



# **CROSSIRON MILLS EAST MASTER SITE DEVELOPMENT PLAN**

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293055 Crossiron Road  
East Balzac  
Rocky View County

**APRIL 2023**



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# PREAMBLE

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## PURPOSE

The CrossIron Mills East Master Site Development Plan (MSDP) has been prepared on behalf of Ivanhoe Cambridge, the owners of the property located at 293055 Crossiron Road in East Balzac, Rocky View County (Herein referred to as the Subject Property). As per the Balzac East Area Structure Plan (Bylaw C-5177-2000), this MSDP envisions the development of approximately 128 acres of land to establish a new employment district that provides a range of service commercial/retail, and industrial/warehousing land uses. This MSDP provides guidance and direction for future subdivision and development permit applications on the Subject Property.

Given that the Subject Property is to be further subdivided in the future, this document intends to provide direction and an evaluative framework which can be applied to development projects on the property. This document will be used by Rocky View County, Ivanhoe Cambridge, and future developers/landowners to guide development applications concerning land use, proposed placement of buildings, lighting, and to establish expectations for the architectural treatment and landscape design within the MSDP area.

This document will be accompanied a Land Use Bylaw Amendment, which will further inform the permitted range, type and scale of land uses and provide a regulatory framework for those uses.

## VISION + GUIDING OBJECTIVES

CrossIron Mills East will be developed as an inviting environment that seamlessly incorporates service commercial and retail uses in the West District and employment uses in the East District, with buildings of varying sizes to accommodate a mix of uses. The development will prioritize thoughtful urban design resulting in a seamless transition between the two districts. The built forms will be compatible, integrated and complementary to the surrounding land uses.

Key features such as accessibility, pedestrian connections/walkability, and landscaping/green spaces will be emphasized, creating a welcoming and inclusive environment that supports the present and future needs of the local economy.

Future development within the MSDP plan area will strive to achieve the following guiding objectives:

- Provide for a compatible transition in land use to act as a buffer between the existing uses located within the CrossIron Mills Shopping Centre and the proposed adjacent employment uses.
- Achieve an elevated architectural aesthetic along the westerly frontage sympathetic to and compatible with the surrounding land uses and built forms.
- Promote efficient site circulation, prioritizing the equitable distribution of retail/ service commercial and industrial traffic to minimize impacts to different land uses.
- Provide site design and configuration flexibility to efficiently respond to changing market demand.



## STUDY AREA DEFINED

The property is generally located at the northeast corner of Crossiron Boulevard and Crossiron Drive. Crossiron Road bounds it to the north, Crossiron Boulevard to the west, the Century Downs racetrack to the east, and Crossiron Drive to the south. The area of the property is approximately 128 acres, and it has frontage on Crossiron Mills Boulevard, Crossiron Drive and Crossiron Road.

Given the large size of the Subject Property, the legal limits form a logical boundary for the defined study area for this Master Site Development Plan. The built-up nature of the lands surrounding the subject property eliminates the need to include portions of the adjacent lands within the defined study area.

The Study Area is illustrated in **Figure 1: Study Area**.

For clarity, we note that Sub-Cell A-3, as identified on Schedule B of Direct Control Bylaw C-6234-2006, is not included in the study area and is not affected by this MSDP or the proposed Land Use Bylaw Amendment. Lands located within Sub-Cell A3 are governed by the Crossiron Common Master Site Development Plan Sub-Cell A3 MSDP which was approved in early 2012.

Legend  
 Site Boundary

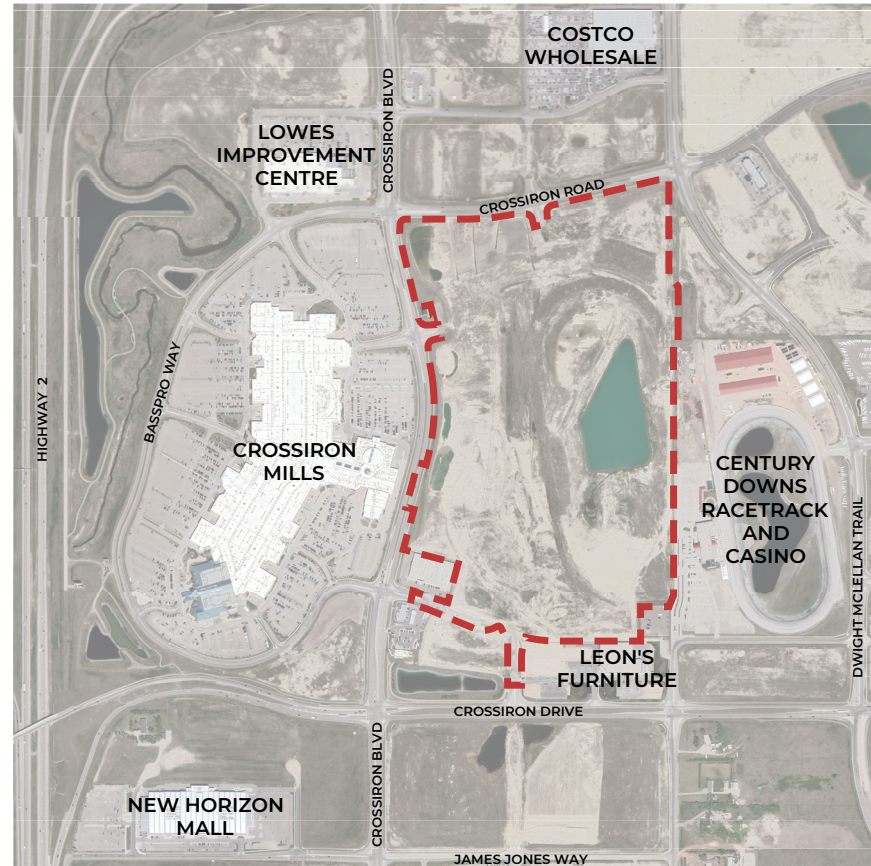


FIGURE 1 - STUDY AREA

# MASTER DEVELOPMENT CONCEPT

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## MASTER PLAN

The Master Plan proposes the development of the Subject Property with two master-planned commercial/retail and employment districts. These districts will leverage the success of the CrossIron Mills Shopping Centre to further diversify the range of service commercial and light employment uses that exist within East Balzac.

The development has been divided into East and West Districts to recognize the two distinct interfaces with the adjacent parcels. The East District is adjacent to the Century Downs Racetrack and Casino, and nearby industrial land uses. The West District is adjacent to the CrossIron Mills Shopping Centre, a vibrant regional shopping centre and a significant draw for shoppers from the County and the City of Calgary. These unique interfaces warrant differing approaches to urban design and site configuration.

The West District is comprised of seven conceptual development parcels ranging in area from approximately 3.0 acres to approximately 5.0 acres. The ultimate size and number of development parcels within the West District will be determined through future subdivision and Development Permit application/s. The West District is planned to be developed with a range of service commercial, retail, and light employment uses. A wide range of building areas are permitted within the West District with maximum building areas being regulated by the proposed zoning regulations. Building setbacks, lot coverage, building height, parking and landscape requirements within the West District will be regulated by the proposed zoning regulations. Permitted and discretionary land uses will be regulated in the proposed zoning regulations.



## LANDSCAPE DESIGN VISION

The landscape design vision seeks to unite the site and complement the character of the proposed and exiting architecture to create an aesthetically pleasing landscape using native and low water-use plant material to replicate a natural prairie ecosystem and promote long-term sustainability. The overall design intent is to create a complimentary landscape consistent with adjacent developments and site interfaces. The landscape will provide a balance of both form and function, offering site users a variety of open space experiences between and adjacent to roads, parking areas, and building structures.

The following content describes the landscape vision in greater detail and outlines requirements associated with design and installation of the landscape. Landscape requirements shown within this document will be supplementary to the requirements within the 'Balzac East Area Structure Plan (BEASP)' and the 'Land Use Bylaw'. Note that the BEASP takes precedence over the Land Use Bylaw. Links to each one of these documents are provided in Appendix B to this MSDP.

# LANDSCAPE DESIGN PRINCIPLES

## INVITING

- A variety of landscape treatments and planting enhancements will be incorporated into main entrances, key pedestrian linkages, and other focal points to enhance the overall aesthetic quality.
- Ornamental planting will be located at the access points of pedestrian spaces to encourage the use of these links.
- Plant materials will be clustered and massed to provide greater impact and add layers of color, texture and aesthetic character to the space.
- A hierarchy of scale and layering strategy will be utilized in the planting design to provide interest and variety in the landscape.

## SUSTAINABLE

- Native / drought tolerant, prairie-adapted, and low water-use species will be utilized throughout the site to promote sustainability and reduce requirements for long-term maintenance of the site.
- Water sensitive design principles will be incorporated to manage stormwater. Peak flows will be mitigated through soil infiltration prior to discharging run-off.
- Adequate growing space and topsoil volume for plants will be accommodated to promote long-term growth of the landscape.

## **FOUR-SEASON**

- The design shall incorporate a variety of deciduous trees and shrubs that flower at various times throughout spring and summer and have colourful fall foliage and aesthetic appeal in winter.
- Evergreen tree and shrub species will provide balance throughout the year and aesthetic interest in the winter months.
- Landscape areas will be designed to accommodate snow storage where possible.

## **PEDESTRIAN AND BICYCLE-FRIENDLY**

- Trees and shrubs will provide buffers to the street and offer safer pedestrian connections.
- Site furnishings will be consistent to create a common design aesthetic that is recognizable and user friendly.

## **FUNCTIONAL**

- Landscape components shall be used for both visual and sound buffering as required throughout the development.
- Planting beds will be strategically located and configured to define spatial boundaries where appropriate.
- Trees, taller shrubs, and perennials may be utilized to frame views through the site.
- Foundation planting principles will be applied to integrate the overall built form of the site and soften the building structures.





A conceptual landscape scheme has been prepared and is illustrated in **Figure 3: Conceptual Landscape Plan**.

FIGURE 3 - CONCEPTUAL LANDSCAPE PLAN

## GATEWAYS AND SITE ENTRYWAYS

Access to the site will not only be from Crossiron Boulevard but will also be accommodated from the north at Crossiron Road and from the south at Crossiron Drive. A hierarchy of entry signage may be provided at one or more of these locations to support wayfinding and promote project identity. Scale of the signage may vary; however, all entry signage shall have a consistent and recognizable aesthetic achieved through similar form, materials, colour, and textures.

Mass plantings of native and drought tolerant shrubs, grasses and perennials will be used around the entry signage to create balance, complement the built-form, anchor the structure, and establish a relationship to the overall site.

The conceptual location of entry signage locations is illustrated in **Figure 4: Potential Entry Feature Signage Locations**.

### LEGEND



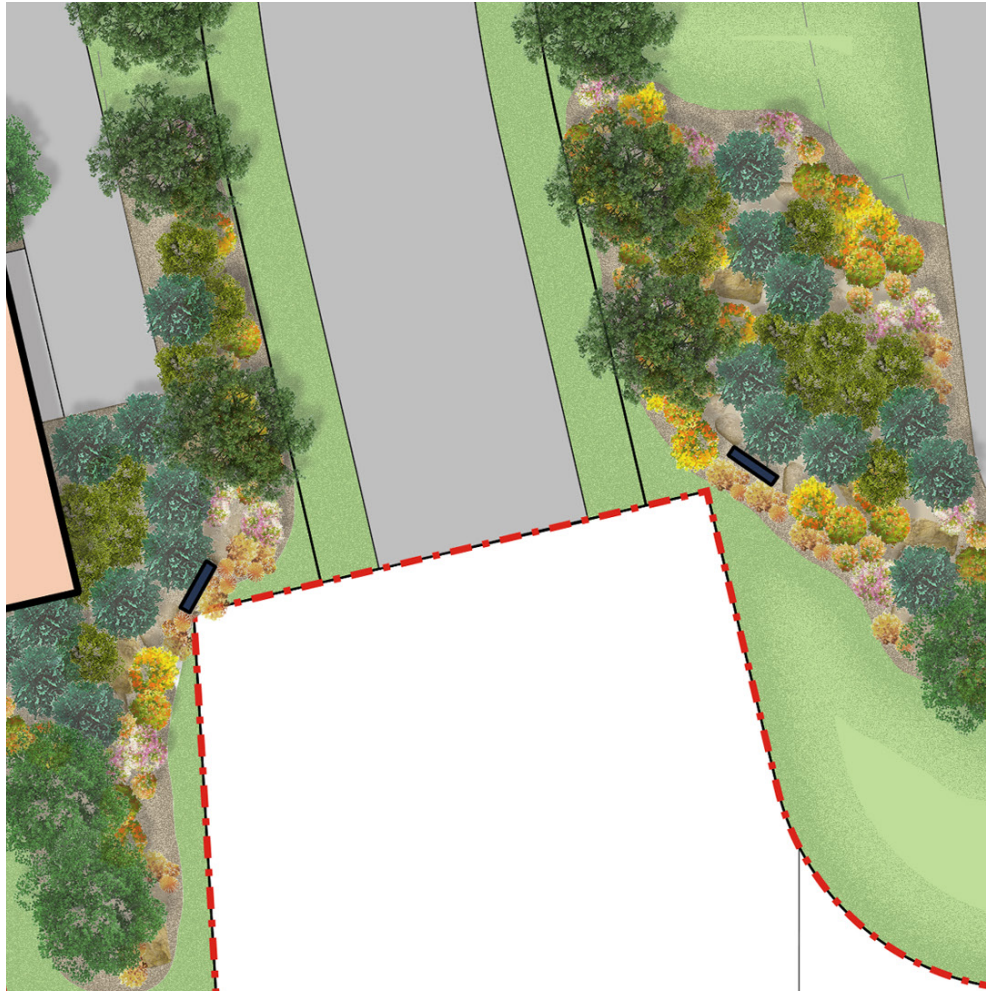
POTENTIAL PRIMARY  
ENTRY FEATURE LOCATIONS



POTENTIAL SECONDARY  
ENTRY FEATURE LOCATIONS



FIGURE 4 - POTENTIAL ENTRY FEATURE SIGNAGE LOCATIONS



A detailed entry feature has been prepared and is illustrated in **Figure 5: Entry Feature Detail**



FIGURE 5 - ENTRY FEATURE DETAIL



## STREETSCAPE

The interface of public boulevards with the site helps introduce the natural landscape character throughout and presents a rhythm that helps set the tone for the user experience. Crossiron Boulevard and Crossiron Road, on the west and north perimeters of the site respectively, will be anchored with organized groupings of street canopy trees, presenting a consistent aesthetic with the landscape treatment associated with existing adjacent development.

The central north-south collector through the site will follow a similar approach, utilizing a single row pattern of street trees to create a design hierarchy and help define the corridor. The trees along the boulevards will be the defining landscape element as the remainder of the landscape footprint in these areas will take a more simplistic approach with standard sod. A typical streetscape treatment is illustrated in **Figure 6: Typical Streetscape Treatment**.

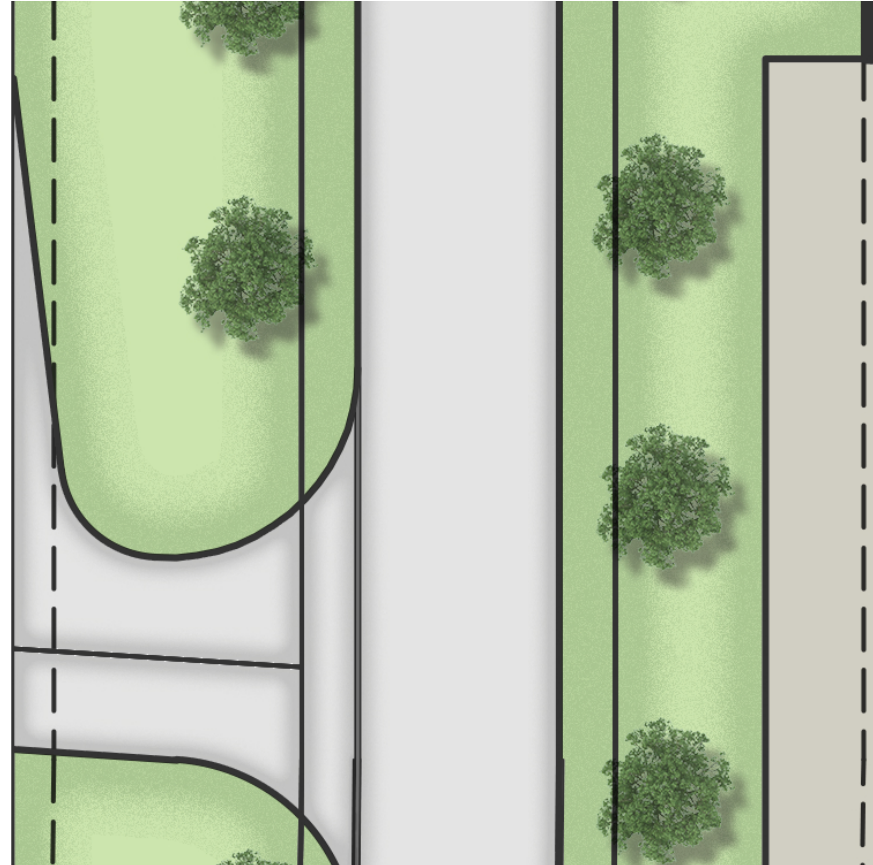


FIGURE 6 - TYPICAL STREETSCAPE TREATMENT

## STORMWATER MANAGEMENT POND

The ponds are integrated into the open space design, serving a predominantly functional purpose as stormwater management facilities, however, they also provide a different natural amenity and aesthetic with the addition of water to the landscape. This presents opportunities for the strategic use of plant material to achieve the following:

- Enhance the overall site landscape quality
- Soften and naturalize pond edges
- Promote biodiversity
- Create habitat in conjunction with the water body

An illustration of a typical pond/landscape interface is illustrated in **Figure 7: Typical Pond/Landscape Interface**

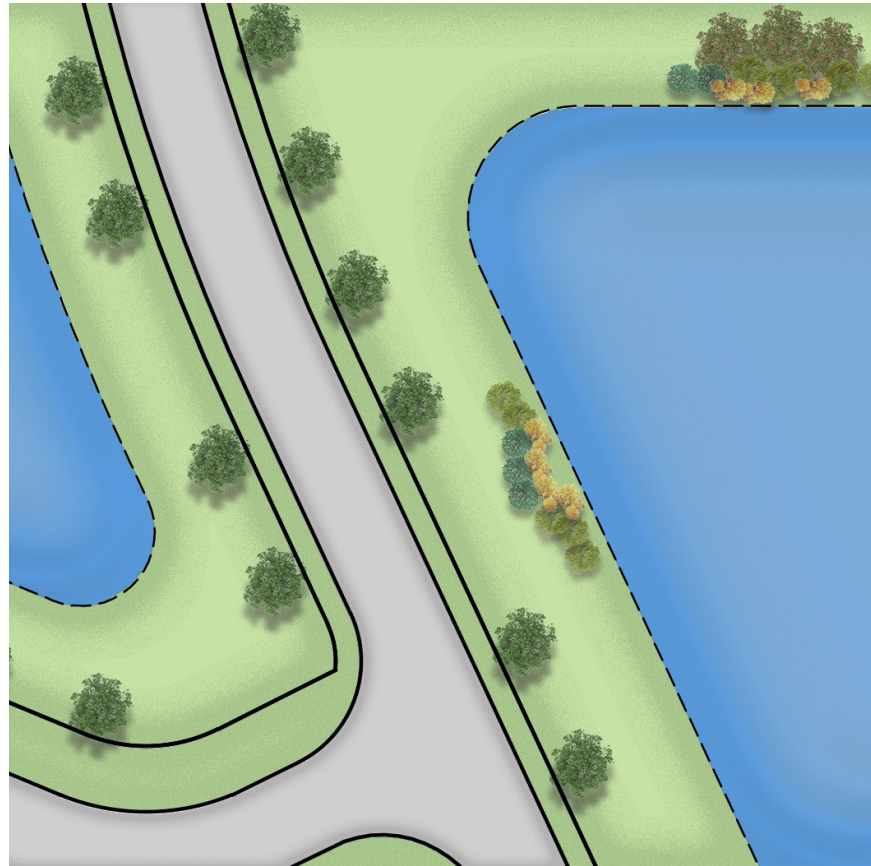


FIGURE 7 - TYPICAL POND/LANDSCAPE INTERFACE

# LANDSCAPE ZONES

The overall development site incorporates a number of different landscape zones that have different approaches to design and address form and function.

**Zone 1 – Low Perimeter:** these areas are adjacent to major internal roads and help provide physical separation between the public roadway and private interface. The intent is to allow viewing to and from the site while providing screening to adjacent parked vehicles. Views in these areas are deemed integral to the site.

**Zone 2 – Tall Perimeter:** these areas are adjacent to major internal roads and provide an alternative to 'Zone 1'. Where possible the intent in this area is to use a single row of deciduous trees to create harmony and a rhythm along the streetscape. In these areas, the views to and from the site are less important than in 'Zone 1'.

**Zone 3 – Landscape Strips / Parking Lots:** landscape strips are oriented perpendicular to parking stalls and are used to subdivide larger parking lots, where appropriate for pedestrian connections to buildings. It is preferred to integrate fewer wider strips than frequent narrow strips for connections. This minimum strip shall be 4.0 meters wide with a landscape strip provided every four (4) rows of vehicle parking with no more than four (4) rows between strips. Row of parking is counted as each row of vehicle parking. These landscape strips could be considered as opportunities to implement bio-swales – planted areas designed to collect storm water run-off from the adjacent hard surfaces.



**Zone 4 – Parking Islands / General:** where possible parking lot islands are to be consolidated from several narrow (2.0 meters width or less) islands into larger islands. This will allow for better survival of plant material due to larger growing medium quantities and help where irrigation is not utilized or limited. Larger islands have been shown to be more viable and provide a greater visual aesthetic within larger parking lots. Diverting storm water run-off from adjacent hard surfaces could be considered in these locations.

**Zone 5 – Parking Islands / Narrow:** these small islands (2.0 meters width or less) are not viable for soft landscape installation. Where possible use boulders, cobbles, stones, gravels, and other decorative hardscapes. Island enhancements with light poles and site-themed features may be used to enrich entrance to buildings. A maximum of thirty percent (30%) of the landscaping applied to parking islands, boulevards and sidewalks, but not parking surfaces, may be in the form of hard landscaping.

An illustration of a typical landscaped area around a parking lot is shown in **Figure 8: Typical Parking Lot Landscape**.



FIGURE 8 - TYPICAL PARKING LOT LANDSCAPE

## LANDSCAPE GUIDELINES

For the purpose of this development, landscaping is required to comply with the landscaping requirements contained within the Land Use By-law. In addition, regard must be had for the landscaping guidelines found in the BEASP and the historic land use By-law.

## RECOMMENDED PLANTING LIST

Future Development Permit applications must consider the use of the identified vegetation in proposed landscape plans. A Recommended Planting List is provided in **Appendix A** to this MSDP.

## TRANSPORTATION AND SITE CIRCULATION

The development is planned around a meandering spine road oriented in a north-south direction. The spine road connects to existing site accesses at Crossiron Drive and Crossiron Road. Two east-west secondary accesses are located along Crossiron Boulevard at Bass Pro Way to the South and at the extension of the mall entrance driveway to the North.

The intended spine road will cater predominantly to heavier traffic associated with the proposed industrial uses, whereas the proposed service commercial uses will be accessed primarily through Crossiron Boulevard and the two east-west connections. It is expected that mutual access agreements will be used to increase circulation between adjacent properties and allow greater circulation efficiency.

Pedestrian site circulation throughout the development is achieved by way of a sidewalk system within the proposed rights of way. The sidewalk system provides convenient links to the adjacent roads and existing sidewalk network at the site's periphery. This system provides for connectivity throughout the entire development.

# ARCHITECTURAL CHARACTER

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## ARCHITECTURAL CONTROL GUIDELINES

Ivanhoe Cambridge, as the developer and owner of significant adjacent landholdings, has a real and vested interest in ensuring that these lands develop according to the overall vision that has been established for the CrossIron Mills East property. To ensure this vision is achieved, Architectural Control Guidelines have been developed to establish a set of minimum requirements for enhancing the development's overall architectural theme and visual appeal. The intent of these Architectural Control Guidelines is to ensure highly uniform construction standards, architectural designs and distinctive built forms are implemented throughout the plan area. Additionally, it is the intent that these Guidelines promote a sense of harmony and design continuity throughout the development.

It should be noted that Ivanhoe Cambridge and Rocky View County have a common interest in achieving high-quality development on this property. In this regard, the Architectural Control Guidelines will be registered on title for the subject property. They will allow for all development proposals to be thoroughly reviewed and approved by Ivanhoe Cambridge prior to the submission of development permit applications to Rocky View County. Through the registration of the Architectural Control Guidelines on title, future purchasers/developers of lands within the plan area will be made fully aware of the requirement for Architectural Control prior to the acquisition of any parcel within the plan area.

## WEST DISTRICT

The West District will offer retail, office, and commercial uses in buildings of varying sizes and designs, occupied by either single or multiple tenants. The district should concentrate on creating visual interest and enhancing the environment at a pedestrian scale.

Buildings in the West District shall provide a high standard of design and landscaping to enhance the appearance of the development. Visual appeal can be achieved by breaking up the facade through changes in plane, reveals, or decorative elements. Large expanses of blank, flat walls facing the street are not desirable. The district should incorporate pedestrian-oriented design, including sidewalks, canopies, glazing and varied building materials to create a pleasant pedestrian experience. Buildings shall also provide street-facing entrances in addition to parking lot-facing entrances.

The building design shall incorporate a generous amount of glazing to promote transparency and permeability from the public realm. This will help establish a comfortable, continuous public realm between entrances, providing pedestrians with safe and convenient connections between properties.

## EAST DISTRICT

The East District will offer a more prominent building footprint with multi-tenant opportunities.

It is important to note that the parking areas should not detract from the overall architecture of the buildings. To integrate the parking areas on site, appropriate landscaping should be utilized to break up the asphalt's expanse and facilitate low-impact development.

Enhancing the pedestrian experience shall be at the forefront when designing the Western faces of the building, including a landscaped buffer for parking, and loading between the built form.

## MASSING

The built form shall enhance the surrounding public realm by utilizing durable and authentic materials and incorporating designs that balance environmental performance, aesthetic excellence and user functionality to contribute to a built environment that attracts employers, shoppers and visitors.

The design of buildings should minimize their impacts on view corridors and natural systems and provide a sense of scale relatable to the street.

Buildings should effectively frame the public realm, aligning with the form of the adjacent street to create a comfortable pedestrian environment.



**PRECEDENT IMAGE: EAST  
DISTRICT BUILDING CONCEPTS**



**PRECEDENT IMAGE: COLONY  
COMMERCE BUSINESS PARK**





**PRECEDENT IMAGE: ARTICULATION  
AND FAÇADE TREATMENTS**

## ARTICULATION & FAÇADE TREATMENT

The architectural treatment of building elevations shall break up the building mass. The treatments may include building articulation, horizontal and vertical joints, various building heights, construction materials, and incorporate landscape features.

Building designs should take the form of a finely grained, repeating façade element that balances a uniform visual identity with subtle variances in expression. To achieve and promote an attractive human-scaled design, the built form should include regular entrances, windows, and changes in colour and material.

Building elevations shall include façade design articulation to enhance the exterior face and structure corners to be articulated with changes in height, massing, or material. The primary entrances shall be oriented towards public streets and incorporate design elements highlighting entry points. Additionally, all building entrances shall be elevated as close to the street level as possible, reducing or eliminating the need for stair/ramped approaches for a more accessible design.



## PARKING AND LOADING AREA REQUIREMENTS

Surface parking areas shall be provided with screening to minimize visual impacts on public streets. This will be achieved through the utilization of the following:

- Vegetation and landscaping features located between surface parking areas and a public right of way;
- Fencing and/or low architectural walls where appropriate.

Importance will be placed on screening loading areas and outdoor storage of trailers. Additional attention shall be paid to orienting these areas to avoid or minimize high visibility exposures.

Loading facilities are encouraged to be located on the sides or rear of buildings, and when facing a road, they must be appropriately screened with either landscaping or screen walls in accordance with the architectural treatment of the adjacent building elevation.

Outdoor display areas may be permitted in compliance with the County's Land Use Bylaw if they are appropriately sited and landscaped.

# SERVICING SUMMARY

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## STORMWATER MANAGEMENT

A Master Drainage Plan (MDP) has previously been established for the entire Study Area, including the CrossIron Mills East lands which are located within portions of catchments of D-A1, D-A3 and D-A4. To revise the stormwater management strategy within the select catchments falling over the CIM East lands, a new stormwater pond will be required to create a flow discharge in alignment with the Approved Master Drainage Plan.

The stormwater management strategy is proposing 20.9 ha (within D-A4 considered to be zero discharge in MDP) to be managed by the new storm pond with a volume discharge target of 25mm as per the Nose Creek Watershed Partnership bulletin interim recommendation.

The average Volume Discharge Target for the CrossIron Mills East MSDP area is 62mm, which is blended from 90mm volume discharge target areas (Catchments D-A1 & D-A3) and 25mm volume discharge areas (D-A4).

The Maximum Pond Discharge Rate is 60.6 l/s (48.2 ha\*1.257 l/s) based on the 48.2 ha site area.

The new proposed stormwater management pond facility is currently based on discharging to Pond 3; however, the proposed stormwater management pond could also discharge to Pond 1. However, Pond 2 downstream of Pond 3 has been found in the approved MDP notes to have utilized the equivalent discharge from catchment D-A4=45.25 ha; which would not allow any release from catchment D-A4 as the area release for this catchment D-A4 was allocated elsewhere in the Approved MDP. However, the new proposed pond facility has adequate storage volume to manage stormwater from the CrossIron Mills East lands.

Stormwater management ponds facilities will be landscaped with appropriate fringe planting/vegetation with informal character as outlined in the previously approved comprehensive landscape strategy. Stormwater management for the individual sites/parcels will conform to release rates and impervious ratios required by the new proposed pond facility.

## WATER SERVICE

Existing Development adjacent to the Subject Property has installed the watermain under the roadways of Crossiron Blvd. Crossiron Road and Crossiron Lane on the perimeter of the Crossiron Mills East Lands. Water Servicing will be extended from the adjacent roadways through the Crossiron Mills East lands within public easements under private roadways (where required).

## STORM SERVICE

Each development parcel will be provided with a storm main to connect into the proposed Crossiron Mills East stormwater management pond that is located upstream of existing Pond 3. Each development parcel will have a stormwater release rate and stormwater design parameters to support private development.

## SHALLOW UTILITIES

Utilities such as gas meters, pad mounted transformers and other physical elements affecting Urban Design shall be appropriately screened from the street with various landscaping elements that are identified in this MSDP. Overhead power lines may be permitted within the municipal road allowance, however all power lines servicing individual development parcels will be buried underground.

## FIRE SERVICE

Fire protection in the plan area is to be provided by way of fire hydrants located throughout the plan area in accordance with County standards for fire protection. Existing Development adjacent to the Subject Property has installed the watermain under the roadways of Crossiron Blvd. Crossiron Road and Crossiron Lane that provide fire protection for the area. Roadways within the Plan Area have been designed to meet the minimum required turning radii for emergency vehicles.

## WASTE COLLECTION & STORAGE

Waste collection within this development will be the responsibility of individual property owners through private waste collection services.

Waste storage facilities and garbage enclosures shall meet the following design and locational criteria:

- Shall not be located within the front yard or between a building and a lot line abutting a street on a corner lot;
- Shall not be visible from a public street;
- Shall be designed with a solid opaque wall or fence with a gate that screens garbage containment;

Waste storage facilities and garbage containers shall be located to optimize collection vehicle access and minimize site circulation impacts.

## TRANSIT

At present, Rocky View County lacks a formal transit system. However, transit connections are available to the City of Calgary transit system as well as other chartered bus services for commuters.

Allowances have been included in the proposed right of way network for the provision of bus laybys and/or bus stops to accommodate future public transit to the area when it becomes available.

# REGULATORY FRAMEWORK

## **SOUTH SASKATCHEWAN REGIONAL PLAN (SSRP)**

The SSRP is a Provincial policy document that establishes a long-term vision for the South Saskatchewan Region intended to align provincial policies at the regional level to balance Alberta's economic, environmental, and social goals. The regional plan also includes strategies for responsible energy development, sustainable farming and ranching, recreation, forest management, and nature-based tourism.

The subject site is located within the "White Area" area identified in Schedule C: South Saskatchewan Regional Plan Map in the SSRP. While the SSRP makes no specific reference to the subject property, based on our review of this policy document, the future development of the subject property for urban land uses would be consistent with the policies contained in the SSRP regarding Land Use Patterns contained within the Implementation Plan section of the policy document.

## **ROCKY VIEW / CALGARY INTERMUNICIPAL DEVELOPMENT PLAN (IDP)**

The IDP is a statutory planning document intended to coordinate intermunicipal planning matters to ensure continued and meaningful communication between the City of Calgary and Rocky View County. The IDP contains circulation and referral sections requiring all development and planning proposals within the Plan area to be circulated per the current IDP policies. Future development applications pertaining to the subject property are required to be circulated to the City of Calgary for review and commentary.

## ROCKY VIEW COUNTY PLAN

This MSDP has been prepared in accordance with the County Plan's policy direction regarding Business Development and the specific policies relating to development that are established in the East Balzac Area Structure Plan as well as Direct Control District Bylaw 109 (DC 109).

The subject property is identified as being within a Regional Business Centre, as shown on Map 1 – Managing Growth of the County Plan. In Section 14 of the County Plan, Regional business centres are described as large areas of commercial and industrial development within the County and serve the purpose of a regional business centre which provides regional and national business services, and local and regional employment opportunities.

Section 14.6 of the County Plan directs that Business development shall address the development review criteria identified in Section 29 of the County Plan. Section 29 of the County Plan identifies Technical Requirements and Submissions and provides direction for the requirement of Master Site Development Plan Submissions under the specified submission requirements.

Section 29.7 of the County Plan directs that a master site development plan should address all matters identified in Appendix C, sections 1 and 3. The development concept proposed in this MSDP has been developed in general accordance with the policy direction in Sections 1 and 3 of Appendix C of the County Plan. This development concept will be refined through future development permit applications for individual parcels.

## EAST BALZAC AREA STRUCTURE PLAN, 2000

The Subject Property is identified as being located within the North Business Node, as shown in Figure 2: Conceptual Vision of the Balzac East Area Structure Plan (BEASP). The BEASP establishes a future land use concept for this parcel which provides a range of uses that may include a wide variety of retail, service, entertainment, business, hospitality, office, education, government, and other uses.

The North Business Node is subdivided into four cells (A through D), with the Subject Property situated within Cell A. This Cell has been planned as one integrated unit to provide various business, commercial, service, hospitality, office and entertainment uses. As per the policy direction in the BEASP, a Master Site Development Plan shall be prepared prior to submission of any development permit application for structures and shall address the location of all major buildings, free-standing signs, parking and loading areas, roads and access points, landscaping, entrances and entry treatments, sensitive and complementary architectural treatment, and internal and regional pathways.

Section 4.4. of the BEASP provides Performance Standards and Development Guidelines for Business Uses. Section 4.4.3. provides specific Development Guidelines intended to be implemented through the development approval process. Future development within the MSDP area is to be designed per the Development Guidelines established in Section 4.4.3 of the BEASP.

## DIRECT CONTROL DISTRICT – 109 (DC 109)

The plan area is located on lands designated Direct Control District-109 (DC-109). The Purpose of DC-109 is to facilitate the development of portions of the Balzac East Area Structure Plan for the purpose of establishing a Super-Regional Shopping Centre and Racing Entertainment Centre. The Subject Property is identified on Schedule B to DC-109 as being located within Sub-Cells A-5 and A-6.

Permitted uses within Sub-Cell A-6 relate solely to a Racing Entertainment Centre. Permitted uses within Sub-Cell A-5 include the following uses:

- Accessory Buildings
- Accessory Uses
- Agricultural Tourism Uses
- Athletic and Recreation Facilities
- Automobile Rental Store
- Banks or Financial Institutions
- Boat Storage
- Brew-pub
- Child Care Facilities
- Drinking Establishment
- General Agriculture
- General Industry Type 1
- Grocery stores
- Health Care Services
- Hotels, Motels
- Laboratories
- Liquor sales
- Offices
- Personal Service Businesses
- Private Clubs
- Temporary Construction Facility
- Public or Quasi-Public Buildings
- Religious Assembly
- Restaurants
- Retail Food Store
- Retail Stores
- Schools Private
- Seasonal Sales of Vegetation
- Service Stations
- Shopping Centre
- Signs



# PHASING

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## PHASING STRATEGY

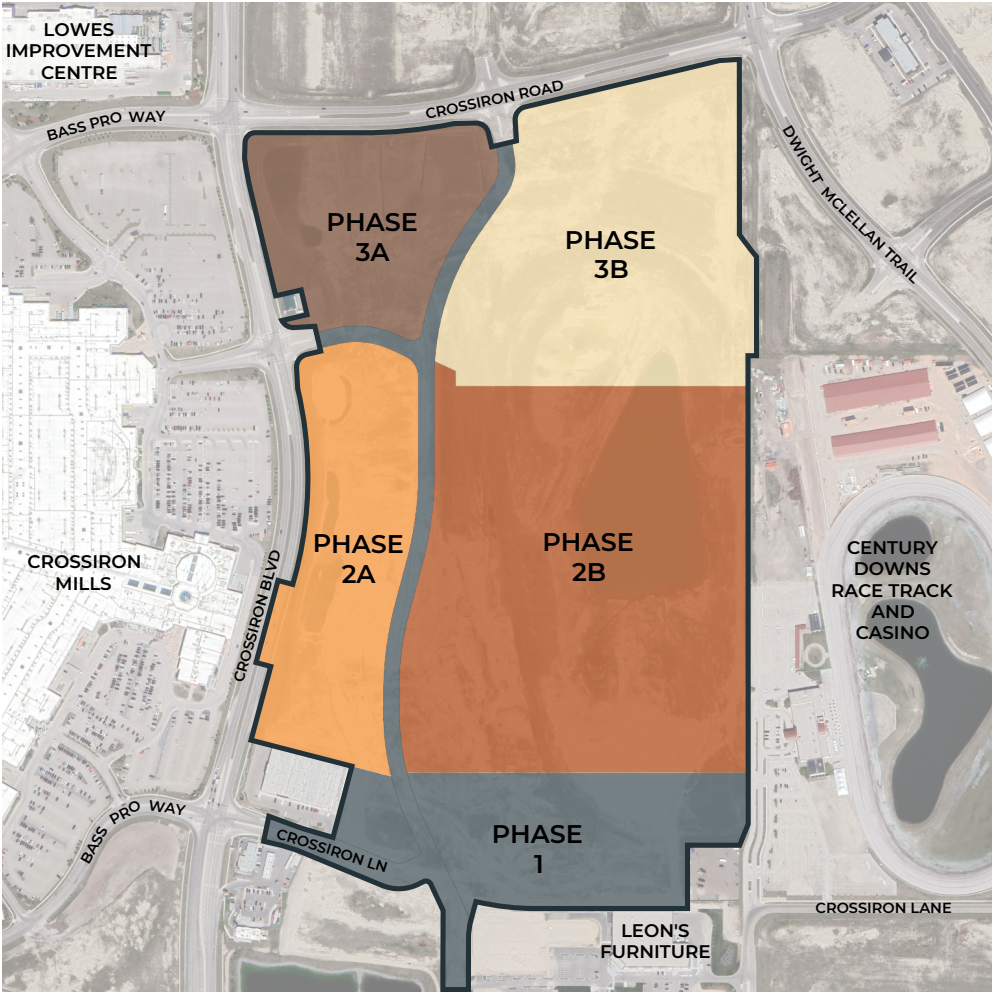
Development phasing within the CrossIron Mills East Master Site Development Plan will generally proceed in a northerly progression. The first development phase will be focused on the southern portion of the Plan area and will likely include the stormwater management facility and the internal road network. As development in Phase 1 progresses, the development will proceed northward into Phase 2 and lastly into Phase 3. Although it is intended that development phasing is to occur sequentially, ultimate development phasing will be determined by market demand.

## MARKET TIMING

Development opportunities within the Plan area are intended to be positioned for market shortly after the required Municipal approvals for MSDP and Subdivision have been secured. Depending on market demand, Ivanhoe Cambridge may market development parcels for sale or may choose to proceed with building construction on individual parcel.

Legend

— Site Boundary



A conceptual phasing plan is included in **Figure 9 – Conceptual Development Phasing**.

FIGURE 9 - CONCEPTUAL DEVELOPMENT PHASING

# IMPLEMENTATION

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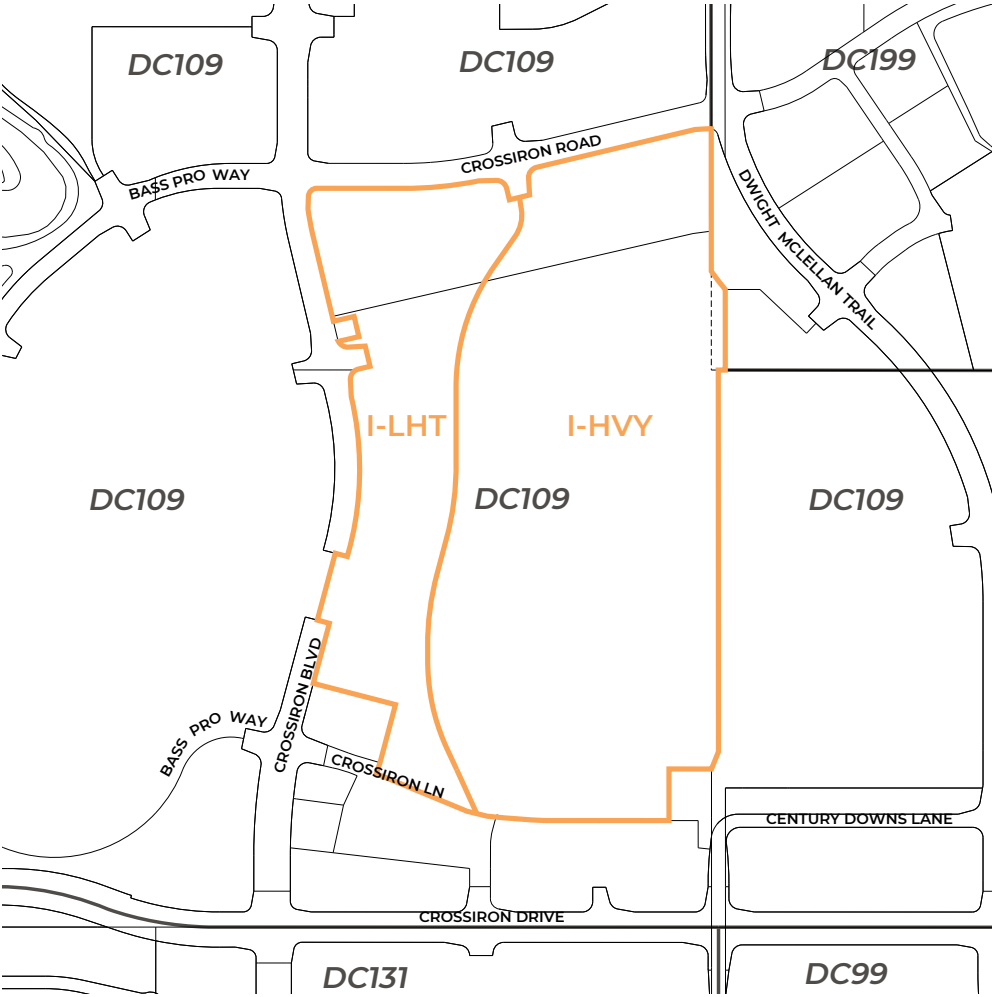
## PROPOSED ZONING REGULATIONS

The Master Site Development Plan will be implemented through an accompanying amendment to the existing Direct Control By-law C-6234-2006 (DC-109). The proposed amendment will remove all portions of the Subject Property that are located within Sub-Cell A-5 and Sub-Cell A-6 from Direct Control By-law C-6234-2006 (DC-109) in their entirety. Figure 11: Proposed Land Use Districts below provides an illustration of the proposed land use districts to be applied to the Subject Property.

Portions of the Subject Property which are located within the West District, those lands lying on the west side of the proposed north/south internal roadway, are proposed to be rezoned to a I-LHT Industrial, Light District land use district under Rocky View County Land Use Bylaw C-8000-2020.

Legend

- Proposed Land Use Redesignation Boundary
- Existing Land Use Boundary
- I-LHT Proposed Land Use
- DC109 Existing Land Use



The proposed land use Districts are shown in **Figure 10 - Proposed Land Use Districts.**

FIGURE 10 - PROPOSED LAND USE DISTRICTS

The following uses are Permitted Uses within the I-LHT Industrial, Light District land use district (Section 439 of Rocky View County Land Use Bylaw C-8000-2020):

- Accessory Building ≤ 500 m<sup>2</sup>
- Automotive Services (Minor)
- Communications Facility (Type B)
- Animal Health (Inclusive)
- Care Facility (Clinic)
- Communications Facility (Type C)
- Auctioneering
- Communications Facility (Type A)
- Industrial (Light)

The following uses are Permitted Uses in an Existing Approved, Discretionary Use Otherwise within the I-LHT Industrial, Light District land use district (Section 439 of Rocky View County Land Use Bylaw C-8000-2020):

- Establishment (Eating)
- Retail (Small)
- Office
- Recreation (Private)
- School, Commercial

The following uses are Discretionary Uses within the I-LHT Industrial, Light District land use district (Section 439 of Rocky View County Land Use Bylaw C-8000-2020):

- Accessory Building > 500 m<sup>2</sup>
- Cemetery and Funeral Services
- Film Production
- Kennel
- Recreation (Outdoor)
- Retail (Restricted)
- Alcohol Production
- Dwelling Unit, accessory
- Industrial (Heavy)
- Outdoor Storage
- Recycling/Compost Facility
- Special Function Business
- Cannabis Retail Store
- Establishment (Drinking)
- Industrial (Medium)
- Post-Secondary
- Retail (Groceries)

The following regulations will be applicable to lands which are located within the I-LHT Industrial, Light District land use district:

PROVISION	REGULATION	
MINIMUM PARCEL SIZE	1.0 ha (2.47 ac)	
MAXIMUM BUILDING HEIGHT	20.0 m	
MINIMUM SETBACKS (For Buildings)	Front Yard	6.0 m
	Side Yard	6.0 m
		2.4 m (7.87 ft) in Hamlets
		No setback abutting a railway
	Rear Yard	6.0 m
		No setback abutting a railway
MINIMUM SETBACKS (for Parking and Storage)	Front Yard	15.0 m from Highway/County road
		8.0 m from service roads
	Side Yard	15.0 m from Highway/County road
		8.0 m from service roads
		No setback from same/similar use
		6.0 m from other parcels
	Rear Yard	15.0 m (49.21 ft.) from any road
		No setback from same/similar use
		6.0 m from other parcels
A minimum of 10% of the lands shall be landscaped		
The quality and extent of landscaping shall be maintained for the life of the development		
Storage shall be screened from public ROWs and adjacent parcels		



Portions of the Subject Property which are located within the East District, those lands lying on the east side of the proposed north/south internal roadway, are proposed to be rezoned to a I-HVY Industrial, Heavy District land use district under Rocky View County Land Use Bylaw C-8000-2020.

The following uses are Permitted Uses within the I-HVY Industrial, Heavy District land use district (Section 446 of Rocky View County Land Use Bylaw C-8000-2020):

- Accessory Building ≤ 500 m<sup>2</sup>
- Communications Facility (Type A)
- Communications Facility (Type B)
- Communications Facility (Type C)
- Industrial (Logistics)
- Industrial (Medium)

The following uses are Permitted Uses in an Existing Approved, Discretionary Use Otherwise within the I-HVY Industrial, Heavy District land use district (Section 446 of Rocky View County Land Use Bylaw C-8000-2020):

- Office
- Industrial (Light)

The following uses are Discretionary Uses within the I-HVY Industrial, Heavy District land use district (Section 446 of Rocky View County Land Use Bylaw C-8000-2020):

- Accessory Building > 500 m<sup>2</sup>
- Alcohol Production
- Auctioneering
- Cannabis Cultivation
- Cannabis Processing
- Cannabis Retail Store
- Dwelling Unit, accessory
- Film Production
- Industrial (Heavy)
- Outdoor Storage
- Recycling/Compost Facility
- Special Function Business
- Station (Bulk Fuel)

The following regulations will be applicable to lands which are located within the I-HVY Industrial, Heavy District land use district:

PROVISION	REGULATION	
MINIMUM PARCEL SIZE	1.0 ha (2.47 ac)	
MAXIMUM BUILDING HEIGHT	20.0 m	
MINIMUM SETBACKS (For Buildings)	Front Yard	15.0 m
	Side Yard	15.0 m
	Rear Yard	6.0 m from an Industrial District
		15.0 m
MINIMUM SETBACKS (for Parking and Storage)	Front Yard	6.0 m from an Industrial District
		15.0 m from Highway/County Road
	Side Yard	8.0 m from service roads
		15.0 m from Highway/County Road
	Rear Yard	8.0 m from service roads
		No setback from an Industrial District
		15.0 m (49.21 ft.) from any road
		No setback from an Industrial District
6.0 m from other parcels		
A minimum of 10% of the lands shall be landscaped		
Storage shall be screened from public ROWs and adjacent parcels		

All other relevant provisions in Rocky View County Land Use Bylaw C-8000-2020 will also apply to the Subject Property.

The Development Authority may allow a variance to these regulations in alignment with the provisions of Rocky View County Land Use Bylaw C-8000-2020.

# APPENDIX A

## **RECOMMENDED PLANTING MATERIAL**

**TABLE 1: RECOMMENDED CONIFEROUS TREES****TREES - CONIFEROUS**

Common Name	Botanical Name	Native (N) / Drought (D)
Rocky Mountain Juniper	Juniperus scopulorum	N / D
Siberian Larch	Larix siberica	
Spruce - White	Picea glauca	N
Spruce - Colorado	Picea pungens	D
Spruce – Hoopsii	Picea pungens ‘Hoopsii’	D
Lodgepole Pine	Pinus contorta	N / D
Scotch Pine	Pinus sylvestris	D

**TABLE 2: RECOMMENDED DECIDUOUS TREES****TREES - DECIDUOUS**

Common Name	Botanical Name	Native (N) / Drought (D)
Maple - Manitoba	Acer negundo	D
Maple - Silver	Acer saccharinum	
Maple - Tatarian	Acer tataricum	
Ohio Buckeye	Aesculus glabra	
River Alder	Alnus tenuifolia	
Hawthorn - Snowbird	Crataegus x mordenensis ‘Snowbird’	
Hawthorn -Toba	Crataegus x mordenensis ‘Toba’	
Russian Olive	Elaeagnus angustifolia	D
Green Ash – Foothills	Fraxinus pennsylvanica ‘Heuver’	N / D
Green Ash - Patmore	Fraxinus pennsylvanica ‘Patmore’	N / D
Green Ash – Prairie Spire	Fraxinus pennsylvanica ‘Rugby’	N / D

**TREES - DECIDUOUS**

Common Name	Botanical Name	Native (N) / Drought (D)
Crabapple - Emerald Spire	Malus x 'Emerald Spire'	
Crabapple - Pink Spire	Malus x 'Pink Spire'	
Crabapple - Spring Snow	Malus x 'Springsnow'	
Crabapple - Thunderchild	Malus x 'Thunderchild'	
*Poplar - Balsam	Populus balsamifera	N / D
Swedish Columnar Aspen	Populus tremula 'Erecta'	D
Trembling Aspen	Populus tremuloides	N / D
*Poplar - Brooks #6	Populus x 'Brooks #6'	
*Poplar - Byland Green	Populus x 'Byland Green'	D
*Poplar - Prairie Sky	Populus x canadensis 'Prairie Sky'	D
Pincherry	Prunus pennsylvanica	D
Schubert Chokecherry	Prunus virginiana 'Schubert'	N
Ussurian Pear	Pyrus ussuriensis	
Burr Oak	Quercus macrocarpa	N
American Mountain Ash	Sorbus americana	

**TABLE 3: RECOMMENDED DECIDUOUS SHRUBS**

<b>SHRUBS - DECIDUOUS</b>		
Common Name	Botanical Name	Native (N) / Drought (D)
Green Alder	<i>Alnus crispa</i>	
Saskatoon	<i>Amelanchier alnifolia</i>	N
Bog/Dwarf Birch	<i>Betula glandulosa</i>	
Globe Caragana	<i>Caragana frutex 'Globosa'</i>	D
Red Osier Dogwood	<i>Cornus sericea</i>	N
Cotoneaster	<i>Cotoneaster acutifolia</i>	D
Wolf Willow/Silverberry	<i>Elaeagnus commutata</i>	N / D
Sea Buckthorn	<i>Hippophae rhamnoides</i>	N / D
Labrador Tea	<i>Ledum groenlandicum</i>	
Sweetberry Honeysuckle	<i>Lonicera caerulea</i>	D
Arnold Red Honeysuckle	<i>Lonicera tatarica 'Arnold Red'</i>	D
Bracted Honeysuckle	<i>Lonicera involucrate</i>	
Creeping Mahonia	<i>Mahonia repens</i>	
Mockorange	<i>Philadelphus lewisii</i>	
Ninebark	<i>Physocarpus opulifolius</i> var.	D
Shrubby Cinquefoil/Potentilla	<i>Potentilla fruticosa</i> var.	D
Double Flowering Plum	<i>Prunus triloba 'Multiplex'</i>	
Chokecherry	<i>Prunus virginiana</i>	N
Three Lobed Sumac	<i>Rhus trilobata</i>	
Alpine Currant	<i>Ribes alpinum</i>	D
Golden Flowering Currant	<i>Ribes aureum</i>	N / D
Gooseberry	<i>Ribes oxycanthoides</i>	N
Gooseberry	<i>Ribes hirtellum</i>	N / D



**SHRUBS - DECIDUOUS**

Common Name	Botanical Name	Native (N) / Drought (D)
Hansa Rose	Rosa rugosa 'Hansa'	D
Prickly Rose	Rosa acicularis	N / D
Prairie Rose	Rosa arkansana	
Common Wild Rose	Rosa Woodsii	N / D
Wild Raspberry	Rubus ideaus	
Thimbleberry	Rubus parviflorus	
Dewberry	Rubus pubescens	
Bebb's Beaked Willow	Salix bebbiana	N / D
Blue Fox Willow	Salix brachycarpa 'Blue Fox'	D
Highbush Cranberry	Viburnum trilobum	N

**TABLE 4: RECOMMENDED CONIFEROUS SHRUBS****SHRUBS - CONIFEROUS**

Common Name	Botanical Name	Native (N) / Drought (D)
Common Juniper	Juniperus communis	N / D
Juniper var.	Juniperus var	D
Globe Blue Spruce	Picea pungens 'Globosa'	D
Dwarf Mugo Pine	Pinus mugo 'Pumilio'	D
Mugo Pine	Pinus mugo 'Mugus'	D

**TABLE 5: RECOMMENDED ORNAMENTAL GRASSES****SHRUBS - CONIFEROUS**

Common Name	Botanical Name	Native (N) / Drought (D)
Bluestem Grass var.	<i>Andropogon gerardii</i> var	N / D
Bulbous Oat Grass	<i>Arrhenatherum bulbosum</i> 'Variegatum'	D
Blue Grama Grass	<i>Bouteloua gracilis</i>	N / D
Feather Reed Grass var.	<i>Calamagrostis acutifolia</i> var.	D
Sedge Grass	<i>Carex pensylvanica</i>	D
*Blue Lyme Grass	<i>Elymus arenarius</i> 'Blue dune'	N / D
Ravenna Grass	<i>Erianthus ravennae</i>	D
Blue Fescue var.	<i>Festuca glauca</i> var	D
Blue Oat Grass	<i>Helictotrichon sempervirens</i>	D
June Grass	<i>Koeleria cristata</i>	N / D
Switch Grass var.	<i>Panicum virgatum</i> var.	D
Ribbon Grass	<i>Phalaris arundinacea</i> var. <i>picta</i>	D
Little Bluestem Grass	<i>Schizachyrium scoparium</i>	N / D
Ivory Tower Yucca	<i>Yucca filamentosa</i> 'Ivory Tower'	D

**TABLE 6: RECOMMENDED PERENNIALS**

**SHRUBS - CONIFEROUS**

Common Name	Botanical Name	Native (N) / Drought (D)
Pussy Toes	Antennaria dioica 'Rosea'	N / D
Rockcress var.	Arabis var.	D
Coneflower var.	Echinacea purpurea var.	D
Cushion Spurge var.	Euphorbia polychrome var.	D
Blanket Flower var.	Gaillardia var.	D
Daylily var.	Hemerocallis var.	D
Hosta var.	Hosta var.	
Iris var.	Iris var.	D
Lavender var.	Lavandula var.	D
Meadow Blazing Star	Liatris ligulistylis	N / D
Poppy var.	Papaver var.	
Sage var.	Perovskia atriplicifolia var.	D
Stonecrop var.	Sedum var.	D
Ivory Tower Yucca	Yucca filamentosa 'Ivory Tower'	D

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# APPENDIX B

## **LINKS TO RELEVANT POLICY/ REGULATORY DOCUMENTS**



BEASP - <https://www.rockyview.ca/Portals/0/Files/BuildingPlanning/Planning/ASP/ASP-Balzac-East.pdf>

Land Use - <https://www.rockyview.ca/land-use-bylaw>

