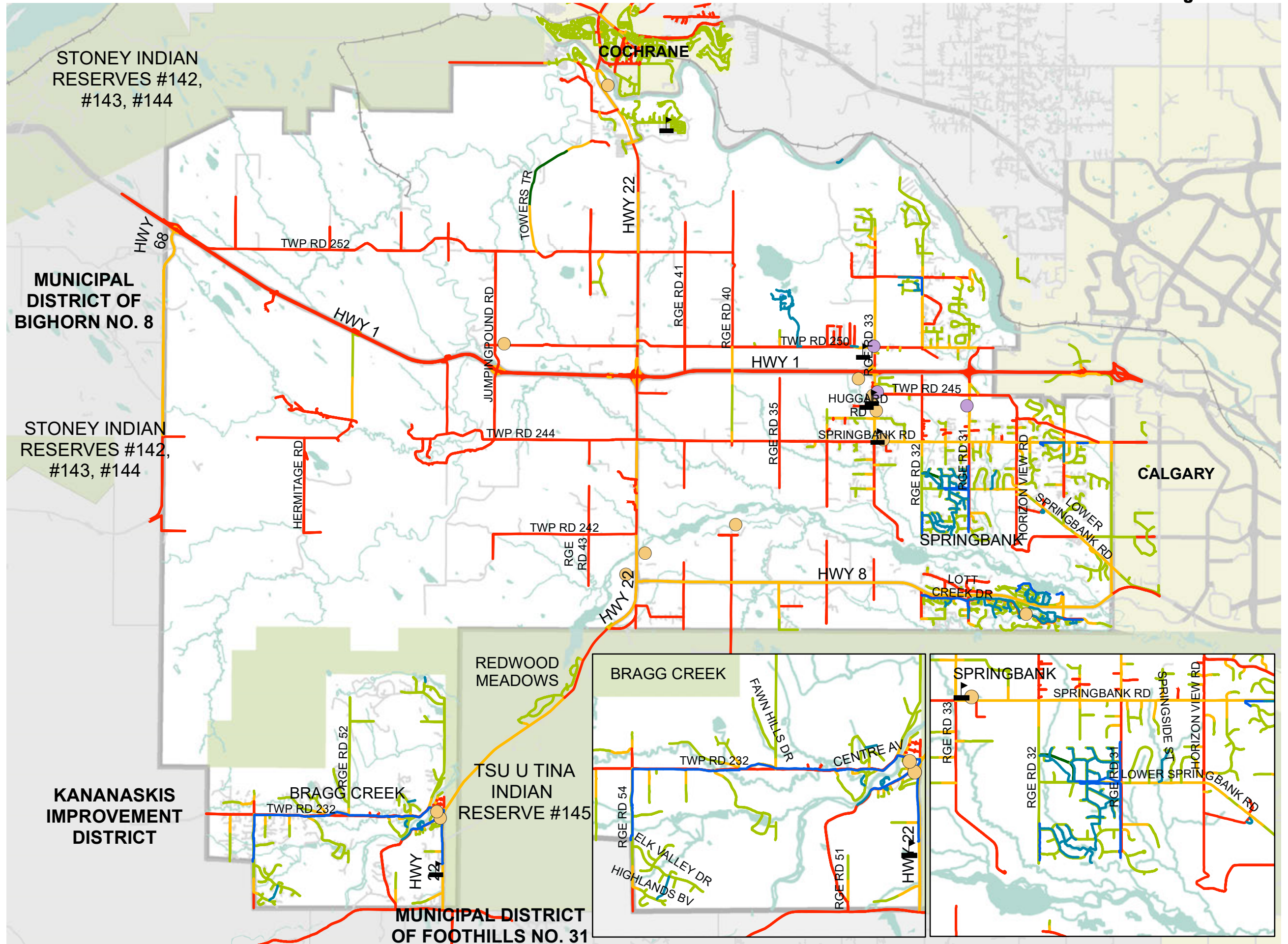
- Existing Regional Pathway
- Existing Local Pathway or Trail
- Existing Sidewalk

## OTHER FEATURES

- School
- Community Facility
- Church
- Railway
- Hydrology



Data provided by Rocky View County, City of Calgary, Town of Cochrane and AltaLis. Map produced February 2018.



# MAP 3B LTS ANALYSIS ROCKY VIEW COUNTY ACTIVE TRANSPORTATION PLAN

## LEVEL OF TRAFFIC STRESS ANALYSIS

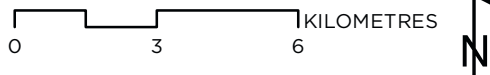
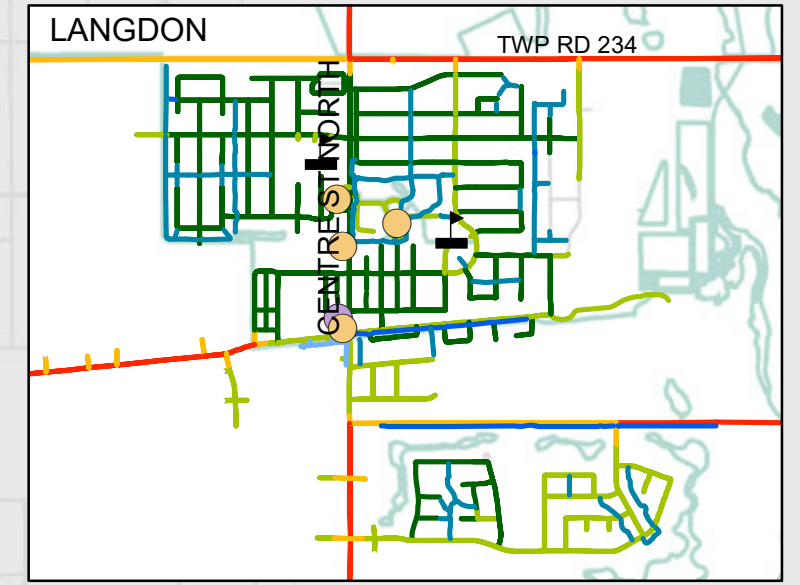
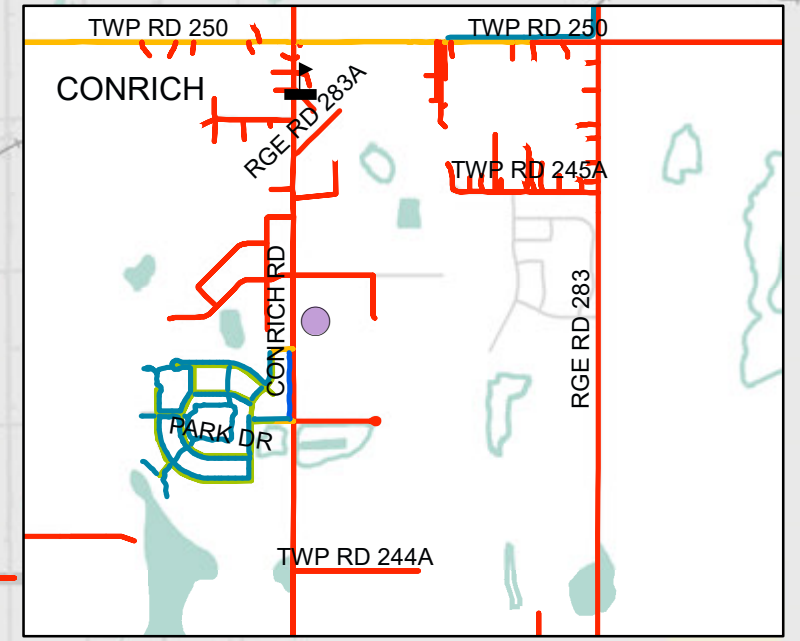
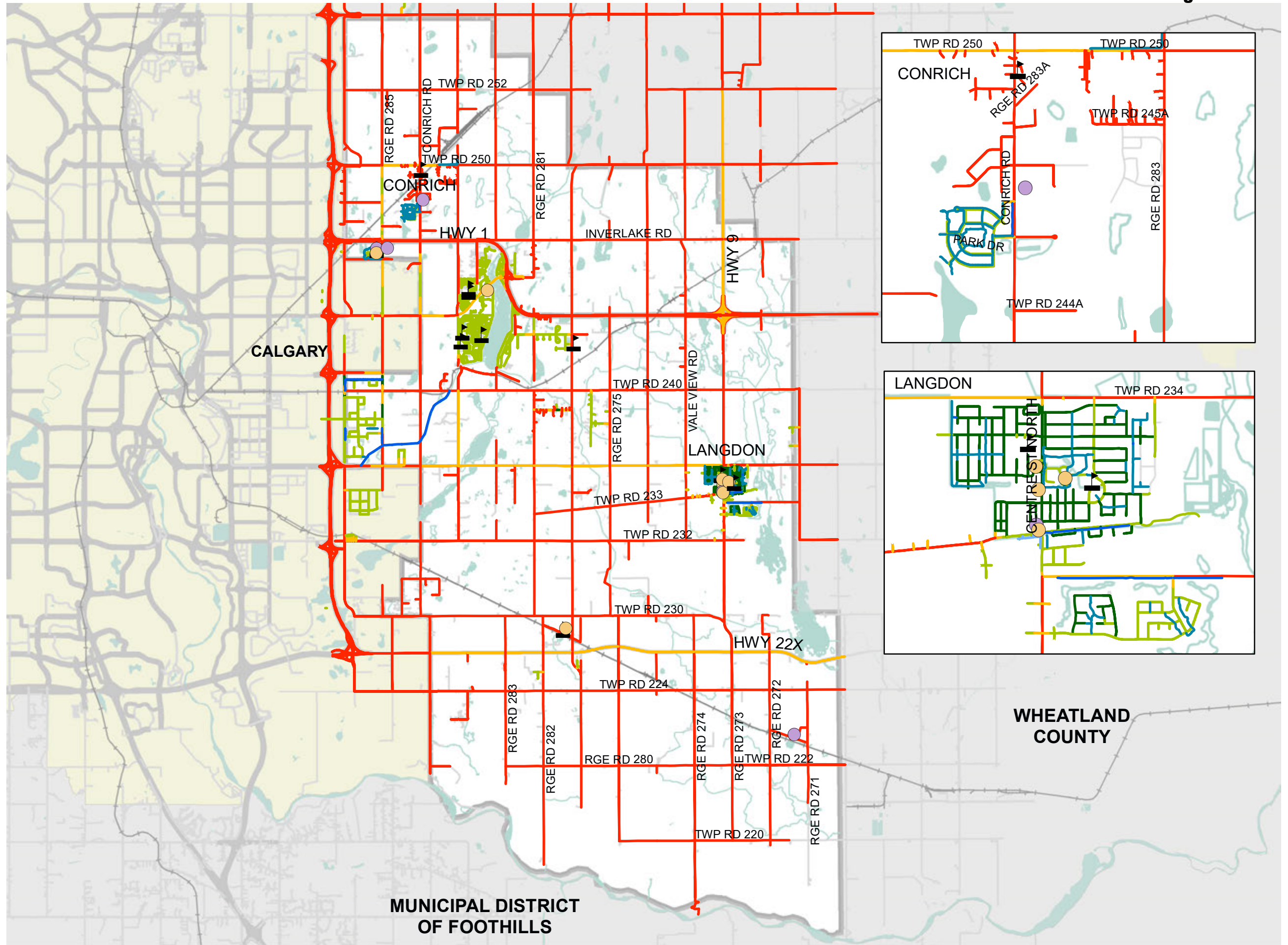
- 1 - Low Stress Roadway
- 2
- 3
- 4 - High Stress Roadway

## ACTIVE TRANSPORTATION INFRASTRUCTURE

- Existing Regional Pathway
- Existing Local Pathway or Trail
- Existing Sidewalk

## OTHER FEATURES

- 🏫 School
- 🏠 Community Facility
- ⛪ Church
- 🚂 Railway
- 💧 Hydrology



Data provided by Rocky View County, City of Calgary, Town of Cochrane and AltaLis. Map produced February 2018.

MUNICIPAL DISTRICT OF FOOTHILLS

## Equity Mapping Analysis

Equity mapping is the process of visually depicting the general areas where historically vulnerable or disadvantaged groups of people reside according to the census. In the simplest of terms, equity recognizes that different people experience different barriers to securing their needs, and working toward equity may mean prioritizing active transportation funding in areas with a greater concentration of disadvantaged populations as opposed to equally based on geography.

Mapping selected indicators such as income, education, and Aboriginal Identity, identifies where vulnerable populations predominantly reside, which can then be used for community and stakeholder selection and engagement, project prioritization, and design implementation decisions.

Using the 2016 Canadian Census, data was retrieved for census tracts in Rocky View County that fall predominantly within the study area. For purposes of analysis, the following socio-economic indicators define the potential underserved populations, as shown on maps 4 through 9:

- Percentage of population aged 19 or younger
- Percentage of population aged 65 or older
- Prevalence of low income population (LIM\_AT %)
- Percentage of population with recognized Aboriginal Identity
- Percentage of population without a high school diploma (or equivalency)

The analysis used the Alberta mean average as a threshold for each of the above indicators, so that census tracts that had a greater value than the Alberta statistical mean was given a score of one (1). For example, if a census tract had an above average number of adults aged 65 and older, and above average population without a high-school diploma, then the census tract was given a score of two (2). The highest equity score possible was a five and the lowest possible score was zero. This total equity score combined with further review of the component parts will be used to identify areas where making active transportation investment can make the most impact.

### Age: Children and Seniors (Map 4 and 5)

The percentage of children in the study area is generally above the Alberta mean, with only the census tract in the northeast section of the study area below the Alberta mean. The percentage of seniors is more evenly split, with above-average populations in both the Elbow River Ranch Lands and Bow River Plains areas. Children under the age of 19 and seniors are more likely to depend on modes of transportation other than private vehicles, due to age and reduced physical capacity relative to healthy adult populations.

## Rocky View County Active Transportation Plan

In using active transportation infrastructure, children and seniors need specialized safety considerations such as strategic use of curb ramps, shorter crossing lengths, and additional shade and resting areas.

**Income: Prevalence of Low Income (Map 6)**

To identify people with low income, the Statistics Canada (2016) definition of the Low-Income Measure - After Tax (LIM-AT) was used. This measure considers low income to be less than 50% of the mean Canadian income (after adjustments for taxes and household size). All census tracts within the study area have fewer low income persons relative to the Alberta mean.

Income effects health in multiple ways, including generally, the ability to live in safer, more active transportation-friendly communities, with a variety of options to obtain healthy food within walking or cycling distances. Additionally, one's income can permit or inhibit the ability to access medical or mental health services.

**Educational Attainment (Map 7)**

Those with more education typically obtain higher earnings, which in turn provides greater access to employment opportunities, safe living and working environments, healthier environments and generally better health outcomes. Within the study area only one census tract, which includes the northern part of the Bow River Plains area is above the Alberta mean.

**Aboriginal People (Map 8)**

The population of Aboriginal people within the study area is below the Alberta mean. Although the health of Aboriginal population has been increasing in recent years, Aboriginal populations continue to show a disproportionate burden of disease or health disparities<sup>1</sup>.

**Total Equity (Map 9)**

The total equity score across the entire study area is two out of five, with exception of the census tract that is predominantly within Cochrane. Areas of inequality are spread throughout the study area, rather than focused in a particular location. Generally, the study area has above average populations of children and seniors, and population without a high-school degree. An active transportation plan for Rocky View County should focus on the needs of seniors and children and making connections to schools and community facilities such as libraries, community centres and recreation centres.

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<sup>1</sup> Adelson, N. (2005). The embodiment of inequality: Health disparities in Aboriginal Canada. Canadian Journal of Public Health, 96(2): S45-S61.



**Latent Demand: Commuting Time (Map 10)**

After the completion of the equity analysis, the concept of latent demand was examined through the lens of commuting time. Travel time can be the determining factor for modal choice, with automobiles more attractive as a mode for longer trips. As shown on Map 10, all census tracts within the study area are below the Alberta mean for people with a commute time of 15 minutes or less. This means that on average people in the study area are commuting further for work, which infers that active transportation options less attractive than the automobile.

# MAP 4 POPULATION AGED 19 AND BELOW

ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

Alberta Mean: 25%

### Population aged 19 and below

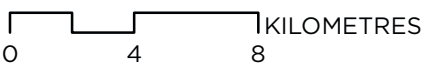
- 23%
- 24-25%
- 26-27%
- 28-29%
- 30-32%

### ROAD NETWORK

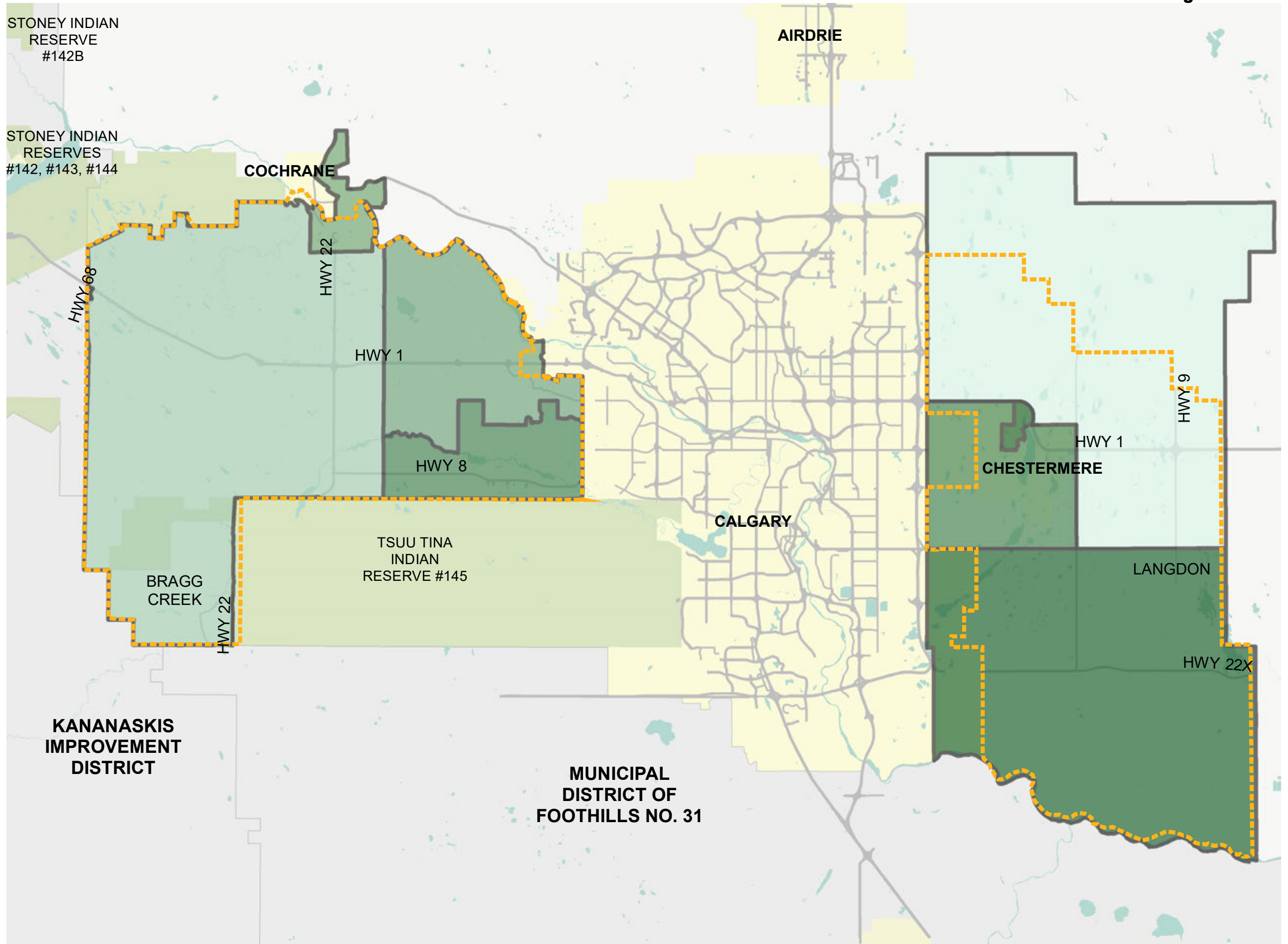
- Primary Hwy
- Secondary Hwy
- Major
- Collector

### OTHER FEATURES

- Hydrology
- Study Area



Data provided by Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced February 2018.

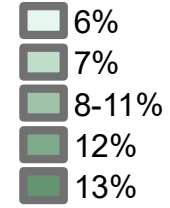


# MAP 5 POPULATION AGED 65 AND ABOVE

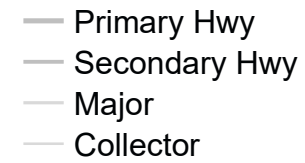
ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

Alberta Mean: 12%

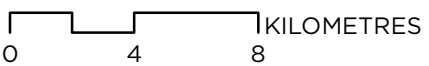
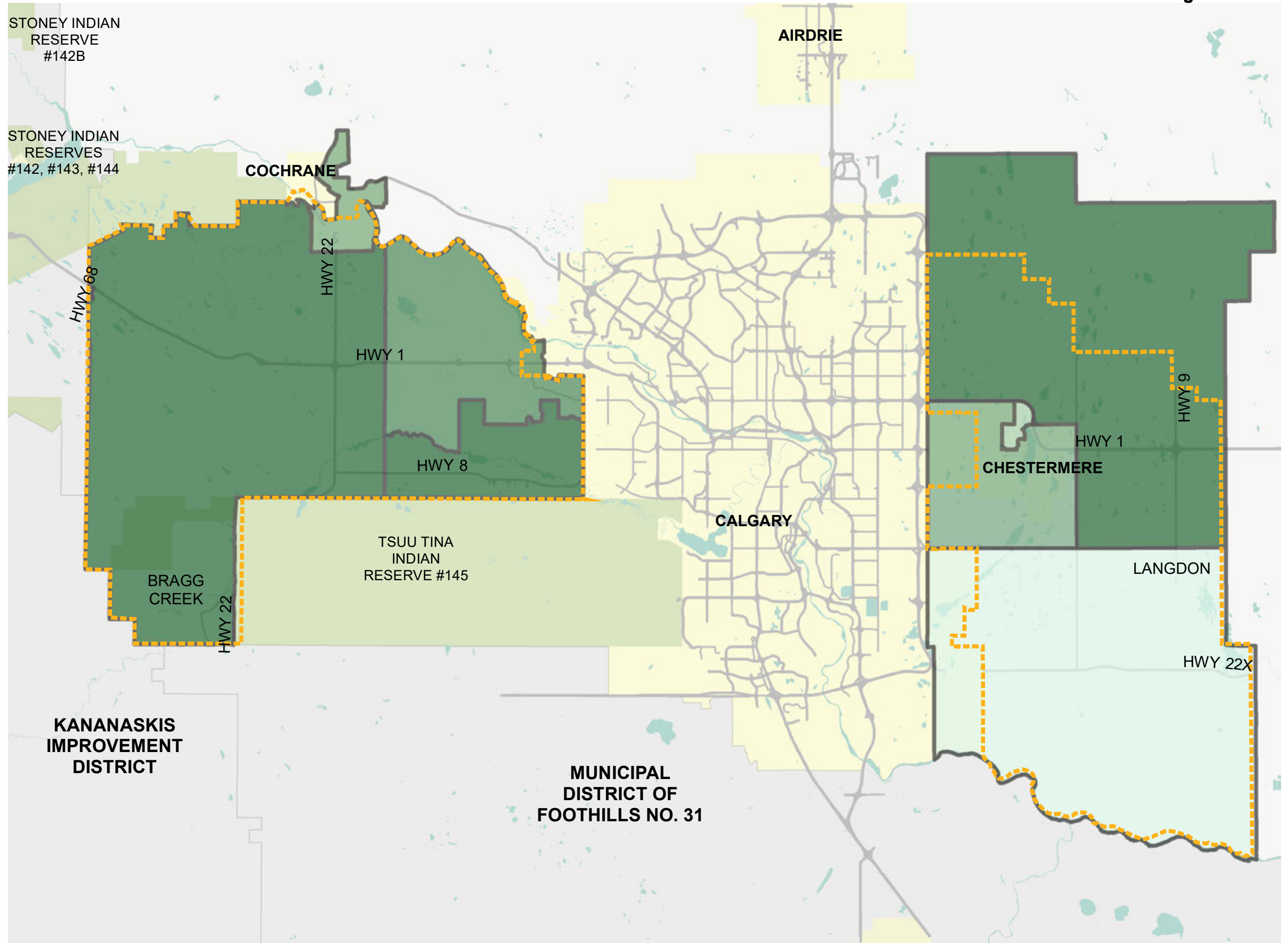
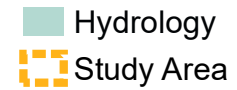
### Population aged 65 and above



### ROAD NETWORK



### OTHER FEATURES



Data provided by Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced February 2018.

# MAP 6 POPULATION LOW INCOME STATUS

ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

Alberta Mean: 9.3%

### Population Low Income Status

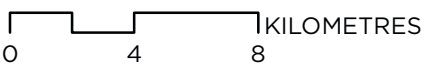
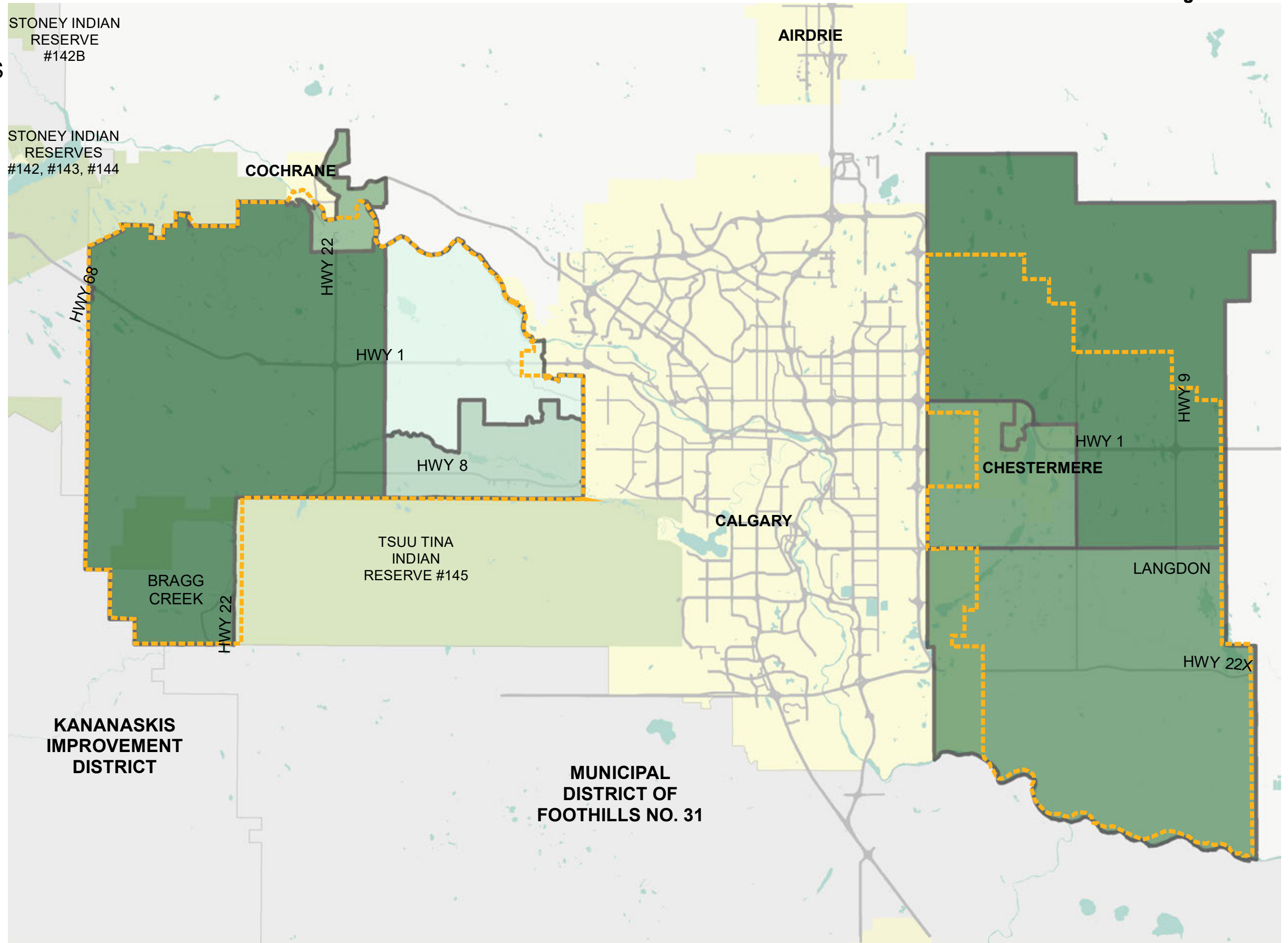
- 3.5%
- 3.6-4.9%
- 5.0-5.5%
- 5.6-5.9%
- 6.0-7.5%

### ROAD NETWORK

- Primary Hwy
- Secondary Hwy
- Major
- Collector

### OTHER FEATURES

- Hydrology
- Study Area



Data provided by Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced February 2018.

# MAP 7 POPULATION WITH NO HIGH SCHOOL DIPLOMA

ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

Alberta Mean: 17%

### No high school diploma

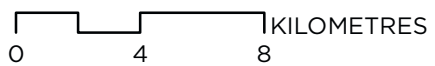
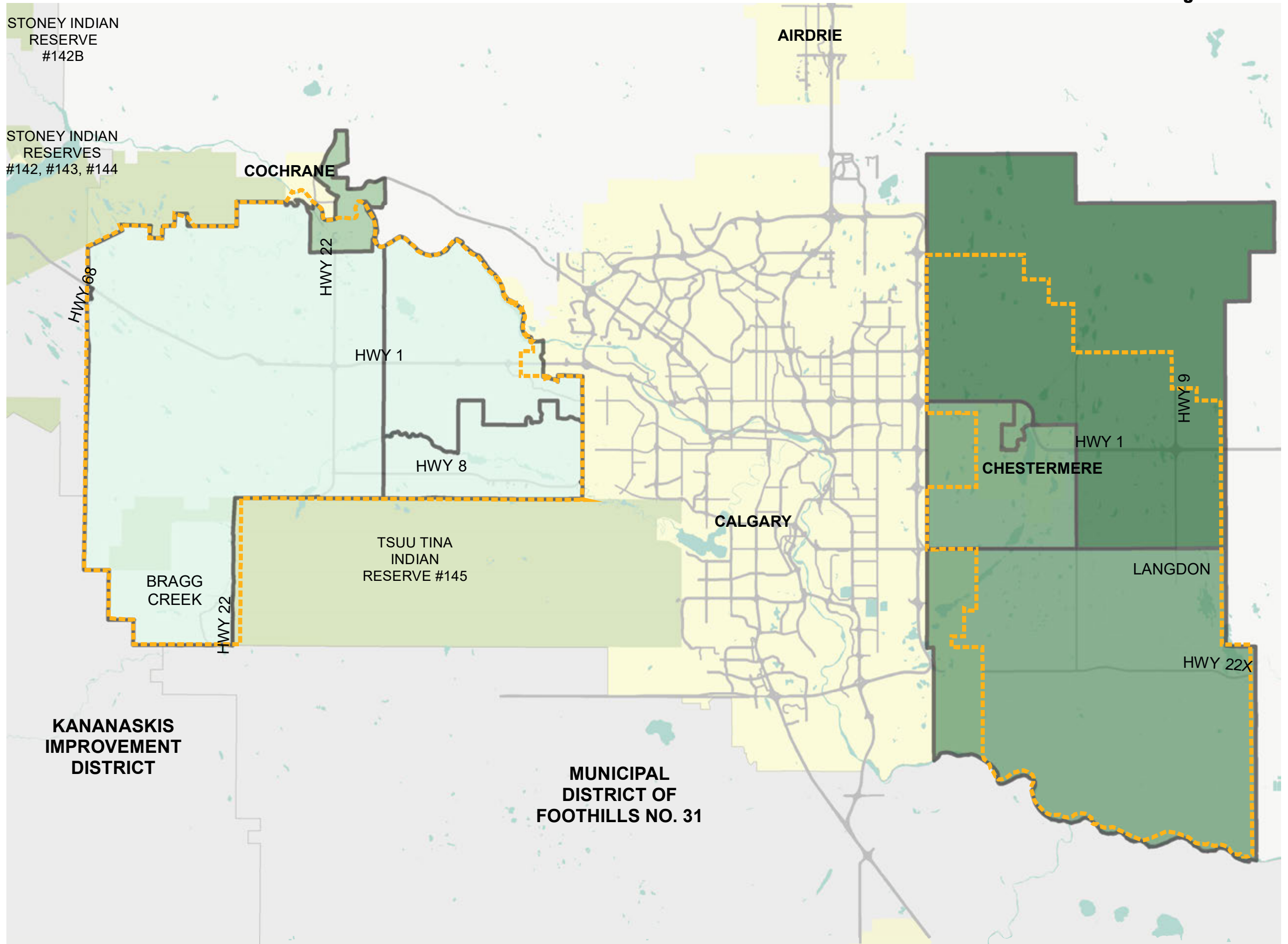
- 9%
- 10%
- 11-17%
- 18-23%

### ROAD NETWORK

- Primary Hwy
- Secondary Hwy
- Major
- Collector

### OTHER FEATURES

- Hydrology
- Study Area



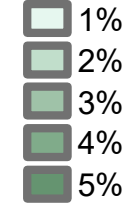
Data provided by Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced February 2018.

# MAP 8 POPULATION WITH ABORIGINAL IDENTITY

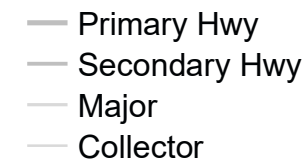
ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

Alberta Mean: 6.5%

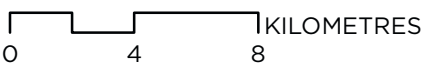
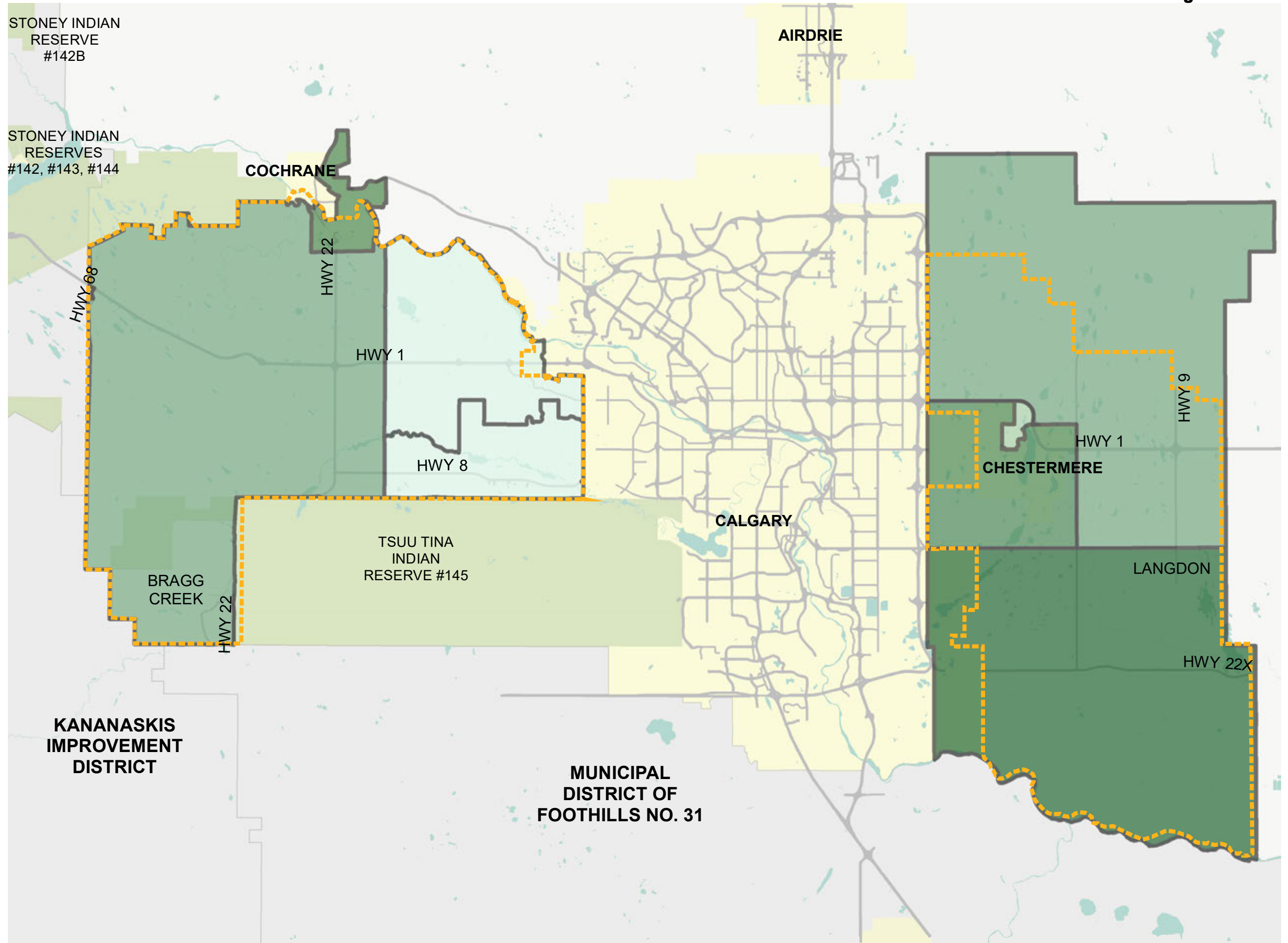
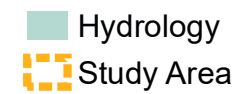
### Population Aboriginal Identity



### ROAD NETWORK



### OTHER FEATURES



Data provided by Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced February 2018.

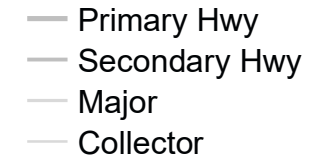
# MAP 9 TOTAL EQUITY SCORE

ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

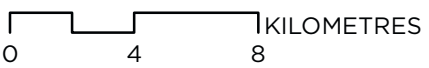
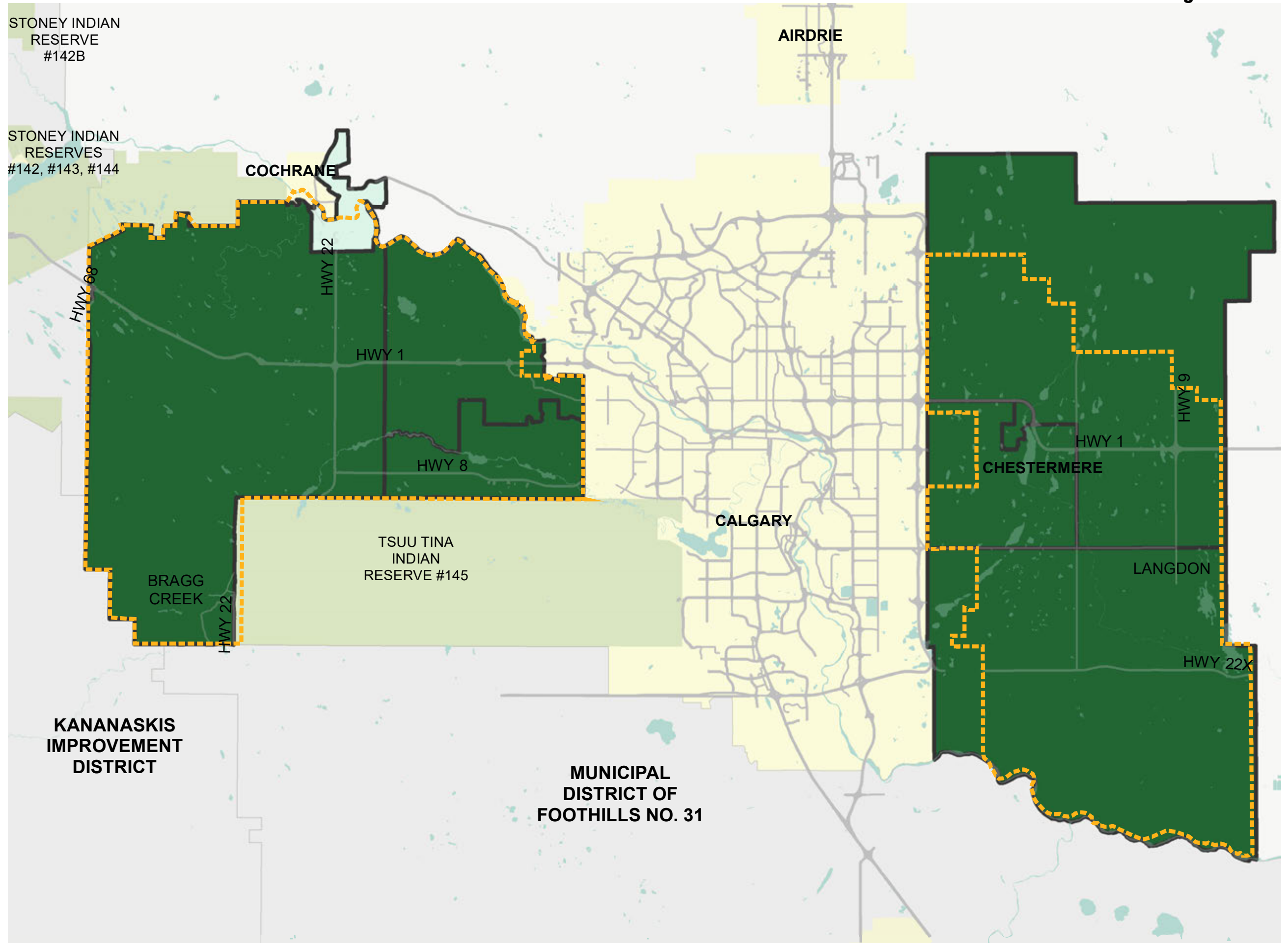
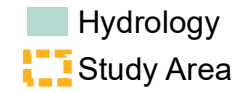
### Total Equity Score



### ROAD NETWORK



### OTHER FEATURES



Data provided by Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced February 2018.

# MAP 10 POPULATION COMMUTE TIME 15 MINS OR LESS

ROCKY VIEW COUNTY  
ACTIVE TRANSPORTATION PLAN

Alberta Mean: 27%

### Commute time 15 mins or less

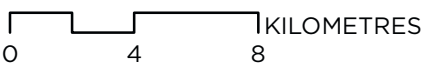
- 9%
- 10-11%
- 12-14%
- 15-16%
- 17-24%

### ROAD NETWORK

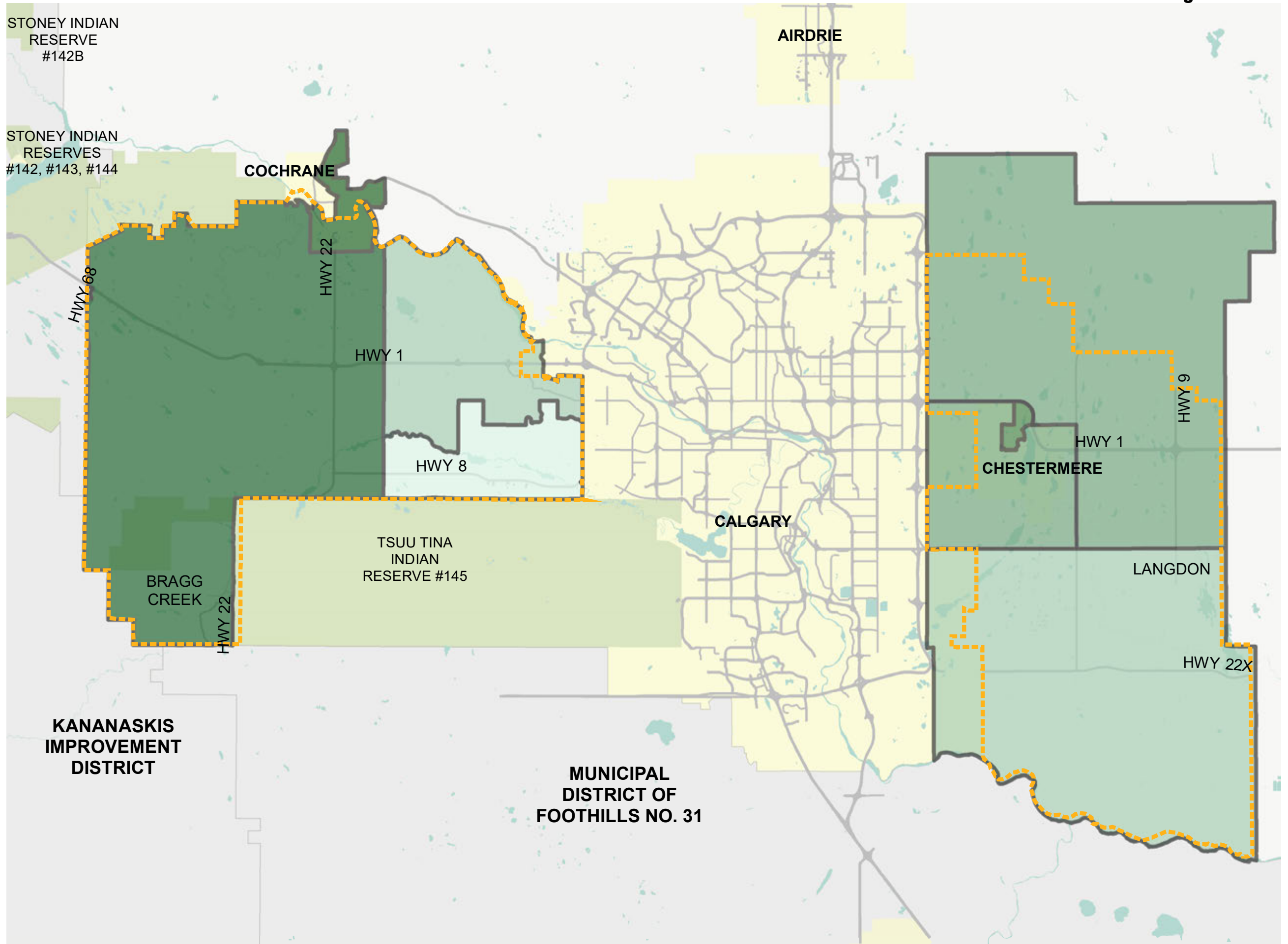
- Primary Hwy
- Secondary Hwy
- Major
- Collector

### OTHER FEATURES

- Hydrology
- Study Area



Data provided by Rocky View County,  
City of Calgary, Town of Cochrane and AltaLis.  
Map produced February 2018.





APPENDIX E

# Needs Assessment

# Active Transportation Needs Assessment

## Technical Background

This appendix provides the technical background supporting the findings presented in Section 3 of the report.

### Developed Rural Core

The needs assessment in these areas are focused on safely and comfortably connecting people to key destinations within their community.

### Walksheds

The convenience and simplicity of walking makes it a practical way to get around for those who live in these more densely populated areas in the County. For most people, 800m (about a 10 minute walk) is a reasonable distance to access local destinations<sup>1</sup>. This means that the pedestrian network within 800m of key destinations should allow for safe, comfortable and direct walking trips.

“Walksheds” are a good measure of the walking network around a destination. A walkshed considers the available transportation network, typically sidewalks, pathways, bike routes and shared streets. The walkshed method differs from a simple 800m radius around a destination because most walking routes are rarely in single straight line. The walkshed definition helps to focus attention on walking networks and infrastructure and can also help identify gaps in the network.

In a well-connected grid street system with sidewalks (or local streets without sidewalks that are comfortable for pedestrians), the walkshed is a diamond shape. **Figure 1** shows an example of a well-connected walking network in Boston. Conversely, **Figure 2** illustrates a poorly connected walking network in the same city, where much of the area within the 800m straight line distance is a much longer walk than 800m and where there walking network is lacking or cut off, either by commuter rail right of way and highway on and off ramps.

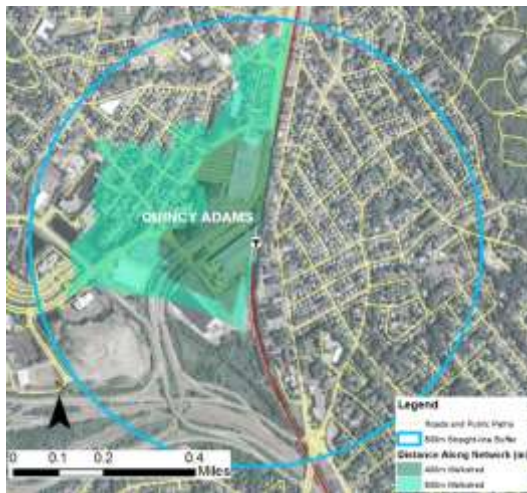
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<sup>1</sup> National Highway Traffic Safety Administration (NHTSA), 2012 National Survey of Bicyclist and Pedestrian Attitudes and Behaviors - Volume 2: Findings Report.  
<https://www.nhtsa.gov/sites/nhtsa.dot.gov/files/811841b.pdf>

Figure 1. Example of a walkshed in well-connected network. Source: MBTA Data Blog (2016)



Figure 2. Example of a walkshed in poorly-connected network. Source: MBTA Data Blog (2016)



Walksheds for 400m and 800m have been developed around schools, retail areas and significant community and recreation centres to identify deficiencies in the existing network and to focus future improvement efforts to maximize the value of investment in walking facilities. Although no walkshed analysis was conducted for smaller neighbourhood parks and tot-lots, there should be priority placed on the 400m walksheds around these facilities as well, with a particular emphasis on safety.

**COMMERCIAL/RETAIL AND COMMUNITY FACILITIES**

Commercial/retail land uses in the context of those in Langdon and Bragg Creek are well-suited for access by active modes. Unlike major regional centres such as Cross Iron Mills, most retail businesses in the County cater to convenience needs and don't involve the need to carry large items. Community centres include significant recreation facilities within hamlets and suburban areas, such as the Bragg Creek Community Centre, sports fields and field house in Langdon, or the recreation centres in Indus and Springbank. The nature of activities associated with these locations attract a wide range of patrons, including those that are dependent on walking or cycling as their primary or only method of travel.



## SCHOOLS

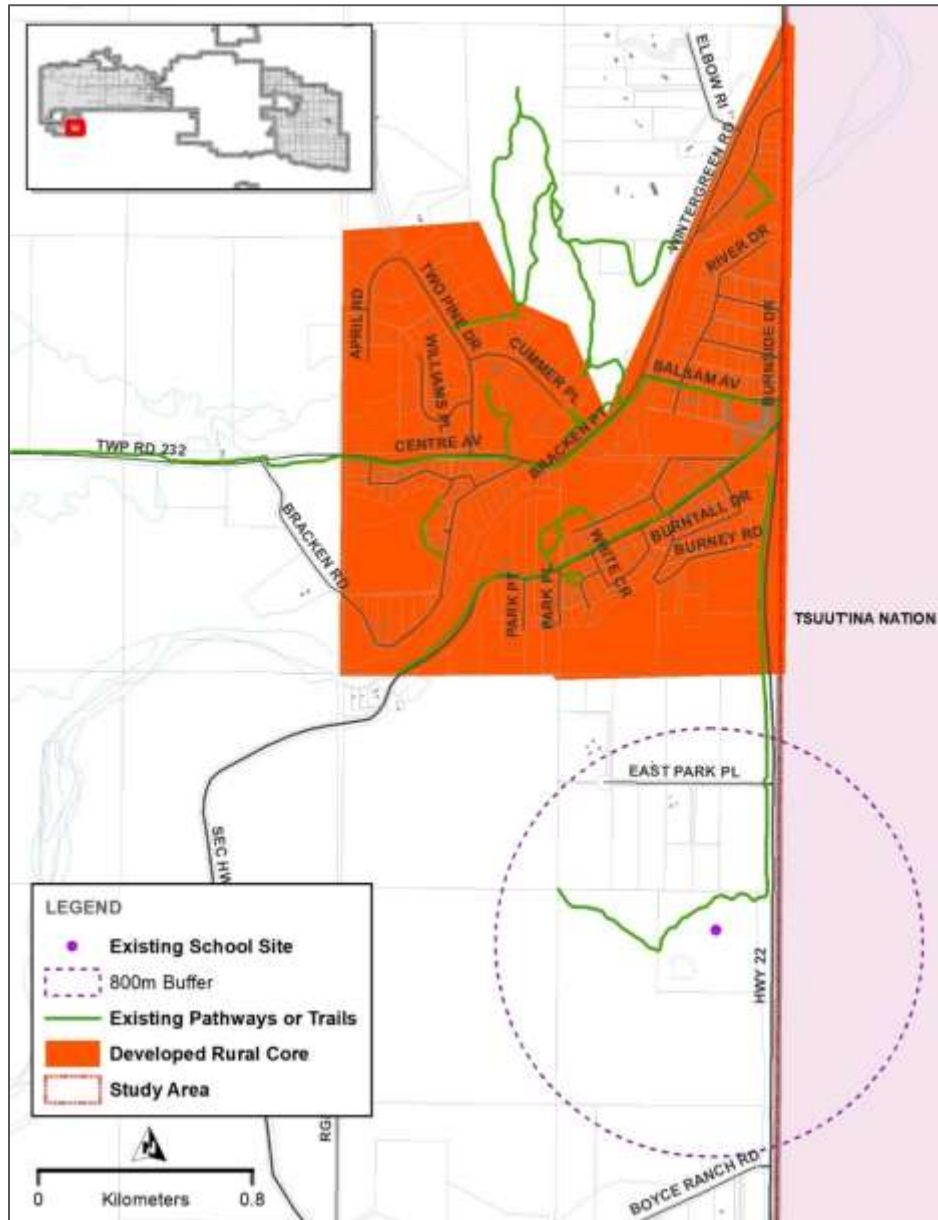
Safe access to schools is a priority within all communities. High traffic volumes around schools along with a lack of safe walking and cycling facilities are a concern for many parents, teachers, and school administrators and result in many children being driven to school rather than walking. This in turn adds to the volume of traffic around schools. Safe walking and cycling facilities around schools are an important component of encouraging active modes and reducing the amount of vehicle traffic around schools.

Additionally, students in junior high and high schools seek a greater level of independence than elementary students and often don't want to be reliant on parents and other adults for travel. This group depends on walking and cycling as their only forms of truly independent travel. For junior high and high schools, safety is a major consideration, recognizing that direct and comfortable routes will help to promote safe behaviour for this age group. Around elementary schools, with a younger demographic that may not have the confidence, road knowledge or skills should focus on safety for all ages and abilities.

### **Bragg Creek**

Bragg Creek includes a residential area, elementary school and commercial centre. Banded Peak School is located on Highway 22, approximately 1.8km south of the core of the hamlet. The area around the school is shown on **Figure 3**. Due to the school's location, there is almost no development within an 800m walking distance of the school. There is an existing aggregate pathway that connects the town centre to the school, but it is not a year-round connection.

Figure 3. Sarah Thompson School near Bragg Creek



Only a small portion of the hamlet is within a 2km cycling distance to the school and cycling on highway 22 on the gravel pathway are neglected for the connection. Given that this is an elementary school, areas beyond 2km are outside the practical cycling distance for most students. Improved active transportation facilities will not have significant benefit for school access.

The Bragg Creek Community Centre is located on the south side of White Avenue (Highway 758) near the intersection with Highway 22. It includes a gymnasium, several meeting rooms and other rooms used for various purposes. The centre is used to host a variety of programs and special events. The Bragg Creek Snowbirds Seniors Fellowship Centre is located on Balsam Avenue. The Seniors Centre has been identified as a significant facility with regard to



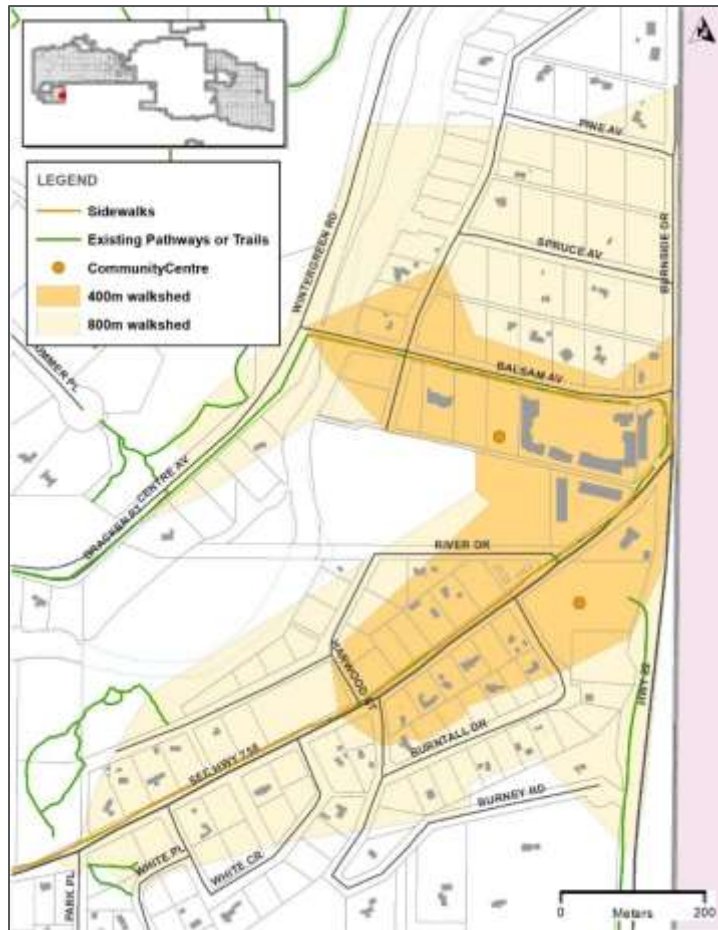
active transportation. Older seniors may have a high reliance on walking, and specifically high quality walking facilities, while more active seniors may be interested in walking and cycling to their destinations for fitness.

**Figure 4** shows the 400m and 800m walksheds for these facilities. The existing multi-use pathway provides good connectivity for much of the populated area of the hamlet within 800m. The local streets noted as walkable, however, with the exception of the multi-use pathways on White Avenue and Balsam Avenue, there are no sidewalks and many of the streets are gravel. There are no pedestrian facilities on Highway 22. Given the high speeds and volumes, it is not considered part of the walkable network and does not provide access to much of the community.



Pathways between the retail centres enhance the connectivity in the area.

**Figure 4. Bragg Creek Community Centres and Retail Centres with 400m and 800m Walksheds**

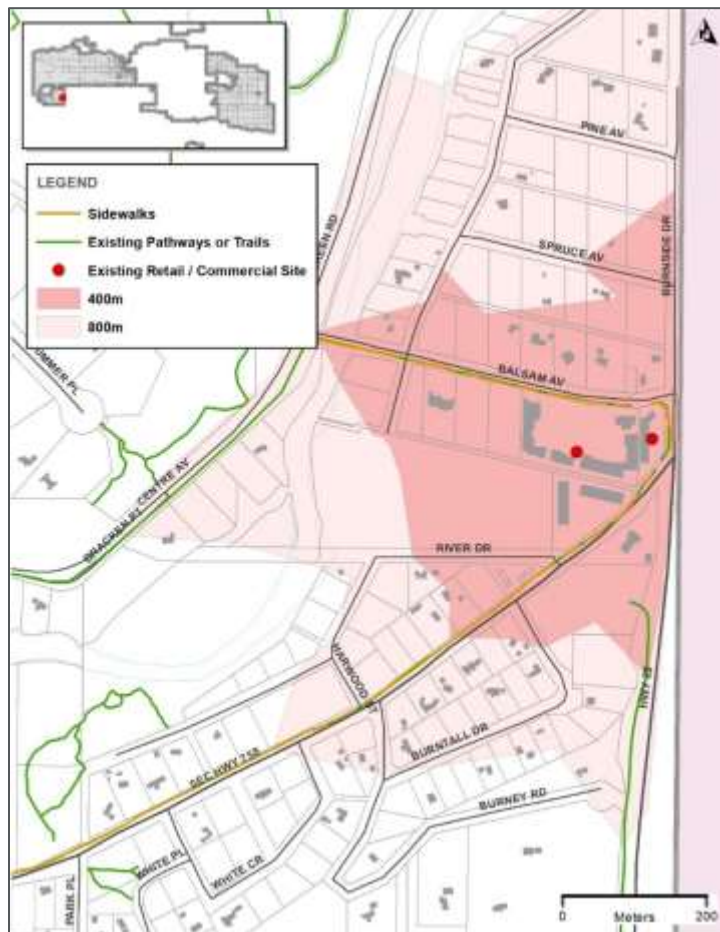


There is a distinct gap in the walking coverage immediately to the west of the Snowbirds Seniors Fellowship Centre. This lack of connectivity is not a significant concern at this time as the level of development on that parcel is low. If the owner intends to develop in the future, active transportation connections between the property and the existing network and directly to the retail centre to the east should be provided.

There is a grouping of commercial/retail businesses at the junction of Highway 22 / White Avenue / Burnside Drive, with frontage onto White Avenue, Burnside Drive and Balsam Avenue. The various properties are well connected through parking lots and short pathways between them.

Figure 5 shows the 400m and 800m walksheds for the retail/commercial centre of Bragg Creek.

Figure 5. Bragg Creek Retail with 400m and 800m walksheds



The 2km cycling routes around the retail/commercial and community centres follow White Avenue (Highway 758 to the southwest). The multi-use pathway continue in various forms for approximately 1.7km, providing a comfortable cycling route between the hamlet and Bragg Creek Provincial Park. Beyond the entrance to the park, there are no paved shoulders on the highway.



To the west, the multi-use pathway on Balsam Avenue continues west and crosses the Elbow River to Wintergreen Road / Centre Avenue. Balsam Avenue is the only crossing of the Elbow River in Bragg Creek. The residences along Bracken Road and Echlin Drive are generally within the 2km cycling distance of the centre of Bragg Creek. There are on Centre Avenue, but are narrow; in the order of 1.0m. The West Bragg Creek Trail parallels Centre Avenue / West Bragg Creek Road and Range Road 54. It is a gravel trail constructed in 2017. There are no shoulders on Wintergreen Road.

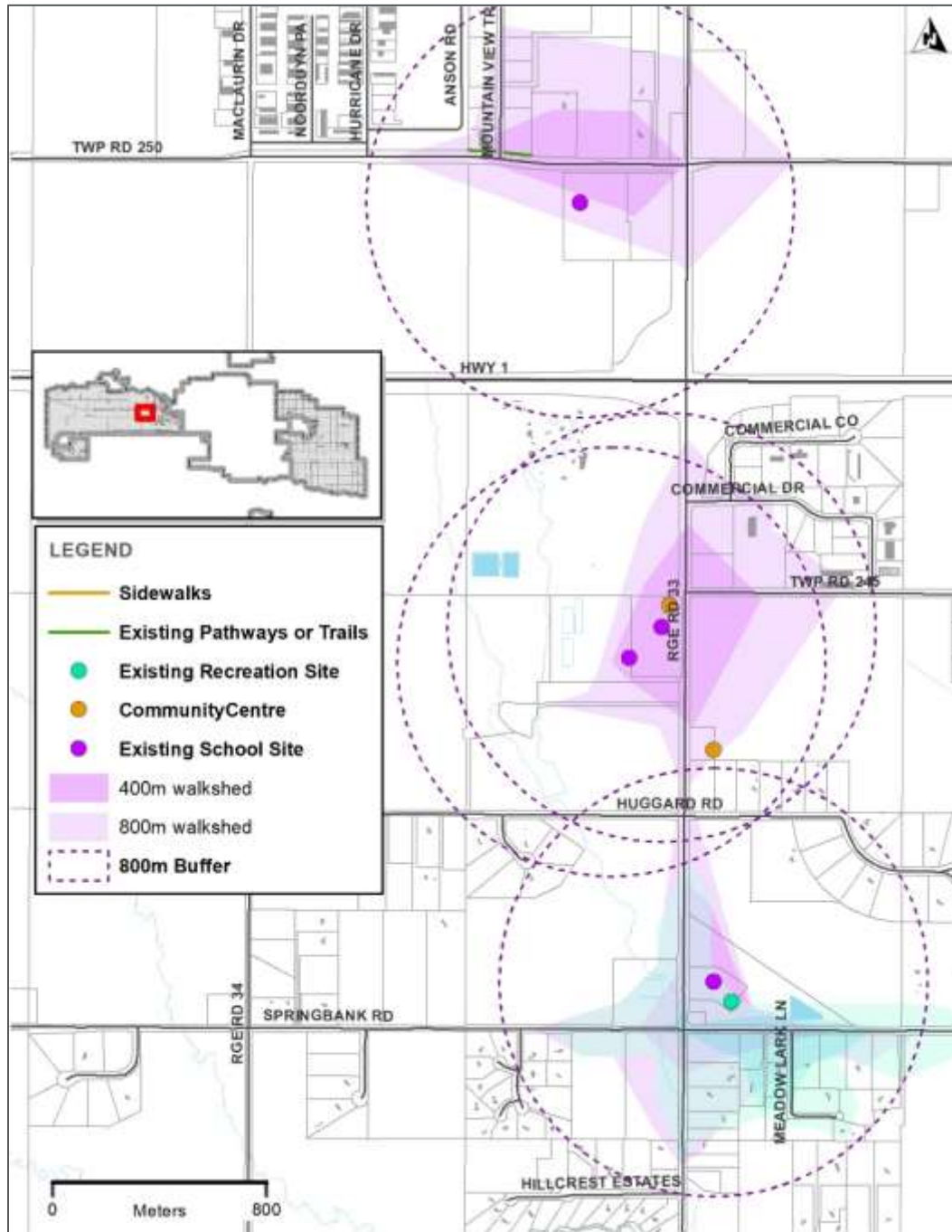
### Springbank

There are several community destinations on Range Road 33, north of Springbank Road, including a recreation centre (Springbank Park) and high school at the intersection of Springbank Road and Range Road 33. Springbank Middle School and Elbow Valley Elementary School are on the west side of Range Road 33, approximately 1.4km north of Springbank Road. There is also a collection of low intensity industrial uses as well as Calaway Park near the junction with Highway 1. The Edge School, a specialty school for high performance athletes is located north of Highway 1 near the corner of Township Road 250 and Range Road 33.

**Figure 6** shows the 400m and 800m walksheds for the schools and recreation centre. As the figures shows, the walksheds generally limited to Range Road 33 and Springbank Road. The posted speeds, traffic volumes and lack of pedestrian facilities render Range Road 33, Springbank Road and Township Road 250 as poor walking environments.



Figure 6: Figure 8: 400m and 800m Walksheds, Springbank Road and Range Road 33 Schools and Recreation Centre



A school zone speed of 30 km/h exists on Springbank Road in front of the high school and on the approach to Range Road 33. However, aside from the signage, there are no other cues to slow drivers along this straight, flat section of roadway. There are also no pedestrian crossings of Springbank Road. Similarly, the school zone speed limit is 30 km/h in the vicinity of the middle and elementary schools.



## Harmony

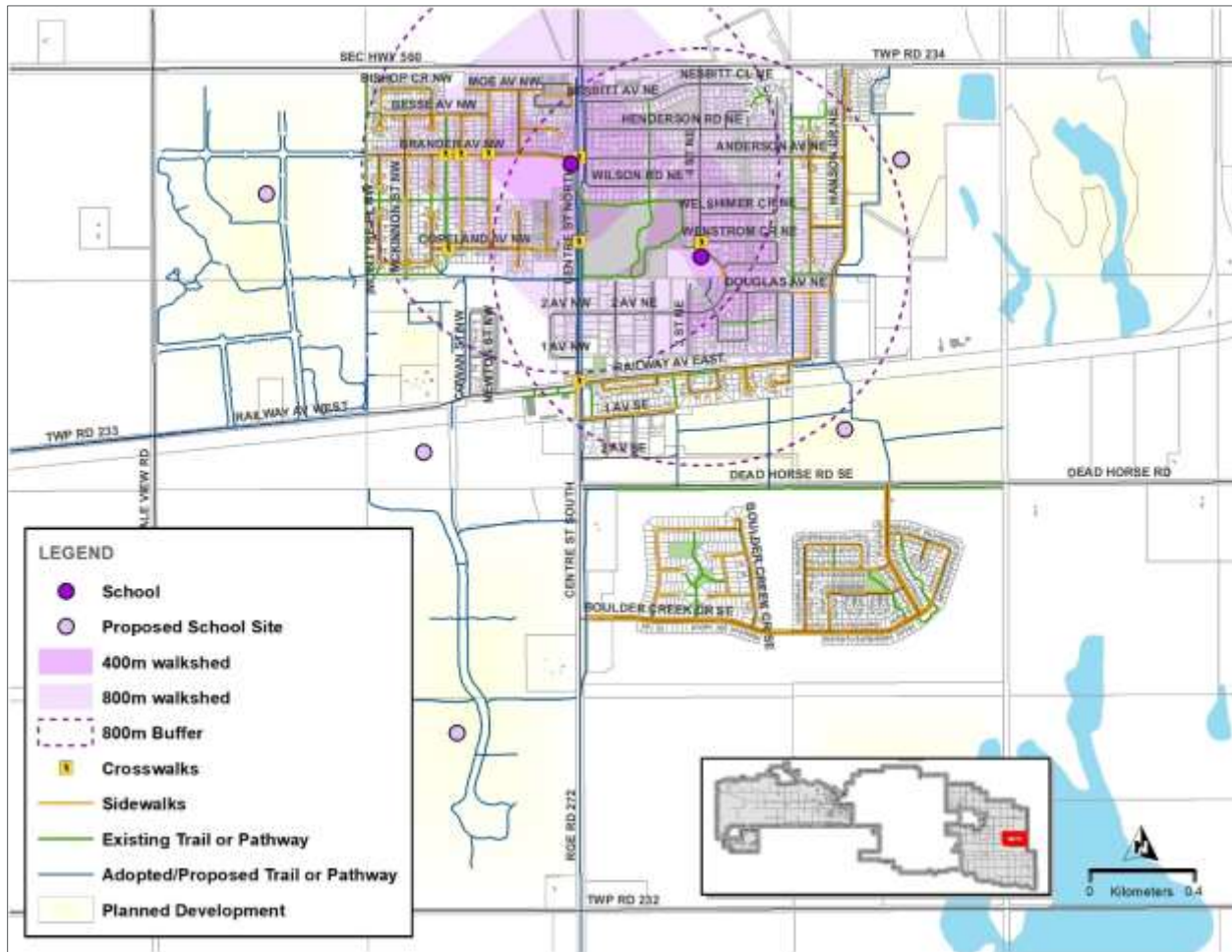
Harmony is a significant future neighbourhood located east and north of the Springbank Airport that is currently being developed. Due to the scale of this development, it is included in the discussion of developed urban core areas. Strong connectivity for active modes is inherent in the Harmony plans. As a principle, the development seeks to have 75% of residential units within a 400m walk of the edge of the village core, and 90% within 800m. This will be achieved through a highly connected trail system.

## Langdon

Langdon is Rocky View County's largest hamlet. The hamlet covers 4.4 km<sup>2</sup> and has a variety of land uses that provide day-to-day destinations. When compared to other developed areas in the County, Langdon is a relatively dense. The form and function is similar to many suburban neighbourhoods in Calgary, Chestermere or Airdrie. Langdon is one of the fastest growing communities in the greater Calgary region, and due to the hamlet's size, scale, demographics, and layout, there is potential to enhance and encourage active mode travel.

Currently, there are two schools in Langdon: an elementary and elementary/middle school. The 400m and 800m walksheds for these schools are shown on **Figure 7**. The figure indicates that both schools are well-connected for walking. A high proportion of the area within these walksheds is residential and therefore represents potential walk-to-school trips. Although the walkshed for the Sarah Thompson Elementary School indicates strong connectivity, the routes rely on students sharing the roadway for most of the trip. As of 2018, the County has funding to enhance active transportation connections along 4 Street NE, north of the school. As for Langdon School, there are sidewalks (one at least one side of the street) and pathways within the 400m and 800m walksheds.

Figure 7: 400m and 800m Walksheds, Langdon School and Sarah Thompson Elementary School

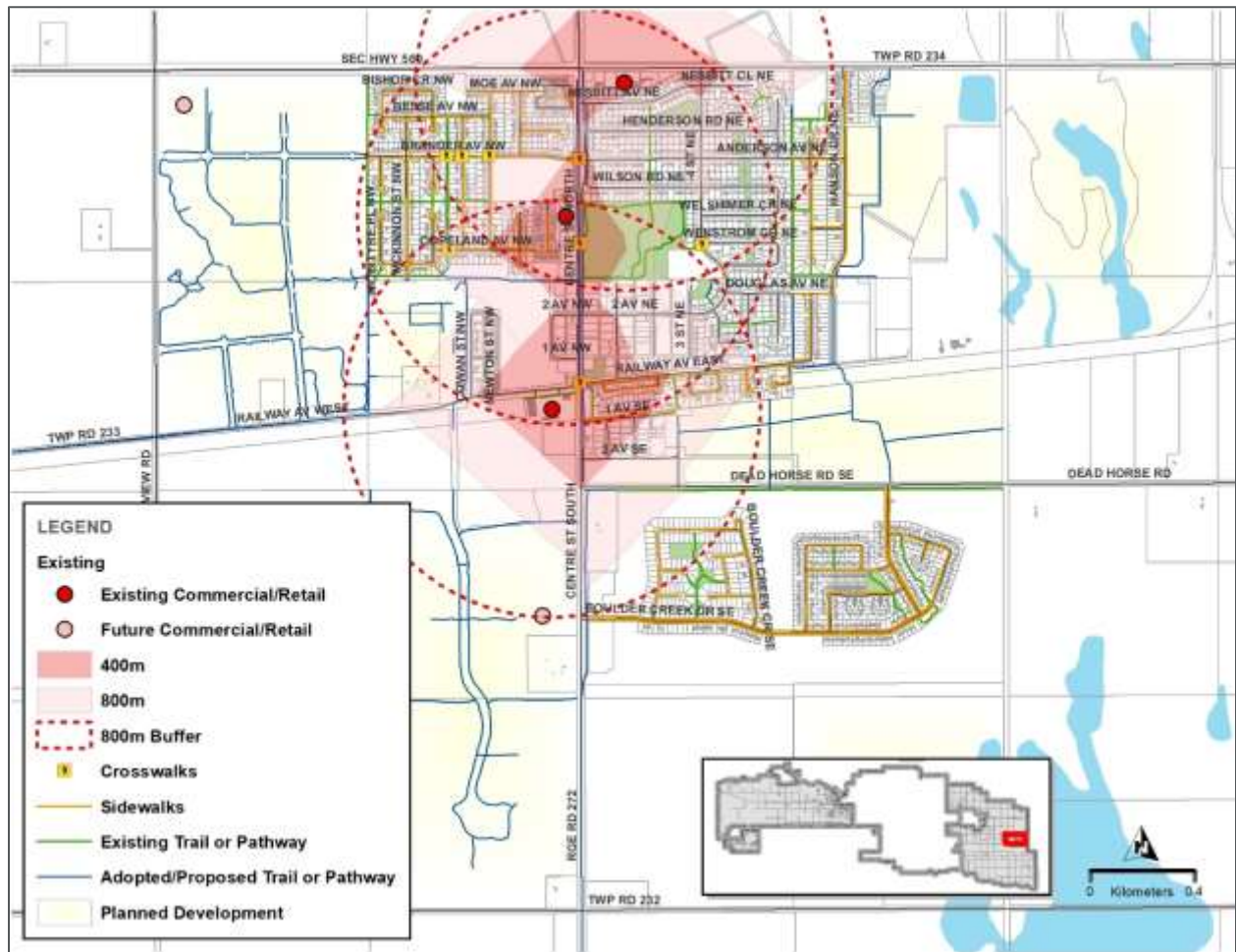


The straight-line distance between Sarah Thompson Elementary School and Boulder Creek is approximately 1km for most of the community, but over 2km cycling distance and relies on cycling on Centre Avenue / Highway 797. The Langdon School is approximately 2.5km cycling distance from Boulder Creek, which is reasonable for older middle school students, but again, the routes rely on cycling on Centre Street.

There are three primary retail areas in Langdon:

- Southeast corner of Range Road 234 (Highway 560)/Centre Street;
- Centre Avenue south of Langdon School; and
- Southwest corner of Centre Street/Railway Avenue.

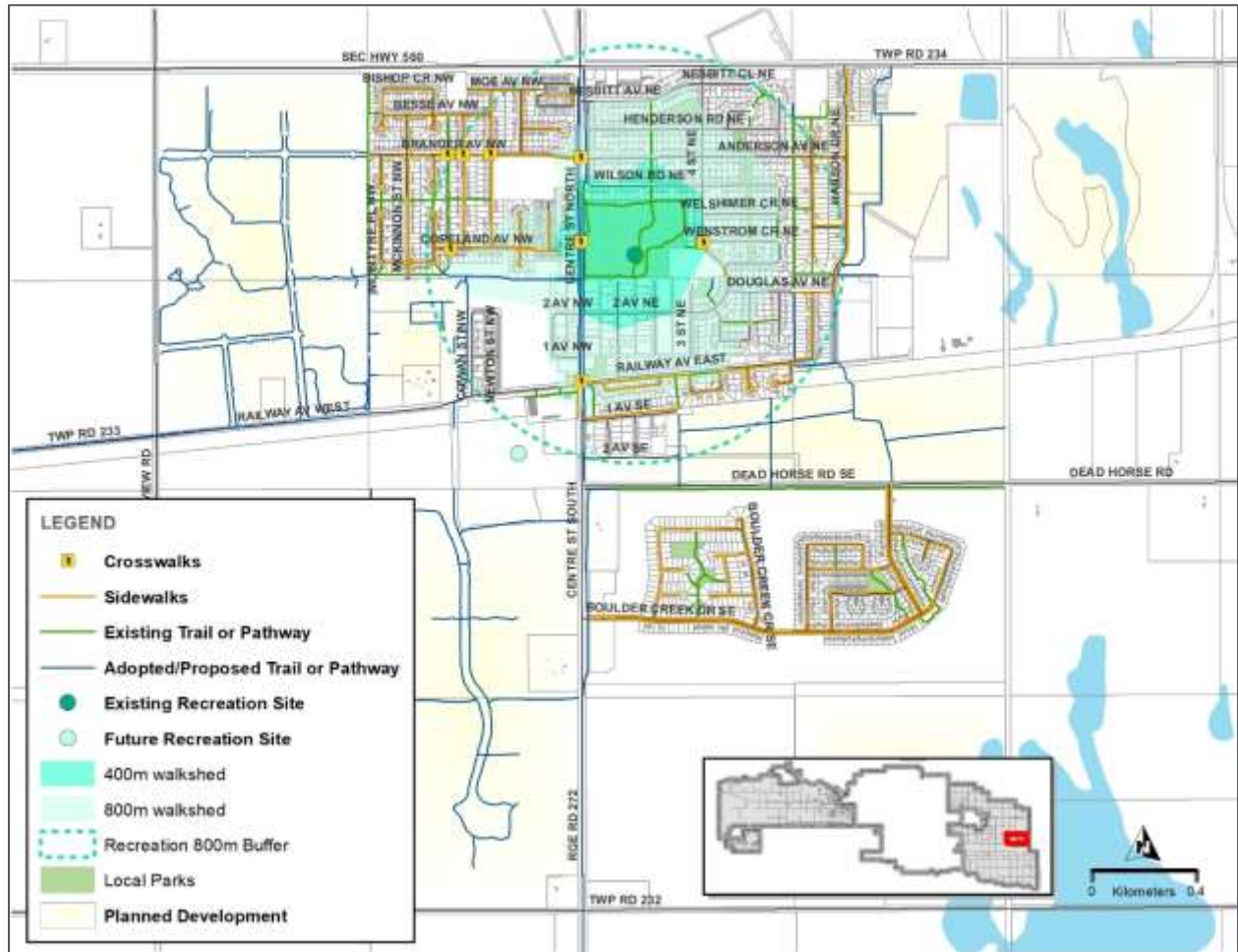
Figure 8: 400m and 800m Walksheds, Langdon Retail



The walksheds for these three areas are shown in **Figure 8**. The walksheds show reasonably strong pedestrian connectivity, but the lack of pedestrian facilities along Centre Street makes pedestrian access to them uncomfortable. The retail centre fronting Range Road 234 has no pedestrian access and pedestrians must walk on the shoulder of Centre Street or Range Road 234 to access the businesses. The retail businesses on Centre Street south of Langdon School are better connected, particularly within the area of Langdon on the west side of Centre Street. The area on Railway Avenue has sidewalks along the property frontage, and a crosswalk on Centre Street at Railway Avenue, but little connectivity beyond that point.

There are parks and recreation facilities of varying scales throughout Langdon, including local parks and tot lots, and major recreation and playfield sites, like Langdon Park. The 400m and 800m walksheds for the park are shown on **Figure 9**. Given the central location, and existing pathway network, the walking connectivity is relatively good, with minor gaps. Along the west side of Langdon Park, along Centre Street, there is a multi-use pathway providing connection to the park.

Figure 9: 400m and 800m Walksheds, Langdon Park

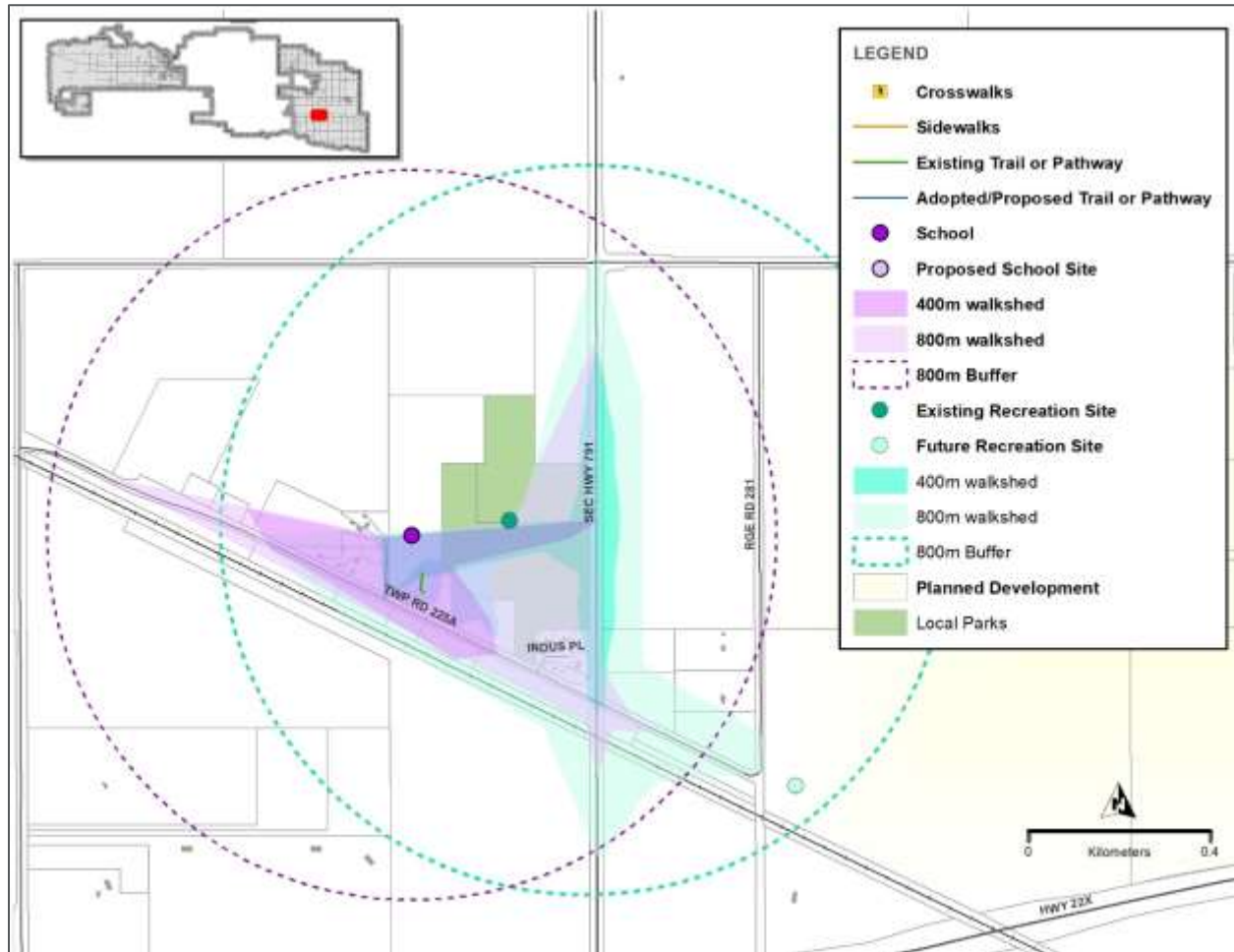


**Indus**

Indus is a small community near the southern edge of the County that includes an elementary school, recreation centre and a few residences. The recreation centre includes a rink, function hall, meeting room, riding arena and softball diamonds. Although next to each other, there is no formal walking connection between the school and recreation centre. Figure X shows the 400m and 800m walksheds around the school and recreation centre. The small size of the community and the lack of road network means the walksheds cover a small area. However, most residences are within or very close to being within the 800m walkshed of the school. With no formal connection between the recreation centre and school, most residences are outside of 800m walking distance from the recreation centre, but it is possible to walk across the school field to significantly shorten the trip.

The distance between the recreation centre and Langdon is approximately 13km, which is beyond the distance that would be considered convenient for day-to-day trips, but given the nature of the recreation centre, there may be some demand for cycling trips between Langdon and Indus.

Figure 10: 400m and 800m Walksheds, Indus School and Indus Recreation Centre



**Conrich**

The hamlet of Conrich is very small, with no notable community facilities that would be destinations for those outside the hamlet. The only school, the Khalsa School Calgary, is a specialty school that draws students primarily from the northeast area of Calgary.

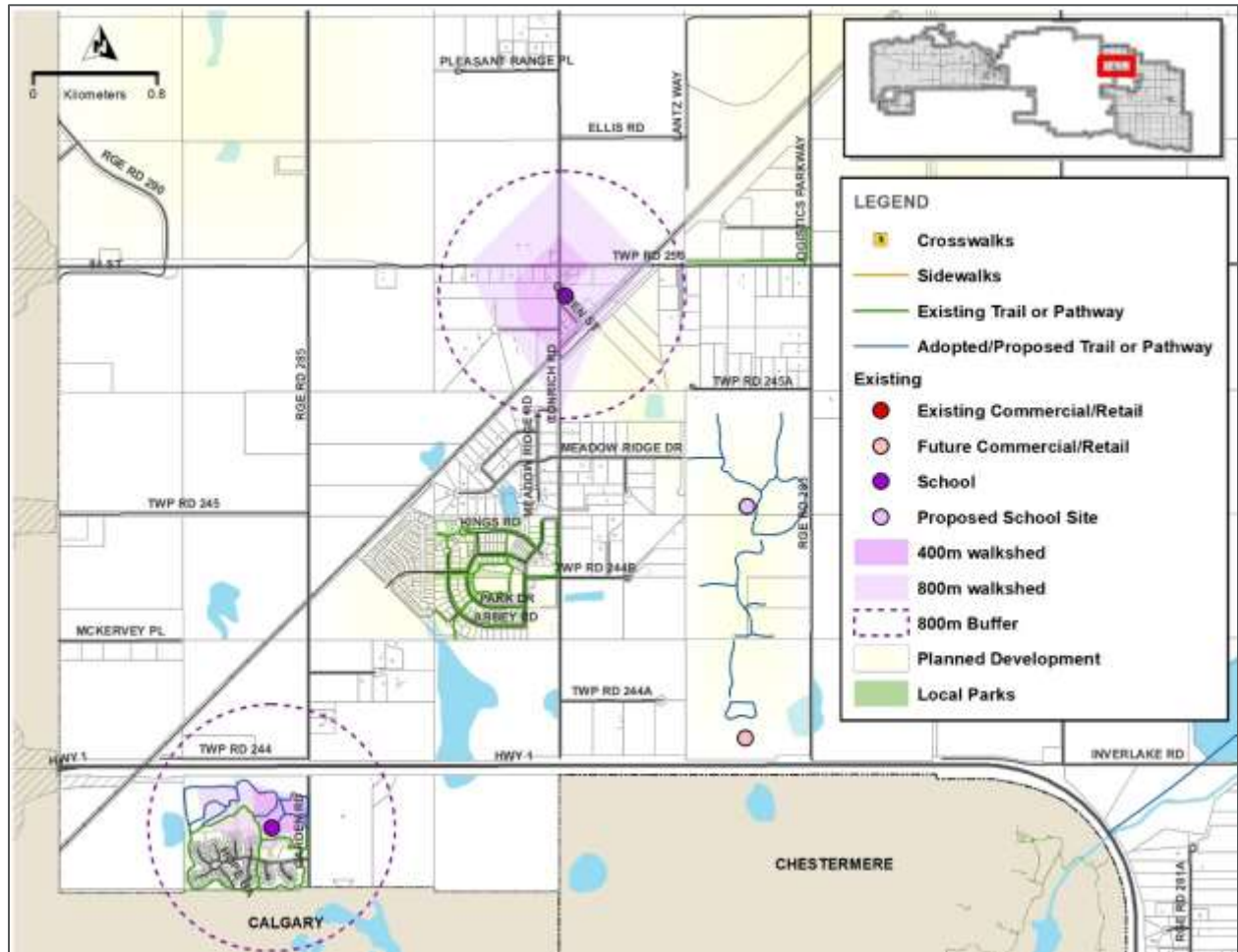
The Cambridge Park neighbourhood, just south of the hamlet is a suburban subdivision with multi-use pathways throughout.

Prince of Peace school is located on the south side of Highway 1 with good connectivity to the community surrounding it.

The Conrich Area Structure Plan includes significant industrial, commercial and residential development, including expansion of the hamlet. Although there is little activity today that will generate active transportation demand, future growth in Conrich and the neighbouring communities will generate future demand.

Figure 11 shows the existing areas of development and active transportation facilities.

Figure 11: Current Land Use and Active Transportation Connections, Conrich



**Other Areas**

There are other existing and planned neighbourhoods that are suburban in nature. In general, newer developments include pathways along major streets, but do not include sidewalks.

APPENDIX F

# Pathway Development Process Guidelines



## Pathway Development Process Guidelines

These guidelines provide a summary of the steps and requirements for planning, designing and implementing new pathways in Rocky View County. The guidelines apply to regional pathways, other pathways within a road right-of-way or any pathway that will ultimately be maintained by the County. The Guidelines do not apply to natural trails, equestrian trails or private pathways that will remain private. The County encourages developers and non-profit organizations to take a lead role in identifying active transportation needs, potential infrastructure and funding opportunities within the context of the Active Transportation Plan. To be part of a long-term network, pathways need to be developed in a logical manner and to a consistent quality throughout the County.

There are five main stages to pathway development:

- Pre-Planning – to identify critical constraints
- Planning – to sufficiently develop a concept to demonstrate feasibility and allow for preliminary approval
- Design – detailed engineering leading to construction documents approved by the County
- Construction – by a qualified contractor procured through a process consistent with Rocky View County procedures
- Operation – to be determined on case-by-case basis, but generally the responsibility of the County

Pathway development is similar to developing a roadway, with all of the same considerations. Although primarily used for pedestrians and cyclists, the same planning, design, construction and operational principles apply to pathways as roadways.

These guidelines are focused primarily on Pre-Planning and Planning, with some discussion of Design. The Construction and Operation stages need to be addressed by the County on a case-by-case basis.

### Pre-Planning

The first step in any project is to determine if it worthwhile pursuing. There are several proposed and approved pathways and trail on various planning documents. Inclusion of a pathway in a planning document does not ensure its feasibility. In most master planning documents, pathway locations are indicative only and represent the general location of a desired pathway or other active transportation facility.

**Purpose:** The pre-planning stage is intended to identify potential “show-stoppers”

**Deliverables:** None

**Preparation:** It is recommended that the proponent obtain property mapping from Rocky View County to help determine if there is sufficient right-of-way, and there should be walk-through of the proposed to observe conditions.

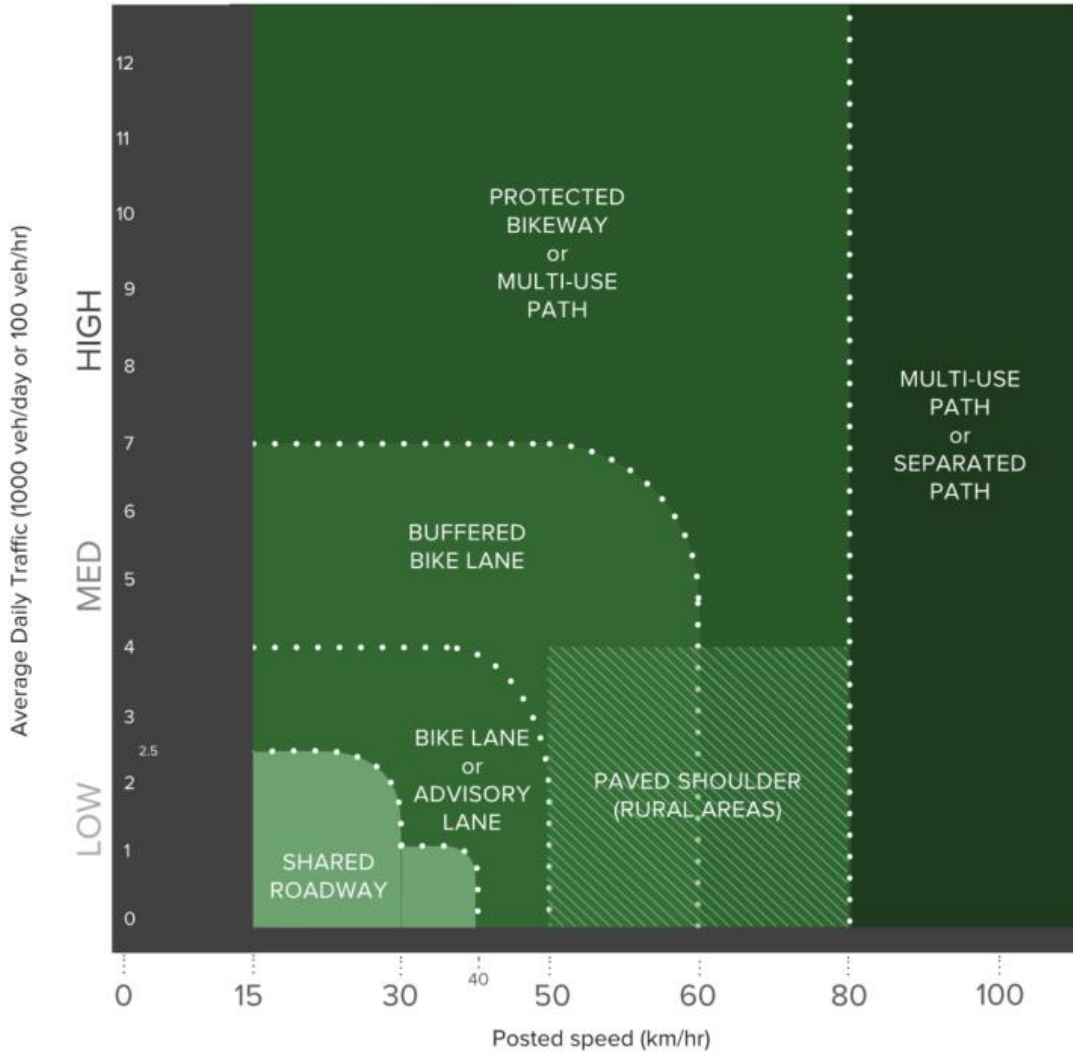
**Approval:** There is no approval at this stage, but proponents are encouraged to meet with Rocky View County staff to review the project and to develop the scope of the analysis for the Planning stage.

**Report, Drawings and Analysis Requirements:** Completion of a pre-planning analysis does not guarantee that a project will be feasible or will be approved. In many cases, further investigation is required to confirm feasibility. However, identification of significant challenges at this stage may save considerable effort in developing a project that has little chance of success within a reasonable cost. Examples of the types of issues that should be considered at the pre-planning stage are:

- Is the facility shown on the Active Transportation Plan – South County network?
- If no, is it shown on as approved or proposed on any other planning document? If yes, please indicate the document:
- Using the facility selection tool, is a shared use pathway an appropriate active transportation facility?
- If the posted speed on the adjacent roadway is 70 km/h or more, is there at least 8.0m from the edge of the lane to the right-of-way line?
- If the posted speed is less than 70 km/h, is there at least 5.0m from the edge of the lane to the right-of-way line?
- Are there steep slopes on the side of the highway that will impede pathway construction.
- Are there stream crossings required, and/or is there significant water in the area the pathway will be constructed?
- Are there multiple driveways or local road intersections with spacing less than 25 apart?
- Are there buried utilities that will impact pathway construction (look for gas line and other signage, fire hydrants, etc. for indicators)

This is a relatively short checklist and only represents a small list of serious issues. Most issues that will determine feasibility require a planning-level review to identify issues and potentially mitigation.

### ROCKY VIEW COUNTY BICYCLE FACILITY SELECTION TOOL



SHARED ROADWAY includes:  
BIKE BOULEVARD, YIELD ROADWAY,  
PEDESTRIAN LANE, AND ADVISORY LANE

## Planning

**Purpose:** The planning stage identifies the general pathway requirements and concept plans. It is used to support funding applications and gain support in principle for the concept.

**Deliverables:** Report, describing the concept, the rationale for the concept and key issues to be addressed in design, along with concept sketches showing the horizontal alignment of the pathway.

**Preparation:** The planning report and concepts will typically be prepared by the project proponent, such as a community group or developer, or possibly Rocky View County. For challenging or constrained locations, it may be prudent to engage an active transportation planner, traffic safety engineer, geometric designer or other professional as appropriate to provide input on specific issues.

**Approval:** The report and concept will be reviewed by Rocky View County staff prior to incurring costs in the design stage. Staff will grant agreement in principle to an acceptable concept to help support funding applications, and may be able to provide advice to reduce risks later in the process.

**Report, Drawings and Analysis Requirements:** A greater effort in the planning stage of the process to identify issues will reduce the potential for unexpected costs and other uncertainties in design and construction, and will increase the potential for long-term project success. The following summarizes the background analysis and steps that should be taken to generate sufficient information to support funding applications and provide reasonable confidence that the project is feasible and will be approved.

### Project Background

- Purpose of the pathway and who it will serve
- Relationship to the Active Transportation Plan
- Relationship to other pathways and initiatives (for example, Trans Canada Trail)
- Start and end points, and destinations along the route
- Factors affecting timing (i.e., answer the question, “why now?”)
- Details of any community engagement as well as supporting stakeholders

### Analysis

- **Current posted speeds and traffic volumes:** Confirm the current posted speeds and daily traffic volumes on the adjacent roadway. Existing traffic volumes can be obtained from Rocky View County recent traffic counts. Where recent traffic counts are not available, the County’s travel demand model can provide a reasonable estimate. Posted speeds can be obtained by checking signs on the roadway, or from Rocky View County’s posted speed sign GIS layer.
- **Pathway Setback Requirements:** Using the references provided in the Active Transportation Facility Guidelines for Shared Use Pathways, use the clear zone distances to determine the pathway setback requirements.
- **Right-of-way:** Using the Active Transportation Facility Guidelines for Shared Use Pathways, confirm that the pathway width can be provided within the recommended

upper and lower limits for pathway width within the setback requirements. A full identification of cut and fill slopes is not needed at the planning stage, but there should be a review of locations where side slope exists and there is potential for cut or fill slope to extend beyond current right-of-way boundaries.

- **Stream Crossings:** Locations where the alignment crosses streams and other water should be identified from maps or Google Earth, but should also be reviewed in the field. Photographs of the existing conditions will help to characterize constraints associated with the alignment. Where possible, the pathway should cross streams at right angles to minimize the environmental effects and cost. If possible, it should be noted whether streams have water intermittently or continuously. For major crossings where the adjacent roadway bridges the stream, consider whether there is sufficient space on the existing bridge to include a pathway, possibly through re-allocation of space. Expansion of the bridge will require further engineering investigation during design.
- **Drainage / Wetlands:** Although drainage issues will be addressed in the design stage, it is useful to note where drainage issues are known to occur. Even anecdotal information on historical trends is useful. Similarly, wetlands mapping and local knowledge can help to identify possible wetland areas, which should be avoided if possible for environmental or engineering reasons. Ideally, the pathway alignment should avoid areas with drainage issues or wetlands, and if they can't be completely avoided, should be noted so that issues can be addressed during design.
- **Road Crossings:** Ideally, the pathway alignment should avoid or minimize the number of roadway crossings. Where crossings are necessary, they should occur in locations that are free of obstructions and on straight sections of roadway. Sight distance evaluations will be required for design, but consideration of good visibility at crossing locations in the planning stage will reduce the risk that the alignment will need to be changed. Pathway approaches to road crossings should be as close to 90° as possible.
- **Accesses/Driveways:** Crossings of accesses and driveways should be minimized. This should be a significant consideration in identifying the preferred side of the road to locate the pathway. The same principles apply to crossing accesses and driveways as crossing roadways, with visibility being most important. Pathway users should be able to see vehicles, and drivers should be able to see pathway users.
- **Topography:** The cross-section and vertical alignment elements will be addressed in the design stage. However, some preliminary work to identify issues will reduce the potential need for changes at the design stage. A review of contour mapping and visual observation of the corridor will help to identify potential issues. Side slopes will have a significant effect on the cross-section of the pathway, and could potentially result in the need for cut or fill slopes that extend beyond the right-of-way boundaries, necessitating unexpected property acquisition. Potential



Example of a pathway that has a short section of steep grades because of a roadway cut (Legacy Trail, Banff National Park)

property acquisition is one of the greatest cost risks associated with a pathway project. Similarly, steep slopes should be avoided. Usually, the adjacent roadway alignment will minimize grades. However, sections of roadway in cut or fill may require the pathway to follow a route that is steeper than the adjacent roadway.

### Conceptual Alignment Sketches

The conceptual plans should show the following information at an appropriate scale for the specific pathway:

- Preferably on orthoimagery (available from Rocky View County)
- Show right-of-way / property lines
- Show proposed centerline horizontal alignment
- Identify existing pathways and connections
- Identify crossings of roads
- Identify stream crossing and locations where bridges and other structures may be required

### Cost Estimate

At the planning level of detail, an order-of-magnitude, conceptual cost estimate is appropriate. This would be based on known pathway costs/metre, plus provisions for stream crossings and other mitigative measures. The best source for the per metre costs is a recently completed pathway in similar conditions. Depending on the level analysis that is completed and the outstanding uncertainties, a contingency of at least 35% should be applied to the conceptual cost, and more likely 50%. Finally engineering costs of 10% should be added.

If there is potential for any of the work associated with the pathway to extend beyond the right-of-way boundary, an additional provision for right-of-way acquisition or other mitigation (eg., retaining walls) will be needed. Property impacts will be assessed at the design stage, but it may be necessary to engage an engineer at the planning stage to identify property issues in order to develop a reasonable conceptual cost estimate.

It is important to not “squeeze” costs simply to meet available funding budgets at this stage. The planning stage is intended to help identify the funding and budget requirements so that the pathway can be constructed properly. An honest and complete estimate of project costs is more likely to result in successful funding and support for the project. Typically costs increase as a project moves into design and construction. It is easier and less expensive to modify the concept or reduce the project to reduce costs if necessary at the planning stage than to run out of money in design or construction.

## Design

**Purpose:** The design stage includes development of preliminary design, which establishes horizontal and vertical geometry, developed to a level that identifies space requirements for the project, and in particular any property acquisition (or mitigation) requirements. The preliminary design also provides sufficient information to confirm feasibility of the preferred horizontal and vertical alignments. The final design provides sufficient design for tender and construction, including sufficient information to develop a reliable engineer’s cost estimate.

**Deliverables:** Preliminary design (at approximately 30% design) including horizontal and vertical alignments, typical cross section, property requirements and a drainage plan. The submissions will be generally in compliance with Section 104.5 of the County Servicing Standards as applicable for the project. The final design will provide all detail necessary for construction and generally aligned with Section 104.6 of the County Servicing Standards.

**Preparation:** The design should be prepared under the supervision of a qualified professional engineer. Where pathways are designed within an existing road right-of-way, the engineering can be complex due to issues related to right-of-way constraints, drainage, ground conditions, existing utilities, traffic operation and various other issues. The specific engineering requirements will be site-specific and the level of effort will vary by location. Sufficient detail in the design and full engineering consideration will reduce the risk of unforeseen circumstances and additional costs during construction.

**Approval:** Designs will be subject to approval by the Engineering Services Department of the County. The final design will be sealed by a qualified professional engineer registered with the Association of Professional Engineers and Geoscientists of Alberta.

**Report, Drawings and Analysis Requirements:** The County Servicing Standards should be referenced for submission requirements. Engineering judgment will be required to determine the level of engineering effort required for specific circumstances. The following outlines the engineering investigation required for most major elements, recognizing that these are guidelines only and the local conditions will dictate the specific needs.

APPENDIX G

# Network Connectivity Index





# Network Connectivity Index

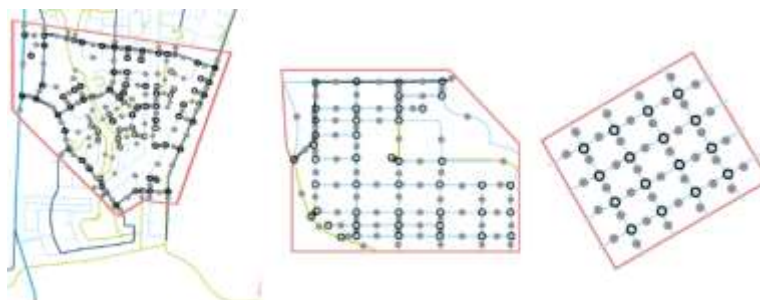
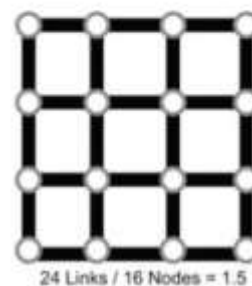
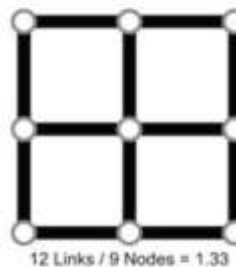
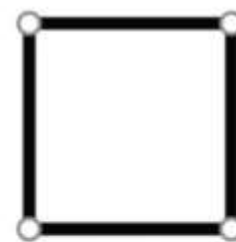
A connected, dense active transportation network can support shorter trips and varied travel options in developed communities. In order to better understand connectivity, many cities, including several in the Calgary region, have adopted tools such as connectivity indices to clearly measure street network connectivity.

## What are Connectivity Indices?

Connectivity indices quantify the connectivity of transportation network. Originally developed to measure the connectivity of a street network, connectivity indices are calculated as the ratio of links to nodes – the more links relative to nodes, the more connected<sup>1</sup>. Nodes are intersections and links are segments between intersections. For active mode connectivity indices, cul-de-sacs must have a pathway in order to be included in the calculation as a link. Pathways are included as separate links and nodes, unless they are immediately adjacent to the street and do not offer any more connection than the street. A connectivity index for active transportation facilities 1.6 is considered to be walkable.

## Connectivity Indices for Active Modes

Typically, connectivity indices for active modes are higher than connectivity indices for streets due to additional pathways that link to the street network. A dense, connected street network supports walking and cycling. The addition of pathways and other active transportation facilities further enhances the walking and cycling environment.



### Rainbow Falls, Chestermere (1.53)

Generally good connectivity  
Pathways typically connect where streets lack  
~2% walk or cycle to work in Chestermere

### South Canmore, Alberta (1.70)

Good connectivity  
Good grid network  
Pathways connect between streets  
18% walk or cycle to work in Canmore

### Nelson, British Columbia (2.0)

Excellent connectivity  
Perfect grid network of sidewalks on streets remove need for pathway links  
~31% walk or cycle to work in Nelson!

Examples of active mode connectivity in Chestermere, Canmore and Nelson, BC are shown to the left.

Rainbow Falls in Chestermere is somewhat well-connected (1.53). In comparison, the Canmore's downtown and Nelson, BC are examples demonstrate dense networks with excellent connectivity (1.70 and 2.0, respectively). While a number of other factors contribute to mode split, connectivity contributes 18% of people in Canmore and 31% of people in Nelson walking or biking to work.

## Connectivity Index (CI) Calculation

$$CI = \text{Links} / \text{Nodes}$$

<sup>1</sup> Ewing, R. Best Development Practices – Doing the Right Thing and Making Money at the Same Time. Urban Land Institute and Florida Department of Community Affairs, 1996.

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## **RECREATION AND COMMUNITY SERVICES**

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**TO:** Council **DIVISION:** 9  
**DATE:** October 23, 2018  
**FILE:** 1006-600  
**SUBJECT:** Request for Capital Contribution – Friends of Westbrook School

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### **<sup>1</sup>POLICY DIRECTION:**

The \$100,000 capital funding application made to the Ranchlands Recreation Board to support redevelopment of Westbrook School requires evaluation as per the Reserves Agreement by the Reserves Coordinating Committee. The following should be considered:

- The Reserves Agreement is dated.
- The Reserves Agreement established a Reserves Coordinating Committee to review and make recommendations respecting the utilization of any Municipal and School Reserve.
  - To activate this committee, requires commencement of a meeting to consider the capital grant application.
- Updating the Reserves agreement would provide a forum for municipal support for future joint projects.

### **EXECUTIVE SUMMARY:**

The purpose of this report is to inform Council of a recent capital funding application made to the Ranchlands Recreation Board for \$100,000 to support the redevelopment of Westbrook School. Attached is a proposed letter to notify Rocky View Schools (RVS) that the County seeks to convene the Reserves Coordinating Committee to discuss appropriate financial support for the project and future of the Reserves Agreement.

### **BACKGROUND:**

In 1998 the County and RVS signed the Reserves Agreement (Attachment A). Relevant points are;

- Committee structure established to coordinate land assembly processes for MR designation;
- Municipal Reserve fund management and formula for cash in lieu distribution between school boards; and
- Municipal responsibility for the funding of gymnasium development above the Alberta Education standard size for an individual school. This may occur when additional community use is identified beyond educational needs.

In November of 2017 - Rocky View Schools decided to expedite the capital replacement process and 'self-fund' a replacement school in Westbrook with \$6 MIL from internal reserve funds; proposed completion is December 2019.

On September 28, 2018 the Friends of Westbrook School, which is the fundraising arm of the school Council, submitted a capital funding application through Community Funding Policy 317 for \$100,000 to assist with the redevelopment of Westbrook School specifically to offset of costs for an expanded gymnasium design. A letter from RVS dated September 27 (Attachment B) outlines the benefits of extended public use should municipal support be forthcoming.

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<sup>1</sup> **Administration Resources**

Corwin McCullagh, Recreation & Community Services



**DISCUSSION:**

As school replacement is comparable to the development of a new school, the capital application from the Friends of Westbrook falls under the jurisdiction of the Reserves Agreement and not Policy 317. Under the agreement there is an established 'Reserves Coordinating Committee', consisting of appointed Councillors and RVS Board members, which acts as the forum to discuss appropriate municipal financial support for gymnasium development.

Other capital projects on school lands such as artificial turf installation in Chestermere and community hall development in Springbank further drive the need to establish a consistent approach to funding and standard for public use.

The Letter of Notification to Convene the Reserves Coordinating Committee (Attachment C) outlines the County's intent to convene the committee to discuss:

- 1) the project and capital request of \$100,000;
- 2) replacement of the current reserves agreement; and
- 3) future joint projects that may require municipal support.

**BUDGET IMPLICATION(S):**

There are no budget implications at this time. Should Council wish to support this initiative, a budget adjustment will be prepared for Council's consideration.

**OPTIONS:**

Option #1: THAT Administration be directed to provide a Letter of Notification (Attachment "C") to Rocky View Schools.

Option #2: THAT alternative direction be provided.

Respectfully submitted,

Concurrence,

"Chris O'Hara"

"Rick McDonald"

\_\_\_\_\_  
General Manager

\_\_\_\_\_  
Interim County Manager

CM/rp

**ATTACHMENTS:**

ATTACHMENT "A": Reserves Agreement (1998)

ATTACHMENT "B": RVS Letter of Support for Application

ATTACHMENT "C": Proposed Letter of Notification to Rocky View Schools

THIS AGREEMENT made as of the 23 day of APRIL 1998.

BETWEEN:

**THE MUNICIPAL DISTRICT OF  
ROCKY VIEW NO. 44**  
a Municipal Corporation pursuant  
to the laws of the Province of Alberta  
  
(referred to as the "Municipality")

OF THE FIRST PART

- and -

**THE BOARD OF TRUSTEES OF  
THE ROCKY VIEW SCHOOL DIVISION NO. 41**  
A body corporate, incorporated pursuant to the  
*School Act*, being S.A. 1988, Ch. S-3.1 as amended  
  
(referred to as the "School Division No. 41")

OF THE SECOND PART

- and -

**THE BOARD OF TRUSTEES OF THE  
CALGARY ROMAN CATHOLIC  
SEPARATE SCHOOL DISTRICT NO. 1**  
A body corporate, incorporated pursuant to the  
*School Act*, being S.A. 1988, Ch. S-3.1 as amended  
  
(referred to as the "Calgary Separate School Authority")

OF THE THIRD PART

**RESERVES AGREEMENT**

**RECITALS:**

1. **WHEREAS** the School Division is a party to an agreement entitled Joint Use Agreement made with the "Municipality" as of the 16<sup>th</sup> day of April, 1991 (the "Prior Agreement");
2. **AND WHEREAS** the Calgary Separate School Authority has subsequently been formed within the boundaries of the Municipality;

*De*  
*per*

3. **AND WHEREAS** it is desirable that the Prior Agreement be terminated and be replaced by this Agreement which includes the Calgary Separate School Authority;

4. **AND WHEREAS** the prior Agreement contained recitals which shall continue to apply to this Agreement as follows:

- (a) pursuant to Section 661 and limited by Section 663 of the Municipal Government Act, the Approving Authority is authorized to require the registered owner of a parcel of land that is the subject of a proposed subdivision to:
  - (i) provide lands which may then be designated as Municipal Reserve, School Reserve or Municipal and School Reserve ("Reserve Lands"), and to specify the amount, type and location of such lands;
  - (ii) provide for payment of Cash-in-Lieu Monies in place of all or a portion of such lands; or
  - (iii) provide a combination of lands and Cash-In-Lieu monies;
- (b) pursuant to Section 666 and 667 of the Municipal Government Act, the Approving Authority is also authorized upon subdivision to:
  - (i) determine the allocation of any Municipal Reserve, School Reserve and Municipal and School Reserve to be set aside by the registered owner between the Municipality and each School Authority as joint owners or as separate owners;
  - (ii) determine the allocation of Cash-In-Lieu Monies to be provided by the registered owner between the Municipality and each School Authority as joint owners or as separate owners; and
  - (iii) determine the allocation of any combination of Municipal Reserve, School Reserve, Municipal and School Reserve and Cash-in-Lieu Monies to be set aside or provided, as the case may be, by the registered owner between the Municipality and each School Authority as joint owners or as separate owners;

all in accordance with this Agreement between the Municipality and the School Authorities.



- (c) Section 671(2) of the Municipal Government Act, restricts the use of Reserve Lands held by the Municipality or by a School Authority, or by them jointly to public parks, public recreation areas, School Authority purposes or to separate areas of land that are used for different purposes;
- (d) Section 671(3) and (4) of the Municipal Government Act entitles the Municipality and each School Authority to enter into any Agreement they consider necessary respecting the use to which the Municipality or a School Authority may put Reserve Lands and any Cash-in-Lieu Monies received in place of such Reserve Lands; and
- (e) The Municipality and each School Authority recognize that certain of their respective functions relating to recreation and education overlap, and that by pooling their respective resources, the residents of the Municipality may be benefited and that, the Municipality may be able to supplement the funds available to the School Authority for recreational purposes from the Municipality's Cash-in-Lieu Monies.

**NOW THEREFORE** in consideration of the mutual covenants and agreements contained in this Agreement the parties agree as follows:

1. **DEFINITIONS:**

In this Agreement the following definitions apply:

- (a) "Approving Authority" means the Subdivision Approving Authority with respect to any application for subdivision of land within the Municipal District, pursuant to Section 623 of the Municipal Government Act, and includes both the respective Subdivision and Development Appeal Board and, the Municipal Government Board where a decision is made by either appellate body on an appeal to it.
- (b) "Cash-in-Lieu Monies" means any monies received by the Municipality with respect to a subdivision of land within the Municipality approved by the Approving Authority pursuant to Section 667 of the Municipal Government Act;
- (c) "Joint Use Purposes" means the use by a party to this Agreement of the facilities of another party to this Agreement for the benefit of the residents of the

Municipality and also includes the use of the facilities of a party to this Agreement by third parties pursuant to an Outside Lease.

- (d) "Land Titles Act" means the Land Titles Act, R.S.A. 1980, c.L-5, as amended from time to time.
- (e) "Municipal District" means the area within the boundaries of the Municipal District of Rocky View No. 44.
- (f) "Municipal Reserve Fund" means those funds held for the Municipal District pursuant to the provisions of Clause 4 of this Agreement.
- (g) "Outside Lease" means a lease of Reserve Lands to any person other than the Municipality or a School Authority for a purpose consistent with the provisions of Section 671(2) of the Municipal Government Act.
- (h) "Playground Equipment" means climbing apparatus, monkey bars, swings and other related equipment played on by younger children. *built to CSA standards*
- (i) "Playing Field Extension" means any part of Reserve Lands which is used as a playing field. *per [signature]*
- (j) "Prior Agreement" means the agreement entitled "Joint Use Agreement" made between the School Division No. 41 and the Municipality as of the 16<sup>th</sup> day of April, 1991;
- (k) "Reserve Co-ordinating Committee" means the committee pursuant to Clause 5.1.1 of this Agreement.
- (l) "Reserve Lands" means any lands which have been provided by a registered owner as Municipal Reserve, School Reserve or Municipal and School Reserve (in each case such terms shall not include lands held as Environment Reserve) under the provisions of the Municipal Government Act.
- (m) "School Authority" means either the School Division No. 41, or the Calgary School Authority, and "School Authorities" means both of them.
- (n) "School Building Envelopes" means any part of the School Reserve upon which school facilities and buildings, related ornamental lawn areas, parking lots and paved play areas are located.
- (o) "School Development" means the construction of school facilities and buildings including related ornamental lawn areas, parking lots and paved play areas.

*[Handwritten signature]*

*[Handwritten signature]*

- (p) “School Reserve Fund” means with respect to each School Authority those funds that are held, if any, for each such School Authority pursuant to the provisions of Clause 4 of this Agreement.

2. **PURPOSES:**

2.1 **Range of Application**

- 2.1.1 The parties shall endeavour to optimize the services they provide for the benefit of the residents of the Municipality and the residents of the cities, towns and villages situated within the boundaries of the Municipal District and the School Authorities with the exception of the residents of the City of Calgary, by
- (a) coordinating the planning, development, maintenance and operation of all Reserve Lands;
  - (b) equitably sharing all costs associated with such coordination; and
  - (c) coordinating the disposition of any Reserve Lands which are found to be surplus to their needs.

3. **RESERVE LANDS:**

3.1 **Protection of All Reserve Lands**

- 3.1.1 All Reserve Lands shall be dealt with in accordance with the provisions of this Agreement and in compliance with the provisions of both the Municipal Government Act and the School Act, and the respective applicable Regulations.

3.2 **Priority of Use**

- 3.2.1 The following priorities of use shall be utilized as guidelines by the Reserves Coordinating Committee in determining their recommendations with respect to all Reserve Lands:
- (a) Priority #1: Neighbourhood needs, which includes School Authority operated Elementary Schools or their equivalent;
  - (b) Priority #2: Community needs, which includes School Authority operated Junior High Schools or their equivalent, and park land required for open space linkages or to provide setbacks beyond the land reserved for environment reserve

*AC*

*go*



under the Municipal Government Act at the tops of escarpments or adjacent to water courses; and

- (c) Priority #3: Regional needs, which includes School Authority operated Senior and Vocational High Schools or their equivalent, pools, arenas, athletic parks and other recreational facilities.

### 3.3 Optimum Planning Sites

- 3.3.1 The Approving Authority shall determine upon receipt of a subdivision application, in accordance with this Agreement and the Municipal Government Act, the geographical extent of Reserve Lands to be set aside by a registered owner or Cash-in-Lieu Monies to be provided in place of Reserve Lands.
- 3.3.2 The parties shall recommend to the Approving Authority the location, size and configuration of all Reserve Lands which may be committed to Joint Use Purposes at the subdivision approval stage so as to optimize the utilization of available Reserve Lands.
- 3.3.3 The parties shall recommend to the Approving Authority the location and size of the applicable School Building Envelopes and any Municipal and School Reserve which is to be adjacent to such School Building Envelopes. Without limiting the ability of the parties to recommend the establishment of such areas as may be appropriate in any particular case, the parties agree to the following guidelines for the area of such parcels:

	School Building Envelopes	Municipal and School Reserve	Total
(a) Elementary School	4 acres	6 acres	10 acres
(b) Junior High School	5 acres	10 acres	15 acres
(c) Senior High School	7 acres	13 acres	20 acres

### 3.4 Gymnasium

- 3.4.1 Each School Authority shall endeavour to ensure that each new school building will have showers, dressing rooms and a gymnasium in accordance with Alberta Education standards for Junior High Schools.

3.4.2 Subject to the Approval of the Municipality, or municipalities, as the case may be, and Alberta Education, where the Reserves Coordinating Committee recommends that the recreational needs of the residents will be best served by the provision of a larger gymnasium than defined by Alberta Education for the type and size of the school being constructed, the Municipality or municipalities, as the case may be, shall be responsible for the incremental capital costs over and above the costs of construction of the gymnasium of the size that would otherwise have been built.

3.4.3 Subject to the approval of the Municipality, the Reserves Coordinating Committee shall be responsible for negotiating a cost sharing Agreement on behalf of the Municipality with such city, town or village for the construction costs associated with the larger gymnasium. In the event that the Reserves Coordinating Committee is unable to reach a suitable cost sharing Agreement to the Municipality's satisfaction, the Municipality shall be under no obligation to participate financially with plans to construct a larger gymnasium.

### **3.5 Maintenance Responsibilities**

3.5.1 Maintenance of Municipal Reserves shall be the responsibility of the Municipality.

3.5.2 Maintenance of Municipal and School Reserves shall be the responsibility of both the Municipality and the affected School Authority, and the proportional share of the cost and the total cost of such maintenance born by each of the parties on a site by site basis shall be a matter of negotiation between the Municipality and the affected School Authority based on a recommendation by the Reserves Coordinating Committee.

3.5.3 Maintenance of School Reserves shall be the responsibility of the School Authority to which such School Reserve has been allocated.

### **3.6 Declaration of Surplus Sites**

3.6.1 In the event that a School Authority wishes to close a school, it shall first advise the Reserves Coordinating Committee whether such closure is intended to be a permanent



closure or a temporary closure pending a return to circumstances under which such school may be reopened or used for alternate School Authority purposes.

### 3.7 Temporary Closure of Schools

- 3.7.1 If a School Authority, with the written approval of the Minister of Education, determines that the temporary closure of one of its schools is required, such school authority, subject to Section 674 of the Municipal Government Act where applicable, may enter into an Outside Lease, which Outside Lease shall be on terms and conditions as determined by such School Authority including both, the retention by such School Authority of any revenue derived from the Outside Lease and the use of the Reserve Lands to be for all or any of the purposes referred to in Section 671 of the Municipal Government Act.

### 3.8 Permanent Closure

- 3.8.1 If a School Authority, with the written approval of the Minister of Education, determines that the permanent closure of one of its schools is required, such School Authority shall so advise the Municipality. Subject to Section 674 of the Municipal Government Act, where it may be applicable, the School Authority may:
- (a) Enter into an Outside Lease for all or a part of the school building and School Reserve and, the adjacent Municipal and School Reserve where applicable; or
  - (b) Sell the school building and the School Reserve and, the adjacent Municipal and School Reserve where applicable, which sale shall be subject to:
    - (i) The written approval of the Minister of Education being obtained for such sale; and
    - (ii) Clause 3.8.3 of this Agreement; or
  - (c) Demolish the school building or any portion of it, which demolition shall be subject to the written approval of the Minister of Education for such demolition.
- 3.8.2 In the event that an Outside Lease is concluded, which Outside Lease shall be on terms and conditions as determined by the School Authority including the retention by the School Authority of any revenue derived from the Outside Lease, the School Authority

shall inform and advise the Municipality of the pertinent details relating to the provision of any Joint Use Purposes.

- 3.8.3 In the event that a school building and the applicable Reserve Lands, which have been declared by a School Authority to be permanently surplus to school needs are to be sold:
- (a) the School Authority shall comply with the respective School Act Regulation, and where applicable, Section 674 of the Municipal Government Act, dealing with the required procedures to be followed for the sale of the respective property;
  - (b) the sale proceeds derived from the respective sale shall, to the extent possible, be allocated as follows:
    - (i) firstly, the payment of all disposal costs shall be addressed;
    - (ii) secondly, the Municipal Reserve Fund shall receive fifty (50%) percent of the sale proceeds of the applicable Reserve Lands with the other fifty (50%) percent of the sale proceeds of the applicable Reserve Lands being allocated to the School Reserve Fund held for the School Authorities. Each School Authority's portion of the proceeds shall in turn be allocated based upon the percentage ratio that is derived when the number of students that reside in the Municipal District of each respective School Authority is divided by the total number of students that reside in the Municipal District and that are enrolled in both School Authorities, all of which shall be calculated as of the September 30<sup>th</sup> prior to the date of the disposition of the applicable Reserve Lands.
    - (iii) thirdly, the retirement of any outstanding debentures and any and all outstanding debt associated with the sale of such a school building as required pursuant to the respective Regulation under the School Act shall be addressed;
    - (iv) fourthly, the value of the school building sale proceeds shall:
      - A. only be applied by the School Authority to a future school building project based upon the percentage ratio that is derived when any School Foundation Program Fund Grant contribution that was



originally contributed to any building project of the school is divided by the total amount of that school building project; and

- B. only be applied by the School Authority to its future capital expenditures based upon the percentage ratio that is derived when any School Authority contribution that was originally contributed to any building project of the school is divided by the total amount of that school building project,

all of which shall be in compliance with the requirements of the respective Regulation under the School Act;

- (v) fifthly, subject to clauses 3.8.3(b)(iii) and (iv) of this Agreement, the value of any residual school building sale proceeds shall be paid to any third party based upon the percentage ratio that is derived when any financial contribution that was originally contributed by a third party to any building project of the school is divided by the total amount of that school building project; and
- (vi) sixthly, subject to clauses 3.8.3(b)(iii), (iv) and (v) of this Agreement, the Municipality and the affected School Authority as the case may be shall, share the sale value of any non-school building and improvements on a pro-rata basis based upon the amounts originally contributed by each party to pay for and improve any such non-school building and improvements.

3.8.4 If a school building which has been declared by a School Authority to be permanently surplus to school needs is to be demolished:

- (a) the demolition and site rehabilitation costs shall be borne out of the School Reserve Fund held for such School Authority; and
- (b) the retirement of any outstanding debentures and all outstanding debt associated with such a school building shall be the responsibility of the School Authority obligated pursuant to such debentures and outstanding debt.

3.8.5 In the event that demolition of a school building occurs as provided in Clause 3.8.4 above, then the School Reserve upon which the school building was situated and any

Municipal and School Reserve associated with the school building shall be redesignated as Municipal Reserve and registered as such upon there being compliance with Sections 674 and 675 of the Municipal Government Act. Legal title to such Municipal Reserve shall vest in the Municipality as provided in Section 672 of the Municipal Government Act.

3.8.6 The consideration payable by the Municipality for the transfer of such redesignated Municipal Reserve shall be \$10.00.

3.8.7 When a School Reserve or a Municipal and School Reserve have been declared permanently surplus to school needs and legal title has been transferred to the Municipality as part of redesignation, the Municipality shall assume responsibility for the maintenance of such newly designated Municipal Reserve.

### 3.9 Disposal of Reserve Lands to Third Parties

3.9.1 Subject to Clause 3.8, the parties to this Agreement shall consult with each other prior to taking any steps under Section 674 of the Municipal Government Act in connection with the disposal of Reserve Lands so as to ensure that the future needs of each of the parties are taken into consideration.

## 4. MUNICIPAL AND SCHOOL RESERVE FUNDS:

### 4.1 Sources

4.1.1 The Municipality shall allocate all Cash-in-Lieu monies provided in place of Reserve Lands as follows:

- (a) Cash-in-Lieu monies received with respect to subdivisions approved by the Approving Authority prior to October 1<sup>st</sup>, 1997 shall have fifty percent (50%) of such monies directed to the Municipality and fifty percent (50%) of such monies directed to the School Division 41; and
- (b) Cash-in-Lieu monies received with respect to subdivisions approved by the Approving Authority after September 30<sup>th</sup>, 1997, shall have fifty percent (50%) of such monies allocated to the Municipality and fifty percent (50%) of such monies

directed to the School Authorities. Each School Authority's portion of the proceeds shall in turn be allocated based upon the percentage ratio that is derived when the number of students that reside in the Municipal District of each respective School Authority is divided by the total number of students that reside in the Municipal District and that are enrolled in both School Authorities, all of which shall be calculated as of September 30<sup>th</sup> prior to the date of the disposition of the applicable Reserve Lands.

- 4.1.2 Notwithstanding Clause 4.1.1, in the event that a new public school authority is established within the jurisdiction of the Municipality which desires to participate with the Municipality and the School Authorities in the allocation of Cash-in-Lieu Monies, the Reserves Coordinating Committee shall consider the new public school authority's request.
- 4.1.3 The proceeds derived from the sale, assignment or other transfer of any Reserve Lands that were established by way of a subdivision of land within the Municipal District pursuant to Section 661 of the Municipal Government Act and which do not have any school buildings or non-school buildings situated on them, shall be allocated as follows:
- (a) the sale proceeds of those lands set out on Schedule "A" to this Agreement, shall be allocated exclusively to the Municipal District;
  - (b) the sale proceeds of those Reserve Lands received with respect to a subdivision of land approved by the Approving Authority prior to October 1st, 1997, shall be allocated on the basis of fifty percent (50%) being paid to the Municipality and fifty percent (50%) being paid to the School Division No. 41; and
  - (c) the sale proceeds of those Reserve Lands received with respect to a subdivision of land approved by the Approving Authority after September 30<sup>th</sup>, 1997, shall be allocated on the basis of fifty percent (50%) being paid to the Municipality and fifty percent (50%) being paid to the School Authorities. Each School Authority's portion of the proceeds shall in turn be allocated based upon the percentage ratio that is derived when the number of students that reside in the Municipal District of each respective School Authority is divided by the total



number of students that reside in the Municipal District and that are enrolled in both School Authorities, all of which shall be calculated as of the September 30<sup>th</sup> prior to the date of the disposition of the applicable Reserve Lands.

4.1.4 Cash-in-Lieu monies and monies paid pursuant to Clauses 4.1.1 and 4.1.3, that have been allocated to the Municipality shall be held in a reserve fund (the Municipal Reserve Fund) to be administered by the Municipality, and cash-in-lieu monies and monies paid pursuant to Clauses 4.1.1 and 4.1.3 that have been allocated to each School Authority shall be held in a reserve fund (the School Reserve Fund) maintained for each School Authority, which shall also be administered by the Municipality. Interest earned on monies administered by the Municipality on behalf of each School Authority shall be accrued to each School Authority's School Reserve Fund. Subject to the Municipal Council being informed, payments from each School Reserve Fund to the School Authority for which such fund is maintained, to enable it to meet the funding requirements for any use authorized by Clause 4.2.1, as reviewed by the Reserves Coordinating Committee as part of its coordination and administration of this Reserves Agreement, shall be made to the School Authority upon a written request by the School Authority.

#### 4.2 Use of the Municipal School Reserve Fund

- 4.2.1 Subject to the provisions of Section 671 of the Municipal Government Act, the Municipal Reserve Fund may be expended for such purposes as the Municipality may determine, and each School Reserve may be expended for such purposes as the School Authority for which such School Reserve Fund is maintained may determine, provided however, that in all cases the Municipal Reserve Fund may only be used for capital expenditures as outlined in Policy 603 of the Municipal District of Rocky View No. 44 Policy Handbook, as amended from time to time, and each School Reserve Fund may only be used for either:
- (a) capital expenditures for school or land improvements or purchases within the boundaries of the Municipal District by the School Authority for which such School Reserve Fund is maintained; or





- (b) for capital expenditures for school or land improvements or purchases not located within the Municipal District but at which children who reside within the Municipal District are educated by the School Authority for which such School Reserve is maintained. This clause does not apply to capital expenditures for schools or land improvements or purchases located within the boundaries of the City of Calgary; or
- (c) for the purposes outlined in Clause 4.2.3 or 4.2.4.

4.2.2 School Reserve Fund expenditures made pursuant to Clause 4.2.1(b) shall be limited to the portion of total capital expenditures to be incurred as determined by the following ratio: the number of students that reside in the Municipal District enrolled in the school for which capital expenditures are to be incurred as compared to the total number of students enrolled in the school for which capital expenditures are to be incurred. For a capital expenditure on an existing school building or surrounding lands, enrollments shall be as of September 30 prior to the proposed expenditures. For a capital expenditure on a proposed new school building or land, enrollment shall be based on projected first year enrollments provided by the School Authority.

4.2.3 The School Authorities agree that if the Municipality approves a request for funds from the Municipal Reserve for the purpose of purchasing and installing Playground Equipment to be located on a School Authority's land or on the adjacent Playing Fields Extension, then fifty percent (50%) of the funds approved, up to a maximum of \$15,000.00 per request, may be paid towards the cost of such equipment from the School Reserve of the School Authority on whose land the Playground Equipment is to be located.

4.2.4 If the Municipality approves a request for funds from the Municipal Reserve for the purpose of purchasing recreational facilities or equipment, except Playground Equipment to be located on a School Authority's land or on the Playing Field Extension, and if the School Authority on whose land or Playing Field Extension the recreational facilities or equipment is to be located also approves a request for funds, then such funds as approved

by the School Authority may be paid from the School Reserve of the School Authority on whose land the recreation facilities or equipment are to be located towards the cost of such equipment.

5. **OPERATIVE FRAMEWORK:**

5.1 **Composition of Reserves Coordinating Committee**

5.1.1 The parties shall create an administrative committee to be known as the Reserves Coordinating Committee. This committee shall consist of one (1) representative appointed by the School Division No. 41, one (1) representative appointed by the Calgary Separate School Authority, and two (2) representatives appointed by the Municipality.

5.2 **Duties of the Reserves Coordinating Committee**

5.2.1 The Reserves Coordinating Committee shall:

- (a) coordinate and administer this Agreement on behalf of the parties;
- (b) notwithstanding Clause 6.1.1, report to the Chief Administrative Officers of the Municipality and each School Authority at any time they feel such a report is necessary;
- (c) meet as often as is necessary;
- (d) operate on a consensual basis on the understanding that on those issues on which agreement cannot be reached, reference is to be made to the Chief Administrative Officers of the respective parties. If no consensus is reached between the Chief Administrative Officers, then reference is to be made to the Council of the Municipality and the Board of Trustees of each School Authority involved for direction as to final resolution;
- (e) set-up sub-committees where appropriate;
- (f) review and make recommendations respecting the utilization of any Municipal and School Reserve.

**6. REVIEW, AMENDMENT AND TERMINATION OF THIS AGREEMENT:****6.1 Review**

6.1.1 The terms of this Agreement shall be reviewed and a report shall be made by the Reserves Coordinating Committee to the Chief Administrative Officers of the parties:

- (a) on January, 2002 and every fifth year afterwards; and
- (b) after any changes are made to the Municipal Government Act which may affect the Reserve provisions of this Agreement.

Such report may recommend amendments to this Agreement which may be necessitated by the passage of time or the changes to the Municipal Government Act.

**6.2 Amendments**

6.2.1 At any time and from time to time any one of the parties to this Agreement may submit for review by the Reserves Coordinating Committee any proposed amendments which they believe are necessary to ensure the proper framework for the carrying out of their respective responsibilities under this Agreement.

6.2.2 Any recommendation made by the Reserves Coordinating Committee under Clause 6.1.1 and any amendments submitted to the Reserves Coordinating Committee in accordance with Clause 6.2.1 shall be reviewed by the Chief Administrative Officers of the parties within six (6) calendar months of such amendments or recommendations being proposed.

6.2.3 If the Chief Administrative Officers of the parties unanimously agree upon the necessity for the content of the proposed amendments arising from the recommendations of the Reserves Coordinating Committee, then the Chief Administrative Officers shall submit such proposed amendments for the approval of the Council of the Municipality and the Board of Trustees of each School Authority.

6.2.4 In the event that the Chief Administrative Officers of the parties do not agree upon such proposed amendments, then the proposed amendments shall be referred to the Council of the Municipality and the Board of Trustees of each School Authority along with a synopsis of such disagreement for their consideration. If the Council of the Municipality



and the Board of Trustees of each School Authority cannot agree upon such proposed amendments, then this Agreement shall not be amended.

### 6.3 Termination

6.3.1 The parties agree that this Agreement shall be for an initial term of three (3) years during which it may not be terminated except as provided in Clause 6.3.1(a), and at the end of such initial three (3) year term shall be continued for an indefinite term until it is terminated in accordance with Clause 6.3.1(a) or 6.3.1(b) as outlined below:

- (a) upon the Agreement of the parties; or
- (b) upon a notice in writing of at least one (1) year having been given by any party to the other parties.

6.3.2 The parties agree that this Agreement shall commence on October 1, 1997 notwithstanding that it may have been executed subsequent to that commencement date.

### 7. PROVISIONS FOUND INVALID:

7.1 If one or more provisions of this Agreement are, for any reason, held to be invalid, the parties agree that all remaining provisions are to remain in full force and effect.

### 8. ARBITRATION:

8.1 In the event of a dispute arising between the parties regarding the interpretation, application, operation or alleged violation of this Agreement, such dispute shall be determined by arbitration in accordance with this Agreement.

8.2 The party(ies) alleging a dispute (the "first party") shall notify the other party(ies) (the "opposite party") in writing of the details of the nature and extent of the dispute.

8.3 Within ten (10) working days from receipt of notice, the opposite party shall in writing notify the first party of the matter referred to in the initial notice for which it accepts responsibility and proposes to take remedial action.

- 8.4 The terms of reference for arbitration shall be those areas of dispute referred to in the initial notice with respect to which the opposite party has not admitted responsibility or proposed to take remedial action to the satisfaction of the first party.
- 8.5 The first party shall, within fifteen (15) working days of the establishment of the terms of reference pursuant to Section 8.4, submit in writing to the opposite party the name or a list of names of the person or persons that it accepts as an arbitrator.
- 8.6 The opposite party shall respond in writing to the first party within ten (10) days of receipt of the name or list of names of the proposed arbitrator provided by the first party, by way of providing the name or list of names of their proposed person or persons that it accepts as an arbitrator or, agreeing to the first party's proposed person, or one of the proposed persons to serve as arbitrator. If the parties cannot agree on an arbitrator within thirty (30) days, either party may request the Director of Mediation Services with the Department of Labour to have the arbitrator appointed.
- 8.7 Within thirty (30) working days of the appointment of the arbitrator, or such further period as may be agreed upon by the parties, the arbitrator shall resolve the matters in dispute referred to in the terms of reference.
- 8.8 The decision of the arbitrator shall be binding upon all parties.
- 8.9 The costs of the arbitrator shall be borne equally by all the parties.
- 8.10 Each party shall bear all of their own legal costs and disbursements incurred in retaining, if necessary, legal counsel to represent their respective interests in the arbitration.
9. **COSTS OF SUBDIVISION, REDESIGNATION AND TRANSFER:**
- 9.1 The Municipality agrees that any fees which would otherwise be payable by a School Authority to the Municipality in connection with both subdivision or redesignation applications made by the School Authority and any transfer of land associated with such



subdivision or redesignation are hereby waived on the condition that the School Authority be solely responsible for all legal costs associated with such subdivision or redesignation applications.

10. **MISCELLANEOUS:**

10.1 The address for delivery of notices or other documents required or permitted under this Agreement shall be at the address adjacent to the party's names as follows or such other address as the parties may have been duly notified:

(a) Municipal District of Rocky View No. 44  
911 – 32<sup>nd</sup> Avenue N.E.  
P.O. Box 3009, Station B  
Calgary, Alberta  
T2M 4L6

Attention: Municipal Manager

(b) The Board of Trustees of the  
Rocky View School Division No. 41  
2616 – 18<sup>th</sup> Street N.E.  
Calgary, Alberta  
T2E 7R1

Attention: Secretary-Treasurer  
Associate Superintendent

(c) The Board of Trustees of the  
Calgary Roman Catholic  
Separate School District No. 1  
1000 – 5<sup>th</sup> Avenue S.W.  
Calgary, Alberta  
T2P 4T9

Attention: Superintendent of Finance  
and Business Services

10.2 Any notice shall be delivered to the address indicated and shall be deemed to have been delivered to and received by the named addressee on the date of delivery.

10.3 This Agreement shall be governed by and construed in accordance with the laws of the Province of Alberta.

10.4 This Agreement shall be binding upon the parties, and their respective successors and assigns.


10.5 The Prior Agreement is terminated effective September 30, 1997.



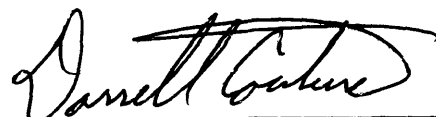
IN WITNESS WHEREOF the parties to this Agreement have duly executed these presents the day and year first above written.

**THE MUNICIPAL DISTRICT OF  
ROCKY VIEW NO. 44**

PER:  (C.S.)  
REEVE ROBERT CAMERON


PER:  (C.S.)  
VALERIE SCHMALTZ  
MUNICIPAL SECRETARY

**THE BOARD OF TRUSTEES OF THE  
ROCKY VIEW SCHOOL DIVISION NO. 41**

PER:  (C.S.)  
Darrell Couture,  
Secretary-Treasurer  
Associate Superintendent

**THE BOARD OF TRUSTEES OF THE  
CALGARY ROMAN CATHOLIC  
SEPARATE SCHOOL DISTRICT NO. 1**

PER:  (C.S.)  
Deborah Achen  
Superintendent of Finance  
and Business Services

PER:  (C.S.)



**SCHEDULE "A"**

Attached to, and forming part of, the Joint Use Agreement dated the 23 day of APRIL 1998, between the Municipal District of Rocky View No. 44, The Board of Trustees of the Rocky View School Division No. 41, and The Board of Trustees of the Calgary Roman Catholic Separate School District No. .

There are no more Municipal Reserves available at this time.



**JOINT USE AGREEMENT**

THIS AGREEMENT made as of the 16<sup>th</sup> day of APRIL, 1991.

BETWEEN:

**THE MUNICIPAL DISTRICT OF ROCKY VIEW #44, a  
Municipal Corporation**

- and -

**THE BOARD OF TRUSTEES OF THE ROCKY VIEW SCHOOL  
DIVISION #41, a corporation established  
pursuant to the provisions of the School Act**

WHEREAS the Council of the Municipality is the subdivision Approving Authority for the allocation and use of land within the Municipality;

AND WHEREAS pursuant to Part 5 Division 3 of the Planning Act the Approving Authority is authorized to require the registered owner of a parcel of land that is the subject of a proposed subdivision to:

- (a) provide Reserve Lands which may then be designated as Municipal Reserve, School Reserve or Municipal and School Reserve and to specify the amount, type and location thereof;
- (b) provide for the payment of Cash-in-Lieu Monies in place of all or a portion of such Reserve Lands; or

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- (c) provide a combination of Reserve Lands and Cash-in-Lieu Monies;

AND WHEREAS pursuant to Part 5 Division 3 of the Planning Act the Approving Authority is also authorized upon subdivision to:

- (a) determine the allocation of any Municipal Reserve, School Reserve and Municipal and School Reserve set aside by the registered owner between the Municipality and the School Authority as joint owners or as separate owners;
- (b) determine the allocation of any Cash-in-Lieu Monies provided by the registered owner between the Municipality and the School Authority as joint owners or as separate owners; and
- (c) determine the allocation of any combination or Municipal Reserve, School Reserve and Municipal and School Reserve and Cash-in-Lieu Monies set aside or provided, as the case may be, by the registered owner between the Municipality and the School Authority as joint owners or as separate owners.

AND WHEREAS Section 111(2) of the Planning Act provides for restrictions on the use of Reserve Lands held by the Municipality or by the School Authority, or by them jointly to public parks, public recreation areas, schools or separate areas of land that are used for different purposes;

AND WHEREAS Section 111(3) of the Planning Act entitles the Municipality and the School Authority to enter

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into any agreement they consider necessary respecting the use to which the Municipality or the School Authority may put Reserve Lands and any Cash-in-Lieu Monies received in place thereof.

AND WHEREAS IT IS DESIRABLE for the Municipality and the School Authority to enter into this Joint Use Agreement respecting the use of the Reserve Lands, the Municipal Reserve Fund and the School Reserve Fund.

AND WHEREAS the Municipality and the School Authority recognize that certain of their respective functions relating to recreation and education overlap and that by pooling their respective resources the residents of the Municipality may be benefited and in such connection that the Municipality may be able to supplement the funds available to the School Authority for recreational purposes from its Cash-in-Lieu Monies.

NOW THEREFORE THIS AGREEMENT WITNESSETH and the parties hereto covenant and agree as follows:

1. DEFINITIONS

In this Joint Use Agreement the following definitions apply:

- (a) "Approving Authority" means the Council of the Municipality, being the body authorized by Section 87 of the Planning Act to act as the Approving Authority with respect to any applications for subdivision of land within the Municipality, except that in the case of a subdivision approved by the Alberta Planning Board on

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appeal from a decision of the Approving Authority as hereinbefore defined, "Approving Authority" shall mean the Alberta Planning Board.

- (b) "Cash-in-Lieu Monies" means any monies which, with respect to a subdivision of land within the Municipality after January 1, 1991, have been required to be provided by a registered owner as money in place of Municipal Reserve, School Reserve or Municipal and School Reserve under the provisions of the Planning Act.
- (c) "Joint Use Coordinating Committee" means the committee created pursuant to paragraph 5.1.1 hereof.
- (d) "Joint Use Purposes" means the use by a party hereto of the facilities of the other party hereto for the benefit of the residents of the Municipality and also includes the use of the facilities of either party by third parties pursuant to an Outside Lease.
- (e) "Land Titles Act" means the Land Titles Act, R.S.A. 1980, c. L-5, as amended to the date hereof.
- (f) "Municipality" means the Municipal District of Rocky View #44, a Municipal Corporation.
- (g) "Old Reserve Lands" means any lands which, with respect to a subdivision of land within the Municipality prior to January 1, 1991, were provided by a registered owner as Municipal Reserve, School Reserve or Municipal and School Reserve (in each case said term shall not include lands held as Environmental Reserve) under the provisions of the Planning Act and shall also include

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any facilities constructed thereon excepting thereout all of such lands which are subsequently removed from Reserve Land status pursuant to planning legislation then in force.

- (h) "Outside Lease" means a lease of Reserve Lands to any person other than the Municipality or the School Authority for a purpose consistent with the provisions of Section 111(2) of the Planning Act and specifically includes any lease of Reserve Lands to a community association incorporated pursuant to the laws of the Province of Alberta for the purpose of erecting premises for community and recreational uses.
- (i) "Planning Act" means the Planning Act, R.S.A. 1980, c. P-9 as amended to the date hereof.
- (j) "Playing Field Extensions" means any part of a School Reserve which is used as a playing field.
- (k) "Reserve Lands" means any lands which, with respect to a subdivision of land within the Municipality after January 1, 1991, have been provided by a registered owner as Municipal Reserve, School Reserve or Municipal and School Reserve (in each case said term shall not include lands held as Environmental Reserve) under the provisions of the Planning Act and shall also include any facilities constructed thereon excepting thereout all of such lands which are subsequently removed from Reserve Land status pursuant to planning legislation then in force.

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- (l) "School Authority" means the Board of Trustees of the Rocky View School Division #41.
- (m) "School Building Envelopes" means any part of the School Reserve upon which school facilities and buildings, related ornamental lawn areas, parking lots and paved play areas are located.
- (n) "School Development" means the construction of school facilities and buildings including related ornamental lawn areas, parking lots and paved play areas.

2. PURPOSES

2.1 Range of Application

2.1.1 The parties shall endeavour to optimize the services they provide for the benefit of the residents of the Municipality and the residents of the cities, towns and villages situated within the jurisdiction of the School Authority by (a) coordinating their planning, development, maintenance and operation of all Reserve Lands; (b) equitably sharing all costs associated therewith; and (c) coordinating the disposition of any Reserve Lands which are found to be surplus to their needs.

3. RESERVE LANDS

3.1 Protection of All Reserve Lands

3.1.1 All Reserve Lands will be dealt with in accordance with the provisions of this Joint Use Agreement and in compliance with the provisions of the Planning Act.

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### 3.2 Priority of Use

3.2.1 The following priorities of use shall apply with respect to all Reserve Lands:

1. Priority #1: Neighbourhood needs, which include School Authority operated Elementary Schools or the equivalent thereof, and neighbourhood parks;
2. Priority #2: Community needs, which include School Authority operated Junior High Schools or the equivalent thereof and park land required for open space linkages or to provide setbacks beyond land reserved for environmental reserve under the Planning Act at the tops of escarpments or adjacent to water courses; and
3. Priority #3: Regional needs, which includes School Authority operated Senior and Vocational High Schools or the equivalent thereof, pools, arenas, athletic parks and other recreational facilities.

### 3.3 Optimum Planning of Sites

3.3.1 The Approving Authority shall determine the geographical extent of Reserve Lands to be set aside by a registered owner on subdivision or Cash-in-Lieu Monies to be provided in place thereof. Without limiting the Approving Authority's discretion, the Approving Authority shall consider recommendations made by the Municipality and/or the School Authority as to the nature and geographical extent of Reserve Lands to be set aside by a registered owner on



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subdivision or Cash-in-Lieu Monies to be provided in place thereof.

3.3.2 The parties shall determine the location, size and configuration of all Reserve Lands which are to be committed to Joint Use Purposes at the "Area Structure Plan" stage so as to optimize the utilization of available Reserve Lands.

3.3.3 The parties shall determine the location and size of the School Building Envelopes and any Municipal and School Reserve which is to be adjacent thereto. Without limiting the ability of the parties to establish such areas as may be appropriate in any particular case, the parties hereby establish the following guidelines for the area of such parcels:

	<u>School Building Envelopes</u>	<u>Municipal and School Reserve</u>	<u>Total</u>
(a) Elementary School	4 acres	6 acres	10 acres
(b) Junior High School	5 acres	10 acres	15 acres
(c) Senior High School	7 acres	13 acres	20 acres

#### 3.4 **Gymnasia**

3.4.1 The School Authority shall endeavour to ensure that each new school building will have showers, dressing rooms and a gymnasium of Junior High School standards, provided the size of the school building makes this financially feasible.

3.4.2 Subject to the approval of the Municipality, where the Joint Use Coordinating Committee recommends that the recreational needs of the residents of the Municipality will be best served by the provision of a gymnasium of Senior High

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School standards at schools other than Senior High Schools which are to be constructed in the Municipality, the Municipality shall be responsible for the incremental capital costs over and above the costs of construction of the gymnasium of Junior High School standards that would otherwise be built. Where a school is situate in a city, town or village, responsibility for such incremental capital costs shall be shared pro rata by the Municipality and such city, town or village based upon the projected use by the residents of the city, town or village of such gymnasium.

3.4.3 Subject to the approval of the Municipality, the Joint Use Coordinating Committee shall be responsible for negotiating a suitable cost sharing agreement on behalf of the Municipality with such city, town or village. In the event that the Joint Use Coordinating Committee is unable to reach a suitable cost sharing agreement, the Municipality shall be under no obligation to participate financially with plans to construct a gymnasium of any higher standards.

### 3.5 Maintenance Responsibilities

3.5.1 Maintenance of all Municipal Reserve shall be the responsibility of the Municipality.

3.5.2 Maintenance of Municipal and School Reserve and all Playing Field Extensions shall be the responsibility of both Municipality and the School Authority and the amount of the cost of such maintenance born by each of the parties shall be a matter for negotiation between the Municipality and the School Authority based on a recommendation of the Joint Use Coordinating Committee.

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3.5.3 Maintenance of all School Reserve shall be the responsibility of the School Authority.

**3.6 Declaration of Surplus Sites**

3.6.1 In the event that the School Authority wishes to close a school, it shall first advise the Joint Use Coordinating Committee whether such closure is intended to be a permanent closure or a temporary closure pending a return to circumstances under which such school may be reopened or used for alternate School Authority purposes.

**3.7 Temporary Closure of Schools**

3.7.1 If the School Authority states that the school closure is intended to be temporary, the School Authority, subject to Section 113 of the Planning Act, may enter into an Outside Lease, which Outside Lease shall be on terms and conditions as determined by the School Authority including the retention by the School Authority of any revenue derived from the Outside Lease, and thereby use the Reserve Lands for another purpose.

**3.8 Permanent Closures**

3.8.1 If the School Authority states that the school closure is intended to be permanent, the School Authority shall so advise the Municipality. Subject to Section 113 of the Planning Act, the Municipality may enter into an Outside Lease, which Outside Lease shall be on terms and conditions as determined by the Municipality, and thereby use the Reserve Lands for another purpose.

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3.8.2 In the event an Outside Lease is concluded, the Municipality shall inform and advise the School Authority of the pertinent details relative to the accommodation of the Joint Use Purpose.

3.8.3 In the event that a school building which has been declared by the School Authority to be permanently surplus to school needs cannot become the subject matter of an Outside Lease, then the Municipality shall consider a sale of such school as is, and in such event:

1. the Joint Use Coordinating Committee shall arrange for an appraisal to be made of the land, buildings and improvements to be the subject of the sale;
2. the sale proceeds shall, to the extent possible, be allocated as follows:
  - (i) firstly, the Municipality and the School Authority shall each receive one-half of the appraised value of the land; and
  - (ii) secondly, the Municipality and the School Authority shall share the appraised value of the building and improvements on a pro rata basis depending upon the amounts originally contributed by each party to pay for and improve the building and improvements.
3. the retirement of any outstanding debentures associated with such school shall be a School Authority responsibility;

- 12 -

3.8.4 If a school building which has been declared by the School Authority to be permanently surplus to school needs may not be sold as aforesaid and as such should be demolished:

1. the demolition and site rehabilitation costs shall be borne out of the School Reserve Fund and such costs shall also form a first charge on the proceeds of any subsequent sale of the Reserve Land on which such school building is situate.
2. the retirement of any outstanding debentures associated with such school shall be a School Authority responsibility.

3.8.5 In the event that demolition of a school building occurs as provided in paragraph 3.8.3 above, then the School Reserve upon which the school building was situate shall be redesignated as Municipal Reserve and registered as such. Legal title to such Municipal Reserve shall vest in the Municipal Authority as provided in Section 113 of the Planning Act.

3.8.6 The consideration payable by the Municipal Authority for the transfer of such redesignated Municipal Reserve shall be \$1.00.

3.8.7 When School Reserve has been declared permanently surplus to school needs and legal title has been transferred to the Municipality as part of a redesignation, the Municipality shall assume responsibility for the maintenance of such new Municipal Reserve.

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### 3.9 Disposal of Reserve Lands to Third Parties

3.9.1 Subject to paragraph 3.8, the parties hereto shall consult with each other prior to taking any steps under Section 115 of the Planning Act in connection with the disposal of Reserve Lands so as to ensure that the future needs of each of the parties hereto are taken into consideration.

## 4. MUNICIPAL AND SCHOOL RESERVE FUNDS

### 4.1 Sources

4.1.1 The Approving Authority shall be advised of the existence of this Joint Use Agreement and as such requested to allocate in accordance with the provisions of this Joint Use Agreement one-half of all Cash-in-Lieu Monies provided in place of Reserve Lands to the Municipality and one-half to the School Authority.

4.1.2 Notwithstanding paragraph 4.1.1, but subject to section 6.2 hereof, in the event that a new school authority is established within the jurisdiction of the Municipality which desires to participate with the Municipality and the School Authority in the allocation of Cash-in-Lieu Monies, the Approving Authority shall continue to allocate one-half of Cash-in-Lieu Monies to the Municipality and the remaining one-half of Cash-in-Lieu Monies shall be allocated between the School Authority and such new school authority as appropriate in proportion to the number of resident students for which each School Authority is responsible in the school district in which the relevant subdivision is situated.

- 14 -

4.1.3 Cash-in-Lieu Monies allocated to the Municipality shall be held in a reserve fund to be administered by the Municipality (the "Municipal Reserve Fund") and Cash-in-Lieu Monies allocated to the School Authority shall be held in a reserve fund which shall also be administered by the Municipality (the "School Reserve Fund"). Payments from the School Reserve Fund to the School Authority to enable it to meet the funding requirements for any use authorized by paragraph 4.2.1 hereof which were discussed and reviewed by the Joint Use Coordinating Committee as part of its coordination and administration of this Joint Use Agreement shall be made to the School Authority upon a written request by the School Authority.

4.1.4 The proceeds derived from the sale, assignment or other transfer of any Reserve Lands or any Old Reserve Lands, which sale, assignment or other transfer occurs after January 1, 1991, shall be allocated one-half to the Municipal Reserve Fund and one-half to the School Reserve Fund.

**4.2 Use of the Municipal Reserve Fund  
and the School Reserve Fund**

4.2.1 Subject to the provisions of Section 111 and 112 of the Planning Act, the Municipal Reserve Fund may be expended for such purposes as the Municipality may determine and the School Reserve Fund may be expended for such purposes as the School Authority may determine, provided however that in all cases the Municipal Reserve Fund may only be used for capital expenditures as outlined in Policy 603 of the Municipal District of Rocky View No. 44 Policy Handbook and the School Reserve Fund may only be used for capital expenditures associated with School Building Envelopes. The Municipality

- 15 -

and the School Authority shall establish mutually agreeable procedures whereby the School Reserve Fund may be invested in a manner which will yield a reasonable return on the funds and at the same time ensure that they are available to the School Authority when they are required.

5. OPERATIVE FRAMEWORK

5.1 **Need for Joint Committee**

5.1.1 The parties shall create an administrative committee to be known as the "Joint Use Coordinating Committee" which committee shall consist of two representatives from the management of the School Authority to be appointed by the School Authority and two representatives from the management of the Municipality to be appointed by the Municipality.

5.2 **Duties of the Joint Use Coordinating Committee**

5.2.1 The Joint Use Coordinating Committee shall:

1. coordinate and administer this Joint Use Agreement on behalf of the parties;
2. notwithstanding paragraph 6.1.1, report to the Chief Administrative Officers of the Municipality and the School Authority at any time and from time to time when they feel such a report is necessary;
3. prepare in writing an "Annual Report" which summarizes the previous year's activities for submission to the Chief Administrative Officers;



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4. meet as often as is necessary and at least on a quarterly basis;
5. operate on a consensus basis on the understanding that on those issues on which agreement cannot be reached reference is to be made to the Chief Administrative Officers. If no consensus is reached between the Chief Administrative Officers, then reference is to be made to the Council of the Municipality and the Board of Trustees of the School Authority for direction as to final resolution;
6. set up sub-committees and delegate functions and responsibilities to them; and
7. review and make recommendations respecting the utilization of any Municipal and School Reserve.

6. **REVIEW, AMENDMENT AND TERMINATION OF  
THIS JOINT USE AGREEMENT**

6.1 **Review**

6.1.1 The terms of the Joint Use Agreement shall be reviewed and a report shall be made by the Joint Use Coordinating Committee to the Chief Administrative Officers of the parties:

1. every five years; and

- 17 -

2. after any changes are made to the Planning Act which may affect the Reserve Land provisions of this Joint Use Agreement;

in either case recommending any amendments to be made to the Joint Use Agreement necessitated by the passage of time or the changes to the Planning Act.

## 6.2 Amendments

6.2.1 At any time and from time to time either party hereto may submit for review by the Joint Use Coordinating Committee any proposed amendments which they believe are necessary to ensure the proper framework for the carrying out of their respective responsibilities under this Joint Use Agreement.

6.2.2 Any recommendation made by the Joint Use Coordinating Committee under paragraph 6.1.1 and any amendments submitted to the Joint Use Coordinating Committee in accordance with paragraph 6.2.1 shall be reviewed by the Chief Administrative Officers of the parties hereto within six (6) calendar months of such amendments or recommendations being proposed.

6.2.3 If the Chief Administrative Officers of the parties both agree upon the necessity for and the content of the proposed amendments arising from the recommendations of the Joint Use Coordinating Committee, then the Chief Administrative Officers shall submit such proposed amendments for the approval of the Council of the Municipal Corporation and the Board of Trustees of School Authority.

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6.2.4 In the event that the Chief Administrative Officers of the parties do not agree upon such proposed amendments, then the proposed amendments shall be referred to the Council of the Municipality and the Board of Trustees of the School Authority along with a synopsis of such disagreement for their consideration. If the Council of the Municipality and the Board of Trustees of the School Authority cannot agree upon such proposed amendments, then this Joint Use Agreement shall not be amended.

### 6.3 Term and Termination

6.3.1 The parties agree that this Joint Use Agreement shall be for an initial term of three years during which it may not be terminated except as provided in 1. below and at the end of such initial three year term shall be, subject to the following, continued for an indefinite term until it is terminated:

1. upon the agreement of the parties; or
2. upon a notice in writing having been given by either party to the other of at least one year, which notice may be given at any time after the end of the second year of the initial term, in which case this Joint Use Agreement shall terminate at the end of such one year notice period.

### 7. PROVISIONS FOUND INVALID

7.1 If one or more provisions of this Joint Use Agreement are, for any reason, held to be invalid, the

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parties intend that all remaining provisions are to remain in full force and effect.

8. ARBITRATION

8.1 If the parties should have any disputes arising out of the operation or the interpretation of this Joint Use Agreement, then such dispute may, at the written request of either party, be submitted to arbitration in accordance with the provisions of the Arbitration Act (Alberta) R.S.A. 1980, c. A-43. The Board of Arbitration shall consist of three members, one appointed by the Municipality and one appointed by the School Authority and a third member appointed by the persons so appointed, provided however that if either party shall fail to appoint a member within twenty days after the written request for arbitration and the other party shall have appointed a member, then the Board of Arbitration shall consist of the member who shall be the person appointed by the other party. The arbitrators so appointed shall have all the powers accorded by said Arbitration Act. The decision of the Board of Arbitration shall, subject to appeal, be final and binding upon the parties hereto. The costs of the arbitration shall be shared equally by the parties.

9. PRE-JANUARY 1, 1991 RESERVE LANDS  
AND CASH-IN-LIEU FUNDS

9.1 All reserve lands held by the Municipality prior to January 1, 1991, all cash-in-lieu monies received in connection with a subdivision approved by the Approving Authority prior to January 1, 1991 and all proceeds from the sale of any reserve lands held by the Municipality prior to

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January 1, 1991 shall continue to belong exclusively to and be administered solely by the Municipality.

10. COSTS OF SUBDIVISION, REDESIGNATION AND TRANSFER

10.1 The Municipality agrees that any fees which would otherwise be payable by the School Authority to the Council in connection with subdivision or redesignation applications made by the School Authority and any transfer of land associated with such subdivision and redesignation are hereby waived on the condition that the School Authority be solely responsible for all legal costs associated therewith.

11. MISCELLANEOUS

11.1 The address for delivery of notices or other documents required or permitted hereunder shall be at the address adjacent to the party's name as follows or such other address as the other parties hereto may have been duly notified:

Municipal District of Rocky View #44  
[Address]

Attention: Municipal Manager

The Board of Trustees of the Rocky View  
School Division #41  
[Address]

Attention: Superintendent of Schools

11.2 Any notice shall be delivered to the address indicated above and shall be deemed to have been delivered to

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and received by the addressee thereof on the date of delivery.

11.3 This Joint Use Agreement shall be governed by and construed in accordance with the laws of the Province of Alberta.

IN WITNESS WHEREOF the parties hereto have caused this Agreement to be executed as of the 16<sup>th</sup> day of APRIL, 1991.

THE MUNICIPAL DISTRICT OF ROCKY VIEW #44

Per: [Signature]

Per: [Signature]

THE BOARD OF TRUSTEES OF THE ROCKY VIEW SCHOOL DIVISION #41

Per: [Signature]

Per: [Signature]



2651 Chinook Winds Dr. SW, Airdrie, Alberta T4B 0B4  
403.945.4000 p 403.945.4001 f  
[www.rockyview.ab.ca](http://www.rockyview.ab.ca)

September 27, 2018

Sent by E-mail

Ranch Lands Recreation Board  
C/O Susan de Caen  
Rocky View County  
911 - 32 Ave NE  
Calgary, AB T2E 6X6

**Re: Proposal for Additional Gym Area within the New Westbrook School Facility Design**

To Members of the Ranch Lands Recreation Board:

Rocky View Schools (RVS) supports a proposal, drafted by the Westbrook School Community, in which it is proposed RVS will reconsider the designs for the new Westbrook School, particularly the gym, with the understanding that funding would need to be obtained. The proposal requests that the gym be enlarged, allowing for a more versatile space and providing room for bleachers and/or spectators.

The replacement school is funded in partnership with Alberta Education, Alberta Infrastructure and RVS. The main portion of the school is funded through RVS and the addition of the modular units (classrooms) is to be supported by Government. Unfortunately, the area allocation RVS would be able to support (a total of 465 m<sup>2</sup>, calculated based on school student capacity and area) does not accommodate very much (if any) room for spectators.

Having a larger gym, with room for spectators, would be a wonderful addition to the community, allowing for a much wider range of uses by the entire community. The gym would be available to the larger community, within the scope of Board Policy 22, for reasonable rental rates after school hours. The design allows the gym to be a separate space, located next to the serverly, which can be used while still maintaining the security of the school, showing RVS' commitment to community use of the gym.

RVS is willing to loan Westbrook School, on behalf of the Westbrook School Community, \$100,000 if the Ranch Lands Recreation District matches the amount to support the gym expansion. RVS is willing to provide the loan as this project is time sensitive and this will allow Friends of Westbrook time to raise the funds needed. The adjustments to the gym size will be at minimal cost as construction and design are already occurring. Construction of this schools is anticipated to start in summer 2019, therefore, RVS needs to decide whether or not to move forward with the expanded gym by November of this year.

Thank you for considering this initiative and proposed venture. If you have any questions, please do not hesitate to contact me either by phone (403-945-4010) or e-mail ([cwinter@rockyview.ab.ca](mailto:cwinter@rockyview.ab.ca)).

Sincerely,



Colette Winter  
Director of Facility Planning

CW/sd

Cc: Christine Parker, Principal, Westbrook School  
Kristi Purnel, Westbrook School Community  
Karen Allison, Westbrook School Community





ROCKY VIEW COUNTY  
Cultivating Communities

TEL 403-230-1401  
FAX 403-277-5977

911-32 Ave NE | Calgary, AB | T2E 6X6  
www.rockyview.ca

October XX, 2018

File: 2025-100

Greg Luterbach, Superintendent of Schools  
Rocky View School Division No. 41  
2651 Chinook Winds Drive Southwest  
Airdrie, AB T4B 0B4

Dear Mr. Luterbach,

**Re: Letter of Notification to Convene Reserves Coordinating Committee**

The Friends of Westbrook School (FOW) have made a \$100,000 capital grant application to the County to assist with school redevelopment. As the grant funding request is for a new school expanded gymnasium, policy direction comes from the Reserves Agreement (1998) between the County and Rocky View Schools (RVS). Under this agreement the County has appointed Councillors Schule and McKylor to the RV School District/RVC Joint Working Committee of the Board of Trustees. The committee is the forum to discuss municipal financial support for gymnasium development.

This funding request would also provide an opportunity for the County and RVS to update the reserves agreement which is dated. Consequently *this letter serves as notification that Rocky View County requires a commencement of the Committee to discuss the capital grant application for Westbrook School.*

I have suggested County staff meet with your team to set a meeting date to gain a better understanding of RVS plans for Westbrook School and discuss a Committee meeting to discuss the potential for municipal funding and other joint use capital projects expected in the near future such as the artificial turf field in Chestermere and community hall development in Springbank. I have asked Corwin McCullagh, Manager of Recreation & Community Services, to follow this letter up by contacting Colette Winter to arrange a meeting.

Regards,

J.R. (Rick) MacDonald  
**Interim County CAO**

cc: Colette Winter, Director of Facility Planning, Rocky View Schools  
Kristi Purnel, Westbrook School Community  
Richard Barss, Manager, Intergovernmental Affairs  
Chris O'Hara, General Manager, Development Services  
Crystal Kissel, Councillor, Division 9



## ENGINEERING SERVICES

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**TO:** Council  
**DATE:** October 23, 2018 **DIVISION:** 9  
**FILE:** PL20160018  
**SUBJECT:** Bylaw C-7835-2018 to Revise Road Closure Bylaw C-7745-2017

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### **<sup>1</sup>POLICY DIRECTION:**

The road closure application, which was approved at the June 12, 2018 Council meeting, was evaluated against Rocky View County Policy #433, "Road Allowance Closure and Disposal," and the *Municipal Government Act* and was found to be compliant:

- This portion of road allowance is not part of the County's 30 Year Long Range Transportation Network Plan (LRTNP) nor does Administration have any plans to construct within the road allowance;
- While Township Road 284 and Horse Creek Road are in close proximity to the LRTNP, the road closure and consolidation would not negatively impact on those roads; and
- The closure and consolidation would not restrict access to any adjacent parcels nor would it create any landlocked parcels, as remaining open road allowance is available for the adjacent parcels from both the north and south road allowances.

The required revision to Road Closure Bylaw C-7745-2017 is in accordance with the *Municipal Government Act* and road closure bylaw revision guidelines provided by Alberta Transportation.

### **EXECUTIVE SUMMARY:**

A revision is required to the legal description for Road Closure Bylaw C-7745-2017 so it can be endorsed by Land Titles. Administration is consistently challenged by this provincial department in regards to road closure bylaw legal descriptions, requiring the bylaws to be revised. Other municipalities also experience the same challenges when dealing with Land Titles. Administration provides the best possible legal description but the decision to accept the bylaw for registration ultimately lies with Land Titles.

Section 22 of the the *Municipal Government Act* gives Council the authority to close a road by bylaw with the requirements being that the bylaw be advertised, that the bylaw must receive approval by the Minister of Transportation prior to second and third reading, and that before passing the bylaw any person who claims to be prejudicially affected by the bylaw must be given an opportunity to be heard by Council. This bylaw revision does not require ministerial consent because it does not change the substance of the original road closure bylaw which went through the process as noted above.

### **DISCUSSION:**

The applicant requested this road closure to close and consolidate the road area into their property for the purpose of gaining access in the form of a panhandle to their property for the ultimate goal of building a dwelling. On January 9, 2018, Road Closure Bylaw C-7745-2017 was brought to Council for a Public Hearing and first reading, which was granted, and the Bylaw was then sent to the Minister of Transportation for approval, which was granted on March 21, 2018. The bylaw was then brought back to Council on June 12, 2018 where it was given second and third readings by Council.

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<sup>1</sup>**Administrative Resources**  
Angela Pare, Engineering Services Support Technician



After second and third readings were given, Administration sent Bylaw C-7745-2017 to Land Titles for registration. The bylaw was rejected due to the size of the parcels being closed not exactly matching the survey plan prepared after the creation of the bylaw. Land Titles has instructed Administration to revise the description to match the survey plan so they can then proceed with registering the bylaw.

Bylaw C-7745-2017 requires the following change to the description:

PARCEL 1

A PORTION OF THE ORIGINAL GOVERNMENT ROAD ALLOWANCE ADJACENT TO THE SOUTH HALF OF THE NORTH EAST QUARTER SECTION 30, TOWNSHIP 28, RANGE 4, WEST OF THE 5<sup>TH</sup> MERIDIAN CONTAINING ~~0.79 HECTARES (1.96 ACRES)~~ **0.81 HECTARES (2.00 ACRES)** MORE OR LESS EXCEPTING THEREOUT ALL MINES AND MINERALS

PARCEL 2

THE ORIGINAL GOVERNMENT ROAD ALLOWANCE ADJACENT TO THE EAST HALF OF THE SOUTH EAST QUARTER SECTION 30, TOWNSHIP 28, RANGE 4, WEST OF THE 5<sup>TH</sup> MERIDIAN CONTAINING ~~4.63 HECTARES (4.02 ACRES)~~ **1.62 HECTARES (4.00 ACRES)** MORE OR LESS EXCEPTING THEREOUT ALL MINES AND MINERALS

As Shown on PLAN \_\_\_\_\_, Schedule 'A' attached to and forming part of this bylaw.

**OPTIONS:**

- Option #1      Motion #1      THAT Bylaw C-7835-2018 be given first reading.
- Motion #2      THAT Bylaw C-7835-2018 be given second reading.
- Motion #3      THAT Bylaw C-7835-2018 be considered for third reading.
- Motion #4      THAT Bylaw C-7835-2018 be given third and final reading.
- Option #2      THAT alternative direction be provided.

Respectfully submitted,

Concurrence,

"Byron Riemann"

"Rick McDonald"

\_\_\_\_\_  
General Manager

\_\_\_\_\_  
Interim County Manager

AP/bg

**ATTACHMENTS:**

- ATTACHMENT 'A' - Proposed Bylaw C-7835-2018
- ATTACHMENT 'B' - Bylaw C-7745-2017
- ATTACHMENT 'C' - Mapset



## BYLAW C-7835-2018

### A Bylaw of Rocky View County, in the Province of Alberta, to Revise Road Closure Bylaw C-7745-2017.

**WHEREAS** the Council of Rocky View County is of the opinion that a revision to Bylaw C-7745-2017 is required to clarify the legal description of the portion of road being closed;

**AND WHEREAS** the *Municipal Government Act*, c M-26, RSA 2000, as amended from time to time, permits changes to the substance of the bylaw to bring out more clearly what is considered to be the meaning of Bylaw C-7745-2017;

**AND WHEREAS** the Chief Administrative Officer of Rocky View County certifies that the proposed revisions have been prepared in accordance with Section 63(4) of the *Municipal Government Act*, c M-26, RSA 2000, as amended from time to time;

**NOW THEREFORE** the Council of Rocky View County, duly assembled, does hereby revise the legal description contained in Bylaw C-7745-2017 as follows:

#### PARCEL 1

A PORTION OF THE ORIGINAL GOVERNMENT ROAD ALLOWANCE ADJACENT TO THE SOUTH HALF OF THE NORTH EAST QUARTER SECTION 30, TOWNSHIP 28, RANGE 4, WEST OF THE 5<sup>TH</sup> MERIDIAN CONTAINING ~~0.79 HECTARES (1.96 ACRES)~~ **0.81 HECTARES (2.00 ACRES)** MORE OR LESS EXCEPTING THEREOUT ALL MINES AND MINERALS

#### PARCEL 2

THE ORIGINAL GOVERNMENT ROAD ALLOWANCE ADJACENT TO THE EAST HALF OF THE SOUTH EAST QUARTER SECTION 30, TOWNSHIP 28, RANGE 4, WEST OF THE 5<sup>TH</sup> MERIDIAN CONTAINING ~~1.63 HECTARES (4.02 ACRES)~~ **1.62 HECTARES (4.00 ACRES)** MORE OR LESS EXCEPTING THEREOUT ALL MINES AND MINERALS

As Shown on PLAN \_\_\_\_\_, Schedule 'A' attached to and forming part of this bylaw.

#### TRANSITIONAL:

Bylaw C-7835-2018 is passed when it receives third reading and is signed by the Reeve/Deputy Reeve and the CAO or Designate as per the *Municipal Government Act*.

**Division:** 9  
**File:** PL20160018



READ A FIRST TIME IN COUNCIL this \_\_\_\_\_ day of \_\_\_\_\_, 2018

READ A SECOND TIME IN COUNCIL this \_\_\_\_\_ day of \_\_\_\_\_, 2018

*UNANIMOUS PERMISSION FOR THIRD READING this \_\_\_\_\_ day of \_\_\_\_\_, 2018*

READ A THIRD TIME IN COUNCIL this \_\_\_\_\_ day of \_\_\_\_\_, 2018

\_\_\_\_\_  
Reeve

\_\_\_\_\_  
CAO or Designate

\_\_\_\_\_  
Date Bylaw Signed



## BYLAW C-7745-2017

**A Bylaw of Rocky View County in the Province of Alberta for the Purpose of closing to public travel and creating title to portions of public highway in accordance with Section 22 of the *Municipal Government Act*, Chapter M26.1, Revised Statutes of Alberta 2000, as amended.**

The Council of Rocky View County enacts as follows:

### WHEREAS

The lands hereafter described are no longer required for public travel; and

### WHEREAS

Application has been made to Council to have the highway closed; and

### WHEREAS

Rocky View County Council deems it expedient to provide for a bylaw for the purpose of closing to public travel certain roads, or portions thereof, situated in the said municipality, and therefore disposing of the same; and

### WHEREAS

Notice of the intention of Council to pass a bylaw has been given in accordance with Section 606 of the Municipal Government Act, and was published in the Rocky View Weekly on Tuesday December 12<sup>th</sup>, 2017 and December 19<sup>th</sup>, 2017 the last of such publications being at least one week before the day fixed for the Public Hearing of this Bylaw; and

### WHEREAS

Rocky View County Council was not petitioned for an opportunity to be heard by any person claiming to be prejudicially affected by the bylaw.

**NOW THEREFORE BE IT RESOLVED** that the Council of Rocky View County in the Province of Alberta does hereby close to public travel for the purpose of creating title to the following described highway. Subject to the rights of access granted by other legislation:

#### PARCEL 1

A PORTION OF THE ORIGINAL GOVERNMENT ROAD ALLOWANCE ADJACENT TO THE SOUTH HALF OF THE NORTH EAST QUARTER SECTION 30, TOWNSHIP 28, RANGE 4, WEST OF THE 5<sup>TH</sup> MERIDIAN CONTAINING 0.79 HECTARES (1.96 ACRES) MORE OR LESSEXCEPTING THEREOUT ALL MINES AND MINERALS

#### PARCEL 2

THE ORIGINAL GOVERNMENT ROAD ALLOWANCE ADJACENT TO THE EAST HALF OF THE SOUTH EAST QUARTER SECTION 30, TOWNSHIP 28, RANGE 4, WEST OF THE 5<sup>TH</sup> MERIDIAN CONTAINING 1.63 HECTARES (4.02 ACRES) MORE OR LESS EXCEPTING THEREOUT ALL MINES AND MINERALS

As Shown on PLAN \_\_\_\_\_, Schedule 'A' attached to and forming part of this bylaw.

Division: 9  
File: PL20160018

READ A FIRST TIME IN COUNCIL this 9<sup>th</sup> DAY OF January, 2018

PUBLIC HEARING WAS HELD IN COUNCIL this 9<sup>th</sup> DAY OF January, 2018

[Signature]  
REEVE / DEPUTY REEVE

[Signature]  
CAO or DESIGNATE

APPROVED BY  
ALBERTA TRANSPORTATION:

APPROVED THIS 15<sup>th</sup> DAY OF March, 2018

Approval Valid for \_\_\_\_\_ Months

[Signature]  
MINISTER OF TRANSPORTATION

READ A SECOND TIME IN COUNCIL this 12<sup>th</sup> DAY OF June, 2018

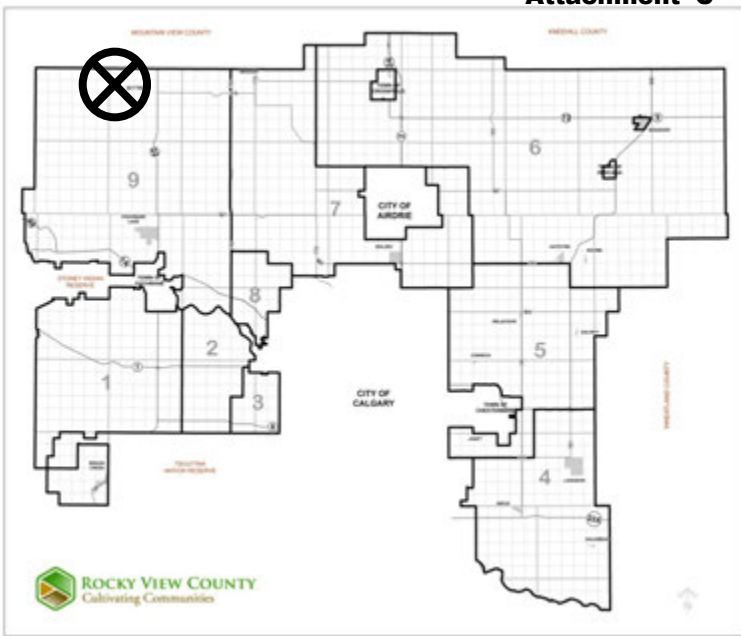
READ A THIRD TIME IN COUNCIL this 12<sup>th</sup> DAY OF June, 2018

[Signature]  
REEVE / DEPUTY REEVE

[Signature]  
CAO or DESIGNATE







# LOCATION PLAN

## NE/SE-30-28-04-W05M

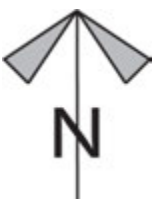




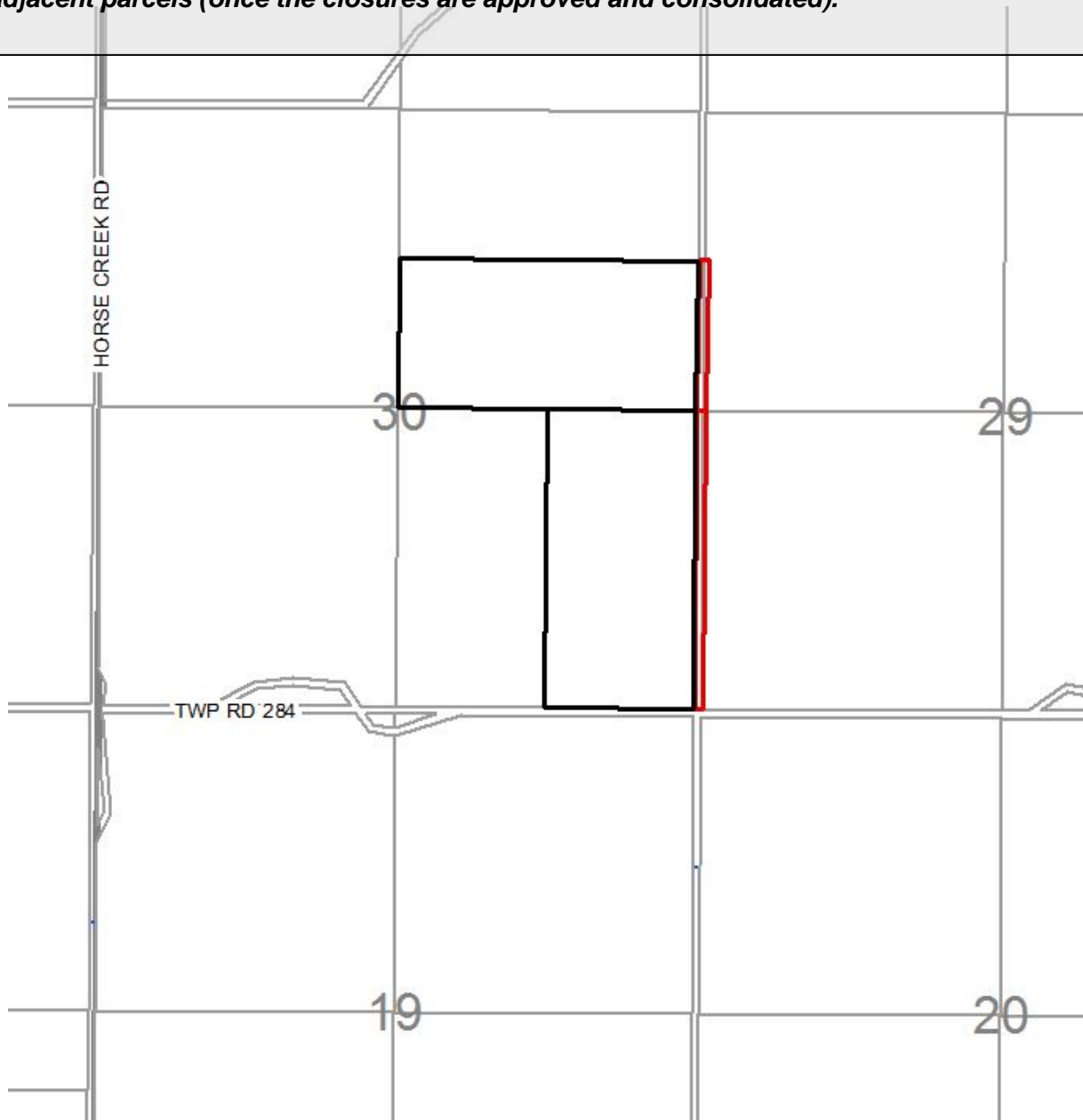
Parcel 1 to be Consolidated into NE-30  
 Parcel 2 to be Consolidated into SE-30

**TENTATIVE PLAN**

**NE/SE-30-28-04-W05M**



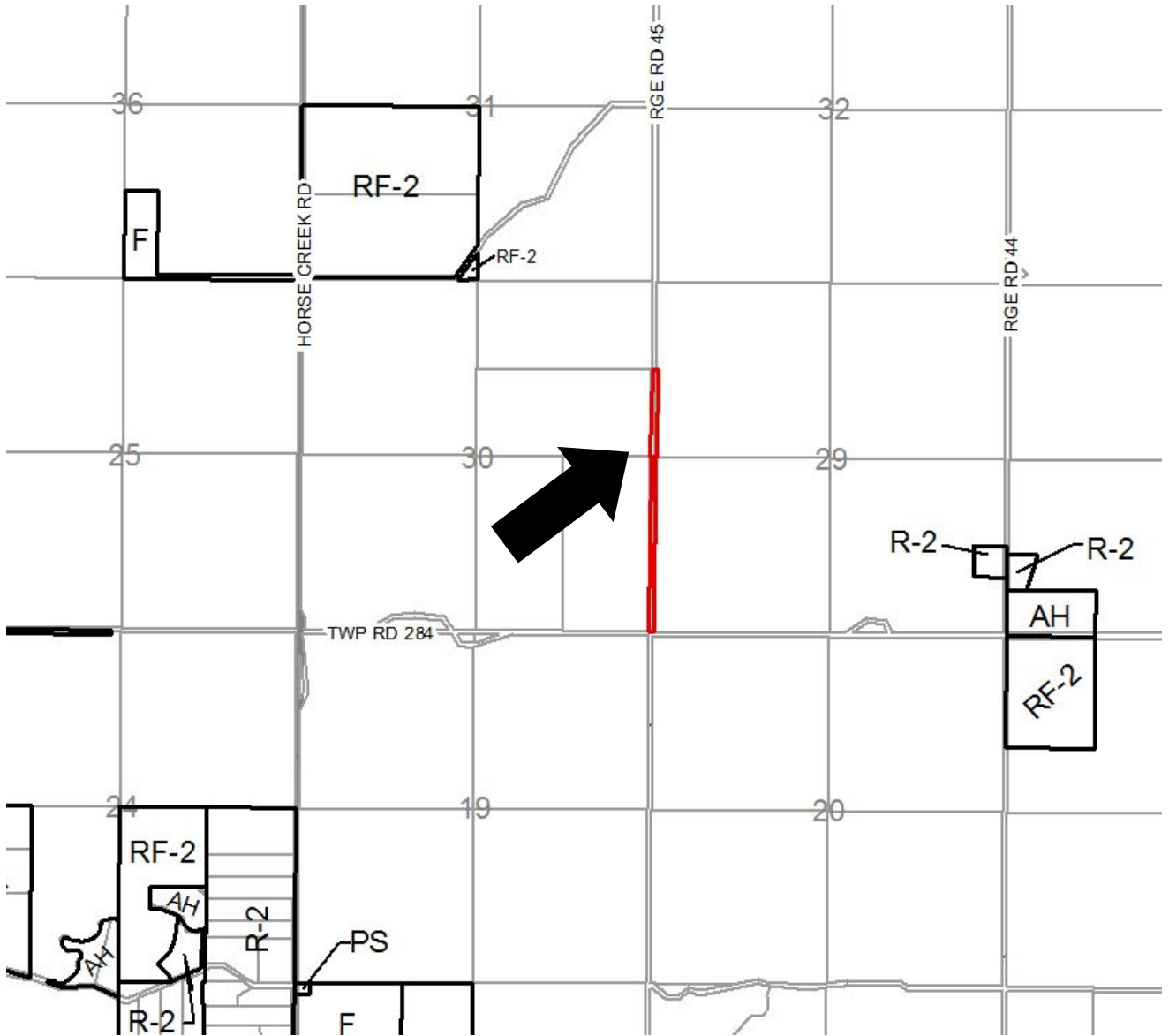
**ROAD CLOSURE PROPOSAL: Joint Application to Close for Consolidation** Page 10 of 14  
 purposes, 2 portions of Road Allowance Adjacent to the NE/SE-30-28-04-W5M. Parcel 1 (+/- 1.96 Acres) would be consolidated with the NE Quarter. Parcel 2 (+/- 4.02 Acres) would be consolidated with the SE Quarter. *NOTE: Both Applicants are prepared to enter into Access Easement Agreements to allow access to adjacent parcels (once the closures are approved and consolidated).*



# ROAD CLOSURE PROPOSAL

## NE/SE-30-28-04-W05M





RF2	Ranch and Farm Two	B-1	Highway Business
RF3	Ranch and Farm Three	B-2	General Business
AH	Agricultural Holding	B-3	Limited Business
F	Farmstead	B-4	Recreation Business
R-1	Residential One	B-5	Agricultural Business
R-2	Residential Two	B-6	Local Business
R-3	Residential Three	NRI	Natural Resource Industrial
DC	Direct Control	HR-1	Hamlet Residential Single Family
PS	Public Service	HR-2	Hamlet Residential (2)
		HC	Hamlet Commercial
		AP	Airport

# LAND USE MAP

## NE/SE-30-28-04-W05M



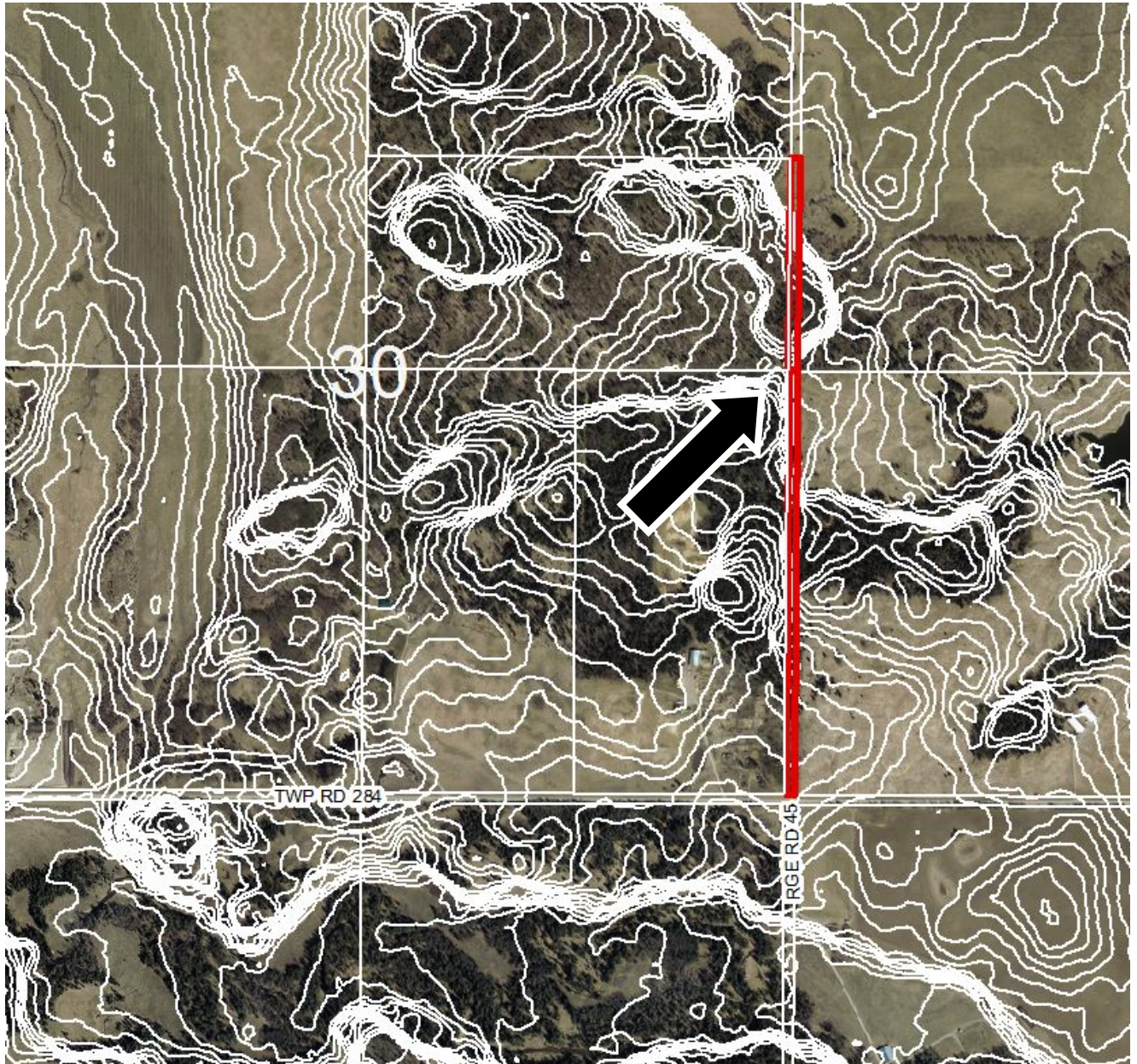


Note: Post processing of raw aerial photography may cause varying degrees of visual distortion at the local level.

**AIR PHOTO**  
Spring 2016

**NE/SE-30-28-04-W05M**



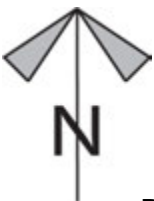


Contours are generated using 10m grid points, and depict general topographic features of the area. Detail accuracy at a local scale cannot be guaranteed. They are included for reference use only.



# TOPOGRAPHY



*Contour Interval 2 M*

## NE/SE-30-28-04-W05M





 **OPPOSE**  
 **SUPPORT**

Legend	
	Circulation Area
	Subject Lands

# LANDOWNER CIRCULATION AREA

## NE/SE-30-28-04-W05M



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## PLANNING SERVICES

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**TO:** Council  
**DATE:** October 23, 2018 **DIVISION:** 2  
**FILE:** 05714035 **APPLICATION:** PL20180045  
**SUBJECT:** Redesignation Item – Residential Two District to Residential One District

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### **<sup>1</sup>POLICY DIRECTION:**

The application was evaluated against the Central Springbank Area Structure Plan (ASP), and the lands are categorized therein as Infill Residential. The proposed redesignation application was found to be in compliance:

- The proposal is consistent with the policies in Sections 2.9.2 – General Residential Development Policies, and 2.9.3 – Infill Residential Areas Policies.

### **EXECUTIVE SUMMARY:**

The purpose of this application is to redesignate the subject land from Residential Two District to Residential One District. This would facilitate a subdivision to create a ± 1.01 hectare (± 2.50 acre) parcel with a ± 1.29 hectare (± 3.18 acre) remainder.

This application was presented to Council on September 25, 2018, and received first reading. Subsequently, Council passed the following motion:

*MOVED by Councillor McKylor that Administration be directed to provide the following additional information for Bylaw C-7824-2018 prior to second reading at the October 23, 2018 Council meeting:*

- a) *Remaining acreage size excluding the escarpment and restrictive covenant area; and*
- b) *If the remaining size can support two proposed houses, two septic fields, and two water wells.*

In order to address this matter, Administration created a to-scale site plan that demonstrates there is sufficient space to properly develop two lots on the subject lands (Appendix 'A'). The site plan shows:

- Total developable area = ± 4.42 acres:
  - Equals the total lot area (5.66 acres) *minus* the Restrictive Covenant area (1.24 acres);
  - ± 2.21 acres of developable area *per lot*.
- One 3,500 ft<sup>2</sup> house with attached garage *per lot*:
  - Minimum 60.00 m setback from Bow River;
  - Minimum 3.00 m side yard setback;
  - Minimum 45.00 m front setback from County Road, or 15.00 m setback from internal road.
- Two proposed 15' x 15' septic tanks *per lot*:
  - Minimum 10.00 m setback from Bow River;
  - Minimum 1.00 m setback from property lines;
- Two proposed 20' x 150' septic fields *per lot*.
  - Minimum 90.00 m setback from Bow River.
- One water well *per lot*:

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### **<sup>1</sup> Administrative Resources**

Lindsey Ganczar, Planning Services  
Eric Schuh, Engineering Services





- o Minimum 1.00 m setback from property lines.

The subject property currently contains an accessory building (which is proposed to be removed), and is currently serviced by water well and a conventional septic system. The proposed lots would continue to be serviced by water wells, and a private sewage treatment system would be required for wastewater servicing for both Lot 1 and Lot 2. A Deferred Services Agreement would be required to be registered on the title of each parcel, requiring the owners to connect to Rocky View County services when they become available in the future.

The western-most portion of the subject lands (with frontage onto Springbank Heights Way) is very steep, and a restrictive covenant is registered on title to prevent development in this location. As such, legal access to the subject parcel is provided from the parcel to the north via an access easement agreement.

All technical considerations for a new parcel would be addressed through the subdivision application. Administration determined that the application meets policy.

**CONCLUSION:**

The to-scale site plan prepared by Administration demonstrates that there is sufficient space to properly develop two lots on the subject lands, and the application is consistent with the policies of the Central Springbank ASP and the Land Use Bylaw.

**OPTIONS:**

- Option # 1:    Motion #1        THAT Bylaw C-7824-2018 be given second reading.  
                  Motion #2        THAT Bylaw C-7824-2018 be given third and final reading.
- Option # 2:    THAT Application PL20180045 be refused.

Respectfully submitted,

Concurrence,

“Chris O’Hara”

“Rick McDonald”

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General Manager

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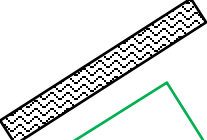
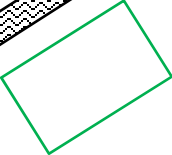
Interim County Manager

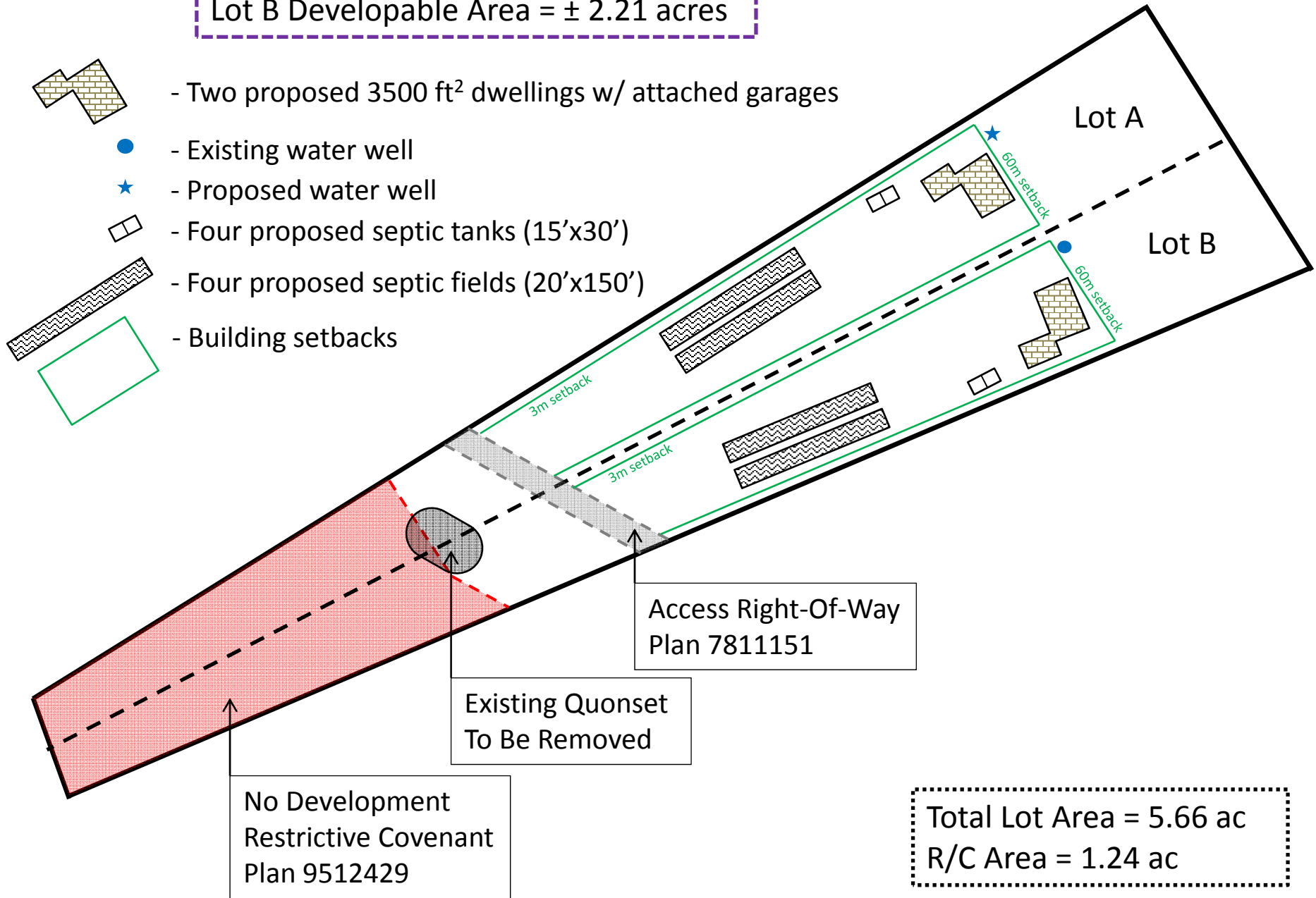
LG/rp

**APPENDICES:**

- APPENDIX ‘A’: Proposed Site Plan
- APPENDIX ‘B’: Original September 25, 2018, Staff Report package

Lot A Developable Area = ± 2.21 acres  
Lot B Developable Area = ± 2.21 acres

-  - Two proposed 3500 ft<sup>2</sup> dwellings w/ attached garages
-  - Existing water well
-  - Proposed water well
-  - Four proposed septic tanks (15'x30')
-  - Four proposed septic fields (20'x150')
-  - Building setbacks





## PLANNING SERVICES

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**TO:** Council  
**DATE:** September 25, 2018 **DIVISION:** 2  
**TIME:** Afternoon Appointment  
**FILE:** 05714035 **APPLICATION:** PL20180045  
**SUBJECT:** Redesignation Item – Residential Two District to Residential One District

---

### **<sup>1</sup>POLICY DIRECTION:**

The application was evaluated against the Central Springbank Area Structure Plan (ASP), and the lands are categorized therein as Infill Residential. The proposed redesignation application was found to be in compliance:

- The proposal is consistent with the policies in Sections 2.9.2 – General Residential Development Policies, and 2.9.3 – Infill Residential Areas Policies.

### **EXECUTIVE SUMMARY:**

The purpose of this application is to redesignate the subject land from Residential Two District to Residential One District. This would facilitate a subdivision to create a  $\pm 1.01$  hectare ( $\pm 2.50$  acre) parcel with a  $\pm 1.29$  hectare ( $\pm 3.18$  acre) remainder.

The subject property contains only an accessory building, and is currently serviced by water well and a conventional septic system. The proposed lots would continue to be serviced by water wells, and a private sewage treatment system would be required for wastewater servicing for both Lot 1 and Lot 2. A Deferred Services Agreement would be required to be registered on the title of each parcel requiring the owners to connect to Rocky View services when they become available in the future.

The western-most portion of the subject lands (with frontage onto Springback Heights Way) is very steep, and a restrictive covenant is registered on title to prevent development in this location. As such, legal access to the subject parcel is provided from the parcel to the north via an access easement agreement.

All technical considerations for a new parcel would be addressed through the subdivision application.

Administration determined that the application meets policy.

**DATE APPLICATION RECEIVED:** May 1, 2018  
**DATE DEEMED COMPLETE:** May 9, 2018

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**PROPOSAL:** To redesignate the subject land from Residential Two District to Residential One District to facilitate the creation of a  $\pm 1.01$  hectare ( $\pm 2.50$  acre) parcel with a  $\pm 1.29$  hectare ( $\pm 3.18$  acre) remainder.

**LEGAL DESCRIPTION:** Lot 10, Block B, Plan 9512428 within NW-14-25-03-W5M

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### **<sup>1</sup> Administrative Resources**

Lindsey Ganczar, Planning Services  
Eric Schuh, Engineering Services



<b>GENERAL LOCATION:</b>	Located approximately 1.80 km (1.12 miles) east of Rge. Rd. 33 and 1.20 km (0.75 miles) north of Twp. Rd. 252 on the east side of Springbank Heights Way.
<b>APPLICANT:</b>	Lighthouse Studios Inc. (Scott Clements)
<b>OWNERS:</b>	Global Advisory Services Inc.
<b>EXISTING LAND USE DESIGNATION:</b>	Residential Two District
<b>PROPOSED LAND USE DESIGNATION:</b>	Residential One District
<b>GROSS AREA:</b>	± 2.29 hectares (± 5.66 acres)
<b>SOILS (C.L.I. from A.R.C.):</b>	<p><b>Class 6T,E</b> – Production is not feasible due to adverse topography and erosion damage.</p> <p><b>Class 4M,P</b> – Severe limitations due to low moisture holding, adverse texture, and excessive surface stoniness.</p> <p><b>Class 3C</b> – Moderate limitations due to climate.</p>

#### **PUBLIC & AGENCY SUBMISSIONS:**

Notification letters were circulated to 51 neighbouring property owners; three letters of objection were received (Appendix 'D'). The application was also circulated to a number of internal and external agencies. Those responses are available in Appendix 'A'.

#### **HISTORY:**

**1995** Subdivision Plan 9512428 was registered at Land Titles, creating the subject parcel. Municipal Reserves were previously provided as Block R-5 on Plan 7811150.

#### **BACKGROUND:**

The subject property currently contains an accessory building (quonset). The parcel is serviced by water well and a conventional septic system. Access to the subject parcel is provided from the parcel to the north via an access right-of-way (Plan 7811151), which extends through the subject parcel to the two adjacent sites to the south. Should a future subdivision be approved, the access agreement would need to be amended to include the additional parcel. The surrounding properties are a mix of R-2 and R-1 parcels.

#### **POLICY ANALYSIS:**

The subject parcel falls within the *Infill Residential Area* in the Central Springbank Area Structure Plan (ASP). Section 2.9.2(c) of the ASP states that conceptual schemes are required to guide residential development and must be appended to the ASP. However, there are exceptions to this policy listed in Section 2.9.2(f), the criteria for which are as follows:

- Direct road access is available;
- One lot is being created;
- The proposed lot is 0.8 hectares (2 acres or greater in size); and
- The creation of the new lot will not adversely affect or impede future subdivision of the balance lands.

Therefore, the subject application is exempt from requiring a conceptual scheme. The ASP states that new infill residential parcels shall range in size from 0.8 to 1.6 hectares, and the proposed new lots are ±1.01 and ±1.29 hectares in size.



The Land Use Bylaw states that minimum parcel size in the R-1 district is 0.8 hectares, and the proposed plan meets that rule as well.

**CONCLUSION:**

The application is consistent with the policies of the Central Springbank ASP and the Land Use Bylaw.

**OPTIONS:**

- Option # 1:    Motion #1        THAT Bylaw C-7824-2018 be given first reading.  
                   Motion #2        THAT Bylaw C-7824-2018 be given second reading.  
                   Motion #3        THAT Bylaw C-7824-2018 be considered for third reading.  
                   Motion #4        THAT Bylaw C-7824-2018 be given third and final reading.
- Option # 2:    THAT Application PL20180045 be refused.

Respectfully submitted,

Concurrence,

“Chris O’Hara”

“Rick McDonald”

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General Manager

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Interim County Manager

LG/rp

**APPENDICES:**

- APPENDIX ‘A’: Application Referrals  
 APPENDIX ‘B’: Bylaw C-7824-2018 and Schedule A  
 APPENDIX ‘C’: Map Set  
 APPENDIX ‘D’: Landowner Comments



## APPENDIX A: APPLICATION REFERRALS

AGENCY	COMMENTS
<i>School Authority</i>	
Rocky View Schools	No objection.
Calgary Catholic School District	No comments received.
Public Francophone Education	No comments received.
Catholic Francophone Education	No comments received.
<i>Adjacent Municipalities</i>	
City of Calgary	No concerns.
<i>Province of Alberta</i>	
Alberta Environment	No comments received.
Alberta Transportation	Not required for circulation.
Alberta Sustainable Development (Public Lands)	Not required for circulation.
Alberta Culture and Tourism (Historical Resources)	The applicant should obtain <i>Historical Resources Act</i> approval prior to proceeding with any land surface disturbance associated with subdivision development by submitting a Historic Resources Application.
Energy Resources Conservation Board	No comments received.
Alberta Health Services	No concerns.
<i>Public Utility</i>	
ATCO Gas	No comments received.
ATCO Pipelines	No objections.
AltaLink Management	No comments received.
FortisAlberta	No comments received.
Telus Communications	No objections.
TransAlta Utilities Ltd.	No comments received.



AGENCY	COMMENTS
<i>Other External Agencies</i>	
EnCana Corporation	No comments received.
Calgary Airport Authority	Not required for circulation.
Rocky View Water Co-op	Not required for circulation.
<i>Rocky View County - Boards and Committees</i>	
Rocky View West Recreation Board	No concerns.
<i>Internal Departments</i>	
Agricultural Services	No concerns.
Municipal Lands	No concerns.
GeoGraphics	No comments received.
Building Services	Not required for circulation.
Enforcement Services	No concerns.
Fire Services	No comments at this time.
Infrastructure and Operations-Maintenance – Engineering Services	<p data-bbox="618 1192 797 1220"><u>Geotechnical:</u></p> <ul data-bbox="618 1245 1448 1612" style="list-style-type: none"> <li data-bbox="618 1245 1448 1444">• There is a restrictive covenant on title of the subject lands (Instrument 951 243 728, Plan 9512429) which restricts development on what is approximately the west most one third of the subject lands. This has been placed on title as there are slopes of approximately 33% on this portion of the subject lands.</li> <li data-bbox="618 1449 1448 1612">• On the east of the subject lands along the bank of the Bow River, there are slopes of approximately 50% and 5 metres in height. According to Land Use Bylaw section 34, a setback of 12 metres from the top of the bank is required, unless a Slope Stability Assessment is required.</li> </ul> <p data-bbox="618 1633 813 1661"><u>Transportation:</u></p> <ul data-bbox="618 1686 1448 1879" style="list-style-type: none"> <li data-bbox="618 1686 1448 1879">• The subject lands currently have frontage along Springbank Heights Way, which is a paved road. However, the subject lands access through neighboring properties via an Access Easement Agreements registered on title (Instrument 781 154 031, Plan 7811151; Instrument 961 126 463, Plan 9611147).</li> </ul>



AGENCY	COMMENTS
	<p>The applicant is proposing to continue to use this existing access easement for this subdivision, as there is a restrictive covenant prohibiting development on the slopes on the west of the subject lands, making an approach from Springbank Heights Way not feasible.</p> <ul style="list-style-type: none"> <li>• The existing Access Easement Agreement (Instrument 781 154 031) stipulates that “owners of each of the said parcels of land shall maintain the portion of the roadway located on the said right-of-way within the boundaries of the land owned by such person, to specifications and standards which shall be stipulated from time to time by the Municipal District of Rockyview No. 44, or failing the stipulating of any such specifications and standards, to a fair and reasonable standard of maintenance for a gravel access road.”</li> <li>• The existing Access Easement Agreement (Instrument 781 154 031) is expected to carry forward to the new titles created and continue to provide access to Lots 1 &amp; 2 (Remainder). This Instrument has carried forward in past subdivisions of these lands.</li> <li>• As a condition of future subdivision, the applicant shall ensure both proposed parcels have sufficient legal access by confirming the current Access Easement Agreement will carry forward to the new titles, or providing a new Access Right-of-Way Plan and Access Easement Agreement, to be registered on title of all affected parcels.</li> <li>• As a condition of future subdivision, as a new shared driveway from the existing access right-of-way is to be used for Lots 1 &amp; 2, the applicant shall provide an Access Right-of-Way Plan and Access Easement Agreement to be registered on title of Lots 1 &amp; 2.</li> <li>• As a condition of future subdivision, the applicant is required to provide payment of the Transportation Off-site Levy in accordance with the applicable levy at time of subdivision approval, for the total acreage of proposed Lots 1 &amp; 2 (Remainder), as the applicant is proposing to subdivide a Residential One District parcel. <ul style="list-style-type: none"> <li>○ Base Levy = \$4595/acre. Special Area 4 Levy = \$11,380/acre. Acreage = 5.66 acres. Estimated TOL payment = (\$15,975/acre)*(5.66 acres) = \$90,419.</li> </ul> </li> </ul> <p><u>Sanitary/Waste Water:</u></p> <ul style="list-style-type: none"> <li>• At the time of future subdivision, the applicant shall submit a Level 4 PSTS Assessment in accordance with the requirements of the County Servicing Standards, for proposed Lot 1. <ul style="list-style-type: none"> <li>○ The subject lands are adjacent to the Bow River. Therefore, in accordance with the Model Process Tool, a Level 4 PSTS Assessment is required.</li> </ul> </li> </ul>






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**AGENCY**
**COMMENTS**


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- It is noted that in accordance with County Policy 449, for parcel sizes less than 3.95 acres and greater than 1.98 acres, the County requires the use a Package Sewage Treatment Plant meeting BNQ standards.
- As a condition of future subdivision, the Owner shall enter into a Site Improvements / Services Agreement with the County, which shall be registered on title of Lot 1 and shall include the following:
  - The system to be in accordance with the Level 4 PSTS Assessment to be submitted at the time of future subdivision;
  - For the construction of a Packaged Sewage Treatment Plant meeting Bureau de Normalisation du Quebec (BNQ) standards.
- As a condition of future subdivision, a Deferred Services Agreement shall be registered against each new certificate of title (lot) created, requiring the owner to tie into municipal services when they become available.

Water Supply And Waterworks:

- The application indicates that there is an existing groundwater well on the proposed Lot 2 (Remainder).
- The applicant submitted a Phase 1 Groundwater Supply Evaluation (Sedulous Engineering Inc. – August, 2018). The report meets the requirements of the County Servicing Standards and concludes that the aquifer underlying the subject lands can supply water to the proposed Lot 1 at a rate of 1250m<sup>3</sup>/year without causing adverse effects on existing users.
- As a condition of future subdivision, the applicant will be required to drill new well on Lot 1, and provide the County with a Phase 2 Aquifer Testing Report, prepared by a qualified professional, in accordance with procedures outlined in the County Servicing Standards. The report shall include a Well Driller's Report confirming a minimum pump rate of 1.0 igpm for the well.
- As a condition of future subdivision, a Deferred Services Agreement shall be registered against each new certificate of title (lot) created, requiring the owner to tie into municipal services when they become available.

Storm Water Management:

- As a condition of future subdivision, the applicant shall be required to submit a Site Specific Stormwater Implementation Plan, in accordance with the requirements of the County Servicing Standards.
-



AGENCY	COMMENTS
	<ul style="list-style-type: none"> <li>○ The Stormwater management requirements in Springbank are: Average Annual Runoff Volume Target of 45mm and the Max Release Rate of 1.714 L/s/ha (A Report on Drainage Strategies for Springbank – Westhoff Engineering Resources Inc. – 2004).</li> <li>○ As the proposed location of the dwellings is adjacent to the Bow River, the SSIP shall identify any ESC measures required.</li> </ul>
	<p data-bbox="618 562 808 590"><u>Environmental:</u></p> <ul style="list-style-type: none"> <li>● In accordance with Land Use Bylaw section 41, the Riparian Setback from the banks of the Bow River is 30 to 60 metres, depending on the soil classification. During future subdivision stage, the applicant shall give consideration to this setback, which may be relaxed in accordance with the requirements of the Land Use Bylaw. At future subdivision stage, a restrictive covenant may be registered on title to prohibit development within the riparian area.</li> <li>● Any approvals required through Alberta Environment shall be the sole responsibility of the Applicant/Owner.</li> </ul>
Infrastructure and Operations-Maintenance	No concerns.
Infrastructure and Operations-Capital Delivery	No concerns.
Infrastructure and Operations-Operations	No concerns.
Infrastructure and Operations-Utility Services	No concerns.

Circulation Period: May 18, 2018 – June 20, 2018



**BYLAW C-7824-2018**  
**A Bylaw of Rocky View County to amend Bylaw C-4841-97,**  
**being the Land Use Bylaw.**

The Council of Rocky View County enacts as follows:

**PART 1 – TITLE**

This Bylaw shall be known as Bylaw C-7824-2018.

**PART 2 – DEFINITIONS**

In this Bylaw, the definitions and terms shall have the meanings given to them in Land Use Bylaw C-4841-97 and the *Municipal Government Act*.

**PART 3 – EFFECT OF BYLAW**

- THAT** Part 5, Land Use Map No. 57 and No. 57 SE of Bylaw C-4841-97 be amended by redesignating Lot 10, Block B, Plan 9512428 within NW-14-25-03-W5M from Residential Two District to Residential One District as shown on the attached Schedule 'A' forming part of this Bylaw.
- THAT** Lot 10, Block B, Plan 9512428 within NW-14-25-03-W5M is hereby redesignated to Residential One District, as shown on the attached Schedule 'A' forming part of this Bylaw.

**PART 4 – TRANSITIONAL**

Bylaw C-7824-2018 is passed when it receives third reading, and is signed by the Reeve/Deputy Reeve and the Municipal Clerk, as per Section 189 of the *Municipal Government Act*.

**Division: 2**  
**File: 05714035/PL20180045**

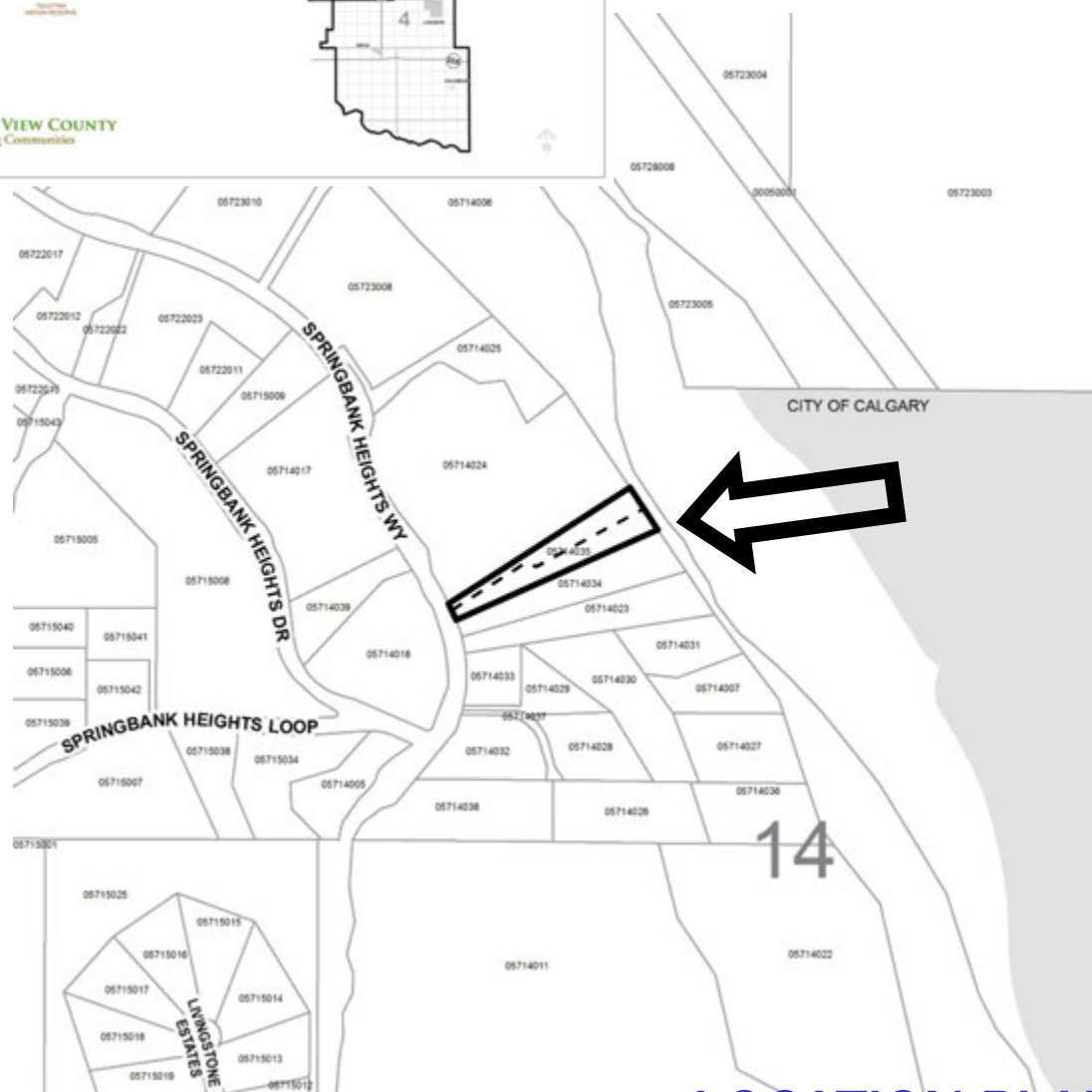
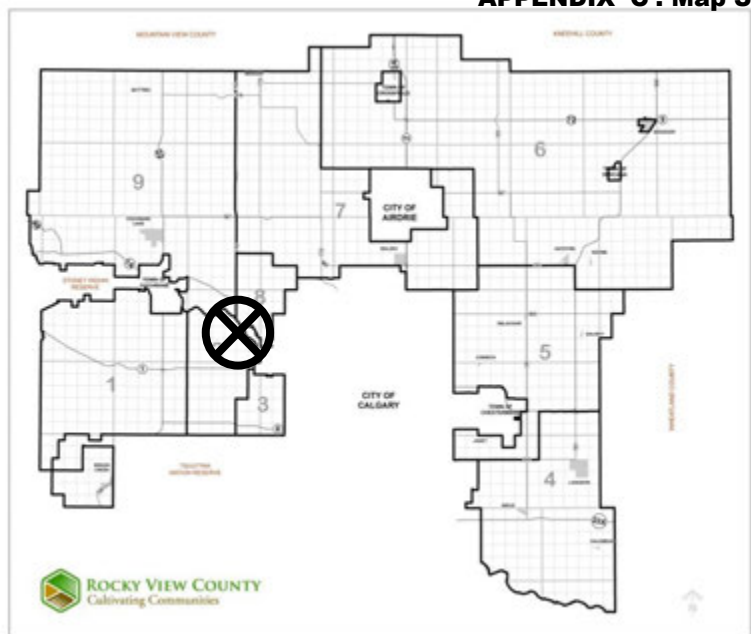
<i>PUBLIC HEARING WAS HELD IN COUNCIL</i> this	day of	, 2018
READ A FIRST TIME IN COUNCIL this	day of	, 2018
READ A SECOND TIME IN COUNCIL this	day of	, 2018
<i>UNANIMOUS PERMISSION FOR THIRD READING</i>	day of	, 2018
READ A THIRD TIME IN COUNCIL this	day of	, 2018

\_\_\_\_\_  
Reeve

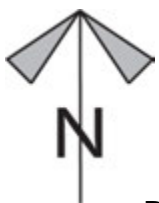
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CAO or Designate

\_\_\_\_\_  
Date Bylaw Signed





**LOCATION PLAN**



**Lot:10 Block:B Plan:9512428**  
**NW-14-25-03-W05M**

Date: May 11, 2018

Division # 2

File: 05714035

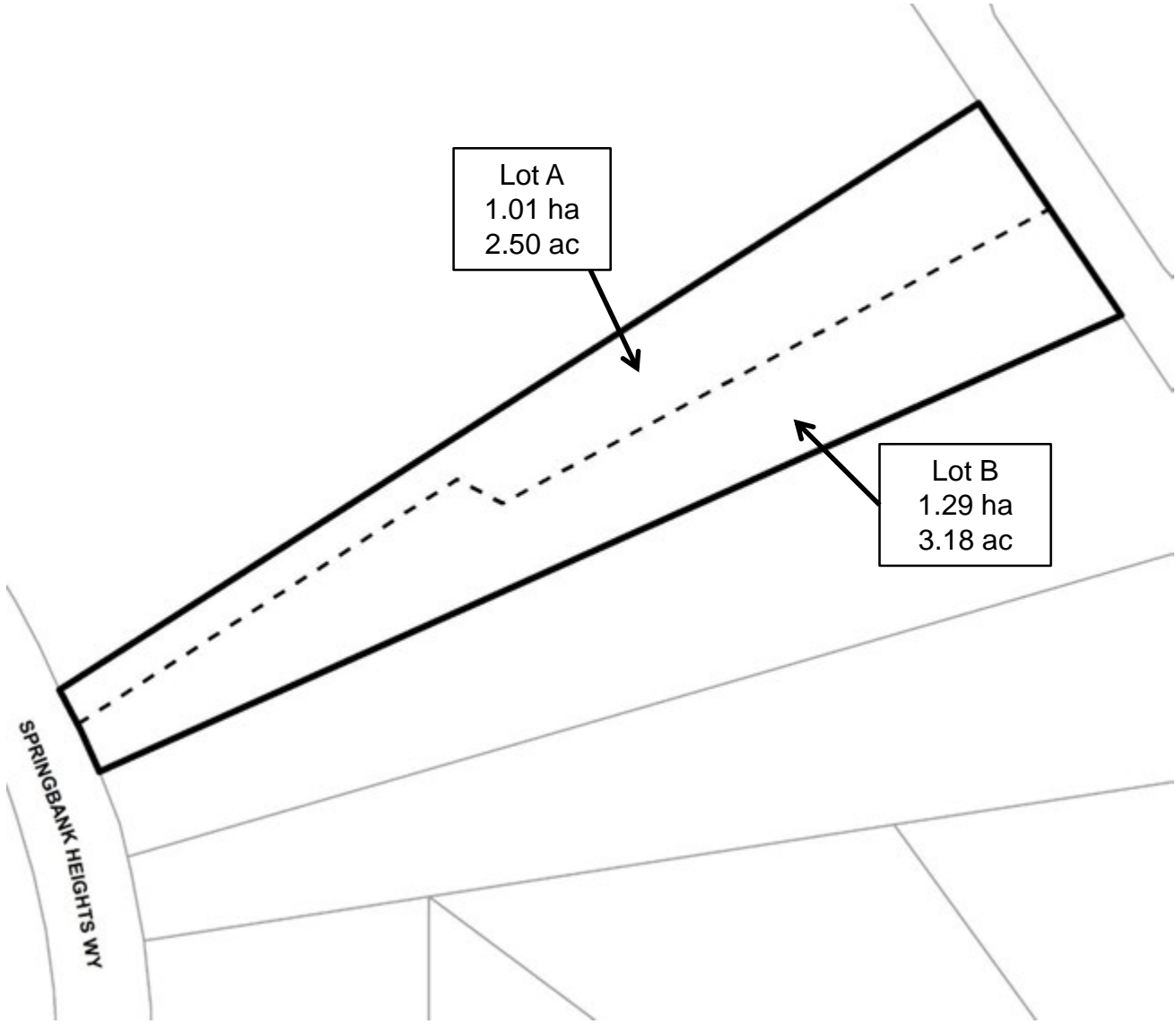
**AGENDA**

Page 389 of 446

**Redesignation Proposal:** To redesignate the subject lands from Residential Two District (R2) to Residential One District (R1) in order to facilitate the creation of a ± 1.01 hectare (2.50 acre) parcel (Lot A) and a ± 1.29 hectare (3.18 acre) parcel (Lot B).

Lot A  
1.01 ha  
2.50 ac

Lot B  
1.29 ha  
3.18 ac



# DEVELOPMENT PROPOSAL

**Lot:10 Block:B Plan:9512428  
NW-14-25-03-W05M**

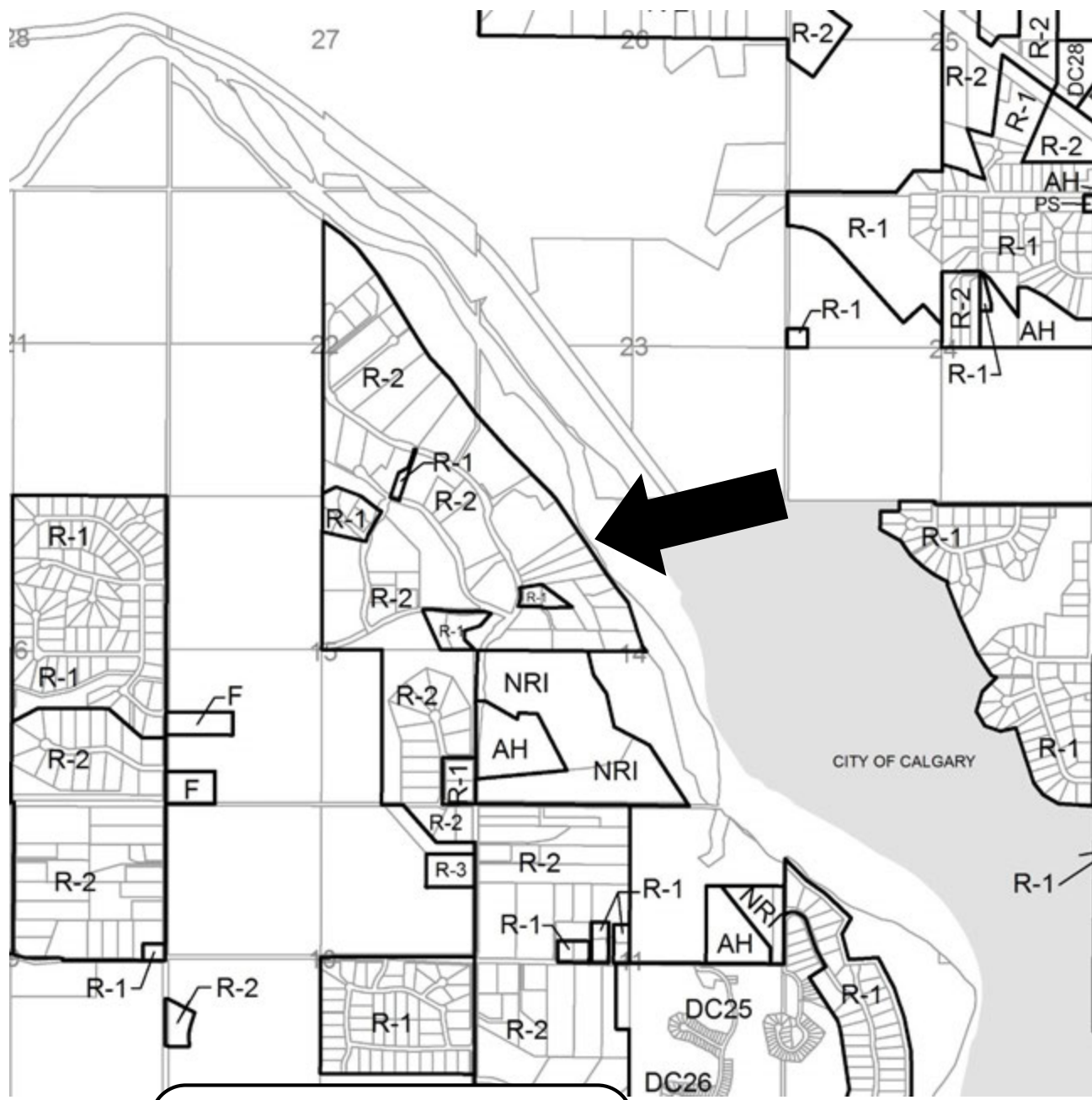


Date: May 11, 2018

Division # 2

File: 05714035

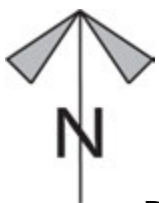
**AGENDA**



Ranch and Farm	B-1 Highway Business
RF2 Ranch and Farm Two	B-2 General Business
RF3 Ranch and Farm Three	B-3 Limited Business
AH Agricultural Holding	B-4 Recreation Business
F Farmstead	B-5 Agricultural Business
R-1 Residential One	B-6 Local Business
R-2 Residential Two	NRI Natural Resource Industrial
R-3 Residential Three	HR-1 Hamlet Residential Single Family
DC Direct Control	HR-2 Hamlet Residential (2)
PS Public Service	HC Hamlet Commercial
	AP Airport

# LAND USE MAP

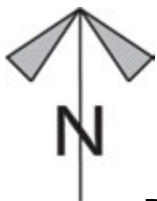
**Lot:10 Block:B Plan:9512428**  
**NW-14-25-03-W05M**





Contours are generated using 10m grid points, and depict general topographic features of the area. Detail accuracy at a local scale cannot be guaranteed. They are included for reference use only.

**TOPOGRAPHY**  
*Contour Interval 2 M*



**Lot:10 Block:B Plan:9512428**  
**NW-14-25-03-W05M**

Date: May 11, 2018

Division # 2

File: 05714035

**AGENDA**

Page 392 of 446





Note: Post processing of raw aerial photography may cause varying degrees of visual distortion at the local level.

**AIR PHOTO**

Spring 2016

**Lot:10 Block:B Plan:9512428  
NW-14-25-03-W05M**



Date: May 11, 2018

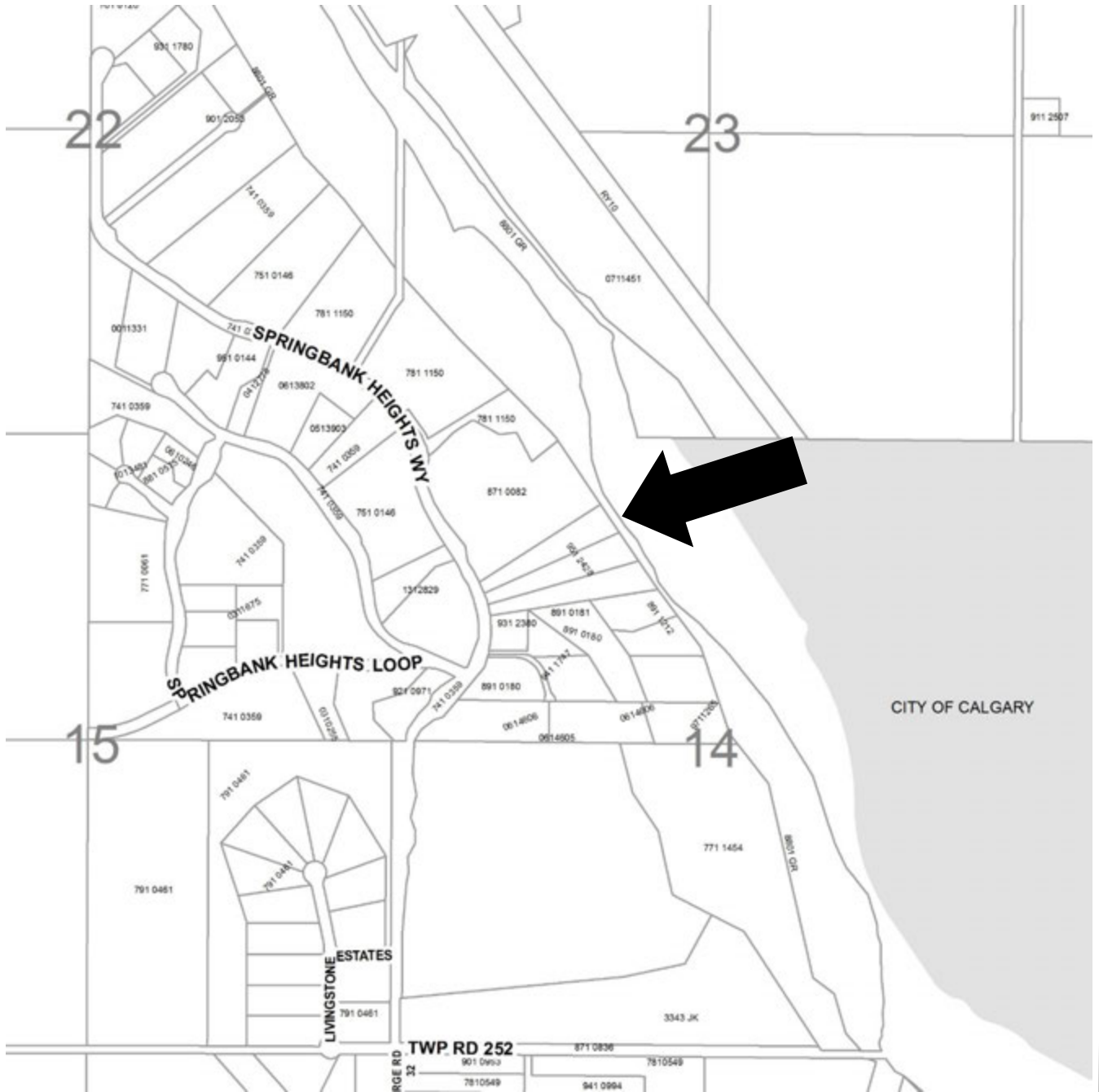
Division # 2

File: 05714035

**AGENDA**

Page 393 of 446



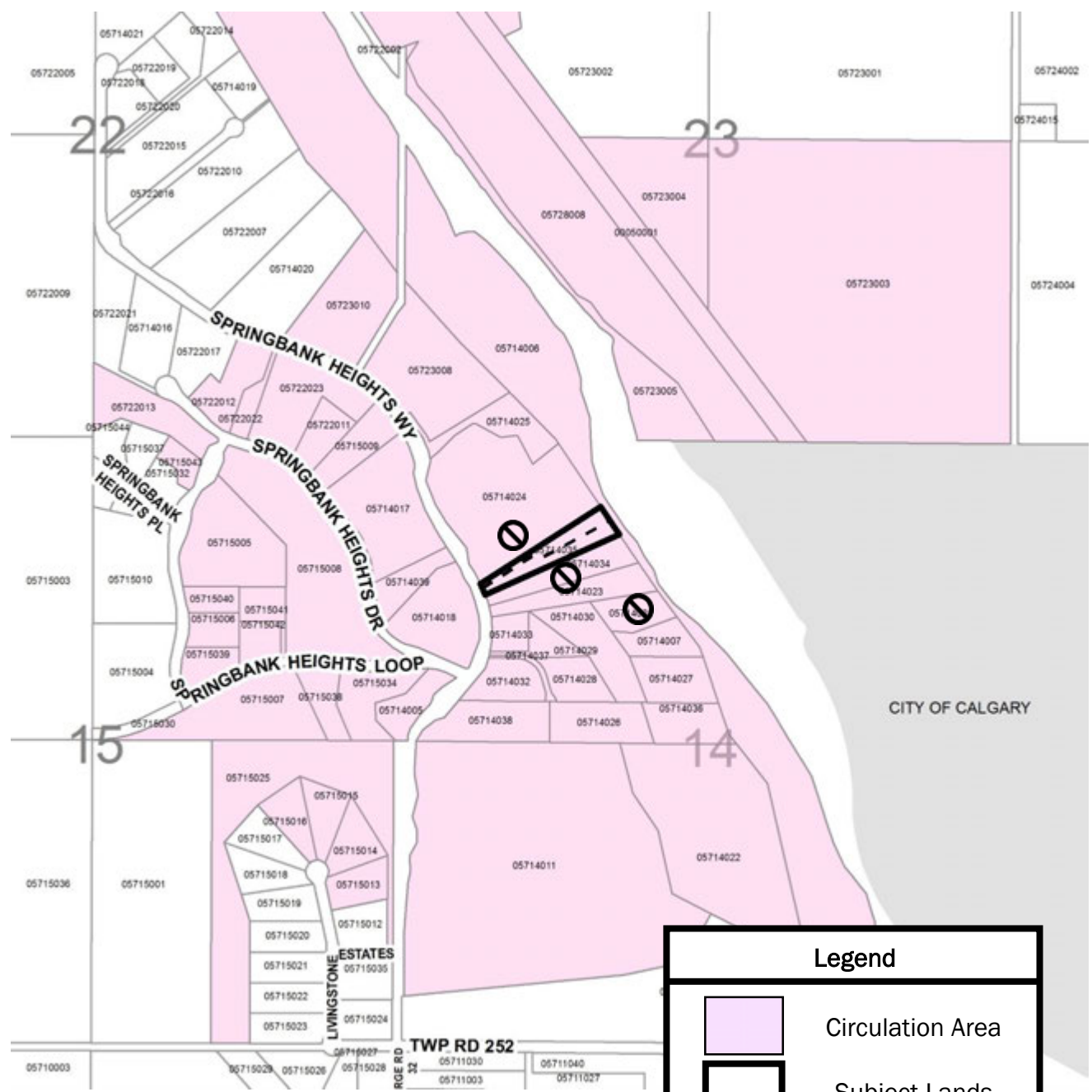


**Legend – Plan numbers**



- First two numbers of the Plan Number indicate the year of subdivision registration.
- Plan numbers that include letters were registered before 1973 and do not reference a year

## HISTORIC SUBDIVISION MAP

**Lot:10 Block:B Plan:9512428  
NW-14-25-03-W05M**

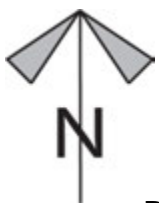


⊗ Letters in Opposition  
 ★ Letters in Support

Legend	
	Circulation Area
	Subject Lands

# LANDOWNER CIRCULATION AREA

**Lot:10 Block:B Plan:9512428  
 NW-14-25-03-W05M**



Rocky View County  
911 – 32 Avenue NE  
Calgary, Ab, T2E 6X6  
Attention: Lindsey Ganczar, Planning Services

June 6, 2018

Dear Sir / Madame,

Re: **File Number – 05714035**

**Application Number – PL 20180045**

**“NOT IN FAVOR”** of Proposal to re-designate zoning for **Lot 10, Block B, Plan 9512428, NW – 14 – 25 - 03 – W05M**

I am writing in response to the proposal to re-designate the address identified under  
As an adjacent land owner of the **2 lots** to the south

I am not in favor of the proposal to change zoning from R2 to R1. As I own the 2 lots mentioned, I would like it documented that this in fact representative of lots. My reasons for this are as follows:

**HIGHER DENSITY** – I recently purchased my property with an understanding that Springbank Heights Way is a lower density property conflict in with the already over density issues within Spring Bank

**WATER Well** and sewage – The property proposing re-designation currently has access to the well on my property. No testing has been done on this well for capacity, but its likely it would not be sufficient enough for increased density. I am concern they will try to use one well for 2 houses. Additional sewage untreated and proximity of the Bears paw dam is also a concern.

**Private shared access ROADWAY** – The current road is a private driveway and is already a concern for the properties owners that use it for access. Conflicts have arisen between neighbors over the road, and I strongly believe additional residential density will create more conflict. There are existing maintenance issues, plus any of the land owners that utilize the road could actually close access to the other owners, based on its current setup. These are issues that were not properly dealt with at the onset of the original lot being subdivided. Maintenance of the access road has been a problem

**The protected Hill from development and** it currently being a wildlife sanctuary is also my concern potential fencing and barbed wire hidden within the bushes is already cause the life of one horse in the last 30 days.

If you have any questions regarding this letter, please don't hesitate to contact myself  
or my consultant John

Regards,



Stan Swiatek

Rocky View County  
911 – 32 Avenue NE  
Calgary, Ab, T2E 6X6  
Attention: Lindsey Ganczar, Planning Services

June 8, 2018

Dear Lindsey,

Re: **File Number – 05714035**

**Application Number – PL 20180045**

**“NOT IN FAVOR”** of Proposal to re-designate zoning for **Lot 10, Block B, Plan 9512428, NW – 14 – 25 - 03 – W05M**

I am writing in response to the proposal to re-designate the address identified under **File # 05714035**.

As an adjacent land owner of the lot to the north I am not in favor of the proposal to change zoning from R2 to R1. My reasons for this are as follows:

**HIGHER DENSITY** – I live in this area as I have an understanding that Springbank Heights Way is a lower density area and live here for this reason.

**Private shared access ROADWAY** – The current road is a private driveway and is already a concern for the property owners that use it for access. Conflicts have arisen between neighbors over the road, and I strongly believe additional residential density will create more conflict. There are existing maintenance issues, plus any of the land owners that utilize the road could actually close access to the other owners, based on its current setup. These are issues that were not properly dealt with at the onset of the original lot being subdivided.

If you have any questions regarding this letter, please don't hesitate to contact me directly

Regards,



Jack Anderson

June 7, 2018

Lindsey Ganczar  
Planning Services, Rocky View County

**Re: File #05714035**  
**Application #PL20180045**

Dear Sir/Madame,

Regarding the above file: Please note that, as long-time residents of this neighborhood, we are **NOT IN FAVOUR** of this subdivision as proposed. Our reasons are as follows:

**Environmental:** When our subdivision was approved in the late 80's, provision was mandated for a wildlife corridor (note that parcel 05714030 is a designated ER, protecting the hillside above us). The Bow River Escarpment is a fragile and ecologically important piece of our neighborhood, and is a very major wildlife corridor. It is our opinion that the ER / wildlife corridor should be protected, and made continuous all along the valley.

**Road:** this subdivision is serviced by an easement road with a non-conforming fence. At barely 7.5M between fenceposts, this road is very difficult to navigate with any opposing traffic. It is our opinion that this road is already at capacity.

**Water:** despite the proximity to the Bow River, water wells in this subdivision tend to yield low flow and poor quality. We understand the precedent has been set for 2-acre lots in the subdivision, but these are situated above the escarpment and therefore are likely accessing a different aquifer. The currently proposed lots are extremely narrow, and our concern is for a potential narrowly-focused drawdown on the lower aquifer which could adversely affect adjacent properties.

Respectfully submitted,

*(signed)*

Brian & Lynn Robb,



**PLANNING SERVICES**

**TO:** Subdivision Authority  
**DATE:** October 23, 2018 **DIVISION:** 1  
**FILE:** 03901008 **APPLICATION:** PL20180087  
**SUBJECT:** Subdivision Item – Residential One District

**<sup>1</sup>POLICY DIRECTION:**

The application was evaluated in accordance with Section 654 of the *Municipal Government Act*, Section 7 and Section 14 of the Subdivision and Development Regulations, and the Greater Bragg Creek Area Structure Plan (GBCASP). Administration determined that it was compliant for the following reasons:

- The proposal is consistent with the requirements of the GBCASP;
- The subject lands hold the appropriate land use designation; and
- All technical considerations are addressed through the conditions of subdivision approval.

**EXECUTIVE SUMMARY:**

The purpose of this application is to create a ± 0.94 hectare (± 2.32 acre) parcel with a ± 1.03 hectare (± 2.53 acre) remainder. The subject lands are located in south Bragg Creek, surrounded by agricultural and residential development.

Access is currently available from an existing approach off Boyce Ranch Road. The lands are currently party to an access easement agreement with the lands to the east. The Applicant would be required to upgrade the existing approach to a mutual standard. Proposed Lot 2 currently contains an existing dwelling, single detached. The lands are serviced by means of a water well and private sewage treatment system. The well is currently located on Lot 1. Technical studies were submitted that confirm servicing is feasible for further residential development.

Administration determined that the application meets policy.

<p><b>PROPOSAL:</b> To create a ± 0.94 hectare (± 2.32 acre) parcel with a ± 1.03 hectare (± 2.53 acre) remainder.</p>	<p><b>GENERAL LOCATION:</b> Located approximately 0.5 km (1/3 mile) west of Highway 22, and approximately 0.25 km (1/6 mile) north of Highway 66.</p>
<p><b>LEGAL DESCRIPTION:</b> Lot 1, Block B, Plan 9511735; SE-01-23-05-W05M</p>	<p><b>GROSS AREA:</b> ± 1.96 hectares (± 4.85 acres)</p>
<p><b>APPLICANT:</b> Element Land Surveys Inc. <b>OWNER:</b> Jova and Aranka Vujinovic</p>	<p><b>RESERVE STATUS:</b> Municipal reserves were previously provided through a cash-in-lieu payment as per Plan 9511735.</p>
<p><b>LAND USE DESIGNATION:</b> Residential One District (R-1)</p>	<p><b>LEVIES INFORMATION:</b> The Transportation Off-Site Levy is applicable in this case.</p>

<sup>1</sup>**Administration Resources**  
 Paul Simon, Planning Services  
 Eric Schuh, Engineering Services





<p><b>DATE SUBDIVISION APPLICATION RECEIVED:</b> July 17, 2018  <b>DATE SUBDIVISION APPLICATION DEEMED COMPLETE:</b> July 17, 2018</p>	<p><b>APPEAL BOARD:</b> Municipal Government Board</p>
<p><b>TECHNICAL REPORTS SUBMITTED:</b></p> <ul style="list-style-type: none"> <li>• Level Two Private Sewage Treatment System Assessment (September 2017)</li> <li>• Phase 2 Aquifer Testing Report (February 2018)</li> </ul>	<p><b>LAND USE POLICIES AND STATUTORY PLANS:</b></p> <ul style="list-style-type: none"> <li>• County Plan (Bylaw C-7280-2013)</li> <li>• Greater Bragg Creek Area Structure Plan (Bylaw C-6260-2006)</li> <li>• Land Use Bylaw (Bylaw C-4841-97)</li> </ul>

**PUBLIC & AGENCY SUBMISSIONS:**

The application was circulated to 25 adjacent landowners. No letters were received in response. The application was also circulated to a number of internal and external agencies. Those responses are available in Appendix 'B'.

**HISTORY:**

**June 12, 2018** Redesignation application PL20170053 was approved by Council, redesignating the subject lands to the Residential One (R-1) District.

**July 25, 1995** Plan 9511735 was registered, creating two ± 4.84 acre (± 1.96 hectare) parcels and two 4.00 acre (± 1.62 hectare) parcels.

**TECHNICAL CONSIDERATIONS:**

This application was evaluated in accordance with the matters listed in Section 7 and Section 14 of the Subdivision and Development Regulation, which are as follows:

a) The site's topography:

Based on the County's LIDAR data, the average slope on the subject lands is approximately 10%. The topography does not appear to inhibit the ability to develop the new proposed lot for residential purposes.

Conditions: None.

b) The site's soil characteristics:

The subject lands contain Class 5 soils, with very severe limitations for production due to temperature factors and low permeability.

Conditions: None.

c) Stormwater collection and disposal:

As a condition of subdivision, the Owner would be required to prepare and implement a site-specific stormwater management plan that conforms to the Bragg Creek Master Drainage Plan, and register any applicable easements.

Conditions: 6.

d) Any potential for flooding, subsidence, or erosion of the land:

The County's wetland mapping indicates that the subject lands do not contain any existing water bodies.



Conditions: None.

e) Accessibility to a road:

The subject lands are currently accessed from Boyce Ranch Road, through an existing access easement agreement with the lands to the east (instrument # 951 165 542, Plan 9511736). As conditions of subdivision, the Owner would need to upgrade the existing approach to a mutual standard, and provide confirmation that Lot 1 would retain legal access through the existing agreement.

Conditions: 2, 3.

f) Water supply, sewage, and solid waste disposal:

As part of the application, the Owner submitted a Level 2 PSTS assessment and Phase 2 Groundwater Evaluation report. The reports confirm that the subject lands are capable of servicing an additional lot for residential purposes. As a condition of subdivision, the Owner would be required to enter into a Development Agreement (Site Improvements/Services Agreement) with the County to ensure that the future private sewage treatment system is constructed in accordance with the findings of the PSTS report. Further, the Owner is required to enter into a Deferred Services Agreement with the County to tie into municipal services when they become available.

Conditions: 4, 5.

g) The use of the land in the vicinity of the site:

The land use in the vicinity of the subject land is residential in nature, with agricultural development immediately north. As a result of the proposed subdivision, no impacts to adjacent land uses have been identified.

Conditions: None

h) Other matters:

*Municipal Reserves*

Municipal Reserves have already been provided through a cash-in-lieu payment on Plan 9511735.

*Transportation Off-Site Levy*

The Applicant/Owner would be required to provide payment of the Transportation Off-Site Levy (TOL) in accordance with applicable levy at time of subdivision approval. The TOL would be applicable on the gross acreage of proposed Lot 1 and 2.

- Base TOL = \$4,595/acre. Acreage = 4.85 acres. TOL payment = (\$4,595/acre)\*(4.85 acres) = \$22,285.75.

Conditions: 8.

**POLICY CONSIDERATIONS:**

The subject lands fall within the Greater Bragg Creek ASP (GBCASP). Specifically, the lands are located within south Bragg Creek and are identified as Infill Residential on Figure 13 of the GBCASP. Infill residential areas refer to parcels that have already experienced subdivision greater than eight lots per quarter where densities are calculated on the basis of a ratio of lots per acre of gross developable area (GDA). Parcels in south Bragg Creek should not be less than 2 acres with a density not greater than 3 acres of GDA. With both parcels greater than 2 acres and an overall density of 1 lot / 5.40 acres of GDA, the application is consistent with the GBCASP.



Conceptual schemes are required to ensure development is undertaken comprehensively in Bragg Creek. However, policy 7.1(d) of the GBCASP states that, “redesignation and subdivision for non-agricultural purposes may proceed in the absence of a conceptual scheme when:

- direct road access is available;
- one lot is being created from a parcel whose boundaries are defined at the time of adoption of this plan;
- the proposed lot is 2 acres or greater in size; and
- the creation of the new lot will not adversely affect or impede future subdivision of the balance lands.”

The subject parcel has legal access provided by way of an existing access easement agreement, and therefore, it does have direct access to a public road. The subject lands were subdivided in 1995, prior to adoption of the GBCASP. Both the proposed lot and remainder are greater than 2 acres in size, and it is the interpretation of Administration that the subdivision would not adversely impact future subdivision of the remainder lands. Therefore, Administration recommends that a conceptual scheme is not required.

The subject lands hold the Residential One designation, with a minimum parcel size of 0.80 hectares (1.98 acres). The land use was granted in June, 2018, and there are no concerns with respect to the subdivided lands complying with the Residential One District provisions in the Land Use Bylaw.

**CONCLUSION:**

The proposal was evaluated in accordance with Section 654 of the *Municipal Government Act*, Section 7 and Section 14 of the Subdivision and Development Regulations, and the Greater Bragg Creek Area Structure Plan (GBCASP). Administration determined that it was compliant for the following reasons:

- The application is consistent with the GBCASP;
- The subject lands hold the appropriate land use designation;
- The technical aspects of the subdivision proposal were considered and are further addressed through the conditional approval requirements.

**OPTIONS:**

Option #1: THAT Subdivision Application PL20180087 be approved with the conditions noted in Appendix A.

Option #2: THAT Subdivision Application PL20180087 be refused as per the reasons noted.

Respectfully submitted,

Concurrence,

“Chris O’Hara”

“Rick McDonald”

General Manager

Interim County Manager

PS/rp

**APPENDICES:**

- APPENDIX ‘A’: Approval Conditions
- APPENDIX ‘B’: Application Referrals
- APPENDIX ‘C’: Map Set

### APPENDIX A: Approval Conditions

- A. The application to create a  $\pm 0.94$  hectare ( $\pm 2.32$  acre) parcel with a  $\pm 1.03$  hectare ( $\pm 2.53$  acre) remainder within Lot 1, Block B, Plan 9511735; SE-01-23-05-W05M, having been evaluated in terms of Section 654 of the *Municipal Government Act* and Section 7 and Section 14 of the Subdivision and Development Regulations, and having considered adjacent landowner submissions, is approved as per the Tentative Plan for the reasons listed below:
1. The application is consistent with the Statutory Policy;
  2. The subject lands hold the appropriate land use designation;
  3. The technical aspects of the subdivision proposal have been considered and are further addressed through the conditional approval requirements.
- B. The Applicant/Owner is required, at their expense, to complete all conditions attached to and forming part of this conditional subdivision approval prior to Rocky View County (the County) authorizing final subdivision endorsement. This requires submitting all documentation required to demonstrate each specific condition has been met, or agreements (and necessary securities) have been provided to ensure the conditions will be met, in accordance with all County Policies, Standards, and Procedures, to the satisfaction of the County, and any other additional party named within a specific condition. Technical reports required to be submitted as part of the conditions must be prepared by a qualified professional, licensed to practice in the province of Alberta, within the appropriate field of practice. The conditions of this subdivision approval do not absolve an Applicant/Owner from ensuring all permits, licenses, or approvals required by Federal, Provincial, or other jurisdictions are obtained.
- C. Further, in accordance with Section 654 and 655 of the *Municipal Government Act*, the application shall be approved subject to the following conditions of approval:

#### *Plan of Subdivision*

- 1) Subdivision to be effected by a Plan of Survey, pursuant to Section 657 of the *Municipal Government Act*, or such other means satisfactory to the Registrar of the South Alberta Land Titles District.

#### *Transportation and Access*

- 2) The Owner shall upgrade the existing approach to a mutual standard as shown on the Approved Tentative Plan.
- 3) The Owner shall demonstrate that Lot 1 has been provided legal access through the existing access easement agreement (instrument # 951165 542). If the existing access easement agreement does not provide legal access to Lot 1, the Applicant/Owner shall:
  - a) Amend the existing access easement agreement (instrument #951165 542) to ensure Lot 1 has legal access; or
  - b) Provide a new access right-of-way plan and prepare and register respective easements on title, where required.

#### *Site Servicing*

- 4) The Owner is to enter into a Development Agreement (Site Improvements / Services Agreement) with the County:
  - a) In accordance with the Level 2 PSTS report from Western Water Resources Inc., dated September 2017.
  - b) For the construction of a Type II packaged sewage treatment plant and LFH at-grade soil based sewage treatment and dispersal system.



- 5) The Owner is to enter into a Deferred Services Agreement with the County, to be registered on title for each proposed Lot 1 and 2, indicating:
  - a) Requirements for each future Lot Owner to connect to County piped water, wastewater, and stormwater systems at their cost when such services become available; and
  - b) Requirements for decommissioning and reclamation once County servicing becomes available.

#### *Developability*

- 6) The Owner is to provide and implement a Site Specific Stormwater Implementation Plan (SSIP) in accordance with the County Servicing Standards and Bragg Creek Master Drainage Plan. Implementation of the SSIP shall:
  - a) Include pre and post-development release rates, volume control targets and water quality;
  - b) Demonstrate that there are no adverse impacts to adjacent properties and downstream lands on drainage routes;
  - c) Include provision of necessary approvals and compensation to Alberta Environment and Parks for wetland loss and mitigation; and
  - d) Require that the Applicant/Owner enter into a Development Agreement (Site Improvements/Services Agreement) with the County should the SSIP indicate that improvements are required.

#### *Payments and Levies*

- 7) The Applicant/Owner shall pay the County subdivision endorsement fee in accordance with the Master Rates Bylaw for the creation of one (1) new Lot.
- 8) The Applicant/Owner shall pay the Transportation Off-Site Levy (TOL) in accordance with Bylaw C-7356-2014 prior to subdivision endorsement:
  - a) The Transportation Off-Site Levy shall be calculated from the total gross acreage of the Lands to be subdivided as shown on the Plan of Survey.

#### *Taxes*

- 9) All taxes owing up to and including the year in which subdivision is to be registered are to be paid to Rocky View County prior to signing the final documents pursuant to Section 654(1) of the *Municipal Government Act*.

#### **D. SUBDIVISION AUTHORITY DIRECTION:**

- 1) Prior to final endorsement of the subdivision, the Planning Department is directed to present the Applicant/Owners with a Voluntary Recreation Contribution Form and ask them if they will contribute to the Fund in accordance with the contributions prescribed in the Master Rates Bylaw.

**APPENDIX B: APPLICATION REFERRALS**

AGENCY	COMMENTS
<b><i>School Authority</i></b>	
Rocky View Schools	No comments received.
Calgary Catholic School District	No comments received.
<b><i>Province of Alberta</i></b>	
Alberta Environment	No comments received.
Alberta Culture and Community Spirit (Historical Resources)	No comments received.
Alberta Energy Regulator	No comments received.
Alberta Health Services	<p>Alberta Health Services, Environmental Public Health, has received the above-noted application. At this time we do not have any concerns with the information as provided. Please contact me if the application is changed in any way, or you have any questions or concerns.</p>
Alberta Transportation	<p>This will acknowledge receipt of your circulation memorandum regarding the above noted proposal, which must meet the requirements of Section 14 of the Subdivision and Development Regulation due to the proximity of Highway 22 and Highway 66. Presently, the application does not appear to comply with any category of Section 14 of the Regulation.</p> <p>The department recognizes that the land involved in this application is removed from the provincial highway system, and relies on the municipal road network for access. It appears that the single residential parcel being created by this application should not have a significant impact on the provincial highway system.</p> <p>Alberta Transportation has no objection to this proposal. The department grants an unconditional variance of Section 14 and/or Section 15 of the Subdivision and Development Regulation. From the department's perspective, any appeals regarding this subdivision application must be heard by the Municipal Government Board.</p>
<b><i>Public Utility</i></b>	
ATCO Gas	No comments received.
ATCO Pipelines	No comments received.



AGENCY	COMMENTS
AltaLink Management	No comments received.
FortisAlberta	No comments received.
Telus Communications	No objections to the above noted subdivision.
TransAlta Utilities Ltd.	No comments received.
Cochrane Lake Gas Coop	No comments received.
<b><i>Other External Agencies</i></b>	
EnCana Corporation	No comments received.
<b><i>Rocky View County Boards and Committees</i></b>	
ASB Farm Members and Agricultural Fieldsmen	No comments received.
Rocky View Recreation Board (All)	No comments received.
<b><i>Internal Departments</i></b>	
Municipal Lands	The Municipal Lands Office has no concerns with this application as applicable reserves have been provided for as a cash in lieu payment as per Plan 9511735.
Development Authority	No comments received.
GeoGraphics	No comments received.
Building Services	No comments received.
Agricultural Services	Because this parcel falls within the Greater Bragg Creek Area Structure Plan, Agricultural Services has no concerns.
Emergency Services	No comments at this time.
Enforcement	Enforcement has no concerns.
Infrastructure and Operations - Engineering Services	<p data-bbox="618 1682 727 1707"><b><u>General</u></b></p> <ul data-bbox="618 1732 1442 1824" style="list-style-type: none"> <li data-bbox="618 1732 1442 1824">• The review of this file is based upon the application submitted. These conditions/recommendations may be subject to change to ensure best practices and procedures.</li> </ul> <p data-bbox="618 1850 1192 1875"><b><u>Geotechnical</u></b> - Section 300.0 requirements:</p> <ul data-bbox="618 1900 1146 1925" style="list-style-type: none"> <li data-bbox="618 1900 1146 1925">• ES has no requirements at this time.</li> </ul>

**AGENCY****COMMENTS****Transportation** - Section 400.0 requirements:

- Both the proposed parcels have frontage on Boyce Ranch Road, which is identified as an oil road;
- The subject lands currently access through the neighboring property (Lot 4, Block B, Plan 9511735) by an Access Easement Agreement (Instrument 951 165 542, Plan 9511736);
- The existing Access Easement Agreement (Instrument 951 165 542) should carry forward to the new titles created and continue to provide access to Lots 1 & 2 (Remainder);
- The proposed panhandle is 12.5 metres in width, which meets the requirements of the County Servicing Standards.
- As a condition of subdivision, the applicant is required to provide payment of the Transportation Off-site Levy in accordance with the applicable levy at time of subdivision approval for the total gross acreage of the lands, as the applicant is proposing to subdivide a Residential One District Parcel less than 9.88 acres in size.
  - Base TOL = \$4,595/acre. Acreage = 4.85 acres. TOL payment = (\$4,595/acre)\*(4.85 acres) = \$22,286.

**Sanitary/Waste Water** - Section 500.0 requirements:

- The applicant submitted a Level 2 PSTS Assessment (Western Water Resources Inc. - September 25, 2017) with the previous Land Use Redesignation application (PL20170053). The report concludes that the subject lands are suitable for a Type II Packaged Sewage Treatment Plant, coupled with a LFH at-grade soil based sewage treatment and dispersal system.
- As a condition of subdivision, the Owner is to enter into a Site Improvements / Services Agreement with the County and shall include the following:
  - In accordance with the Level 2 PSTS Report prepared by Western Water Resources Inc.
  - For the construction of a Type II Packaged Sewage Treatment Plant and LFH at-grade soil based sewage treatment and dispersal system.
- As a condition of subdivision, a Deferred Services Agreement shall be registered against each new certificate of title (lot) created, requiring the owner to tie into municipal services when they become available.

**Water Supply And Waterworks** - Section 600.0 & 800.0 requirements:

- The applicant submitted a Phase 2 Aquifer Testing Report (Western Water Resources Inc., February 14, 2018). The report meets the requirements of the County Servicing

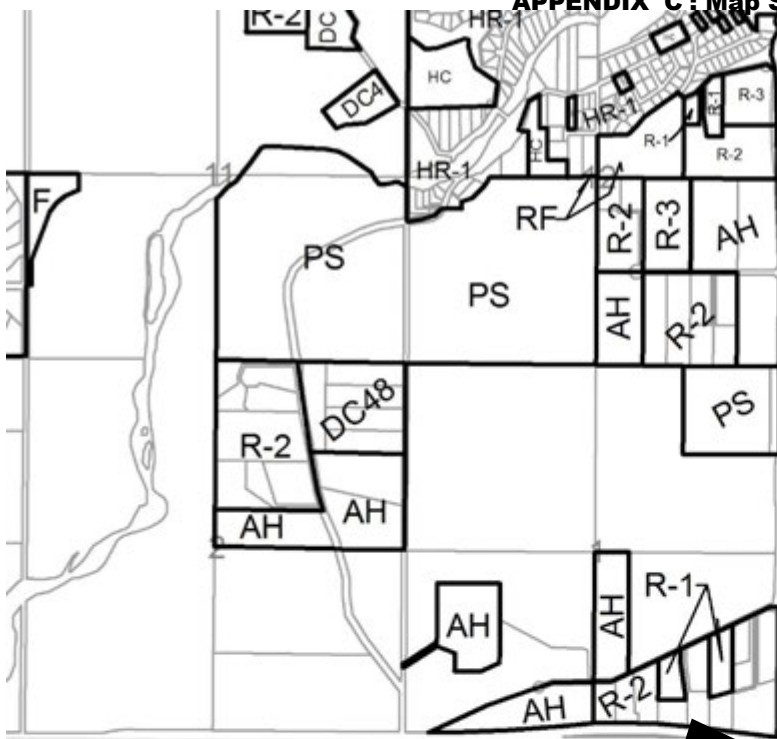




AGENCY	COMMENTS
	<p>Standards, and concludes that the groundwater well can supply water to the proposed lot at a rate of 1250m<sup>3</sup>/year.</p> <ul style="list-style-type: none"> <li>As a condition of subdivision, a Deferred Services Agreement shall be registered against each new certificate of title (lot) created, requiring the owner to tie into municipal services when they become available.</li> </ul> <p><b>Storm Water Management</b> – Section 700.0 requirements:</p> <ul style="list-style-type: none"> <li>As a condition of subdivision, the applicant shall submit a Site Specific Stormwater Implementation Plan (SSIP) for the subject lands, in accordance with the requirements of the County Servicing Standards and the Bragg Creek Master Drainage Plan.</li> </ul> <p><b>Environmental</b> – Section 900.0 requirements:</p> <ul style="list-style-type: none"> <li>ES has no requirements at this time.</li> <li>Any approvals required through Alberta Environment shall be the sole responsibility of the Applicant/Owner.</li> </ul>
Infrastructure and Operations - Maintenance	No issues.
Infrastructure and Operations - Capital Delivery	No concerns.
Infrastructure and Operations – Utility Services	No concerns.
Infrastructure and Operations – Road Operations	Approach required for new parcel.

Circulation Period: July 24, 2018 – August 15, 2018





TSUUT'INA NATION RESERVE

MUNICIPAL DISTRICT OF FOOTHILLS

RF2	Ranch and Farm Two	B-1	Highway Business
RF3	Ranch and Farm Three	B-2	General Business
AH	Agricultural Holding	B-3	Limited Business
F	Farmstead	B-4	Recreation Business
R-1	Residential One	B-5	Agricultural Business
R-2	Residential Two	B-6	Local Business
R-3	Residential Three	NRI	Natural Resource Industrial
DC	Direct Control	HR-1	Hamlet Residential Single Family
PS	Public Service	HR-2	Hamlet Residential (2)
		HC	Hamlet Commercial
		AP	Airport

# LAND USE MAP

Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M



**Subdivision Proposal:** To create a ± 0.94 hectare (± 2.32 acre) parcel with a ± 1.03 hectare (± 2.53 acre) remainder.

Page 13 of 19  
Approach upgraded to mutual standard

BOYCE RANCH RD

Lot 1  
± 0.94 ha  
(± 2.32 ac)



Existing Access Easement Area

Lot 2  
(remainder)  
± 1.03 ha  
(± 2.53 ac)

Legend	
Approach	
Driveway	
Dwelling	
Water Well	
Septic Field	
Road Acquisition Area	
15 m Restrictive Covenant	

**Surveyor's Notes:**

1. Parcels must meet minimum size and setback requirements of Land Use Bylaw C-4841-97.
2. Refer to Notice of Transmittal for approval conditions related to this Tentative Plan.

**TENTATIVE PLAN**

**Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M**





Note: Post processing of raw aerial photography may cause varying degrees of visual distortion at the local level.

**AIR PHOTO**

*Spring 2016*

**Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M**



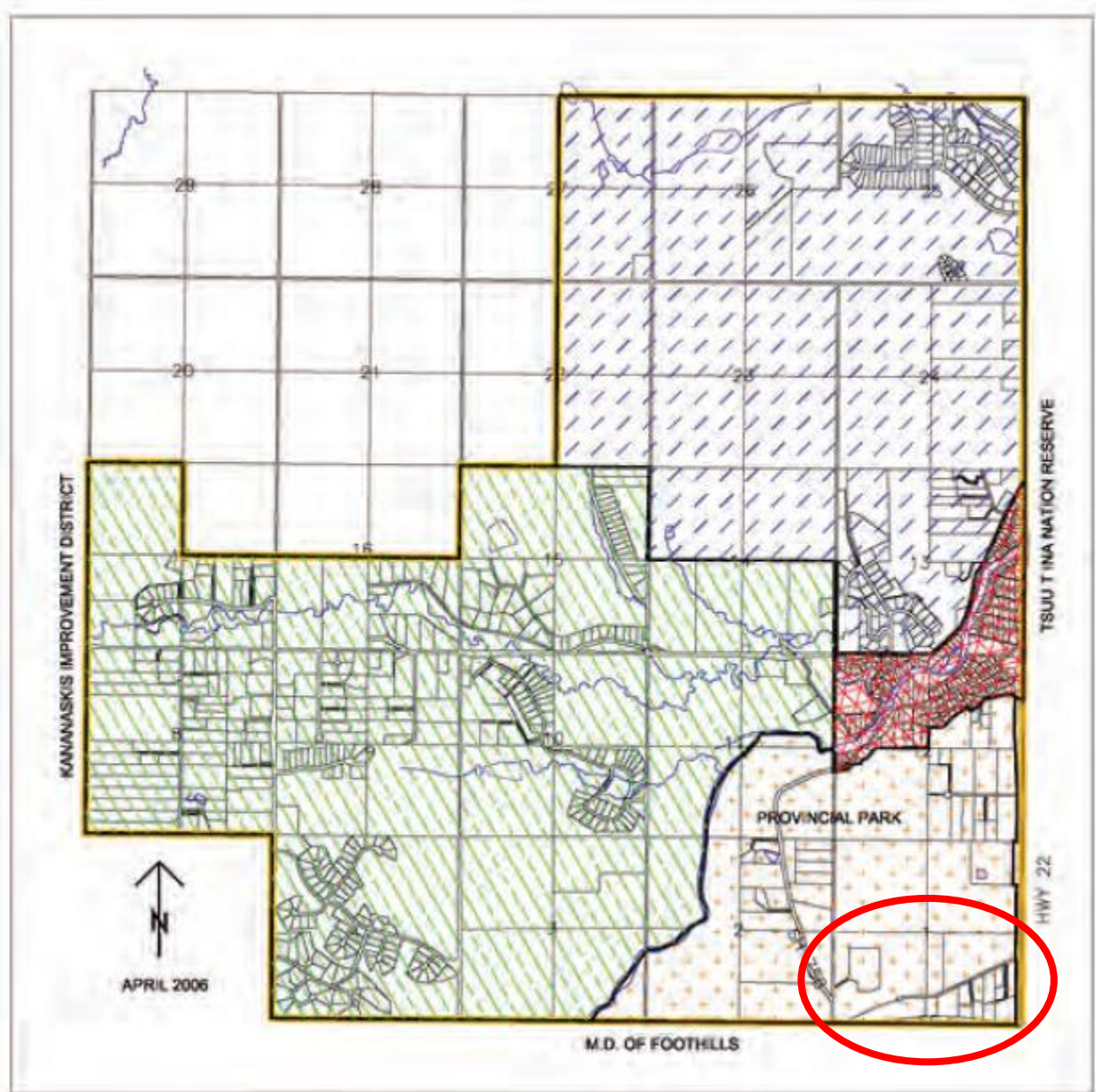
Date: July 20, 2018

Division # 1

File: 03901008

**AGENDA**

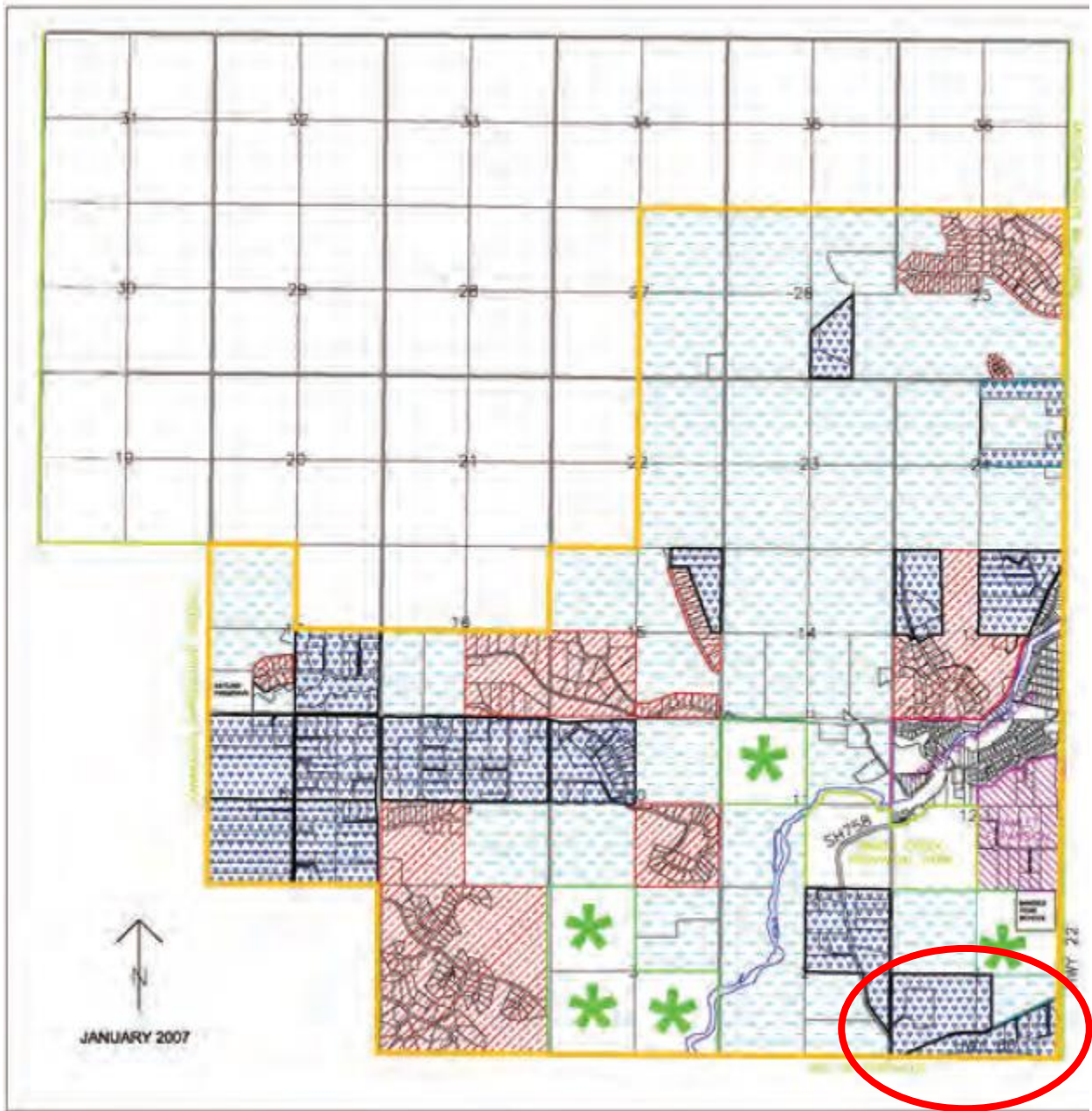
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**FIGURE 6: SOUTH BRAGG CREEK**

**Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M**





**FIGURE 13: INFILL RESIDENTIAL  
AREA**

**Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M**





# TOPOGRAPHY

Contour Interval 1 M (LIDAR)

Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M



Date: July 20, 2018

Division # 1

File: 03901008

**AGENDA**  
Page 416 of 446



**Gross Developable Area (GDA)** means the amount of land that remains once areas that represent constraints to development have been subtracted from the gross area. No topographic or environmental constraints have been determined for this area.

**Policy 7.4.3(d)** states that within south Bragg Creek, parcel sizes should not be less than 2 acres with an overall density of not greater than one lot per 3 acres of GDA. Given no constraints have been identified, GDA is equivalent to the gross area.

**Figure 13** identifies the overall area where density can be calculated (predetermined conceptual scheme boundaries). Based on this area, the GDA density calculation is:

Overall area of lands within predetermined boundary (figure 13)	86.47 acres
Total number of lots within overall area (including proposed and already approved lots)	16
Overall density of GDA	1 lot / 5.40 acres

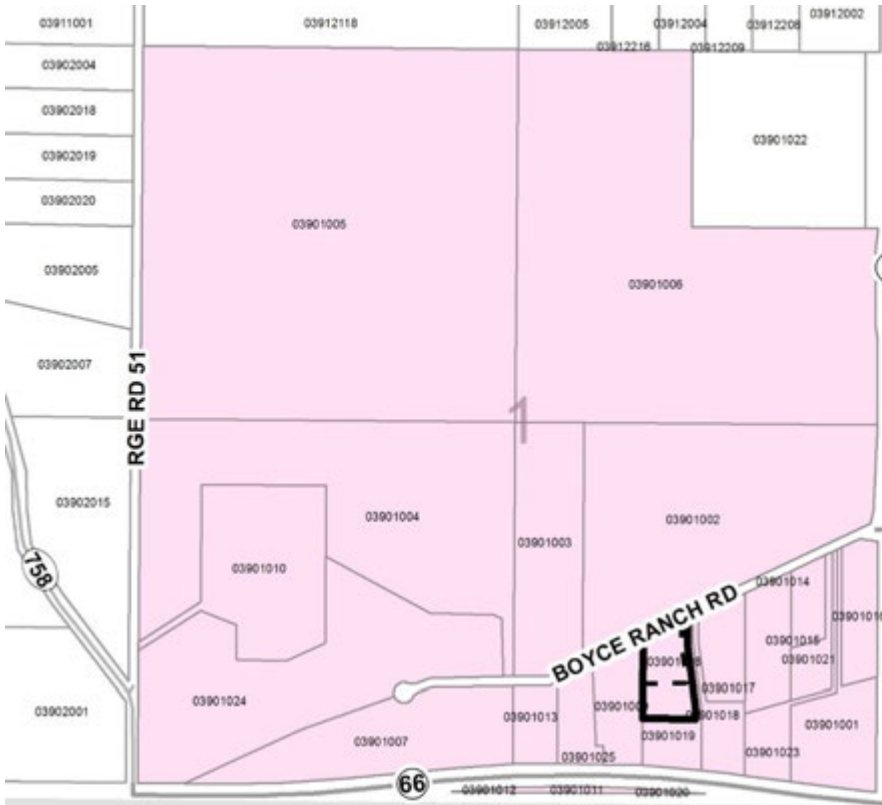


\*Overall area used for calculating density



## GROSS DEVELOPABLE AREA



**Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M**





MUNICIPAL DISTRICT OF FOOTHILLS

-  Letters in Opposition
-  Letters in Support

Legend	
	Circulation Area
	Subject Lands

# LANDOWNER CIRCULATION AREA

**Lot:1 Block:B Plan:9511735  
SE-01-23-05-W05M**





## PLANNING SERVICES

**TO:** Subdivision Authority  
**DATE:** October 23, 2018 **DIVISION:** 5  
**FILE:** 03329006 **APPLICATION:** PL20180066  
**SUBJECT:** Subdivision Item – Industrial – Industrial Activity and Public Services District

**<sup>1</sup>POLICY DIRECTION:**

The application was evaluated against Section 654 of the *Municipal Government Act*, Section 7 of the Subdivision and Development Regulations, the Janet Area Structure Plan (JASP), and the Rocky View County / City of Calgary Intermunicipal Development Plan and was found to be in compliance:

- The application is consistent with the Statutory Policy;
- The subject lands hold the appropriate land use designation; and
- The technical aspects of the proposal were considered and are further addressed through the conditional approval requirements.

**EXECUTIVE SUMMARY:**

The purpose of this application is to create two ± 1.67 hectare (± 4.13 acre) parcels, two ± 1.10 hectare (± 2.72 acre) parcels, two ± 1.01 hectare (± 2.50 acre) parcels, and two ± 1.64 hectare (± 4.05 acre) parcels, with a ± 3.82 hectare (± 9.44 acre) remainder.

The lands are developed with an existing homestead and agricultural operation that cultivates grain crops. Servicing for the proposed development would include the use of sewage holding tanks and potable water cisterns. Stormwater is to be directed to the proposed stormwater evaporation pond on the remainder lot, which would be registered as a Public Utility Lot (PUL). Access would be achieved via a newly constructed intersection with Range Road 285 and an internal road network.

Administration determined that the application meets policy.

<p><b>PROPOSAL:</b> To create two ± 1.67 hectare (± 4.13 acre) parcels, two ± 1.10 hectare (± 2.72 acre) parcels, two ± 1.01 hectare (± 2.50 acre) parcels, and two ± 1.64 hectare (± 4.05 acre) parcels, with a ± 3.82 hectare (± 9.44 acre) remainder.</p>	<p><b>GENERAL LOCATION:</b> Located approximately 1.6 kilometers (1 mile) north of Highway 560 (Glenmore Trail), on the east side of Range Road 285 (Garden Road).</p>
<p><b>LEGAL DESCRIPTION:</b> A portion of NW-29-23-28-W04M.</p>	<p><b>GROSS AREA:</b> ± 16.19 hectares (± 40.00 acres)</p>
<p><b>APPLICANT:</b> Tronnes Geomatics <b>OWNER:</b> Joseph Bleile</p>	<p><b>RESERVE STATUS:</b> Municipal Reserves to be provided by a cash-in-lieu payment.</p>

<sup>1</sup>**Administration Resources**  
 Jamie Kirychuk, Planning Services  
 Gurbir Nijjar, Engineering Services



<p><b>LAND USE DESIGNATION:</b> Industrial – Industrial Activity &amp; Public Services District</p>	<p><b>LEVIES INFORMATION:</b> The Transportation Off-Site Levy and Stormwater Levy are applicable in this case.</p>
<p><b>DATE APPLICATION RECEIVED:</b> June 19, 2018 <b>DATE DEEMED COMPLETE:</b> July 16, 2018</p>	<p><b>APPEAL BOARD:</b> Municipal Government Board</p>
<p><b>TECHNICAL REPORTS SUBMITTED:</b></p> <ul style="list-style-type: none"> <li>• Transportation Impact Assessment (Updated July 16, 2018)</li> <li>• Storm Water Management Plan (May 2017)</li> <li>• Environmental Assessment and Initial Wetland Evaluation (February 6, 2018)</li> <li>• Geotechnical Investigation (January 17, 2018)</li> </ul>	<p><b>LAND USE POLICIES AND STATUTORY PLANS:</b></p> <ul style="list-style-type: none"> <li>• County Plan (Bylaw C-7280-2013)</li> <li>• Land Use Bylaw (Bylaw C-4841-97)</li> <li>• Janet Area Structure (Bylaw C-7418-2014)</li> <li>• Rocky View / Calgary Intermunicipal Development Plan (Bylaw C-7197-2012)</li> </ul>

**PUBLIC & AGENCY SUBMISSIONS:**

The application was circulated to 103 adjacent landowners. No responses were received. The application was also circulated to a number of internal and external agencies. Those responses are available in Appendix ‘B’.

**HISTORY:**

- April 24, 2018** Application PL20170114 was approved by Council to redesignate the subject lands from Ranch and Farm District to Industrial – Industrial Activity District and Public Services District in order to allow for the creation of two ± 1.29 hectare (± 3.19 acre) parcels, four ± 1.0 hectare (± 2.50 acre) parcels, two ± 1.86 hectare (± 4.6 acre) parcels, and a ± 3.82 hectare (± 9.44 acre) remainder.
- November 11, 2014** Janet Area Structure Plan (Bylaw C-7418-2104) adopted.
- 1987** Plan 8810698 is registered, resulting in the separation of the parcel to the south.

**TECHNICAL CONSIDERATIONS:**

This application was evaluated in accordance with the matters listed in Section 7 of the Subdivision and Development Regulation, which are as follows:

a) The site’s topography:

The subject lands are flat and feature no significant waterbodies or topographical features that would inhibit development.

Conditions: None.

b) The site’s soil characteristics:

The soils on site are Class 1 with no significant limitations. There are no concerns that soil conditions would have an impact on future development of the lands.

Conditions: None.



c) Stormwater collection and disposal:

The Applicant submitted a conceptual level stormwater plan in support of the land use redesignation application (PL20170114). This report proposes stormwater infrastructure to support the development, including a stormwater evaporation pond and irrigation disposal infrastructure meeting the zero-release requirement of the Janet Master Drainage Plan. The Applicant proposes that a future Lot Owners' Association is to be responsible for the ownership, operation, and maintenance of the irrigation disposal system.

As a condition of subdivision, the Applicant/Owner would be required to submit a detailed Stormwater Management Report, providing the detailed designs of the Stormwater Management infrastructure necessary to support the proposed development. Alberta Environment and Parks approvals and licensing for the stormwater management infrastructure would be required.

The Owner would be required to enter into a Development Agreement for the construction of the stormwater infrastructure required as a result of the development and as outlined in the final Stormwater Management Plan. The Applicant/Owner would be responsible for the registration of any required easements, utility rights of way, and/or public utility lots.

As a condition of subdivision, the Applicant/Owner would be required to provide an Erosion & Sedimentation Control Plan, providing measures to be implemented during construction.

As a condition of subdivision, the Applicant/Owner would be required to provide payment of the Stormwater Offsite Levy in accordance with Bylaw C-7535-2015. At this time, the estimated levy payment owed at time of subdivision endorsement is \$219,500 (CSMI).

Conditions: 5, 6, 13, 17

d) Any potential for flooding, subsidence, or erosion of the land:

As part of the previous land use application, the Applicant provided a Geotechnical Investigation, prepared by Lone Pine Geotechnical Ltd., dated January 17, 2018. The report provides the subsurface conditions of the subject lands and provides various recommendations for the development of the subject lands. The report concludes that the on-site soils are generally suitable to support the proposed development.

As a condition of subdivision, the Applicant/Owner would be required to implement a groundwater measurement program to determine the levels of the seasonal groundwater table within the subject lands. The Applicant may also be able to conduct further geotechnical investigation to better establish the loading capacity of the on-site soils for stormwater irrigation purposes, as stormwater irrigation is integral to the stormwater management strategy for the development. If the on-site soils are deemed to be inadequate, the report is to provide recommendations (i.e.: topsoil thicknesses) to allow for the adequate loading and absorption of stormwater irrigation.

Conditions: 14

e) Accessibility to a road:

The Applicant previously submitted a Transportation Impact Assessment (TIA) by JCB Engineering, dated May 24, 2017. The TIA indicates that all impacted intersections analyzed in the TIA shall operate at acceptable levels of service at the opening day; however, intersectional improvements along Range Road 285 are warranted at future horizons based on the growth of background traffic and the build out of the Janet ASP. The TIA also indicates that the future widening of Range Road 285 to a four-lane cross section and the implementation of a traffic signal at Range Road 285 and Bluegrass Drive warrant the closure of the site access from Range Road 285 at the 2040 horizon due to inadequate spacing. At that time, access to the development would come via an eastward extension of Bluegrass Drive across Range Road 285 using the future north/south road allowance dedicated via the Road Acquisition Agreement.



The Applicant/Owner would be required to enter into a Road Acquisition Agreement with the County for the future acquisition of the north/south road allowance along the boundaries of proposed lots 1, 2, 3, and 4 to construct a future road (30 m width). This future roadway connection would serve as primary access into and out of the proposed development.

The City of Calgary, with the involvement of both Alberta Transportation and the County, conducted a Functional Planning Study for an all-directional interchange at the intersection of Range Road 285 and Glenmore Trail. The Study also includes an interim, at-grade intersectional improvement, which would increase the current capacity for approximately ten years. The Study was recently approved by the City of Calgary and accepted as information at the October 2 Policy & Priorities Committee.

In conjunction with the Functional Planning Study, on July 18, 2018, the Applicant provided a revised Traffic Impact Assessment (TIA). The revised TIA provides an analysis of the intersection at Highway 560 and Range Road 285, and determines an alternative interim solution to what was recommended within the Functional Planning Study to be implemented to support the proposed development. Engineering Services reviewed the alternative recommendation and has no concerns with the revised solution. The revised TIA was circulated to the City of Calgary and Alberta Transportation on September 26, 2018 for comment. To date, no comments have been received.

The Owner would be required to enter into a Development Agreement for the construction of an internal Industrial/Commercial subdivision road in accordance with the County Servicing Standards and the revised TIA. The Owner would also be required to enter into an additional Development Agreement for all other off-site infrastructure upgrades required to accommodate the development as outlined in the revised TIA, or as required by Rocky View County and Alberta Transportation. The Applicant/Owner would be responsible for any additional acquisitions or rights-of-way necessary to implement the alternative recommendation within the revised TIA.

The Applicant/Owner would be required to dedicate eight meters (8 m) along the entire west boundary of the subject site for future road widening in accordance with the requirements of the SE Industrial Growth Study. Five meters (5 m) shall be dedicated by Plan of Survey, with the remaining three meters (3 m) to be dedicated by caveat.

#### *Transportation Off-Site Levy*

The Applicant/Owner would be required to provide payment of the Transportation Offsite Levy in accordance with Bylaw C-7356-2014.

- At this time, the estimated levy payment owed at time of subdivision endorsement is \$580,400 (Base + Special Area #3 + Special Area #8).

Conditions: 2, 3, 4, 8, 9, 10, 18

#### f) Water supply, sewage, and solid waste disposal:

The Applicant proposes to use potable water cisterns and sewage holdings tanks to service the proposed lots, which aligns with the policies of the Janet ASP.

As a condition of subdivision, the Applicant would be required to provide a drafting hydrant system from the proposed Stormwater pond to service the proposed development. Detailed designs of the fire suppression infrastructure servicing the proposed development, prepared by a qualified professional, would be required.

Conditions: 12

#### g) The use of the land in the vicinity of the site:

The subject lands are located within the Janet ASP, east of the city of Calgary, and southwest of



the city of Chestermere. This is an area of the County that primarily consists of business, commercial, and industrial development, but also features a mix of land uses. The majority of business uses are found to the west, where numerous commercial, industrial, and direct control districts are located. To the east, lands are predominantly agricultural, with scattered pockets of country residential development. Lands immediately to the north are located within the Emcor Business Park Conceptual Scheme. The Heather Glenn Golf Course is located south of the subject lands.

Conditions: None

h) Other matters:

*Municipal Reserves*

Municipal Reserves would be provided through a cash-in-lieu payment for the gross acreage of the subject site. Because the minimum parcel size in the Industrial – Industrial Activity district is 1.01 hectares (2.50 acres) as per section 75.6 of the Land Use Bylaw, further subdivision is not possible; therefore, Reserves should be collected in full.

The reserves owing for the subject site are 10% of the gross acreage of the subject site, which equates to approximately 40.00 acres. This would be confirmed at the time of endorsement through the Plan of Survey as per Condition 1.

- 40.00 acres X 10% = 4.00 acres owing to be provided by cash-in-lieu, in accordance with the Appraisal Report prepared by Outlook Realty Advisors Inc. dated June 20, 2018, in the amount of \$125,000.00 per acre. Estimated total of \$500,000.00.

Conditions: 19

**POLICY CONSIDERATIONS:**

This application was previously assessed and found to be in accordance with the Janet Area Structure Plan and the Rocky View County / City of Calgary Intermunicipal Development Plan. The detailed policy review was provided to Council at the redesignation stage with application PL20170114. The subject land holds the appropriate land use designation for the proposed parcel size, in accordance with the Land Use Bylaw.

**CONCLUSION:**

The application was evaluated in accordance with the Janet Area Structure Plan (JASP), and the Rocky View County / City of Calgary Intermunicipal Development Plan and Administration determined that:

- The application is consistent with the Statutory Policy;
- The subject lands hold the appropriate land use designation;
- The technical aspects of the subdivision proposal were considered and are further addressed through the conditional approval requirements.

**OPTIONS:**

Option #1: THAT Subdivision Application PL20180066 be approved with the conditions noted in Appendix 'A'.

Option #2: THAT Subdivision Application PL20180066 be refused as per the reasons noted.



Respectfully submitted,

“Chris O’Hara”

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General Manager

JK/rp

Concurrence,

“Rick McDonald”

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Interim County Manager

**APPENDICES:**

APPENDIX ‘A’: Approval Conditions

APPENDIX ‘B’: Application Referrals

APPENDIX ‘C’: Map Set



### APPENDIX 'A': Approval Conditions

- A. The application to create two  $\pm 1.67$  hectare ( $\pm 4.13$  acre) parcels, two  $\pm 1.10$  hectare ( $\pm 2.72$  acre) parcels, two  $\pm 1.01$  hectare ( $\pm 2.50$  acre) parcels, two  $\pm 1.64$  hectare ( $\pm 4.05$  acre) with a  $\pm 3.82$  hectare ( $\pm 9.44$  acre) remainder within a portion of NW-29-23-28-W04M, having been evaluated in terms of Section 654 of the *Municipal Government Act* and Section 7 of the Subdivision and Development Regulations, and having considered adjacent landowner submissions, is approved as per the Tentative Plan for the reasons listed below:
1. The application is consistent with the Statutory Policy;
  2. The subject lands hold the appropriate land use designation;
  3. The technical aspects of the subdivision proposal have been considered and are further addressed through the conditional approval requirements.
- B. The Applicant/Owner is required, at their expense, to complete all conditions attached to and forming part of this conditional subdivision approval prior to Rocky View County (the County) authorizing final subdivision endorsement. This requires submitting all documentation required to demonstrate each specific condition has been met, or agreements (and necessary securities) have been provided to ensure the conditions will be met, in accordance with all County Policies, Standards, and Procedures, to the satisfaction of the County, and any other additional party named within a specific condition. Technical reports required to be submitted as part of the conditions must be prepared by a qualified professional, licensed to practice in the province of Alberta within the appropriate field of practice. The conditions of this subdivision approval do not absolve an Applicant/Owner from ensuring all permits, licenses, or approvals required by Federal, Provincial, or other jurisdictions are obtained.
- C. Further, in accordance with Section 654 and 655 of the *Municipal Government Act*, the application shall be approved subject to the following conditions of approval:

#### *Plan of Subdivision*

- 1) Subdivision is to be effected by a Plan of Survey, pursuant to Section 657 of the *Municipal Government Act*, or such other means satisfactory to the Registrar of the South Alberta Land Titles District.
- 2) The Owner is to dedicate, by Plan of Survey, a 5.0 m wide portion of land for road widening along the western boundary of Lots 1 and 2.

#### *Development Agreement*

- 3) The Owner is to enter into a Development Agreement for provision of the following infrastructure and improvements (further details are provided in the various sections below):
  - i. Construction of a public internal road system (Industrial/Commercial – 400.6 Standard) complete with a cul-de-sac bulb and all associated infrastructure;
  - ii. Mailbox locations are to be located in consultation with Canada Post to the satisfaction of the County;
  - iii. Fire servicing via a drafting hydrant system to the satisfaction of the County;
  - iv. Construction of stormwater facilities in accordance with the recommendations of the approved Stormwater Management Plan, and the registration of any overland drainage easements and/or restrictive covenants as determined by the Stormwater Management Plan;
  - v. Implementation of the recommendations of the approved ESC Plan;



- vi. Implementation of the recommendation of the approved Construction Management Plan; and
  - vii. Installation of power, natural gas, and telephone lines;
- 4) The owner shall enter into a Special Improvements Development Agreement for the following required off-site transportation infrastructure in accordance with the approved TIA at the intersection of Range Road 285 and Highway 560, including the following:
- i. Implementation of a southbound left turning lane at the north leg of the intersection;
  - ii. Modification of the traffic islands at the south leg of the intersection at the SE and SW corners;
  - iii. Local widening of Highway 560 through the intersection to a four (4) lane cross-section in accordance with Alberta Transportation guidelines. The length of the widening in each direction shall be determined at the detailed design stage, to the satisfaction of the County and Alberta Transportation; and
  - iv. Modifications to the signal timings to include appropriate phasing required for all left turning movements at the intersection.

OR

Should an intersectional improvement at Range Road 285/Highway 560 be implemented by others that meets or exceeds the upgrades identified by the approved TIA, the Owner shall pay to the County the relevant cost recoveries plus applicable interest for the improvements to the intersection of Range Road 285/Highway 560, in accordance with the Infrastructure Cost Recovery Agreement. The County shall calculate the total amount owing from the gross acreage as shown on the Plan of Survey submitted for endorsement. Should the owner not enter into a Special Improvements Development Agreement for improvements to the intersection of Range Road 285 and Highway 560, payment of cost recovery to others for the intersectional improvements at Range Road 285/Highway 560 shall be satisfactory to satisfy this condition.

#### *Stormwater*

- 5) The Owner is to provide and implement a Stormwater Management Report that meets the requirements outlined in the Janet Master Drainage Plan and CSMI Report. Implementation of the Stormwater Management Plan shall include the following:
  - i. If the recommendations of the Stormwater Management Report require improvements, then the Applicant/Owner shall enter into a Site Improvements / Services Agreement or Development Agreement;
  - ii. Registration of any required easements and/or utility rights-of-way;
  - iii. Necessary approvals and compensation provided to Alberta Environment for wetland loss and mitigation;
  - iv. Necessary Alberta Environment licensing documentation for the stormwater infrastructure system.
- 6) The Owner shall provide a detailed Erosion and Sedimentation Control Plan, prepared by a qualified professional, in accordance with the County Servicing Standards and best management practices.



### *Site Construction*

- 7) The Owner shall provide a Construction Management Plan that is to include, but not be limited to, noise, sedimentation and erosion control, construction waste management, firefighting procedures, evacuation plan, hazardous material containment, construction, and management details. Other specific requirements include:
  - i. Weed management during the construction phases of the project;
  - ii. Implementation of the Construction Management Plan recommendations, which will be ensured through the Development Agreement;

### *Transportation and Access*

- 8) The Owner is to enter into an Agreement, to be registered by caveat, respecting the future acquisition of lands for road widening, and shall include:
  - i. The provision of 3.0 m road widening along the western boundary of the property;
  - ii. The purchase of land by the County for \$1.
- 9) The Owner is to enter into a Road Acquisition Agreement with the County, to be registered by Caveat on the title of Lots 1 and 2, to serve as notice that those lands are intended for future development as a County road, as per the approved Tentative Plan. The Agreement shall include:
  - i. The provision of 30 m (+/- 0.60 ha) road acquisition along the north/south boundaries of Lots 1 and 2;
  - ii. The purchase of land by the County for \$1.
- 10) The Owner is to enter into a Restrictive Covenant, to be registered by Caveat prepared by the County, on the title of Lots 1, 2, 3, 4, 8, and 9 that restricts the erection of any structure on or within 15 meters of a future road right-of-way, as shown on the approved Tentative Plan.
- 11) The Owner shall obtain approval for a road name by way of application to, and consultation with, the County.

### *Site Servicing*

- 12) The Applicant/Owner shall design a central water fire suppression distribution system, including fire hydrants, for firefighting purposes, in accordance with the requirements described in the Fire Hydrant Water Suppression Bylaw (C-7259-2013):
  - i. Construction of the fire suppression collection system shall be included within the Development Agreement;
- 13) The Owner shall legally establish a Lot Owners' Association (LOA), and an encumbrance or instrument shall be concurrently registered against the title of each new lot created, requiring that each individual Lot Owner is a member of the Lot Owners' Association;
  - i. The LOA agreement shall specify the future maintenance obligations of the Lot Owners' Association for Stormwater facilities located on private and public lands.

### *Others*

- 14) The Applicant/Owner is to provide an updated Geotechnical Investigation Report, prepared by a Qualified Geotechnical Professional, licensed to practice in the Province of Alberta, in accordance with the County Servicing Standards, to the satisfaction of the County, which shall include:



- i. Implementation of a groundwater measurement program within the boundaries of the proposed phase, in accordance with the procedures and duration indicated in the County's Servicing Standards, to get an accurate representation of the groundwater table within the subject lands for consideration into the detailed design of the on-site infrastructure; and
- ii. Review of the findings of the groundwater measurement program to determine if the infrastructure design recommendations need to be updated or revised; and
- iii. Establish the loading capacity of the on-site soils for stormwater irrigation purposes. If the on-site soils are deemed to be inadequate, the report is to provide recommendations (i.e.: topsoil thicknesses) to allow for the adequate loading and absorption of stormwater irrigation.

15) Utility Easements, Agreements, and Plans are to be registered to the satisfaction of ATCO Gas.

#### *Payments and Levies*

- 16) The Applicant/Owner shall pay the County subdivision endorsement fee, in accordance with the Master Rates Bylaw, for the creation of eight (8) new Lots.
- 17) The Owner shall pay the Stormwater Off-Site Levy in accordance with Bylaw C-7535-2015 for the gross area of lands to be subdivided prior to entering into the Development Agreement. The County shall calculate the total amount owing:
  - i. From the total gross acreage of the Lands to be subdivided as shown on the Plan of Survey.
- 18) The Applicant/Owner shall pay the Transportation Off-Site Levy (TOL) in accordance with Bylaw C-7356-2014 prior to entering into the Development Agreements. The County shall calculate the total amount owing:
  - ii. From the total gross acreage of the Lands to be subdivided as shown on the Plan of Survey.

#### *Municipal Reserve*

- 19) The provision of Reserve in the amount of 10 percent for the gross area of the subject site, as determined by the Plan of Survey, is to be provided by payment of cash-in-lieu in accordance with the per acre value listed in the land appraisal prepared by Outlook Realty Advisors Inc. dated June 20, 2018, in the amount of \$125,000.00 per acre. The county shall calculate the total amount owing:
  - i. From the total gross acreage of the Lands to be subdivided as shown on the Plan of Survey.

#### *Cost Recovery*

- 20) The County will enter into an Infrastructure Cost Recovery Agreement with the Owner to determine the proportionate recovery of infrastructure money spent by the Owner to construct municipal infrastructure that will consequently provide benefit to other lands:
  - i. This Agreement shall apply to the construction of the improvements to the intersection of Range Road 285 and Highway 560.

OR

Should an intersectional improvement at Range Road 285/Highway 560 be implemented by others that meets or exceeds the upgrades identified by the approved TIA, the Owner shall pay to the County the relevant cost recoveries plus applicable interest for the improvements to the intersection of Range Road 285/Highway 560, in accordance with the Infrastructure Cost



Recovery Agreement. The County shall calculate the total amount owing from the gross acreage as shown on the Plan of Survey submitted for endorsement. Should the owner not enter into a Special Improvements Development Agreement for improvements to the intersection of Range Road 285 and Highway 560, payment of cost recovery to others for the intersectional improvements at Range Road 285/Highway 560 shall be satisfactory to satisfy this condition.

- 21) The Owner shall pay to the County the relevant cost recoveries, plus applicable interest, for the improvements to Range Road 285 development area, in accordance with the Infrastructure Cost Recovery Agreement. The County shall calculate the total amount owing from the gross acreage as shown on the Plan of Survey submitted for endorsement.

#### Taxes

- 22) All taxes owing up to and including the year in which subdivision is to be registered are to be paid to Rocky View County prior to signing the final documents pursuant to Section 654(1) of the *Municipal Government Act*.

#### D. SUBDIVISION AUTHORITY DIRECTION:

- 1) Prior to final endorsement of the subdivision, the Planning Department is directed to present the Applicant/Owners with a Voluntary Recreation Contribution Form and ask them if they will contribute to the Fund in accordance with the contributions prescribed in the Master Rates Bylaw.



**APPENDIX B: APPLICATION REFERRALS**

<b>AGENCY</b>	<b>COMMENTS</b>
School Authority	
Rocky View Schools	Rocky View Schools has no objection to this circulation.
Calgary Catholic School District	No comments received.
Public Francophone Education	No comments received.
Catholic Francophone Education	No comments received.
Province of Alberta	
Alberta Environment and Sustainable Resource Development (Public Lands)	No comments received.
Alberta Transportation	<p>By definition, this proposal must meet the requirements of Section 14 and 15 of the Subdivision and Development Regulation due to the proximity of Highway 560. Presently, the application does not appear to comply with any category of Section 14 of the Regulation.</p> <p>The department recognizes that the land involved in this application is removed from the provincial highway system, and relies on the municipal road network for access. However, future development of the nine proposed industrial lots will generate an increase in traffic likely impacting the safe and efficient operation of the intersection of Hwy 560 (Glenmore Trail) &amp; Range Road 285 / 100 ST SE.</p> <p>Rocky View County should coordinate with the City of Calgary to plan and construct any required intersection improvements at this intersection, in conjunction with Alberta Transportation.</p> <p>The department grants an unconditional variance of Section 14 and/or Section 15 of the Subdivision and Development Regulation. From the department's perspective, any appeals regarding this subdivision application must be heard by the Municipal Government Board.</p>
Alberta Culture and Community Spirit (Historical Resources)	No comments received.
Energy Resources Conservation Board	No comments received.
Alberta Health Services	Alberta Health Services, Environmental Public Health has received the above-noted application. At this time we do not have any



AGENCY	COMMENTS
Public Utility	<p>concerns with the information as provided. Please refer to our comments on the previous application for land use re-designation of the subject property dated August 21, 2017 (attached).</p>
ATCO Gas	<p>This subdivision will be creating 9 lots for industrial use and should be forwarded to engineering for their review as well. ATCO's existing land rights will not allow us to service all lots created by the subdivision but engineering will most likely work with the developer and consultant to ensure that a registered URW plan is created that will provide the necessary land rights to service this subdivision.</p> <ul style="list-style-type: none"> <li>• The landowner(s) is required to contact the ATCO Gas land agent listed below to execute a Utility Right of Way to the satisfaction of ATCO Gas.</li> </ul>
ATCO Pipelines	<p>The Engineering Department of ATCO Pipelines (a division of ATCO Gas and Pipelines Ltd.) has reviewed the above named plan and has no objections subject to the following conditions:</p> <ol style="list-style-type: none"> <li>1. Any existing land rights shall be carried forward in kind and registered on any newly created lots, public utility lots, or other properties.</li> <li>2. Ground disturbances and surface works within 30 meters require prior written approval- from ATCO Pipelines before commencing any work.             <ul style="list-style-type: none"> <li>• Municipal circulation file number must be referenced; proposed works must be compliant with ATCO Pipelines' requirements as set forth in the company's conditional approval letter.</li> <li>• Contact ATCO Pipelines' Land Department at 1-888-420-3464 for more information.</li> </ul> </li> <li>3. Road crossings are subject to Engineering review and approval.             <ul style="list-style-type: none"> <li>• Road crossing(s) must be paved and cross at a perpendicular angle.</li> <li>• Parallel roads are not permitted within ATCO Pipelines' right(s)-of-way.</li> <li>• If the road crossing(s) requires a pipeline alteration, the cost will be borne by the developer/owner and can take up to 18 months to complete.</li> </ul> </li> <li>4. Parking and/or storage is not permitted on ATCO Pipelines' pipeline(s) and/or right(s)-of-way.</li> <li>5. ATCO Pipelines recommends a minimum 15 meter setback from the centerline of the pipeline(s) to any buildings.</li> </ol>

AGENCY	COMMENTS
	<p>6. Any changes to grading that alter drainage affecting ATCO Pipelines' right-of-way or facilities must be adequate to allow for ongoing access and maintenance activities.</p> <ul style="list-style-type: none"> <li>• If alterations are required, the cost will be borne by the developer/owner.</li> </ul> <p>7. Any revisions or amendments to the proposed plans(s) must be re-circulated to ATCO Pipelines for further review.</p>
AltaLink Management	No comments received.
FortisAlberta	<p>Thank you for contacting FortisAlberta regarding the above application for subdivision. We have reviewed the plan and determined that no easement is required by FortisAlberta.</p> <p>FortisAlberta is the Distribution Wire Service Provider for this area. The developer can arrange installation of electrical services for this subdivision through FortisAlberta. Please have the developer contact 310-WIRE (310-9473) to make application for electrical services.</p>
Telus Communications	Telus has no issues or concerns with this proposed subdivision.
TransAlta Utilities Ltd.	No comments received.
Rockyview Gas Co-op Ltd.	No comments received.
Other External Agencies	
City of Calgary	<p>The City of Calgary has reviewed the below noted circulated application referencing the <i>Rocky View/Calgary Intermunicipal Development Plan (IDP)</i> and other applicable policies. It is our understanding that this application complies with the Janet ASP, specifically policies within Sections 10.0 Industrial and 23.0 Stormwater. Please contact me at the below if this is not the case.</p> <p>The City of Calgary has no comments regarding Application # PL20180066 – To create two ± 1.67 hectare (± 4.13 acre) parcels, two ± 1.10 hectare (± 2.72 acre) parcels, two ± 1.01 hectare (± 2.50 acre) parcels, two ± 1.64 ha (±4.05ac) with a ± 3.82 hectare (± 9.44 acre) remainder.</p>
EnCana Corporation	No comments received.
Western Irrigation District	<p>WID has no objection to the subdivision proposal; however there are currently 40 irrigation acres on this parcel, The Irrigation Districts Act requires that the irrigation acres be removed from the parcel, prior to the subdivision being finalized as the industrial designation is inconsistent with the land use for irrigation acres under The Irrigation Districts Act.</p>





AGENCY	COMMENTS
Rocky View County Boards and Committees	Please have the landowner contact WID in regard to the irrigation acres.
ASB Farm Members and Agricultural Fieldman	Agricultural Services Staff Comments: this parcel falls within the Janet Area Structure Plan and therefore Ag Services has no concerns.
Recreation Board	Given that Municipal Reserves were provided by a cash-in-lieu payment on a previous Plan, the Chestermere-Conrich Recreation Board has no comments on this circulation.
Internal Departments	
Municipal Lands	The Municipal Lands Office has no concerns with this application.
Development Authority	No comments received.
GeoGraphics	Please ensure a road naming application is required as a condition of subdivision at approval stage.
Building Services	No comments received.
Emergency Services	<p>Enforcement Services:</p> <ul style="list-style-type: none"> <li>• No concerns at this stage.</li> </ul> <p>Fire Services:</p> <ol style="list-style-type: none"> <li>1. Please ensure that water supplies and hydrants for the development are sufficient for firefighting purposes.</li> <li>2. Dependent on the occupancies, the Fire Service recommends that the buildings be sprinklered, if applicable, as per the Alberta Building Code.</li> <li>3. The Fire Service also recommends that the water co-op be registered with Fire Underwriters.</li> <li>4. Please ensure that access routes are compliant to the designs specified in the Alberta Building Code and RVC's servicing standards.</li> <li>5. The Fire Service also recommends a secondary access route onto the development.</li> </ol>
Infrastructure and Operations - Engineering Services	<p><b><u>General</u></b></p> <ul style="list-style-type: none"> <li>• The review of this file is based upon the application submitted. These conditions/recommendations may be subject to change to ensure best practices and procedures.</li> <li>• As a condition of subdivision, the Owner is required to enter into a Development Agreement pursuant to Section 655 of the Municipal Government Act respecting provision of the following:</li> </ul>

AGENCY	COMMENTS
	<ul style="list-style-type: none"> <li>• Construction of a public internal road system (Industrial/Commercial – 400.6 Standard) complete with a cul-de-sac bulb and all associated infrastructure;</li> <li>• Construction of the necessary off-site improvements as identified in the final approved TIA to the satisfaction of the County;</li> <li>• Mailbox locations are to be located in consultation with Canada Post to the satisfaction of the County;</li> <li>• Fire servicing via a drafting hydrant system to the satisfaction of the County;</li> <li>• Construction of storm water facilities in accordance with the recommendations of the approved storm water Management Plan and the registration of any overland drainage easements and/or restrictive covenants as determined by the storm water Management Plan;</li> <li>• Implementation of the recommendations of the approved ESC Plan;</li> <li>• Implementation of the recommendations of the approved Construction Management Plan; and</li> <li>• Installation of power, natural gas, and telephone lines;</li> <li>• As a condition of subdivision, the applicant will be required to establish a Lot Owner’s Association for the operation and maintenance of the stormwater irrigation disposal system required to support the proposed development. It is to be noted that this system is to be managed by a qualified 3<sup>rd</sup> party operator (under the supervision of a Lot Owner’s association) to the satisfaction of the County.</li> <li>• As a condition of subdivision, the applicant will be required to provide the appropriate cost recoveries for the previous improvement to Range Road 285 in accordance with the current Cost Sharing Agreement. As per the Agreement, the estimated cost recovery owed amounts to \$144,838.40 (\$3620.96 / acre @ 40 acres)</li> </ul>
	<p><b><u>Geotechnical</u></b> - Section 300.0 requirements:</p> <ul style="list-style-type: none"> <li>• As part of the previous land use application, the applicant provided a Geotechnical Investigation, prepared by Lone Pine Geotechnical Ltd. dated January 17, 2018. The report provided the subsurface conditions of the subject lands and provided various recommendations for the development of the subject lands. The report concludes that the onsite soils are generally suitable to support the proposed development.</li> <li>• As a condition of subdivision, the applicant will be required to implement groundwater measurement program to determine the level of the seasonal groundwater table within the subject lands. The applicant may also be able to conduct further geotechnical investigation to better establish the loading capacity of the onsite soils for stormwater irrigation purposes as stormwater irrigation is integral to the stormwater</li> </ul>

AGENCY	COMMENTS
	<p>management strategy for the development. If the onsite soils are deemed to be inadequate, the report is to provide recommendations (i.e.: topsoil thicknesses) to allow for the adequate loading and absorption of stormwater irrigation</p> <p><b>Transportation</b> - Section 400.0 requirements:</p> <ul style="list-style-type: none"> <li>• The applicant previously submitted a Transportation Impact Assessment by JCB Engineering dated May 24, 2017. The TIA indicates that all impacted intersections analyzed in the TIA shall operate at acceptable levels of service at the opening day however intersectional improvements along Range Road 285 are warranted at future horizons based on the growth of background traffic and build out of the Janet ASP. The TIA also indicates that the future widening of Range Road 285 to a four lane cross section and the implementation of a traffic signal at Range Road 285 and Bluegrass Drive warrants the closure of the site access from RR 285 at 2040 horizon due to inadequate spacing. At that time, access to the development will come via an eastward extension of Bluegrass Drive across RR 285 utilizing the future north/south road allowance dedicated via the Road Acquisition Agreement;</li> <li>• The City of Calgary, with the involvement of both AT and the County, have conducted a Functional Planning Study for an all directional interchange at the intersection of RR 285 &amp; Glenmore Trail. The Study also includes an interim, at-grade intersectional improvement which will increase the current capacity for approx. ten years. The Study has been presented and accepted by both City and County Councils respectively;</li> <li>• The applicant provided an updated TIA prepared by JCB Engineering dated August 16, 2018 which provided an assessment of the intersection of Range Road 285 and Highway 560 in conjunction with the recently accepted Functional Planning Study for Glenmore Trail East. The TIA recommends the following improvement to the intersection: <ul style="list-style-type: none"> <li>- Implementation of a southbound left turning lane at the north leg of the intersection;</li> <li>- Modification of the traffic islands at the south leg of the intersection at the SE and SW corners;</li> <li>- Local widening of Highway 560 through the intersection to a four (4) lane cross-section in accordance with Alberta Transportation guidelines. The length of the widening in each direction shall be determined at the detailed design stage to the satisfaction of the County and Alberta Transportation; and</li> <li>- Modifications to the signal timings to include appropriate phasing required for all left turning movements at the intersection</li> </ul> </li> </ul>

AGENCY	COMMENTS
	<p>Should an intersectional improvement at Range Road 285/Highway 560 be implemented by others which also provides service to this development, the owner shall provide their proportionate payment of their share of the costs of the improvements to the satisfaction of the County. Should the owner not enter into a Development Agreement for improvements to the intersection of Range Road 285 and Highway 560, payment of cost recovery to others for the intersectional improvements at Range Road 285/Highway 560 shall be satisfactory to satisfy this condition.</p> <ul style="list-style-type: none"> <li>• As a condition of subdivision, the applicant will be required to enter into a Road Acquisition Agreement with the County for the future acquisition of the north/south road allowance along the boundaries of proposed lots to construct a future road (30m width). Ultimately this future roadway connection will serve as primary access into and out of the proposed development;</li> <li>• Ultimately, the Janet ASP shows that Bluegrass Drive will be extended east across Range Road 285 south of the proposed site. As recommended in the TIA, at such time in the future that this extension is constructed, the existing access off of Garden Road to the business park should be removed and reclaimed, and the Road Acquisition Agreements may be acted on for the site to gain access directly to Bluegrass Drive;</li> <li>• As a condition of subdivision, the applicant will be required to enter into a Development Agreement for the construction of an internal Industrial/Commercial subdivision road in accordance with the County Servicing Standards and the TIA. As well, the applicant will be required to enter into a DA for all other offsite infrastructure upgrades required to accommodate the development as outlined in the final approved TIA or as required by Rocky View County and Alberta Transportation. If acquisition of any additional right of way is necessary to implement the TIA recommendations, this will be the responsibility of the applicant;</li> <li>• As a condition of subdivision, the applicant is required to dedicate 8m along the entire west boundary of the subject site for future road widening in accordance with the requirements of the SE Industrial Growth Study. Five (5) meters shall be dedicated by Plan of Survey with the remaining three (3) meters to be dedicated by caveat;</li> <li>• As a condition of subdivision, the applicant will be required to provide payment of the Transportation Offsite Levy in accordance with Bylaw C-7356-2014. The estimated levy payment owed at time of subdivision endorsement is \$580,280 (Base = \$4,595/ac x 40.00 ac = \$183,800; Special Area 3 = \$9028/ac x 40.00 ac = \$361,120; Special Area 7 =</li> </ul>

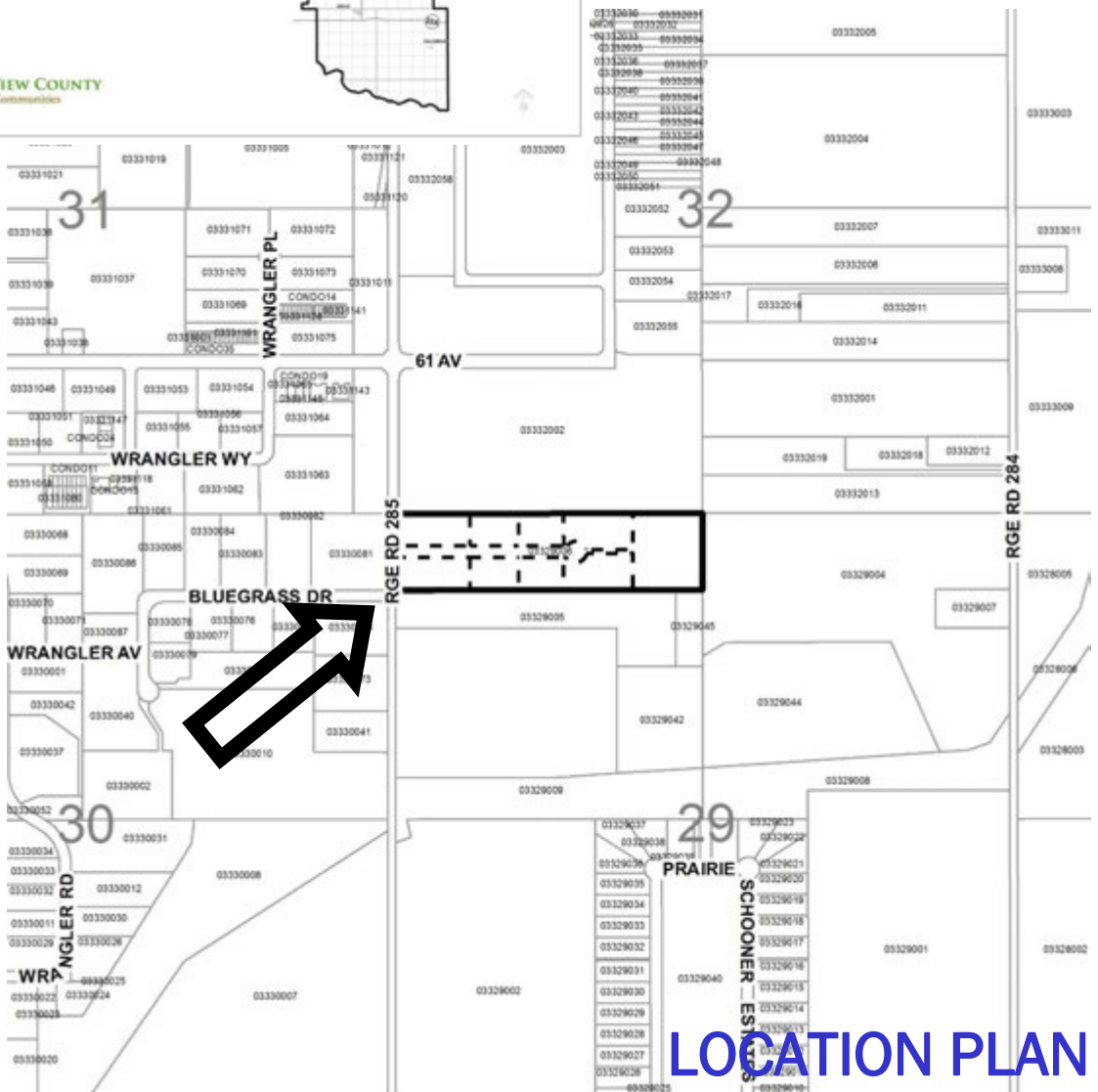
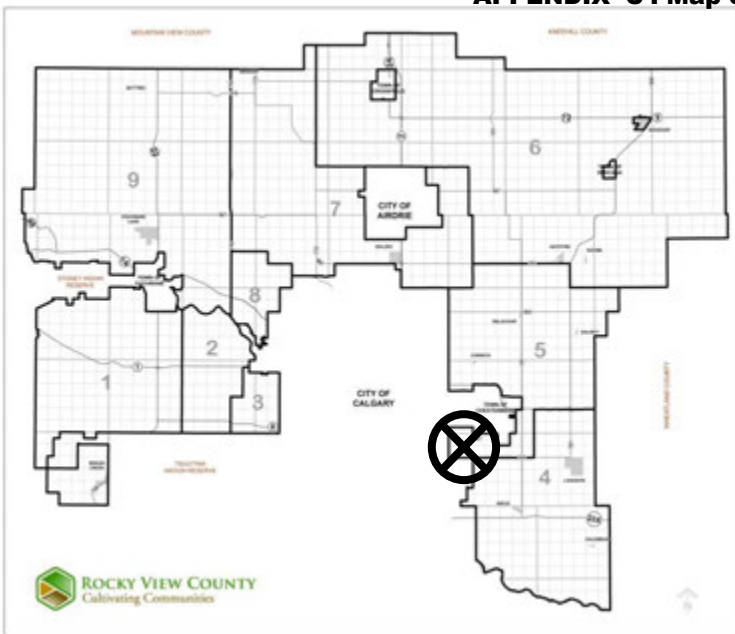


AGENCY	COMMENTS
	<p>\$884/ac x 40.00 ac = \$35,360).</p> <p><b><u>Sanitary/Waste Water</u></b> - Section 500.0 requirements:</p> <ul style="list-style-type: none"> <li>The applicant has proposed to utilize sewage holding tanks to service the proposed lots aligning with County Policy 449 and the Janet ASP. ES has no further concerns</li> </ul> <p><b><u>Water Supply And Waterworks</u></b> - Section 600.0 &amp; 800.0 requirements:</p> <ul style="list-style-type: none"> <li>The applicant has proposed to utilize potable water cisterns to service the proposed lots which align with the policies of the Janet ASP. ES has no further concerns;</li> <li>The applicant has indicated to provide a drafting hydrant system from the proposed stormwater pond to service the proposed development. As a condition of subdivision, the applicant will be required to provide the detailed design, prepared by a qualified professional, of the fire suppression infrastructure servicing the proposed development</li> </ul> <p><b><u>Storm Water Management</u></b> – Section 700.0 requirements:</p> <ul style="list-style-type: none"> <li>The applicant previously provided a conceptual level storm water management plan prepared by Integrated Environments dated May 24, 2017. The report proposes storm water infrastructure to support the development including a storm water evaporation pond (located on a PUL) and irrigation disposal infrastructure meeting the zero-release requirement of the Janet Master Drainage Plan. The applicant has proposed that a future Lot Owners Association is to be responsible for the ownership, operation and maintenance of the irrigation disposal system.</li> <li>As a condition of subdivision, the applicant will be required to submit a detailed stormwater management report, prepared by a qualified professional, providing the detailed designs of the stormwater management infrastructure necessary to support the proposed development</li> <li>As a condition of subdivision, the applicant will be required to provide an Erosion &amp; Sedimentation Control Plan, prepared by a qualified professional, providing the ESC measures to be implemented during construction</li> <li>As a condition of subdivision, the applicant will be required to enter into a Development Agreement for the construction of the storm water infrastructure required as a result of the development and outlined in the final Storm water Management Plan including access from the internal road through the panhandle all in accordance with the County Servicing Standards. The applicant will be responsible for the registration of any required easements, utility right of ways and/or public utility lots is required as a condition of subdivision.</li> </ul>



AGENCY	COMMENTS
	<ul style="list-style-type: none"> <li>As a condition of subdivision, the applicant will be required to provide payment of the Stormwater Offsite Levy in accordance with Bylaw C-7535-2015. The estimated levy payment owed at time of subdivision endorsement is \$219,500 (CSMI = \$5488/ac x 40.00 ac = \$219,500)</li> <li>As a condition of future subdivision, the Applicant will be required to obtaining all AEP approvals and licensing for the storm water management infrastructure.</li> </ul>
	<p><b><u>Environmental</u></b> – Section 900.0 requirements:</p> <ul style="list-style-type: none"> <li>The applicant previously provided a Desktop Environmental Assessment and Initial Wetland Evaluation for the subject lands prepared by Tannas Conservation Services Ltd. dated February 06, 2018. The assessment took into consideration the significance of the existing onsite soils, vegetation, wildlife, historical resources and wetlands and concludes that there are no wildlife, plant, or historical resources of concern on the property. The assessment does indicate that there appears to have been a temporary impact of water on the landscape, but it does not appear to be due to a naturally formed wetland.</li> </ul>
<p>Infrastructure and Operations - Maintenance</p>	<p>No concerns.</p>
<p>Infrastructure and Operations - Capital Delivery</p>	<p>No concerns.</p>
<p>Infrastructure and Operations – Road Operations</p>	<p>No concerns.</p>
<p>Infrastructure and Operations - Utility Services</p>	<p>Need to understand proposed water / wastewater servicing for newly created lots, no information provided.</p>

Circulation Period: June 28 – July 31, 2018



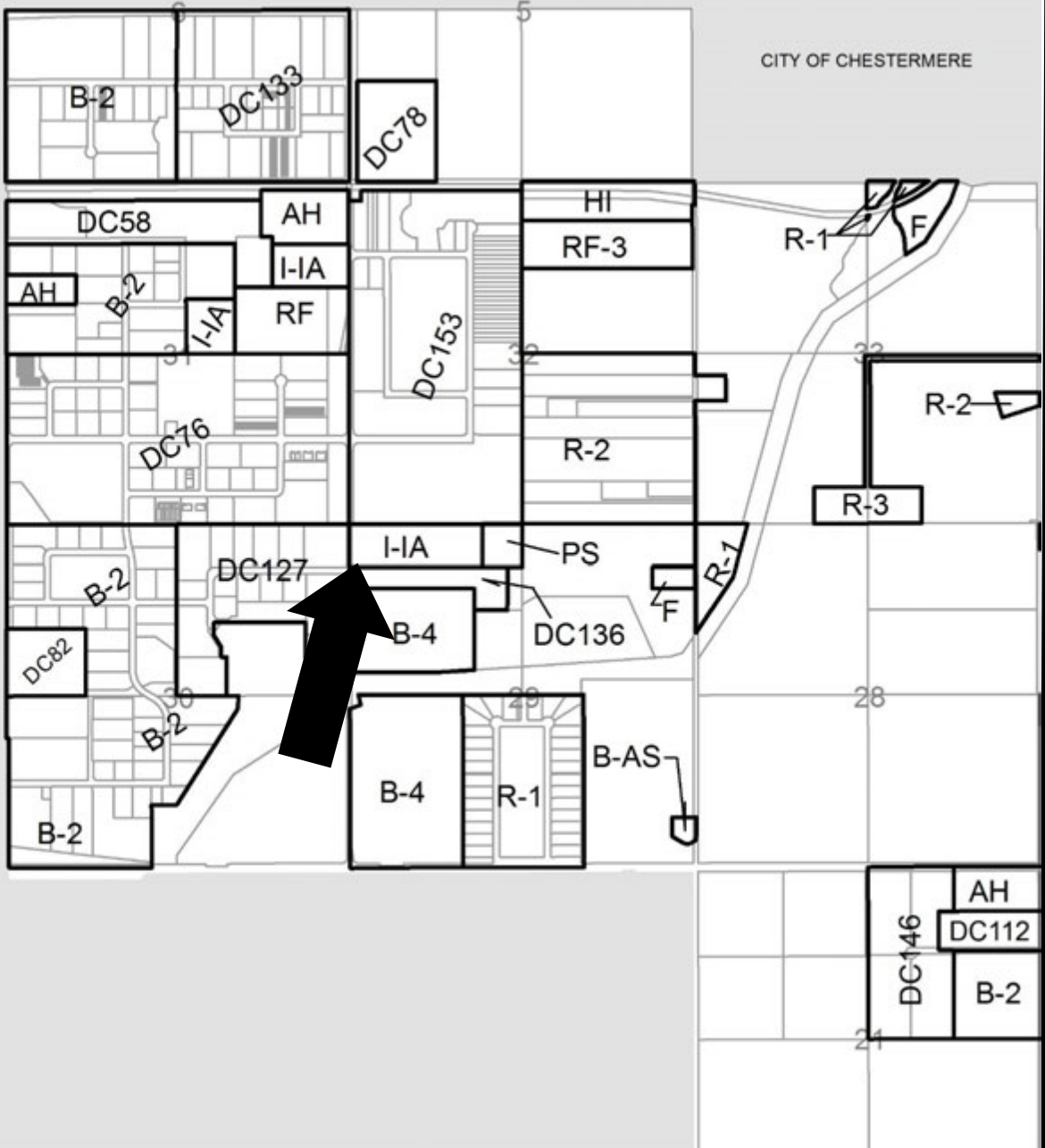
LOCATION PLAN

NW-29-23-28-W04M



CITY OF CALGARY

CITY OF CHESTERMERE



- |                          |                                       |
|--------------------------|---------------------------------------|
| Ranch and Farm           | B-1 Highway Business                  |
| RF2 Ranch and Farm Two   | B-2 General Business                  |
| RF3 Ranch and Farm Three | B-3 Limited Business                  |
| AH Agricultural Holding  | B-4 Recreation Business               |
| F Farmstead              | B-5 Agricultural Business             |
| R-1 Residential One      | B-6 Local Business                    |
| R-2 Residential Two      | NRI Natural Resource Industrial       |
| R-3 Residential Three    | HR-1 Hamlet Residential Single Family |
| DC Direct Control        | HR-2 Hamlet Residential (2)           |
| PS Public Service        | HC Hamlet Commercial                  |
|                          | AP Airport                            |

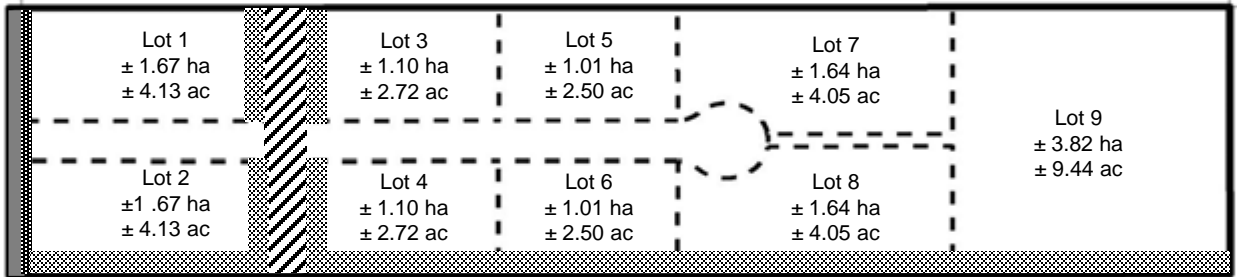
# LAND USE MAP

NW-29-23-28-W04M









**Subdivision Proposal:** To create two ± 1.67 hectare (± 4.13 acre) parcels two ± 1.10 hectare (± 2.72 acre) parcels , two ± 1.01 hectare (± 2.50 acre) parcels , two ± 1.64 ha (± 4.05 ac) parcels , with a ± 3.82 hectare (± 9.44 acre) remainder .



**Legend**

5.0 m Road Dedication (Plan of Survey)	
3.0 m Road Widening (Caveat)	
30.0 m Future Road Acquisition (Caveat)	
15.0 m Restrictive Covenant (Caveat)	

**Surveyor's Notes:**

1. Parcels must meet minimum size and setback requirements of Land Use Bylaw C-4841-97.
2. Refer to Notice of Transmittal for approval conditions related to this Tentative Plan.

**TENTATIVE PLAN**

**NW-29-23-28-W04M**





Contours are generated using 10m grid points, and depict general topographic features of the area. Detail accuracy at a local scale cannot be guaranteed. They are included for reference use only.

# TOPOGRAPHY

*Contour Interval 2 M*



**NW-29-23-28-W04M**



Note: Post processing of raw aerial photography may cause varying degrees of visual distortion at the local level.

**AIR PHOTO**

*Spring 2016*

**NW-29-23-28-W04M**



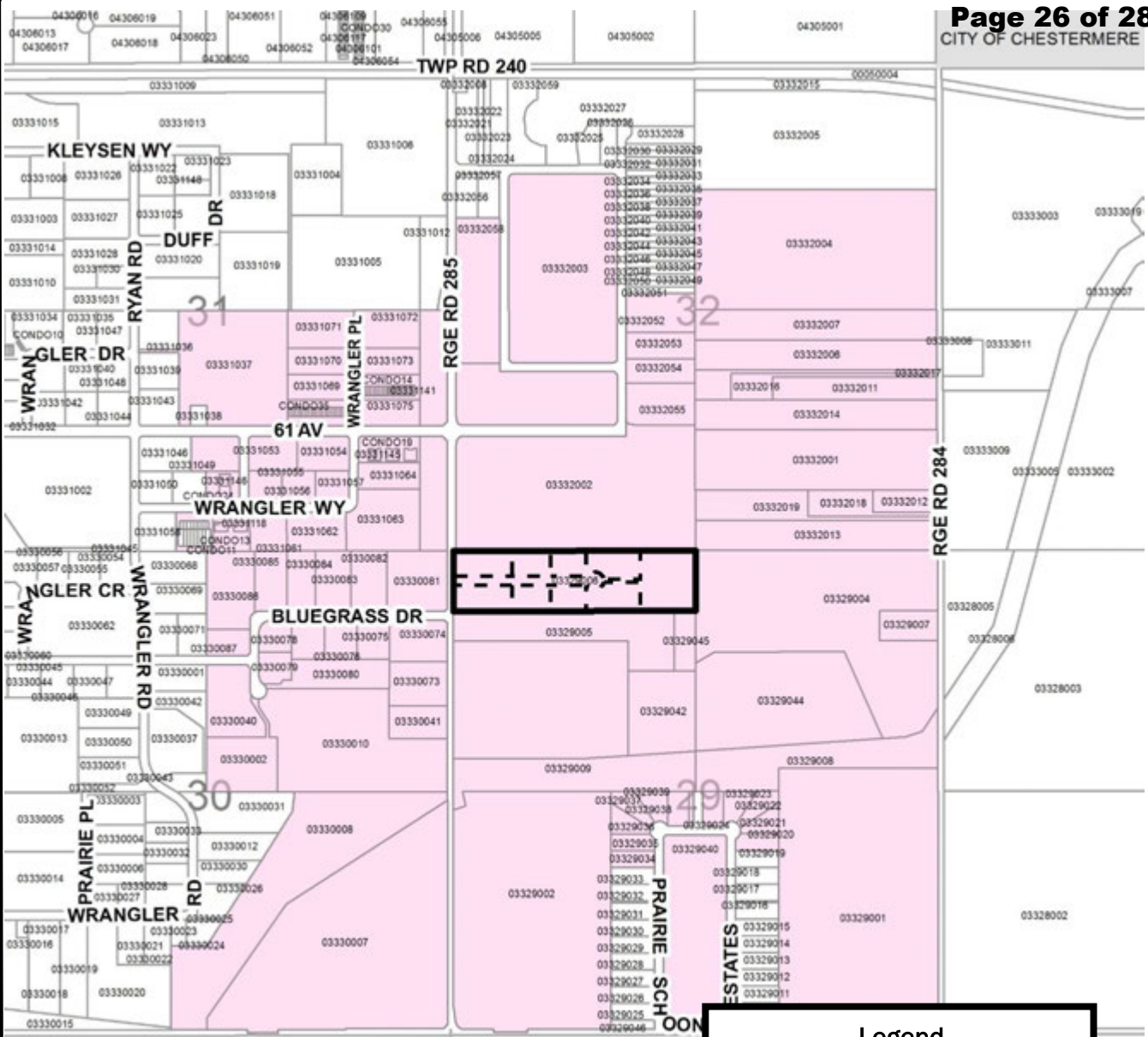
Date: June 25, 2018



Division # 5

File: 03329006

**AGENDA**

**Page 443 of 446**



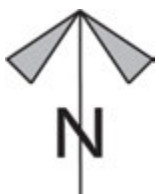
-  Letters in Opposition
-  Letters in Support

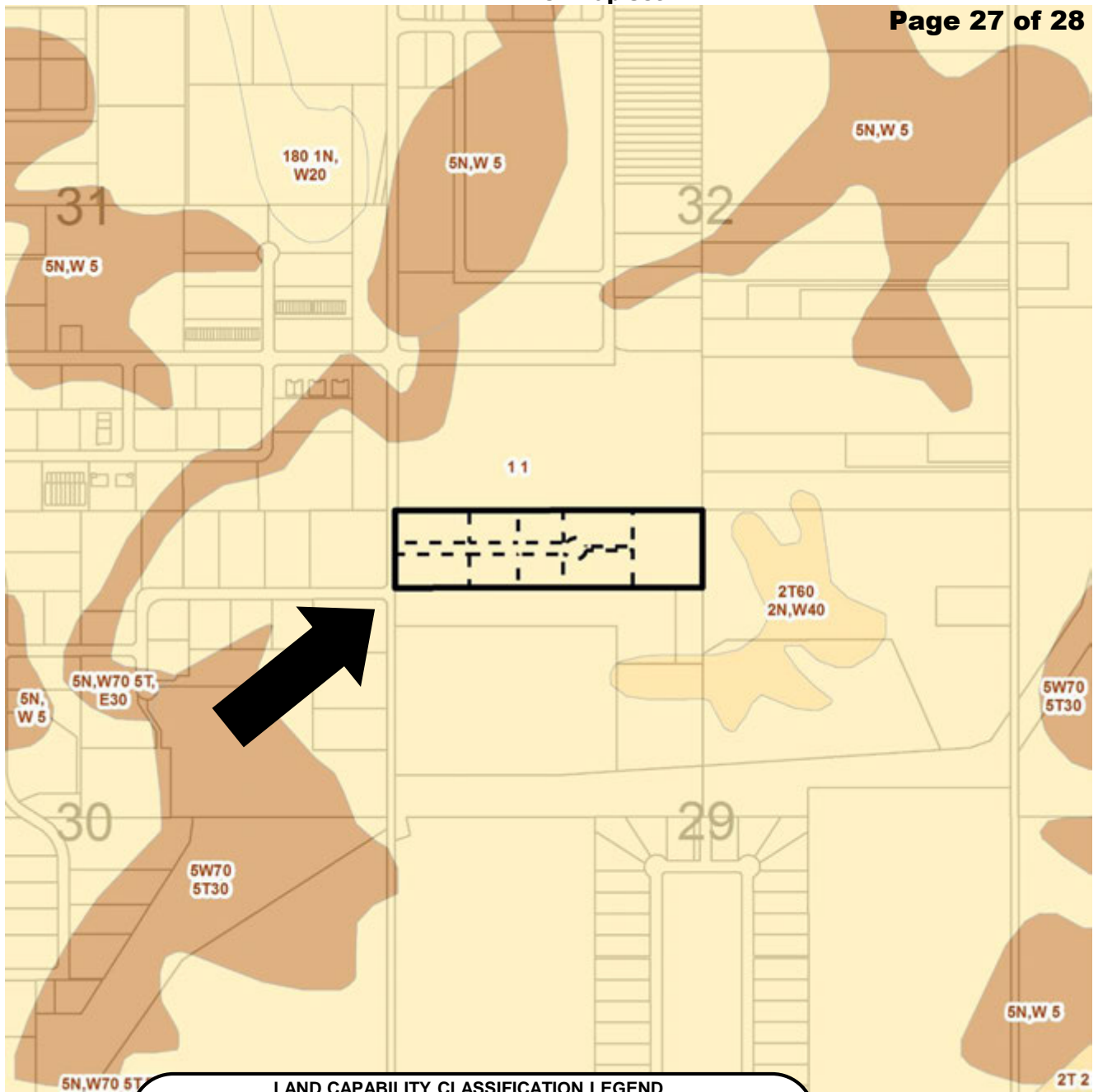
**Legend**

- Circulation Area
- Subject Lands

# LANDOWNER CIRCULATION AREA

## NW-29-23-28-W04M





**LAND CAPABILITY CLASSIFICATION LEGEND**

*Limitations refer to cereal, oilseeds and tame hay crops*

**CLI Class**

- 1 - No significant limitation
- 2 - Slight limitations
- 3 - Moderate limitations
- 4 - Severe limitations
- 5 - Very severe limitations
- 6 - Production is not feasible
- 7 - No capability

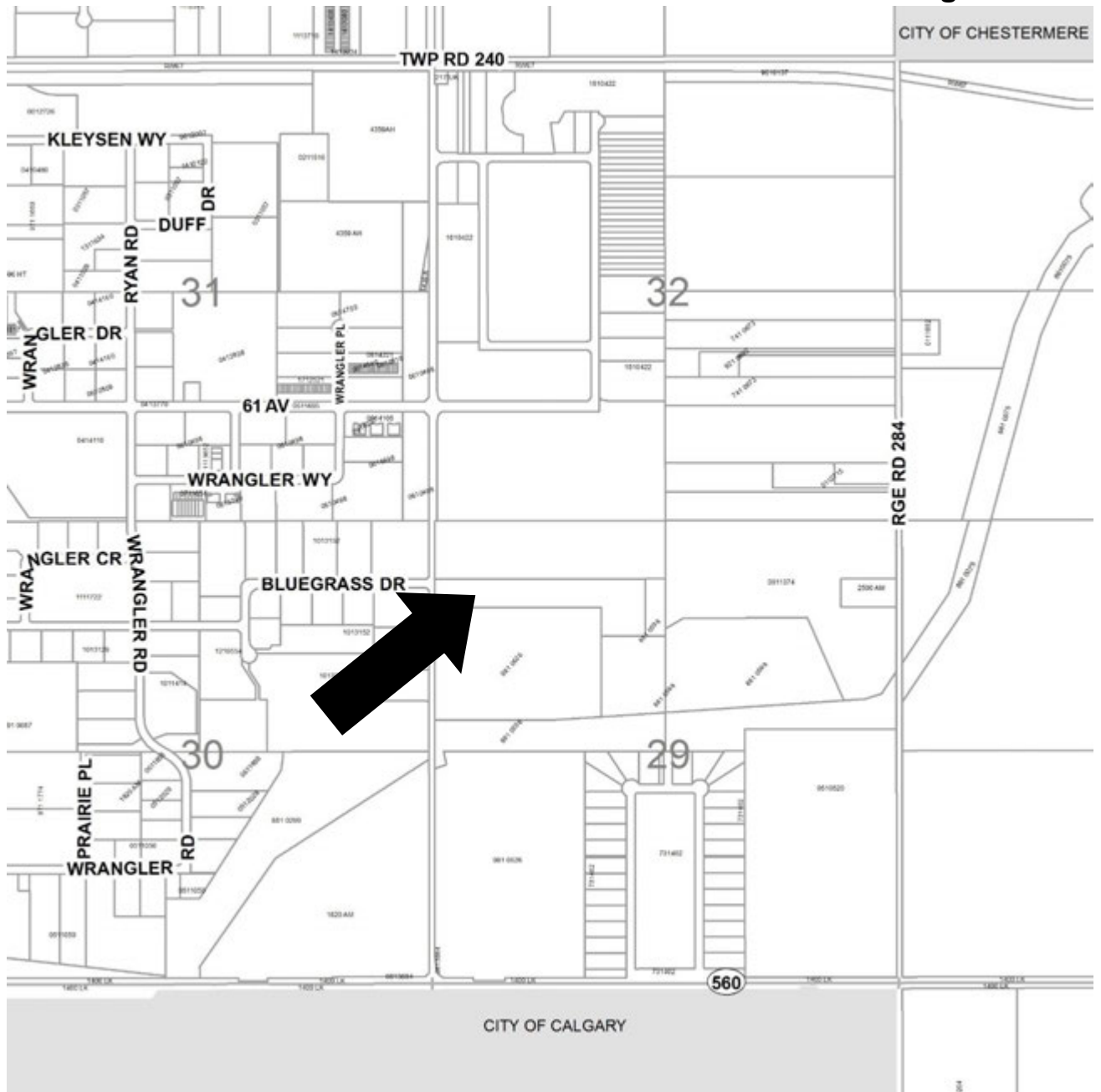
**Limitations**

- B - brush/tree cover
- C - climate
- D - low permeability
- E - erosion damage
- F - poor fertility
- G - Steep slopes
- H - temperature
- I - flooding
- J - field size/shape
- K - shallow profile development
- M - low moisture holding, adverse texture
- N - high salinity
- P - excessive surface stoniness
- R - shallowness to bedrock
- S - high sodicity
- T - adverse topography
- U - prior earth moving
- V - high acid content
- W - excessive wetness/poor drainage
- X - deep organic deposit
- Y - slowly permeable
- Z - relatively impermeable

**SOIL MAP**

**NW-29-23-28-W04M**





**Legend – Plan numbers**

- First two numbers of the Plan Number indicate the year of subdivision registration.
- Plan numbers that include letters were registered before 1973 and do not reference a year

# HISTORIC SUBDIVISION MAP

## NW-29-23-28-W04M

