



SHEPARD

LOGISTICS CENTRE

CONCEPTUAL SCHEME

FEBRUARY 2025



TABLE OF CONTENTS

1. Introduction	1
1.1 Purpose of this Plan	1
1.2 Regional Context	1
1.3 Vision, Goals, & Objectives	3
1.4 Supporting Studies	4
1.5 Design Standards	4
2. Plan area	6
2.1 Location / Context	6
2.2 Legal Description & Ownership	6
2.3 Existing Land Use	8
2.4 Site Conditions	10
2.5 Existing Road Network	20
2.6 Canadian Pacific Kansas City (CPKC) Railway Lands	20
3. Policy Review	23
3.1 Regional	23
3.2 Intermunicipal	23
3.3 Municipal	24
3.4 Prairie Gateway Area Structure Plan	24
4. Development Concept	26
4.1 Development Objectives	26
4.2 Development Concept Plan	26
4.3 Development Statistics	29
4.4 Rail Served Operations	30
4.5 Land Use	38
4.6 Access	43
4.7 Internal Road Network	43
4.8 Wetland Integration & Design	51
4.9 Private Landholdings	52
4.10 Open Space & Pathways	53



5. Transportation	56
5.1 Introduction	56
5.2 Regional Transportation Network	56
5.3 Transit Service Plan	57
6. Servicing	62
6.1 Water	62
6.2 Sanitary	64
6.3 Stormwater	66
6.4 Shallow Utilities	70
6.5 Power Generation Facilities	71
6.6 Protective Services	72
6.7 Fire Protection	72
6.8 Solid Waste & Recycling	72
7. Development Guidelines	74
7.1 Township Road 232 Design Corridor	74
7.2 Interfaces & Landscaping Plan	76
7.3 Lighting, Signage, & Fencing	97
7.4 Site & Building Design	99
8. Public Consultation	104
8.1 Purpose of Consultation	104
8.2 Open Houses	104
8.3 Shepard Community Association	104
9. Implementation	106
9.1 Process	106
9.2 Anticipated Phasing	107
APPENDICES	109
Appendix A: Policy Summary	
Appendix B: Risk Assessment	
Appendix C: Development Concept	



LIST OF FIGURES

Figure 1: Plan Area..... 2

Figure 2: Regional Context..... 7

Figure 3: Existing & Adjacent Land Uses 9

Figure 4: Existing Topography 11

Figure 5: Biophysical Inventory 13

Figure 6: Oil & Gas Infrastructure Summary17

Figure 7: Existing Transportation Network 21

Figure 8: Development Concept28

Figure 9: Rail Served Option 1 34

Figure 10: Rail Served Option 2.....36

Figure 11: Land Use Redesignation39

Figure 12: Local Road Network..... 44

Figure 13: 19m Industrial (4 utilities) with Regional Pathway..... 45

Figure 14: 19m Industrial (4 utilities) 46

Figure 15: 19m Industrial (3 utilities) 46

Figure 16: 22m Industrial47

Figure 17: 16m Industrial 48

Figure 18: 36 m Arterial.....49

Figure 19: Open Space & Pathways..... 54

Figure 20: Regional Transportation Network 58

Figure 21: Transit Service Plan Option 159

Figure 22: Transit Service Plan Option 2.....60

Figure 23: Water Servicing Plan63

Figure 24: Sanitary Servicing Plan.....65

Figure 25: Stormwater Servicing Network68

Figure 26: Stormwater Trunk Network.....69

Figure 27: Plan Area Interfaces..... 81

Figure 28: Cross-section for Interface Condition 1..... 85

Figure 29: Cross-section for Condition 286

Figure 30: Cross-section for Interface Condition 3.....87



Figure 31: Cross-section for Interface Condition 4..... 88

Figure 32: Cross-section for Interface Condition 5.....89

Figure 33: Cross-section for Interface Condition 6.....90

Figure 34: Cross-section for Interface Condition 7..... 91

Figure 35: Cross-section for Interface Condition 8.....92

Figure 36: Cross-section for Interface Condition 9.....93

Figure 37: Cross-section for Interface Condition 1094

Figure 38: Anticipated Phasing.....108



LIST OF TABLES

Table 1 : Summary of Design Standards 4

Table 2 : Legal Description and Ownership 6

Table 3 : Status of Oil and Gas Pipelines 16

Table 4 : Development Statistics29

Table 5 : Proposed Municipal Reserve (MR) Disposition 53



ACRONYMS / ABBREVIATIONS

AER	Alberta Energy Regulator
ASP	Area Structure Plan
BIA	Biophysical Impact Assessment
CPKC	Canadian Pacific Kansas City
CS	Conceptual Scheme
DP	Development Permit
EPEA	Environmental Protection and Enhancement Act
ESA	Environmental Site Assessment
HRA	Historical Resource Assessment
IDP	Intermunicipal Development Plan
LU	Land Use
MGA	Municipal Government Act
RVC	Rocky View County
SSS	Sanitary Servicing Study
SDC	Shepard Development Corporation
SCMDP	Sub-Catchment Master Drainage Plan
TIA	Transportation Impact Assessment



GLOSSARY

232 Design Corridor	200 metres of land located on each side of the Township Road 232 right-of-way and as shown within the Development Concept.
Area Structure Plan (ASP)	Provides a high-level vision for future development with regard to land use, transportation, conservation of the natural environment, emergency services, design, and utility requirements within its plan area.
CANAMEX	The CANAMEX corridor is a series of improvements to freeways and other transportation infrastructure linking Canada to Mexico through the United States. The corridor was established under the North American Free Trade Agreement. Currently the corridor is defined by a series of highways. However, the corridor is also proposed for use by railroads and fiber optic telecommunications infrastructure.
Conceptual Scheme (CS)	Provides a comprehensive policy framework intended to guide and evaluate Rocky View County redesignation, subdivision, and development proposals within its plan area.
Development Concept	The development concept plan/layout for the Conceptual Scheme, indicating the development lands, storm ponds, wetlands, roadways, rail spur options, and other key infrastructure.
Land Development Information Package	A package that displays AER-regulated pipelines, wells (surfacehole), incidents, facilities, coal mines and coal mine permits within a 2 km vicinity of the Plan Area. Additional lookup tables and relevant AER support documents accompany the package.
Non-rail Served Development	Development on a parcel that does not have access to a rail spur.
Plan Area	Land area subject to this CS.
Rail Served Development	Development on a parcel that abuts a rail spur.
Shepard Development Corporation (SDC)	Acting on behalf of Simpson Group of Companies.
The City	The City of Calgary.
The County	Rocky View County.





1 INTRODUCTION

1.1 Purpose of this Plan

A Conceptual Scheme (CS) is a planning document that is adopted via bylaw by the Council of Rocky View County ('the County'). The CS addresses planning and development items including land use, infrastructure provision, environmental considerations, pattern of future subdivision, roadways, and the integration of the development with surrounding land uses and communities. The CS is intended to provide clear and robust policy direction for development of the subject lands.

The Shepard Logistics Centre CS has been prepared to align with the vision and objectives of the Prairie Gateway Area Structure Plan (ASP). This CS establishes a comprehensive planning framework for the future development of 1,287.7 ac (521.1 ha) in Rocky View County, illustrated in **Figure 1: Plan Area Location**. Development of these lands will establish a major logistics hub that leverages the adjacent CANAMEX corridor and Canadian Pacific Kansas City (CPKC) rail line.

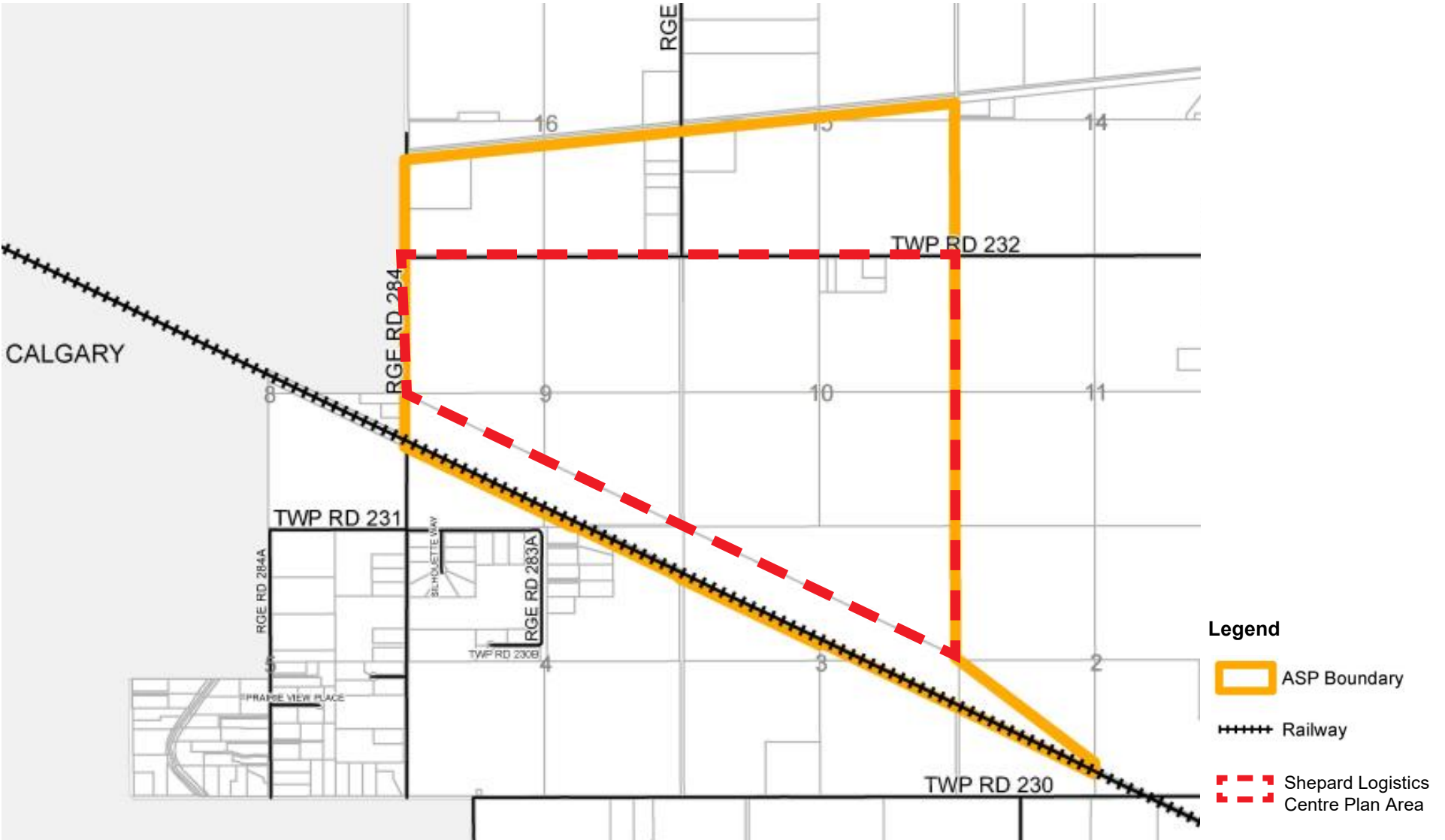
1.2 Regional Context

In January 2023, the County and the City of Calgary ('the City') announced their intent to work collaboratively on a new industrial corridor within the County. As a result, the two municipalities prepared the Prairie Gateway ASP, approved by both Councils (3rd reading) in February 2025. The ASP builds upon the opportunity provided by the merger of Canadian Pacific and Kansas City Southern that occurred in April 2023. The merger of the two rail operators has created a transnational railway connecting Canada, the U.S.A., and Mexico, strengthening the Canada-Mexico (CANAMEX) Trade Corridor.

The CS Plan Area consists of approximately 1,287.7 ac (521.1 ha) of agricultural lands located within the County. These lands border the City to the Plan Area's western boundary and are within the City and the County's Collaborative Planning Project area, as indicated within the Rocky View County / City of Calgary Intermunicipal Development Plan (IDP). Additionally, the IDP identifies the Plan Area as part of the Southeast Railway Corridor. The Prairie Gateway ASP designates the Plan Area as Industrial and within the Rail Served Policy Area.



Figure 1: Plan Area





1.3 Vision, Goals, & Objectives

The Shepard Logistics Centre CS aligns with the Goals as outlined within the Prairie Gateway ASP, which include:

- a. *Promote Rail Served Industrial Development*
- b. *Optimize Rail and Road Access*
- c. *Provide Industrial Development Flexibility*
- d. *Contribute to a Strong Regional Economy*
- e. *Advance Regional Collaboration*
- f. *Reduce Greenhouse Gas Emissions*
- g. *Execute Land Use Compatibility*

The Shepard Logistics Centre CS will create Western Canada's largest industrial distribution and logistics centre while leveraging CPKC's only direct line from Canada to Mexico, moving goods throughout North America and to global markets. It will be a model both for intermunicipal collaboration and future rail served logistics hubs.

The development of rail served industrial significantly reduces greenhouse gas emissions by decreasing the reliance on trucks for transporting goods. Rail transport is inherently more energy-efficient and environmentally friendly, as trains can move a ton of freight over 470 miles on a single gallon of fuel, making them 3-4 times more fuel efficient than trucks. By shifting a substantial portion of freight movement from road to rail, the development will reduce the number of trucks on highways, leading to lower fuel consumption, decreased traffic congestion, and reduction in greenhouse gases. This transition not only supports sustainable freight outcomes, but also enhances the efficiency and reliability of the supply chain.

The CS Plan Area represents the entire portion of the Prairie Gateway ASP area south of Township Road 232 and north of CPKC's rail line right-of-way (ROW). The strategic vision for the Plan Area is to develop an industrial and logistics park with rail access, connecting Western Canada to the U.S.A. and Mexico. A single CS has been prepared for this landholding to:

- *Create an integrated planning and design framework to guide future development with confidence;*
- *Maximize development potential and build out flexibility to capture prospective rail served and industrial investment; and,*
- *Identify and implement ultimate planning, design, and infrastructure measures to support efficient development outcomes.*





1.4 Supporting Studies

This CS has been prepared in conjunction with and is supported by the following studies:

- *Biophysical Impact Assessment (BIA) – Stantec, Dec 2024*
- *Sub-Catchment Master Drainage Plan (SCMDP) – Stantec, Feb 2025*
- *Sanitary Servicing Study (SSS) – Stantec, Feb 2025*
- *Transportation Impact Assessment (TIA) Supplementary Memo – ISL, Dec 2024*
- *Geotechnical Investigation – Stantec, Feb 2025*
- *Phase I and Phase II Environmental Site Assessments (ESA) – Stantec, Dec 2024*
- *Historical Resource Assessment (HRA) Clearance – Stantec,*
- *AER Land Development Information Package – AER, October 2024*
- *Oil and Gas Facilities Risk Assessment – Stantec, Feb 2025*

1.5 Design Standards

For ease of reference, a summary of the design standards utilized for the CS are provided below.

Table 1: Summary of Design Standards

Item	Standard being used
232 Design Corridor	City of Calgary
RR 284	City of Calgary
RR 283	City of Calgary
RR 282	City of Calgary
Internal Roads	City of Calgary
Water (offsite)	City of Calgary
Water (onsite)	Rocky View County
Sanitary (offsite)	City of Calgary
Sanitary (onsite)	Rocky View County
Stormwater (offsite)	City of Calgary
Stormwater (onsite)	Rocky View County
Landscaping (other than roads)	Rocky View County





2 PLAN AREA

2.1 Location / Context

As illustrated in **Figure 2: Regional Context**, the Plan Area is bordered by Township Road 232 (114 Avenue SE in Calgary) to the north, undeveloped Range Road 282 ROW to the east, the CPKC Mainline to the south, and Range Road 284 to the west. The Plan Area is located adjacent to the eastern limits of the City and the City's Shepard Industrial ASP, which is intended to provide industrial and business uses and consists of un-subdivided quarter sections, larger farming parcels, and lands owned by CPKC. The area has been identified as a Southeast Railway Corridor and a Collaborative Planning Project in the IDP between the County and the City.

The Plan Area is well connected to the region's major infrastructure, 3.2 kilometres east of Stoney Trail with access through Township Road 232 / 114 Ave SE and 3.2 kilometres south of Highway 560 / Glenmore Trail with access through Range Road 283. The preferred long-term access to the Plan Area will be east-west access to Stoney Trail via Township Road 232 and a realigned 114 Ave SE.

The CPKC Mainline is located on lands owned by CPKC. Due to the availability of direct access to the CPKC Mainline, adjacent lands within the Plan Area are optimal for a new rail served facility. The CPKC titled area is approximately 275 metres wide at this location and falls under Federal jurisdiction.

2.2 Legal Description & Ownership

Existing ownership, legal descriptions, and areas are illustrated and summarized in **Table 2: Legal Description and Ownership**. The majority of the lands are owned by Shepard Development Corporation (SDC) on behalf of the Simpson Group of Companies.

Table 2 : Legal Description and Ownership

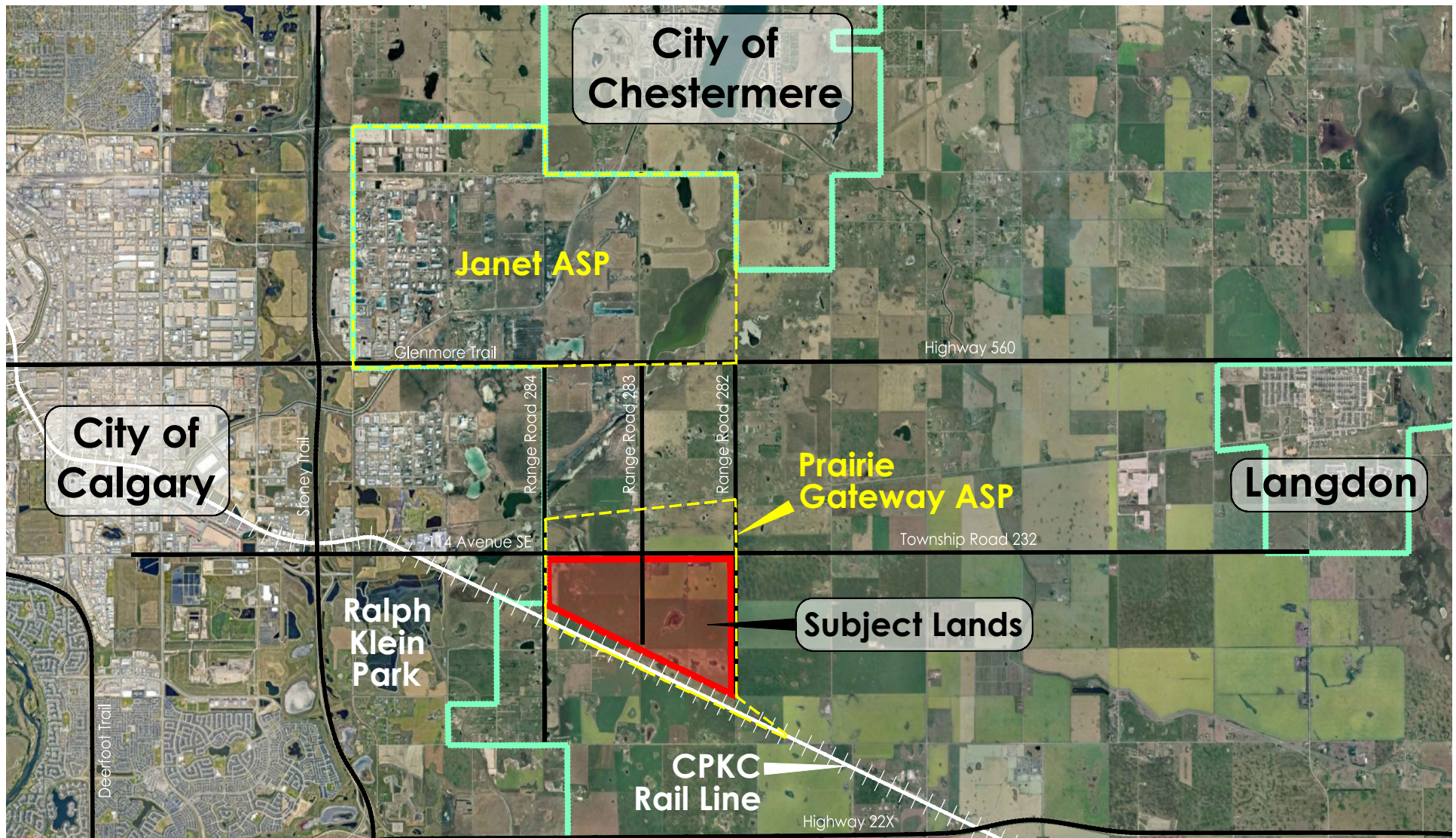
OWNER	LEGAL DESCRIPTION	AREA (± ac)
Shepard Development Corporation on behalf of the Simpson Group of Companies (SDC)	All of: 4;28;23;9;NW	513 ha (1,269 ac)
	All of: 4;28;23;9;NE	
	All of: 0610017;1;2	
	All of: 4;28;23;10;NE	
	Part of: 4;28;23;9;SW	
	Part of: 4;28;23;9;SE	
	All of: 4;28;23;10;SW	
	All of: 4;28;23;10;SE	
Private Landholdings	All of: 1112368;1;1	8 ha (20 ac)
	All of 1310578;1;3	
	All of 1510046;1;4	
	All of 1811875;1;5	
TOTAL		521 ha (1,287 ac)

2.2.1 Private Landholdings

The Plan Area for the CS has been prepared to include a small consolidation of parcels in the northeast corner of the Plan Area, and along Township Road 232. These parcels are not owned by SDC on behalf of the Simpson Group of Companies. They are included in this CS to provide direction and alignment as to how they could be redeveloped and integrated in the future, although no changes to existing land use is proposed at this time. Future integration would require the participation of the respective landowners.



Figure 2: Regional Context





2.3 Existing Land Use

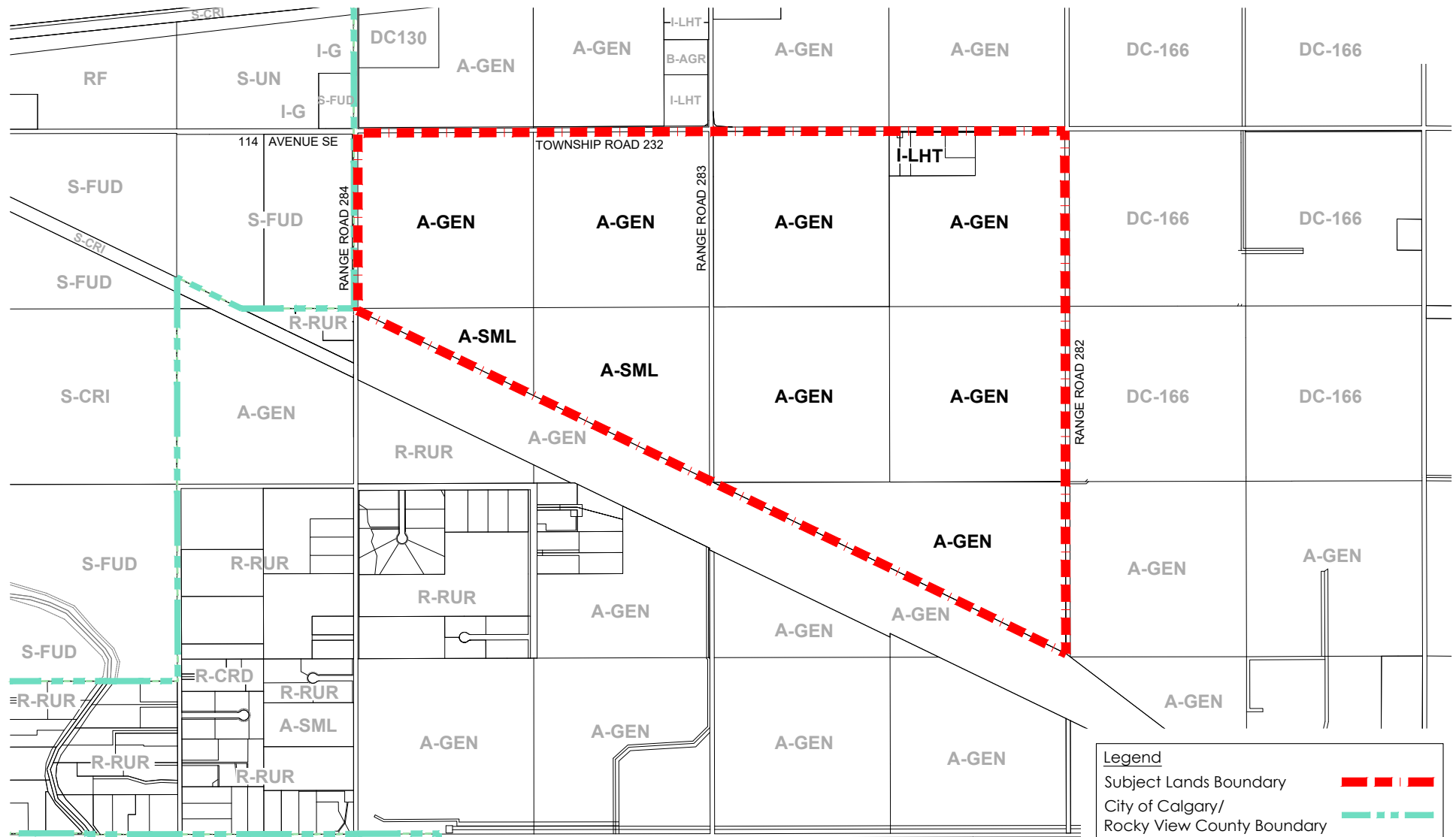
Existing land uses are depicted in **Figure 3: Existing & Adjacent Land Uses**. Land uses within the Plan Area are designated as Agricultural, General District (A-GEN) and Agricultural, Small Parcel District (A-SML) under Rocky View County's Land Use Bylaw C-8534-2024. These Districts are intended for general agricultural and associated rural residential development. In regards to the private landholdings, referenced above in **Section 2.2.1**, three of the four parcels are designated Industrial, Light District (I-LHT) and are currently utilized for light industrial activities.

Outside of the Plan Area, land uses include Residential Rural (R-RUR) District, Light Industrial (I-LHT), Agricultural – General (A-GEN), Agricultural Business (B-AGR), Agricultural Small Parcel (A-SML), Direct Control (DC) District #130, which allows for interim business uses, and DC District #166, which allows for solar farm infrastructure.

Lands to the west, within the City of Calgary, are designated for Industrial / Business uses within the Shepard Industrial Area ASP. They are currently zoned as Special Purpose – Future Urban Development (S-FUD) under Calgary's Land Use Bylaw 1P2007. Land uses surrounding the Plan Area are also important to note and have been addressed through policy identified in **Section 7.3**.



Figure 3: Existing & Adjacent Land Uses





2.4 Site Conditions

This section provides a summary of site conditions associated with the Plan Area. It is supported by a number of technical studies, including Phase I and II Environmental Site Assessment (ESA), a Biophysical Impact Assessment (BIA), and a Geotechnical Investigation.

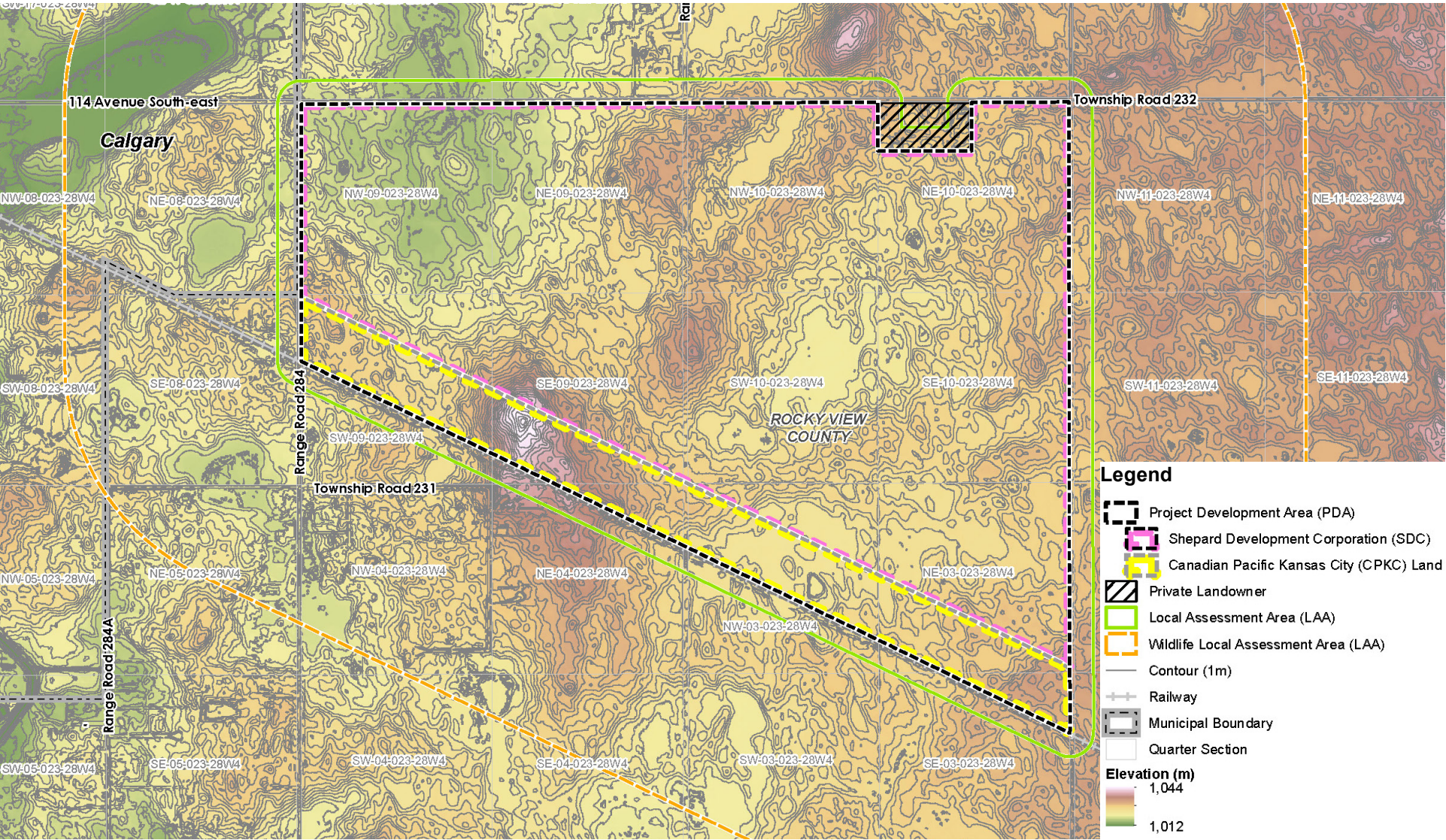
2.4.1 Topography

As illustrated in **Figure 4: Existing Topography**, the topography of the Plan Area is relatively flat with several depressions and wetlands, that are seasonally wet. The Plan Area drains generally towards the Shepard Slough to the north, however, due to the flat topography and local wetlands runoff is minimal.

Surrounding the Plan Area to the west and north are a number of shallow water bodies. Further west and southwest is Ralph Klein Park in the City of Calgary, and a regional park that includes the Shepard reconstructed wetland / stormwater management facility.



Figure 4: Existing Topography





2.4.2 Biophysical and Wetlands

A Biophysical Impact Assessment (BIA) was prepared in support of this CS. The purpose of the BIA is to provide an inventory and assessment of baseline conditions, identify potential mitigation measures and assess the anticipated environmental consequence of the development. Assessment of current land use, geology, terrain, topography and soil features, hydrology, vegetation and wetland communities, wildlife and wildlife habitat was completed. Field inventories of vegetation, soils, wetlands, and wildlife were initiated in the late summer and early fall of 2024 and are shown in **Figure 5: Biophysical Inventory**.

The Plan Area consists of primarily un-subdivided quarter sections, agricultural, and light industrial parcels with small sections of settled (residential) areas. Terrain within the Plan Area is generally flat to undulating with depressional, pothole wetlands present. Soils consist of Orthic Black Chernozems, Black Solodized Solonetz, Rego Humic Gleysols and Humic Luvic Gleysols.

No watercourses occur within the Plan Area. Stormwater pools within larger existing semi-permanent wetlands within the Plan Area, which are responsible for most flood attenuation within the Plan Area. Existing surface flow is mainly towards a manmade ditch in the northwest corner of the Prairie Gateway ASP boundary, which ultimately flows north towards the Shepard Slough Complex.

A total of six landcover types are present within the Plan Area, which include two upland types (i.e., crop and pasture), three disturbance types (i.e., road, settled and industrial, and wetland). Wetlands within the Plan Area are mineral, graminoid marshes that range in permanence from ephemeral (surface water is present in most years, but only for a brief period of days after snowmelt or a heavy rainfall) to semi-permanent (typically surface water is present throughout the year except in years of drought). There are 189 wetlands

identified within the Plan Area, of which, 59 are classified as ephemeral waterbody (EW), 84 as temporary graminoid marsh (MGII), 37 as seasonal graminoid marsh and nine as semi-permanent graminoid marsh , (MGIV). No vegetation species of conservation concern (SOCC) or ecological communities of conservation concern were identified within the Plan Area or within 1 km of the Plan Area.

Seasonal to semi-permanent wetlands provide potential habitat and breeding areas for wildlife such as waterfowl, waterbirds and amphibian species. Two (2) wetlands within the Plan Area are crown-claimed: one in the north-western corner of the Plan Area and along Township Road 232; and a second located centrally.

Boreal chorus frog and wood frog have been identified within the Plan Area, as well as an incidental observation of tiger salamander. A total of 40 bird species including four wildlife SOCC were identified during breeding bird surveys. In addition, five stick nests with potential for raptor usage were identified within the Plan Area and in areas adjacent to the Plan Area.

The following CS policies will support the implementation of recommendations of the BIA.

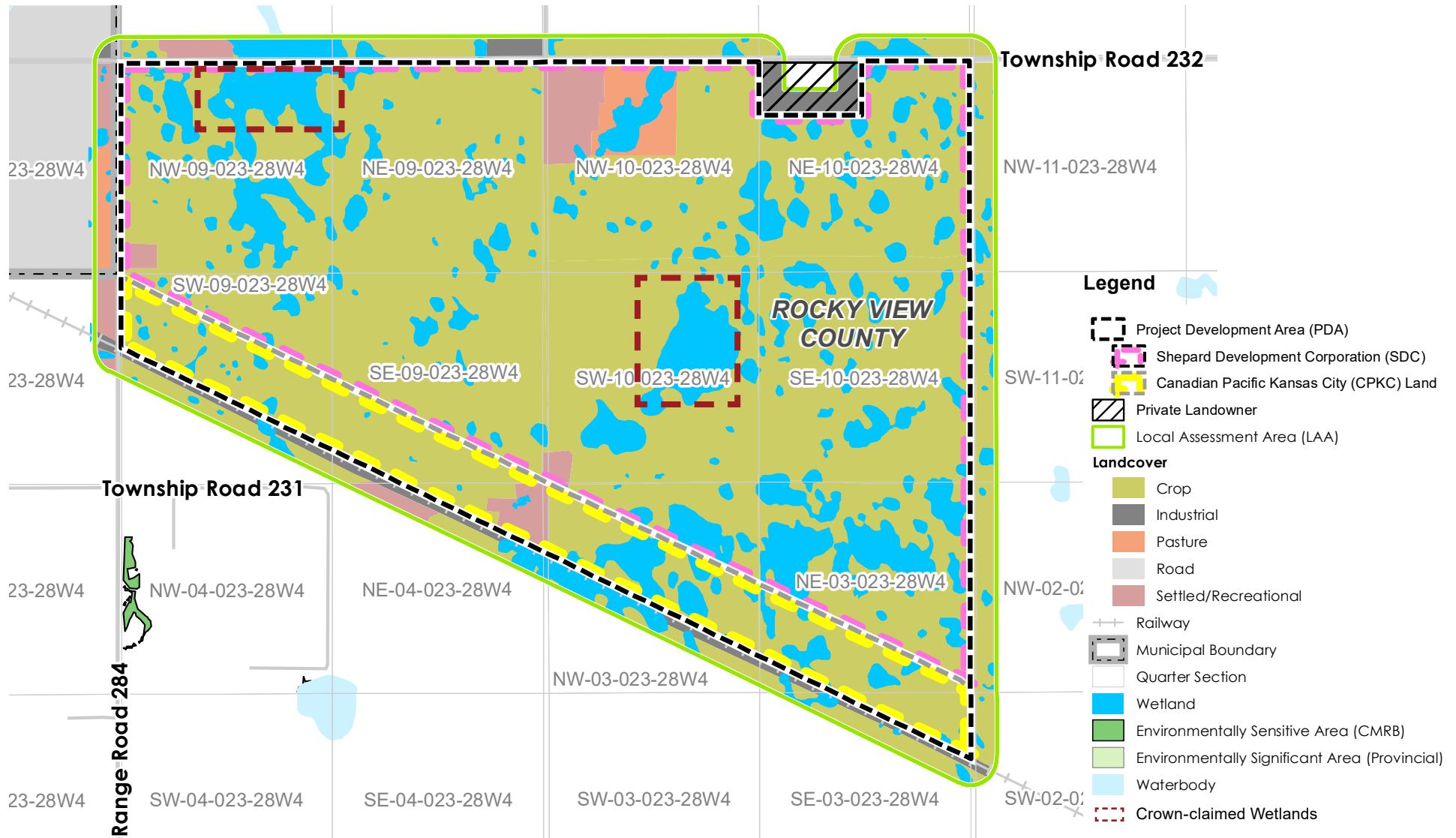
Policies

2.4.2.1 Compensation for any disturbed wetlands will follow standard procedures as established in the Provincial Water Act.

2.4.2.2 The Developer shall obtain required permits / approvals for the proposed development related to all applicable municipal, provincial, and federal legislation, regulations, and policies.



Figure 5: Biophysical Inventory





2.4.3 Environmental Site Assessment

A Phase I Environmental Site Assessment (ESA) was prepared for the Plan Area, followed by a Limited Phase II ESA. The purpose of the Limited Phase II ESA was for due diligence purposes and to assess the presence or absence of environmental impacts in soil at the Plan Area identified in the Phase I ESA.

Based on the results of the Limited Phase II ESA, further investigation of the Site was not considered to be warranted. It is recommended, however, that if impacted soil or groundwater are encountered during construction activities, work should be halted and additional ESA activities be conducted to assess the soil and groundwater quality within the area by an environmental professional. In addition, given the naturally elevated salinity parameter concentrations at the Plan Area, it is not recommended that excavated soil from the Plan Area be removed and re-used at an off-site location without further consideration by an environmental consultant.

2.4.4 Geotechnical

A site-specific geotechnical investigation was prepared in support of this CS to confirm the suitability of subsurface conditions in accordance with the requirements of the County Servicing Standards.

The subsurface soil conditions encountered in the Plan Area generally consisted of a surficial layer of topsoil and subsoil overlying till. The till was typically encountered as clay till; however, layers of silt till, sand till, and gravel till were observed at several investigation locations. Bedrock was encountered underlying the till at several boreholes.

Based on the findings of the geotechnical investigation, the existing conditions in the Plan Area are considered suitable for the proposed development. Recommendations for development of the Plan Area are incorporated within the geotechnical report (January, 2025), with more detailed specific assessments including deep fill reports, foundation evaluations, and pavement assessments required once further design details are known.

Policies

2.4.3.1 If impacted soil or groundwater are encountered during construction activities, work should be halted and additional ESA activities should be conducted to assess the soil and groundwater quality within the area by an environmental professional.

2.4.3.2 Any soil excavated from the Plan Area during construction activities should be assessed by a qualified environmental consultant prior to removal and re-use at an off-site location.

Policies

2.4.4.1 Detailed design and construction shall be undertaken in accordance with recommendations of the site-specific geotechnical investigation (Stantec, January 2025).



2.4.5 Archaeological and Historical

The Plan Area has been subject to a prolonged history of agricultural activity. In support of the ASP and this CS, an application for Historical Resource Clearance was submitted to the Province, with approval granted in accordance with the standard condition of “Reporting the Discovery of Historic Resources”.

The below CS policy is identified to support implementation of the Historical Resource Clearance.

Policies

- 2.4.5.1** The Developer shall follow all rules and policies laid out in the Historical Resources Act regarding the discovery of any historic resources during excavation and construction.





2.4.6 Oil & Gas

In accordance with the Prairie Gateway ASP, a Land Development Information Package from the Alberta Energy Regulator (AER) was obtained to identify all oil and gas related infrastructure in the Plan Area (Appendix C). Subsequent to receiving this package, Stantec undertook a Risk Assessment to identify how this infrastructure is intended and required to be addressed to support development of the Plan Area for industrial purposes. The report identifies requirements for the oil and gas infrastructure, the responsible parties, any required setbacks, and any other development considerations – particularly for abandoned wells, which cannot be removed or relocated.

The following pipelines and wells are located within the Plan Area, identified generally within **Figure 6: Oil & Gas Infrastructure Summary**.

Table 3: Status of Oil and Gas Pipelines

	License	Type	Operator	Status
1	63417	Saltwater Pipeline	MAGA Energy Ltd	<i>Operating</i>
2	34425	Sour Gas Pipeline	LR Processing Ltd (defunct)	<i>Abandoned</i>
3	48662	Natural Gas Pipeline	Ember Resources Ltd	<i>Operating</i>
4	48663	Natural Gas Pipeline	HESC Energy Corporation	<i>Discontinued</i>
5	0035829	Well	Ovintiv Canada ULC	<i>Abandoned</i>
6	189985	Sweet H2S Well	Lexin Resources Ltd (defunct)	<i>Injection</i>
7	0373340	Gas Well	Ember Resources Ltd	<i>Active</i>
8	0373341	Gas Well	Ember Resources Ltd	<i>Active</i>

As shown in the above table, there are two currently operating pipelines, two abandoned or discontinued pipelines, three operating wells, and one abandoned well within the Plan Area.

All oil and gas infrastructure located in the Plan Area will ultimately be decommissioned and removed where possible. The Developer will make all efforts for this to occur prior to development taking place in proximity to this infrastructure. However, encroachment onto existing pipeline rights-of-way and well site lease areas will not occur should development proceed prior to their decommissioning and reclamation.

All pipelines located in the Plan Area will be properly abandoned, decommissioned, and removed, with the land reclaimed to its original state. Pipelines must be emptied, purged, isolated, and left in a safe condition so that there are no risks to the public or environment. This process will be the responsibility of the licensee or the Orphan Well Association (OWA) if licensee defunct, including ensuring any cleanup and environmental requirements are met. The Province of Alberta's Pipeline Act and Pipeline Rules outline the requirements and responsibilities for the discontinuation, abandonment, and removal of pipelines.

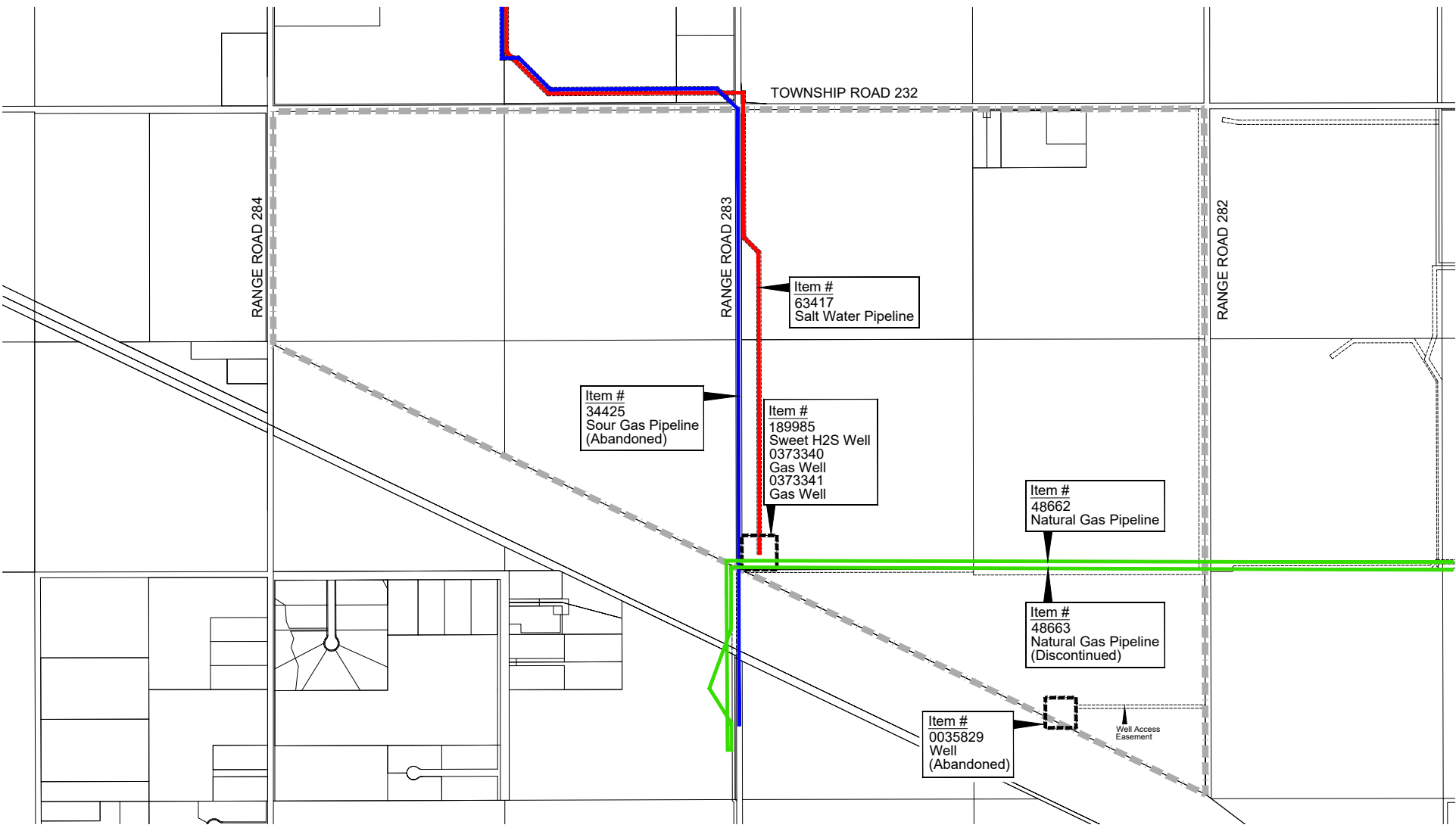
Existing active wells will be abandoned and recertified in accordance with AER requirements. Future development in proximity to these abandoned wells, including setbacks and access, will adhere to the AER's Directive 79 "Surface Development in Proximity to Abandoned Wells."

Specific development requirements for each individual oil and gas item are provided in Appendix B of the *Risk Assessment (Stantec, February 2025)*.

The following CS policies are provided in relation to oil and gas items.



Figure 6: Oil & Gas Infrastructure Summary





Policies

2.4.6.1 Development in proximity to pipeline and well infrastructure shall adhere to all Federal, Provincial, and Municipal regulatory requirements, including but not limited to:

- a. Province of Alberta's Pipeline Act
- b. Province of Alberta's Pipeline Rules
- c. Environmental Protection and Enhancement Act (EPEA)
- d. Conservation and Reclamation Regulation (CRR)
- e. Alberta Energy Regulator (AER):
 - i. Specified Enactment Direction (SED) 002: Application Submission Requirements and Guidance for Reclamation Certificates for Well Sites and Associated Facilities
 - ii. Directive 020: Well Abandonment
 - iii. Directive 77: Pipelines – Requirements and Reference Tools
 - iv. Directive 79: Surface Development in Proximity to Abandoned Wells

2.4.6.2 Specific development requirements for each oil and gas infrastructure item shall comply with Appendix B of the Risk Assessment.

2.4.6.3 During execution of construction activities approved under Subdivision, all abandoned well sites shall be marked with temporary signage identifying the location and depth, if known, of the abandoned well and providing contact information for the Alberta Energy Regulator. Such signage, as well as adequate fencing and any other necessary protective measures, shall be in place during the development process to prevent damage to the abandoned well bore.

2.4.6.4 At the time of a related subdivision or development permit approval, a restrictive covenant shall be registered that prevents the construction of any building within the set-back area associated with an active, suspended, reclaimed, or abandoned well.

2.4.6.5 At the time of a related subdivision, the developer shall ensure adequate access is provided to abandoned oil and pipeline infrastructure, in accordance with AER and/or related operator requirements.

2.4.6.6 The developer shall continue to undertake consultation with AER, the OWA, the County, and the affected operators of identified oil and gas facilities to discuss development planning and implementation.



2.4.6.7 All buildings located in proximity to an abandoned well site shall comply with Alberta's Matters Related to Subdivision and Development Regulation and Alberta Energy Regulator setback requirements or provide a minimum building setback as required by the operator(s), whichever is greater.

2.4.6.8 In conjunction with the preparation of a subdivision or development permit application for any parcel containing any oil and gas infrastructure, the applicant shall provide:

- a. Surveyed locations and depth, if known, of abandoned wells and pipelines and confirmation from the Alberta Energy Regulator of any setbacks;
- b. Phase I Environmental Site Assessment specific to the abandoned well as deemed appropriate by the Approving Authority;
- c. Phase II Environmental Site Assessment specific to the abandoned well if required; d. An evaluation of the integrity of the well abandonment; and
- d. A reclamation certificate for the well, if possible.

2.4.6.9 The Risk Assessment shall be updated as necessary, as changes to oil and gas infrastructure status take place.





2.5 Existing Road Network

The Plan Area is bounded by Township Road 232 / 114 Ave SE to the north, Range Road 282 (unconstructed) to the east, and Range Road 284 to the west, as depicted in **Figure 7: Existing Transportation Network**. The roadways connect to the broader regional highway network, including Stoney Trail to the west, and Highway 560 (Glenmore Trail) to the north.

2.6 Canadian Pacific Kansas City (CPKC) Railway Lands

CPKC railway lands are located immediately south of the Plan Area and provide rail access that the proposed development will connect into, to support rail served development.

CPKC's lands are federally regulated under the Canada Transportation Act. Therefore, any development proposed within the CPKC lands to support the Shepard Logistics Centre, including construction of new railway infrastructure, will be assessed under these federal requirements.

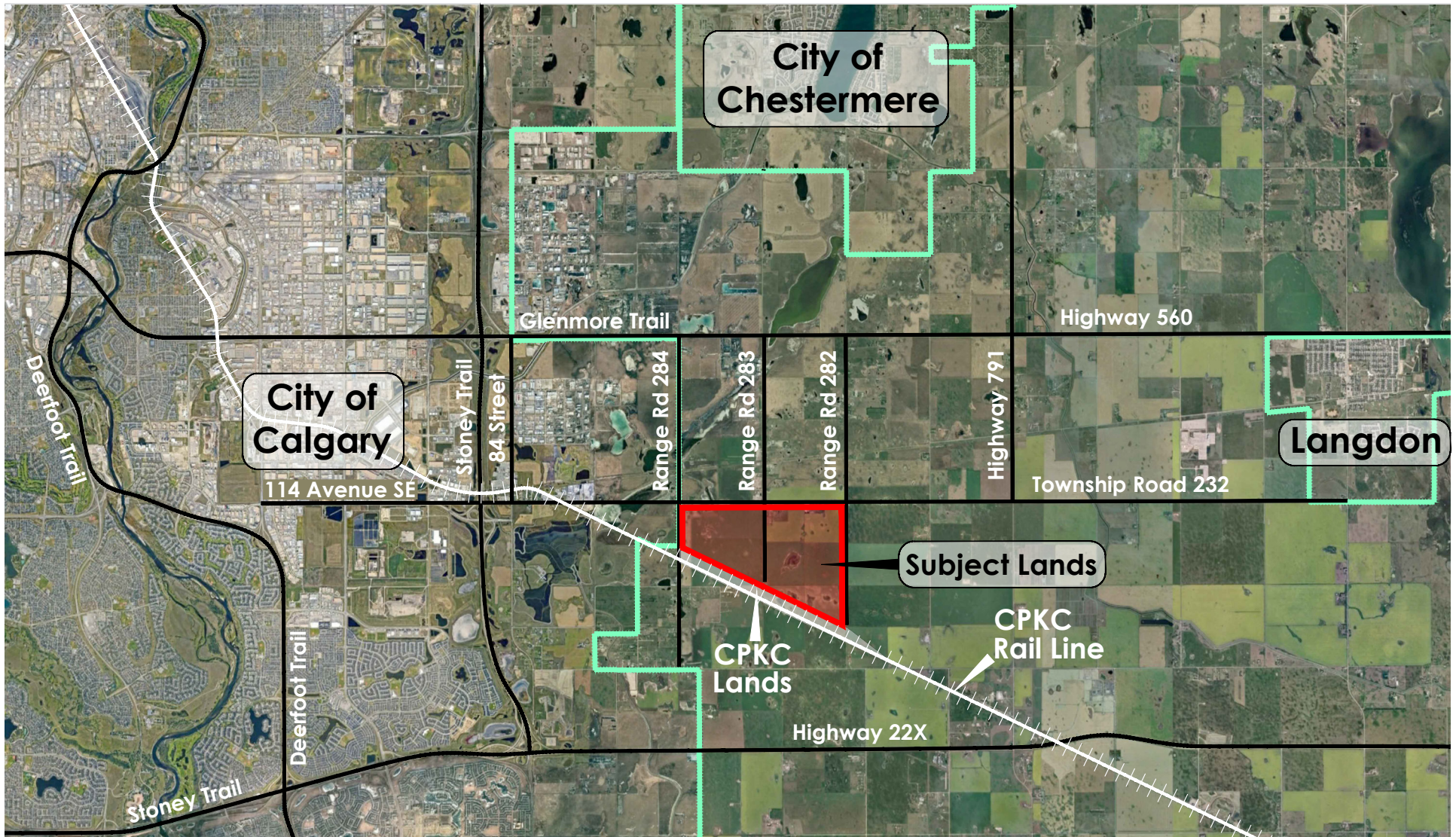
The Developer is collaborating closely to align planning, design, construction, and operational requirements between the Plan Area and CPKC lands. While railway line construction within CPKC lands does not require municipal approval, the Developer will provide ongoing updates to the County to support alignment and understanding of overall development and construction progress. This includes provision of necessary plans and documentation to demonstrate integration of development outcomes between the Developer and CPKC lands.

Policies

- 2.6.0.1** The Developer should provide ongoing updates, as necessary, to the County on the status of proposed development within CPKC lands.
- 2.6.0.2** Should any of the Plan Area come under the jurisdiction of the Canadian Transportation Agency, the Developer will continue to collaborate with and provide updates to the County regarding these lands.



Figure 7: Existing Transportation Network







3 POLICY REVIEW

The Plan has been prepared in compliance with and the context of higher-level statutory plans, regional plans, and County policies. The Plan is to be read in conjunction with the following documents:

- *Municipal Government Act (MGA);*
- *South Saskatchewan Regional Plan (SSRP);*
- *Rocky View County / City of Calgary Intermunicipal Development Plan (IDP);*
- *Rocky View County Municipal Development Plan (MDP);*
- *Rocky View County Land Use Bylaw (LUB); and*
- *Other Rocky View County documents and policies:*
 - *County Servicing Standards,*
 - *Agricultural Boundary Design Guidelines,*
 - *Commercial, Office, and Industrial Design Guidelines,*
 - *Recreation and Parks Master Plan,*
 - *Rocky View County solid Waste Master Plan, and*
 - *Fire Services Master Plan.*

3.1 Regional

At the time of the preparation of the ASP, Rocky View County was a member of the CMRB, whose mandate includes ensuring long-term sustainable growth for the Calgary Metropolitan Region. The ASP was referred to the CMRB for review and approval in accordance with the CMRB Regional Evaluation Framework, with approval granted in November 2024.

During the February 7th, 2025 CMRB board meeting, members voted unanimously to wind down the operations of the CMRB. The decision was influenced by recent provincial changes on the CMRB, which made membership voluntary and ended funding from the province. The CMRB also requested that the Municipal Affairs Minister repeal the CMRB regulation by May 1, 2025. Alternative approaches to regional planning are currently being discussed by related municipalities in lieu of provincial involvement.

3.2 Intermunicipal

The Plan Area has been identified within the Southeast Railway Corridor area and as a Collaborative Planning Area within the IDP (Map 2). The purpose of the IDP is to identify areas of mutual interest, minimize land use conflicts across municipal borders, provide opportunities for collaboration and communication, and outline processes for the resolution of issues that may arise within the Plan Area. The IDP ensures that both jurisdictions work collaboratively to coordinate planning initiatives for issues of mutual interest.



3.3 Municipal

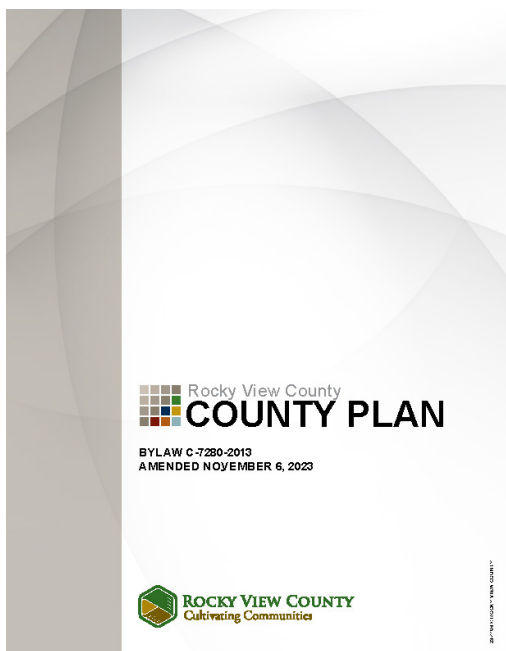
Rocky View County's Municipal Development Plan (County Plan, 2023 as amended) outlines the importance of industrial business development in supporting the County's economy. The County Plan requires Conceptual Schemes to be created to guide subdivision development Section (29.4), with the content of the conceptual scheme determined by the corresponding Area Structure Plan Section (29.5). Appendix C provides further direction as to requirements for conceptual scheme submissions.

This CS is prepared in accordance with the Prairie Gateway Area Structure Plan (ASP) and the relevant conceptual scheme requirements of Appendix C of the County Plan, and will provide the framework for a logistics centre that will provide significant economic benefits to the County and greater Calgary region.

3.4 Prairie Gateway Area Structure Plan

The Prairie Gateway ASP, approved in 3rd reading February 2025, provides the collaborative planning framework between the City of Calgary and Rocky View County to support the proposed development. In addition, the approval of this ASP sets the expectation for development that will support greater opportunities for economic growth, shared servicing, and intermunicipal cooperation. Throughout the planning process to develop this ASP, there was a strong focus on rail-served development, which is supportive of the development concept established by this CS.

To demonstrate compliance of this proposed CS with all policies/requirements of the ASP, a comprehensive compliance assessment has been prepared and is provided in **Appendix A**.







4 DEVELOPMENT CONCEPT

This section provides a comprehensive summary of the proposed CS Development Concept. A summary of the Development Concept is provided in **Figure 8: Development Concept** below, with the full Development Concept Plan provided in **Appendix C**.

The Shepard Logistics Centre will provide a sustainable and efficient solution for transporting goods across North America by accommodating industrial uses connected to direct rail access. The overarching goal is to create a world-class logistics centre that will support a stronger regional economy.

4.1 Development Objectives

The Development Concept was prepared in accordance with the following development objectives, reinforcing and implementing the overall CS vision identified in **Section 1.3**. These objectives were critical in guiding the planning, siting, and design of various components of the Development Concept, and importantly, identifying what development outcomes were prioritized to facilitate a successful rail served logistics centre. The Development Concept:

1. *Prioritizes the safe and efficient design and operation of rail over other planning and design aspects.*
2. *Maximizes flexibility to ensure a wide range of end users (both rail served and non-rail served) can be accommodated as the development advances.*
3. *Provides a high-quality design interface with surrounding lands.*
4. *Ensures higher impact industrial uses (e.g. Heavy Industrial) are appropriately planned and accounted for.*
5. *Provides sufficient development area for supporting uses and activities.*

4.2 Development Concept Plan

In line with the above objectives, the Development Concept has been designed to maximize operational efficiency, enabling large-format industrial, rail-served uses. The Development Concept is focused on the provision of large-scale development parcels that will accommodate a wide range of rail served and non-rail uses, including, but not limited to:

- *Intermodal facilities,*
- *Logistics and distribution,*
- *Manufacturing and assembly,*
- *Warehouse and storage,*
- *Bulk material handling,*
- *Food and beverage processing,*
- *Data processing, and maintenance and repair.*

A detailed description of uses anticipated within rail and non-rail served parcels is provided in **Section 4.5**.

Development areas for supporting uses and services, including commercial, office, light industrial, and food/beverage, are also provided in convenient locations at the main entry points to the development (from Township Road 232).

The Development Concept is characterized by the following:

- *Identification of five (5) potential rail spur locations connecting with the CPKC main line to the south. Not all spur alignments will advance, with two options proposed to provide flexibility for the Developer and end users in advancing rail served development. Details as to rail served options are discussed in **Section 4.4.2**.*



- A straightforward internal road network that aligns with quarter section boundaries within the Plan Area, to create an efficient grid-based road network. Critical to the safe and efficient operation of the rail service is not allowing elimination of any road crossings of the rail spurs. Adequately sized cul-de-sacs will to accommodate vehicle movements and emergency access will be provided across rail spurs as necessary.
- Aligning with the internal road network, access to the Plan Area is facilitated via equitably distributed access points off Township Road 232 to Range Roads 282, 283 and 284. Primary access is provided via Township Road 232, which is supported by the 232 Design Corridor Plan, establishing design guidelines for a 200m wide corridor along this entry point to the Plan Area.
- The internal road network grid and aligned rail spurs facilitate large-scale and regularly shaped development cells to accommodate prospective rail and non-rail served industrial uses in a highly efficient manner. Individual development cells will be determined as part of each subdivision phase to align with individual end user needs. A phasing plan is provided in **Section 9.2** that identifies the intended sequencing of development within the Plan Area.
- Intentional siting of linear ponds along the majority of the Township Road 232 Corridor to support a high-quality design interface that softens the impact of large-scale and rail served industrial development due to industrial uses being located at least 120-150 metres from Township Road 232. Landscape buffers within storm pond boundaries and development parcels within the 200m corridor further soften the impact of large-scale industrial uses.
- The provision of ponds along Township Road 232 is supplemented with smaller development parcels at the primary entry point/intersection to the development at RR 283. These parcels will provide supporting commercial uses and services for industrial development, such as truck stop/gas stations, eating establishments, convenience retail, and office.
- Retention of crown water bodies within the Plan Area, with the provision of appropriate buffers (Environmental Reserve) and integration with the proposed stormwater management solutions to ensure post-development hydrology.
- Identification of suitable locations (on or offsite) for supporting utilities, including the water reservoir, sanitary lift station, and power substation.

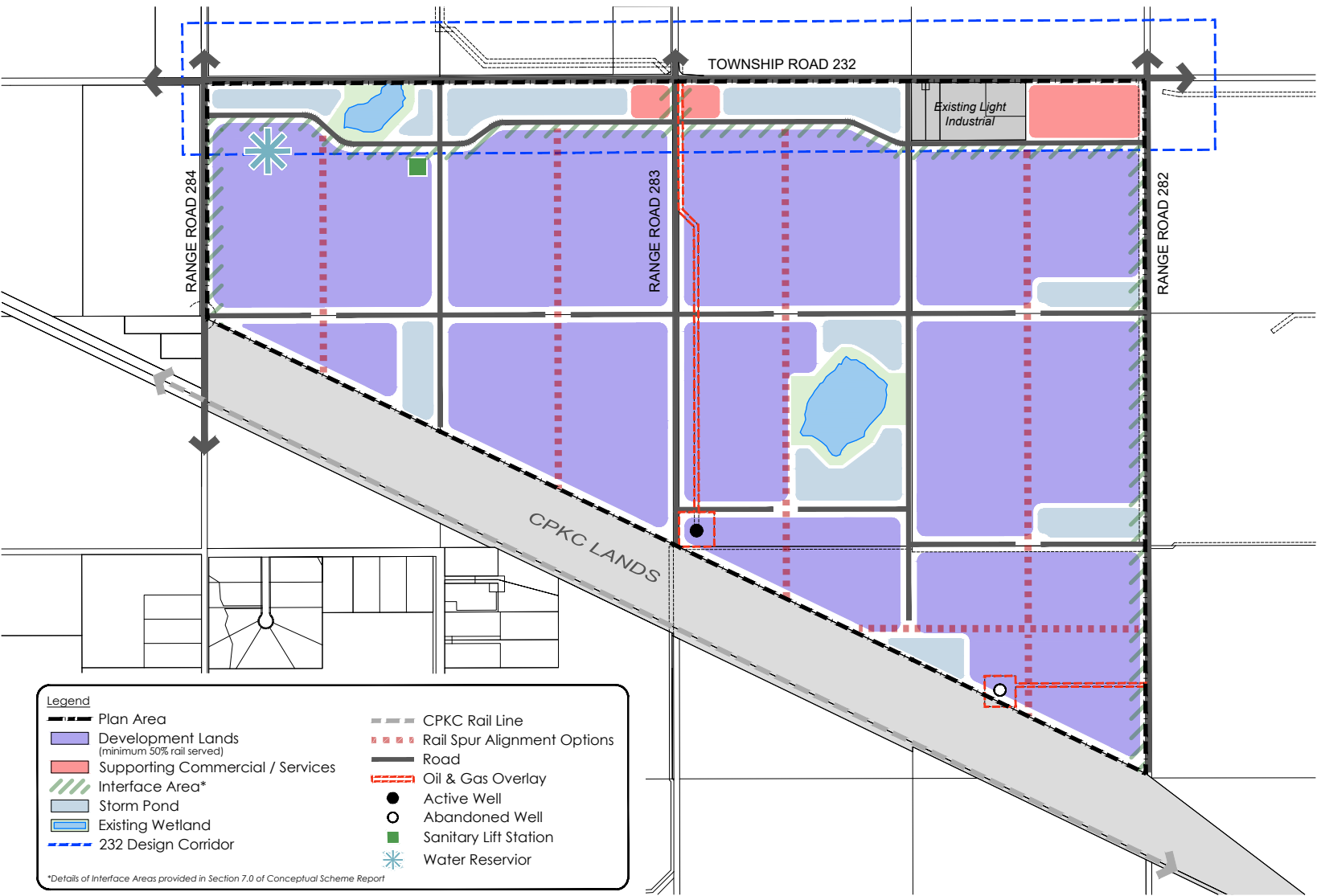
Policies

- 4.2.0.1** Individual development parcels within the Plan Area shall be determined at the subdivision stage.





Figure 8: Development Concept





4.3 Development Statistics

A summary of development statistics relating to the Development Concept is provided in the table below. For details regarding the amount of rail served and non-rail served development, please refer to **Section 4.4.2**.

Table 4 : Development Statistics

CONCEPT PLAN STATISTICS			
TOTAL AREA OUTLINED	521.1 ha	1287.7 ac	
Less ENVIRONMENTAL RESERVE (S-NOS)	15.5 ha	38.4 ac	
Less EXISTING USES (I-LHT / A-GEN)	7.7 ha	19.1 ac	
GROSS DEVELOPABLE AREA (GDA)	497.9 ha	1230.2 ac	100.0%
INDUSTRIAL	416.0 ha	1027.9 ac	83.6%
DIRECT CONTROL - INDUSTRIAL	377.9 ha	933.8 ac	
DIRECT CONTROL - SPUR LINES	26.8 ha	66.3 ac	
DIRECT CONTROL - SUPPORTING COMMERCIAL	11.3 ha	27.8 ac	
PUBLIC DEDICATION	81.9 ha	202.3 ac	16.4%
DIRECT CONTROL - STORM POND FACILITIES	54.4 ha	134.4 ac	
ROADS & LANES	27.5 ha	67.9 ac	



4.4 Rail Served Operations

In facilitating and prioritizing rail served development within the Plan Area, a series of rail spurs, stemming from CPKC's main line to the south, will be constructed and could be operated by a third-party operator or CPKC. Section 4.4.2 provides a rail design shadow plan identifying specific alignments of proposed rail spur options. The following provides a summary of key aspects of the proposed rail operation

Rail Spur Design Characteristics

To maximize efficiency of the development parcels within the Plan Area, rail spur geometry is proposed as straight/direct into the Plan Area from the CPKC lands. Rail spurs could be facilitated in 40-metre-wide ROWs, which will incorporate all track infrastructure for railcar movements and storage, switching operations, supporting utilities, and signage. The specific ROW requirements for these spurs will be determined at the subdivision phase.

Rail Operator

Rail operations will be administered by a third-party operator or CPKC. The third-party operator will be responsible for the preparation and implementation of all operational plans and documentation associated with rail aspects within the Plan Area. If the operator is a third party, the third party operator will coordinate with CPKC to ensure operational plans and documentation are integrated for safety and efficiency of operations.

Operational Hours

Rail served operations could take place 24 hours a day, 7 days a week, to maximize efficiency of operations.

Proximity - Adjacent Rail Served Development

The success of rail served development relies on the ability to safely and efficiently transfer goods to and from railcars to end users. To effectively facilitate this, there will be no space/setbacks between the rail spurs and end user infrastructure (e.g. buildings, structures, loading/unloading facilities).

In addition to the physical alignment and integration of rail spur and end user infrastructure, other design and operational alignments are required. These specific design and operational requirements will be prepared and implemented by the third-party operator as part of implementation activities.

Utility Infrastructure

Utilities required to support rail operations will be incorporated within the rail spur ROW. While not related to the operation of rail, it is noted that the Development Concept also proposes underground utility crossings (water, sanitary, storm) across rail spurs in select locations.

Security & Fencing

Security and fencing are crucial components of rail infrastructure within a rail-served industrial development, to protect goods and materials from theft, prevent unauthorized access, and help monitor and manage the flow of personnel and vehicles, reducing the risk of operational disruptions.

All rail spurs within the Plan Area will be appropriately secured, fenced, and monitored. Development permits for proposed development that abuts rail spurs will need to be designed to integrate with security and fencing requirements of the third-party operator.



Operational Emergency Response Plans

To ensure adequate and immediate emergency response for the development and rail operations, an Emergency Response Plan will be prepared by the third-party operator, in coordination with the Developer, CPKC, the County, and the City.

Waste Management

Rail operations can generate a wide range of types of waste, including chemicals, solvents, packaging, dust, and goods and materials being transported by rail cars. Waste Management Plans are typically prepared and implemented by rail operators and any land requirements (outside rail spur ROWs) will be determined at the detailed design phase.

Signage

Signage is essential for the smooth and safe operation of rail infrastructure. Signage will be strategically placed to guide the movement of railcars, vehicles, and personnel, ensuring clear communication of operational protocols and safety information. This includes directional signs for rail spurs, loading and unloading zones, and access points, as well as warning signs for hazardous areas and speed limits. Additionally, signage will comply with regulatory standards and be easily visible and legible under various weather conditions and lighting.

Policies

- 4.4.0.1** The specific right-of-way requirements of rail spurs shall be determined at the subdivision phase.
- 4.4.0.2** Specific design and operational requirements of rail infrastructure shall be prepared and implemented by the third-party operator.
- 4.4.0.3** Utility crossings of rail right-of-way shall be designed and constructed in accordance with rail design standards and requirements prepared by the third-party operator. The Applicant shall demonstrate compliance with rail design standards at time of subdivision.
- 4.4.0.4** Utilities required to support rail operations should be incorporated within the rail spur right-of-way.
- 4.4.0.5** The minimum building setback from a property line shared with a Rail Right-of-Way is zero metres.
- 4.4.0.6** Development of parcels that abut a rail spur shall be designed and operated in accordance with rail operation standards and manuals. Applications for Development Permits for Rail Served Development (see Section 4.5) shall demonstrate how the proposed development is complying with relevant rail operation standards prepared by the third-party operator for the Plan Area.



4.4.0.7 At Development Permit stage, proposed development that shares a boundary with a rail right-of-way shall demonstrate how the design complies with security and fencing requirements of the third-party operator.

4.4.0.8 Should rail operations require land outside of a rail right-of-way, for the purposes of implementing Waste Management Plans, the developer shall identify these requirements at time of Subdivision.

4.4.0.9 The County shall collaborate with The City of Calgary, CPKC, and any third-party site operator to develop an Emergency Response Plan to mitigate any risks related to Railway Lands and train movements.

4.4.0.10 Development adjacent to the CPKC Mainline should consider best practices for development in proximity to rail infrastructure. This includes:

- a. Integration of loading/unloading facilities to minimize the distance that goods need to be moved. Loading/unloading facilities should also be provided with high-capacity equipment to handle bulk materials efficiently
- b. Ensure rail right-of-way is designed with efficient geometry to support a range of rail cars anticipated to serve the development.

- c. Identify and implement safety measures, including fencing, designated and controlled access points, and clear signage to ensure safe operations and restrict access.

- d. Incorporate appropriate soundproofing materials and vibration-dampening construction techniques to minimize impact of rail operations on buildings and employees.

- e. Design the development of rail-served parcels with clear and unimpeded emergency access routes.

4.4.0.11 Any signage proposed as part of rail infrastructure or operations shall comply with regulatory standards and be easily visible and legible under various weather conditions and lighting.



4.4.1 Rail Design Plan

As outlined within the Development Concept, five (5) potential rail spur locations connecting with the CPKC main line to the south are identified. Two potential options are proposed to provide flexibility for the Developer and end users for rail served development, though not all spur alignments will advance. This approach is being undertaken to maximize flexibility for potential users, as large-scale industrial end users intended to be accommodated within this development have a substantial influence on layout, and the timing and location of the first end users will not be determined until the subdivision phase. These options are described below.

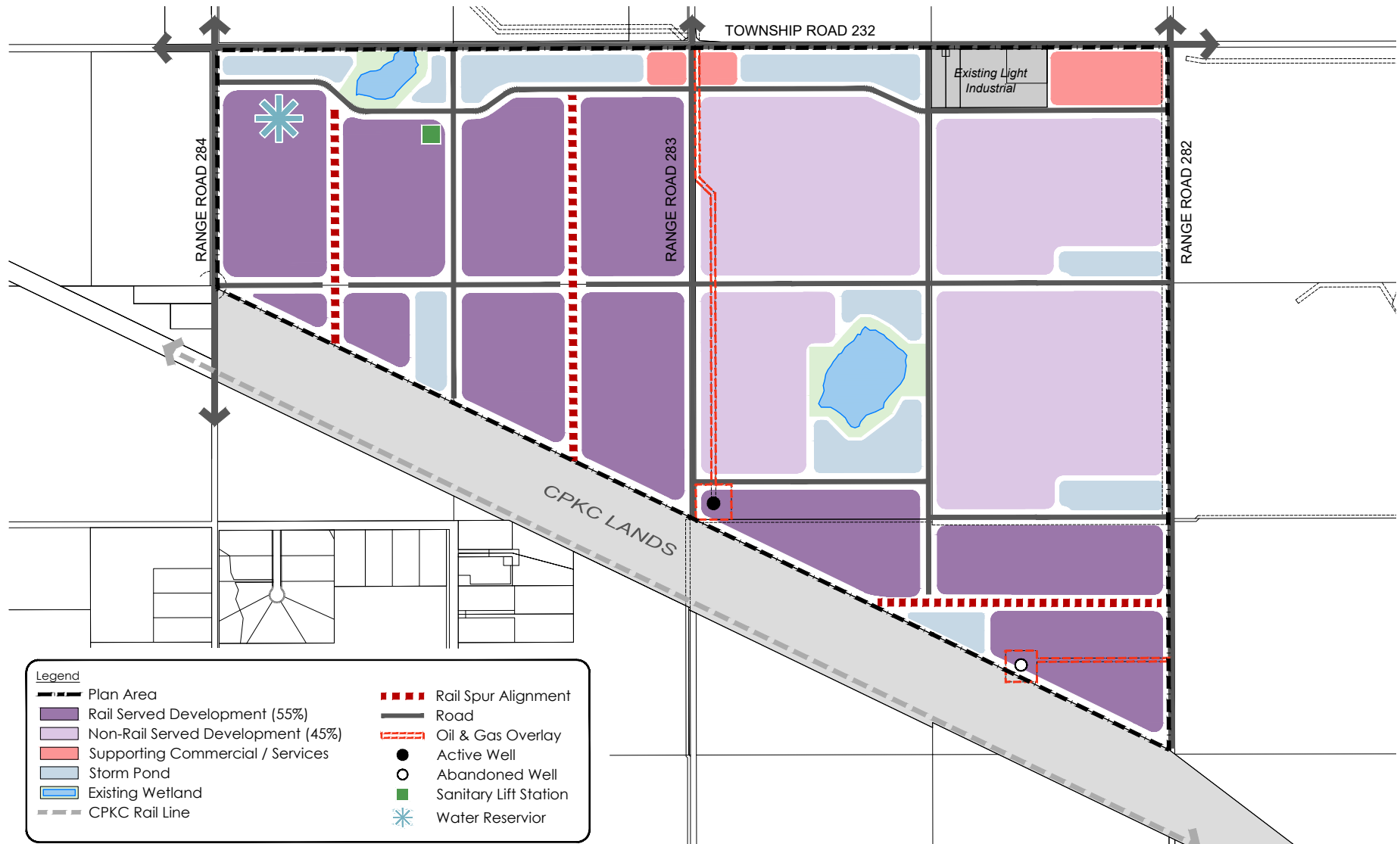
Rail Served Option 1 (Spurs A, B, and E)

This option involves rail spurs A, B, and E being designed and constructed, with rail spurs C and D removed. This option provides ~55% of the Plan Area for rail-served development. As land would not be required to facilitate rail spurs C and D, this land would be utilized for development area, and internal roads that no longer have these rail spur constraints could be connected. In this option, a transit route can service the majority of the northern and eastern portions of the Plan Area.





Figure 9: Rail Served Option 1





Rail Served Option 2 (Spurs B, C, and D)

This option involves rail spurs B, C, and D being designed and constructed, with rail spurs A and E removed. This option provides ~85% of the Plan Area for rail-served development. As land would not be required to facilitate rail spurs A and E, this land would be utilized for development area, and internal roads that no longer have these rail spur constraints could be connected. In this option, a transit route can service the majority of the northern and western portions of the Plan Area.





Figure 10: Rail Served Option 2





It is crucial to emphasize that regardless of the rail served option selected, the proposed internal road network and servicing alignments identified in the Development Concept remain the same, with the exception of the opportunity to connect adjacent cul-de-sacs where rail spurs are not being implemented (i.e. improving connectivity and transit service in that specific location). Both options achieve the required 50% threshold indicated in the ASP for Rail Served Development, with Option 1 providing ~55% and Option 2 achieving ~85%.

Timing of Selection of Preferred Option

As indicated above, the selection of a preferred rail served option will be determined at the first phase of subdivision, when the developer will have more certainty as to confirmed end user requirements.

Policies

- 4.4.1.1** Development within the Shepard Logistics Centre shall include a minimum of 50% rail served development.
- 4.4.1.2** The preferred Rail Served Option shall be determined prior to the submission of the first phase subdivision application.
- 4.4.1.3** Rail operations should be prioritized through all phases of the development process and the operation and ongoing maintenance of the development.





4.5 Land Use

The land use strategy for the CS is characterized into three (3) distinct land use categories. Rail served development will be prioritized while maximizing opportunity for a range of large-scale industrial development within the Plan Area. These land use categories are defined as:

4.5.1 Rail Served Development

This land use category is associated with development of any parcel that abuts a rail spur within the Plan Area. Rail served development would include, but not be limited to, uses such as:

- *Multi-modal terminals,*
- *Distribution and logistics centres,*
- *Manufacturing plants,*
- *Bulk material handling facilities,*
- *Food processing facilities,*
- *Warehousing (including bonded warehousing),*
- *Recycling centres,*
- *Agricultural processing facilities, and*
- *Transload facilities.*

These developments are supported by a range of rail related infrastructure, including gantry cranes (up to 30 metres in height) and straddle carriers to effectively and efficiently handle shipping containers. It is also anticipated that a majority of these uses will involve significant storage requirements (both indoor and outdoor), including the storage of shipping containers/ sea cans which would be appropriately screened.







4.5.2 Non-Rail Served Development

This land use category is associated with development of any parcel that does not abut a rail spur (i.e. no access to) within the Plan Area. Non-Rail served development would include, but not be limited to, uses such as:

- *Warehouses,*
- *Storage facilities,*
- *Data processing facilities,*
- *Distribution, research and development facilities,*
- *Pharmaceutical manufacturing,*
- *Printing and publishing,*
- *Cold storage,*
- *Food processing facilities, and*
- *Various assembly and packaging facilities.*

4.5.3 Supporting Commercial & Services

This land use category is associated with any uses that would service the daily/convenience needs of the Rail and Non-Rail Served Development. This would include, but not be limited to, uses such as gas stations, eating establishments, and offices that support industrial uses, as directed in the Prairie Gateway ASP.

Supporting Commercial & Services will be limited to the Township Road 232 Design Corridor, with the exception of smaller parcels within the Rail Served and Non-rail Served Development areas that are not capable of supporting industrial development. Safety and accessibility will be established while preventing compatibility challenges with other development, such as high levels of vehicular and pedestrian activity in close proximity to high impact industrial uses and rail spurs. Uses will be primarily focused on providing services to employees and the public travelling on Township Road 232 and should not draw the public into the core of the Plan Area.

As each of these land use categories specifically relate to the presence of a rail spur or a particular area of the plan (i.e. 232 Design Corridor), they are defined and addressed within a single Direct Control (DC) District for the Plan Area. Additionally, the Direct Control District identifies specific Permitted and Discretionary Uses for each of these land use categories, ensuring the Rail Served Development area is appropriately utilized for rail served end users.

Policies

- 4.5.3.1** Rail Served Development parcels shall be utilized for uses that require rail access.



4.5.3.2 Development of a rail served development parcel shall demonstrate utilization of adjacent rail infrastructure at the Development Permit stage, including provision of infrastructure to support rail operations such as sidings and loading docks, and compliance with safety regulations for rail operations.

4.5.3.3 Any change in use or tenant for a development parcel designated as Rail Served Development shall maintain the parcel's status as a Rail Served Development. This includes ensuring continued direct access to rail infrastructure and compliance with all relevant regulations and standards for rail operations. Any proposed change in use or tenant must demonstrate development will continue to leverage rail infrastructure, to the satisfaction of Rocky View County.

4.5.3.4 Outdoor Storage Areas shall be designed, located, and screened in a manner that maintains the aesthetic quality of the development and minimizes visual impact from public areas, including roadways. Specifically:

- a. Where possible, outdoor storage areas should avoid being located along the front setback of parcels along Range Road 284. Should outdoor storage be proposed along this roadway, a detailed screening plan should be provided to supplement a Landscape Plan at Development Permit.

- b. Screening shall include higher intensity landscaping and mass planting, berms, low transparency fencing, acoustic barriers, and decorative panels.

4.5.3.5 Supporting Commercial and Services uses should be located within the 232 Design Corridor, with the exception of parcels in the Plan Area that:

- a. Are not of a size or configuration to support development that is either rail-served or non-rail-served; and
- b. Will not negatively impact the operations of existing or planned rail-served or non-rail-served development parcels adjacent to the parcel.

4.5.3.6 Commercial uses should be located within 400 metres of a planned transit stop.

4.5.3.7 Heavy industrial uses with the potential for offsite impacts such as unsightly appearance, noise, odour, emission of contaminants, fire or explosive hazards, or dangerous goods should:

- a. Be located in areas close to, or adjacent to, railway lines or other means of access suitable for the transportation of raw materials and goods;



- b. Mitigate off-site impacts where possible, including the incorporation of noise attenuation techniques recommended as part of a noise mitigation study/analysis undertaken at the Development Permit stage; and
- c. Provide mitigating landscaping where possible.

4.5.3.8 Development shall be in accordance with the County's Commercial, Office, and Industrial Guidelines.

4.5.3.9 Recreational development, institutional development, and private school uses are not compatible with the goals of this Plan and shall not be permitted.

4.5.3.10 Industrial development within the Plan Area should, where possible:

- a. Include opportunities for on-site renewable energy generation;
- b. Consider waste heat recovery and re-use; and
- c. Provide landscaping and passive amenities to workers and visitors to the area.

4.5.3.11 Electric Vehicle charging stations should be included for fleet and public vehicles.

4.5.3.12 Development within parcels adjacent to the residential interface along Range Road 284 should avoid garbage storage, loading bays, loading doors, or other activities creating heavy truck movements being oriented towards the residential interface. Should this design outcome be unavoidable, screening (including acoustic barriers as necessary) should be provided to the satisfaction of the County.

4.5.3.13 At Development Permit, materials proposed to be stored on site shall be specifically identified to determine site specific requirements.



4.6 Access

Primary access into the Plan Area will be facilitated via Township Road 232/114 Ave SE via evenly spaced access points to the internal road network (including Range Road 284, 283, and 282).

Township Road 232 will be upgraded to a 36 metre wide ROW arterial roadway, in accordance with the Prairie Gateway ASP and City of Calgary standards. The proposed Township Road 232 road cross-section is shown in **Figure 18: 36m Arterial**.

4.7 Internal Road Network

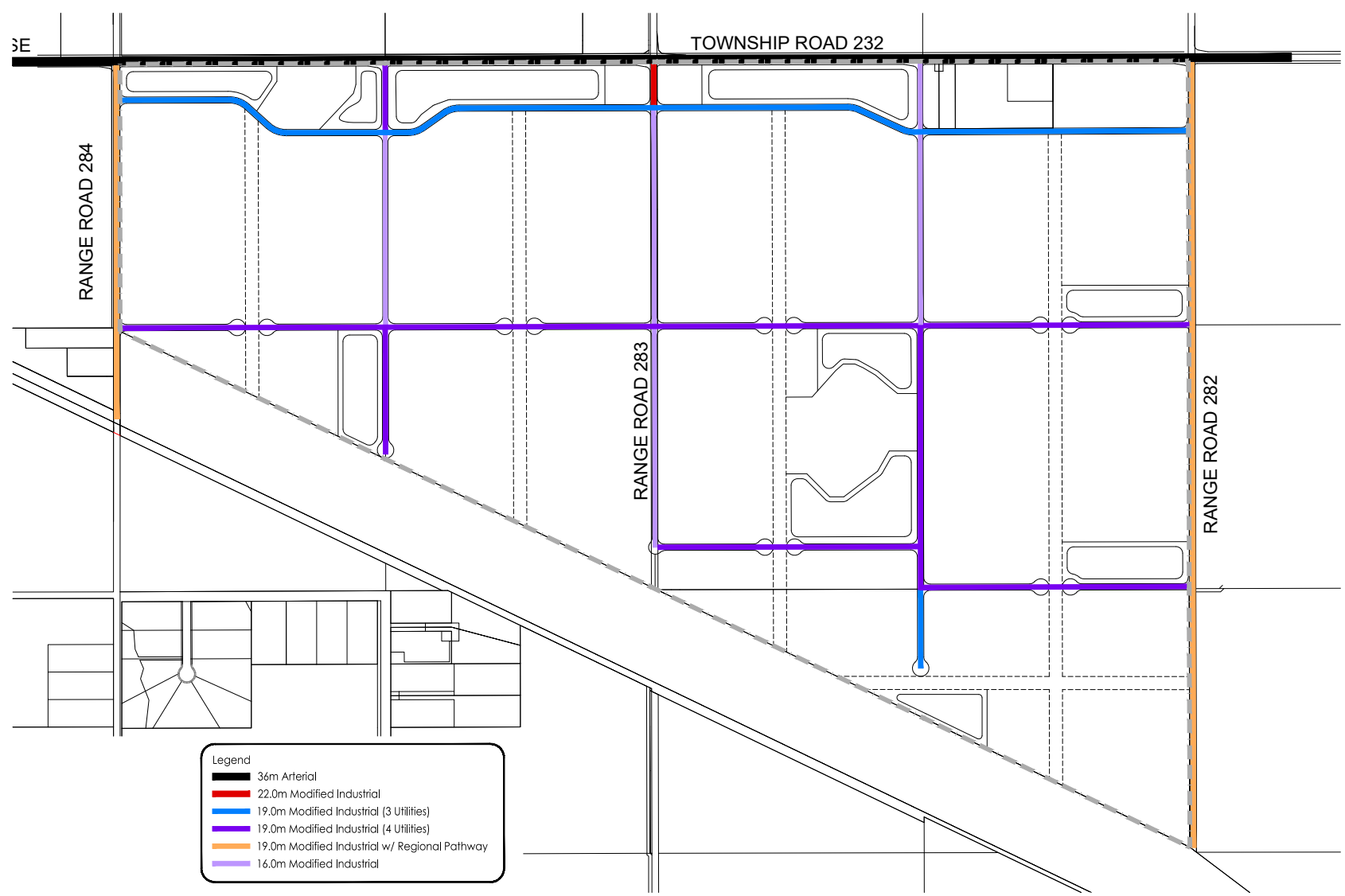
The Development Concept incorporates a logical internal road network that aligns with quarter section boundaries within the Plan Area, to create an efficient grid-based road network. The internal road network avoids any road crossings of proposed rail spurs, with adequately sized cul-de-sacs incorporated to accommodate vehicle movements. Emergency access (connecting cul-de-sacs separated by rail spurs) will be provided across rail spurs as necessary and is discussed Section 4.7.1.

The internal road network aligns with Rocky View County's 2013 Servicing Standards and proposes the use of several custom cross-sections that are identified within the Development Concept (Appendix C) as well as below. These roadways have been appropriately sized to meet anticipated traffic volumes, vehicle types, access needs, pedestrian and cyclist needs, and underground utility servicing ROW requirements.





Figure 12: Local Road Network





19m Industrial

Three versions of the 19m Industrial road are proposed as part of this CS, based on the number of utilities and the type of pathway required. Specifically, these versions are:

- A 19m Industrial accommodating 4 underground utilities and a Regional Pathway
- A 19m Industrial accommodating 4 underground utilities
- A 19m Industrial accommodating 3 underground utilities

The 19m wide ROW applies to roads within the Plan Area that require 3 or 4 utilities (which the proposed 16m cross-section can't accommodate) and along the boundary roads of Range Road 282 and Range Road 284, as projected traffic volumes do not require a 4-lane arterial road to accommodate.

The 19m modified industrial roadway accommodates 2 travel lanes (1 in each direction) with widths of 4.5m each. There is no median and no parking is allowed. Standard curbs are provided along either side. Because landscaping is being provided within the front setbacks of adjacent development parcels, no street tree line assignment is incorporated..

For internal roadways, a 1.5m wide monowalk or separate walk applies, with a Regional Pathway incorporated for Range Road 282 and Range Road 284. Underground utilities include sanitary, storm, water, and gas. Combined power/lights/communications poles are proposed in each boulevard.

Figure 13: 19m Industrial (4 utilities) with Regional Pathway

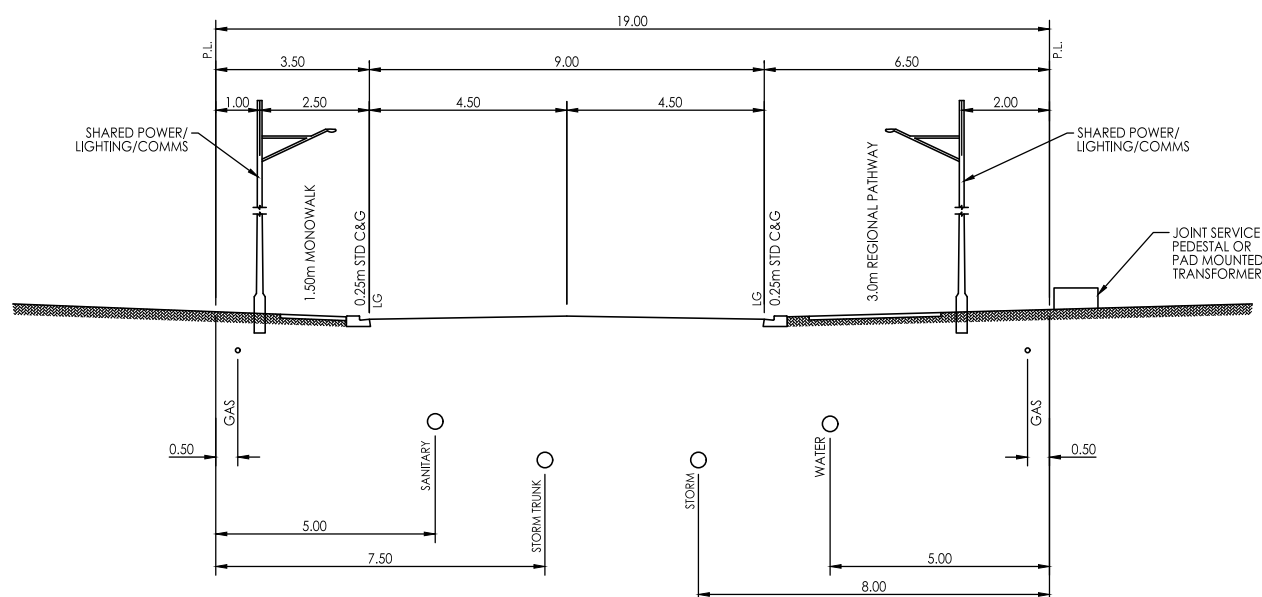




Figure 14: 19m Industrial (4 utilities)

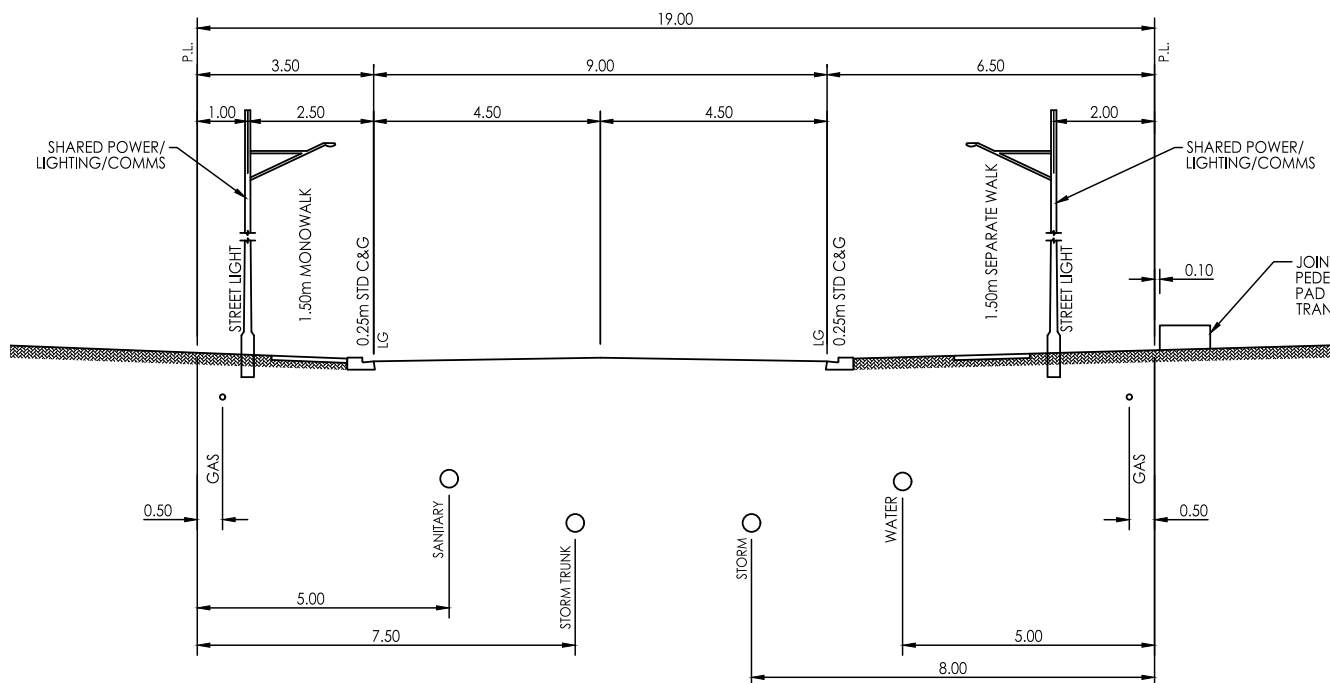
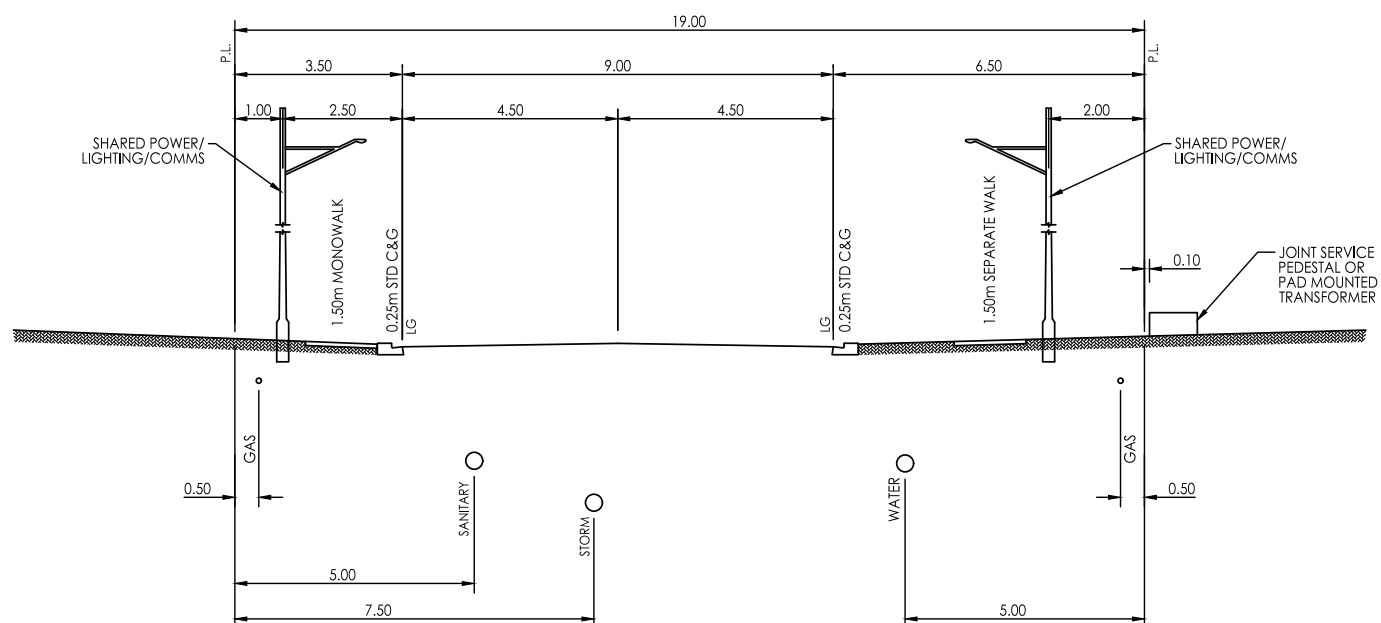


Figure 15: 19m Industrial (3 utilities)





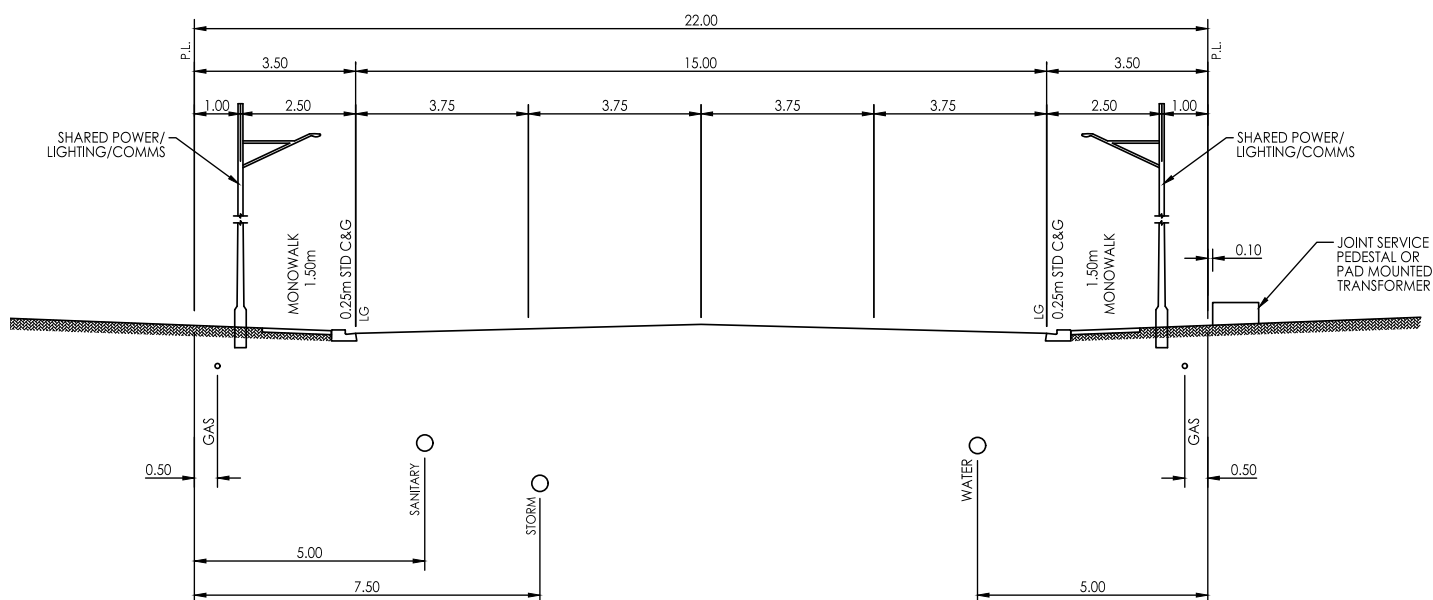
22m Modified Industrial

This 22m wide ROW cross-section applies to the main entry point into the Shepard Logistics Centre, off Township Road 232 and into Range Road 283. This road provides 2 lanes in either direction, allowing for the movement of larger vehicles into the development and providing a lane for through-traffic should vehicles be accessing the adjacent Supporting Commercial & Services parcels via the outside lanes. This ROW tapers to a 16m industrial ROW following the first east-west internal roadway/intersection.

This industrial roadway accommodates 4 travel lanes (2 in each direction) with widths of 3.75m each. There is no median and no parking is allowed. Standard curbs are provided along either side. No street tree line assignment is incorporated, in favour of landscaping being provided within the front setbacks of adjacent development parcels.

1.5m wide monowalk sidewalks are provided along either side. Underground utilities include sanitary, storm, water, and gas. Combined power/lights/communications poles are proposed in each boulevard.

Figure 16: 22m Industrial



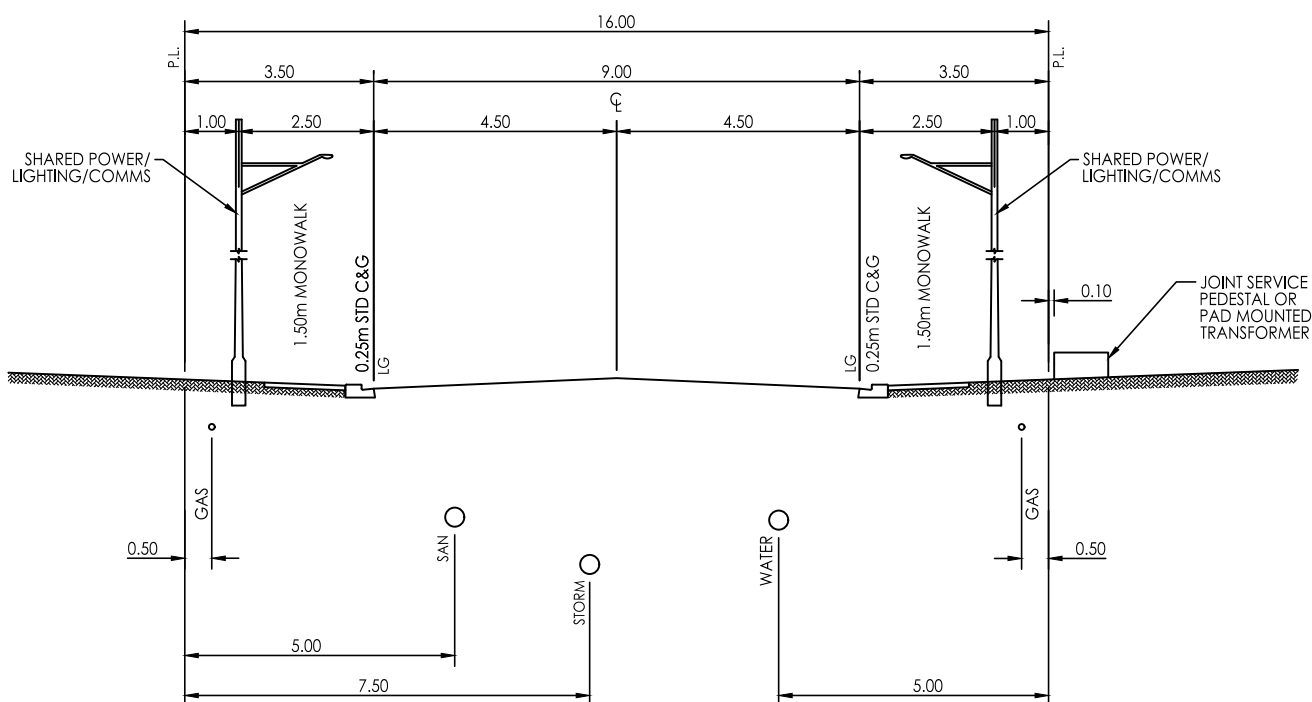


16m Industrial

This 16m wide ROW cross-section applies to the majority of internal roads, where only two lanes (1 in each direction) are required and only 3 underground utilities need to be accommodated. Travel lanes have a width of 4.5m each. There is no median and no parking is allowed. Standard curbs are provided along either side.

1.5m wide monowalk sidewalks are provided along either side. Underground utilities include sanitary, storm, and gas. Combined power/lights/communications poles are proposed in each boulevard. No street tree line assignment is incorporated, in favour of landscaping being provided within the front setbacks of adjacent development parcels.

Figure 17: 16m Industrial





36m Arterial

This 36m wide road right-of-way (ROW) standard applies to Township Road (TR) 232, which runs along the north side of the Plan Area. It provides the primary access to the Plan Area via 5 access points separated by ~800m each. From west to east, they are:

- *Range Road (RR) 284*
- *Halfway between Range Road 284 and Range Road 283*
- *Range Road 283: the main access into the lands and the two commercial sites*
- *Halfway between Range Road 283 and Range Road 282*
- *Range Road 282*

Township Road 232 also provides access to the lands to the north (also forming part of the Prairie Gateway ASP). This roadway becomes 114 Avenue SE when it crosses into the City of Calgary, which subsequently

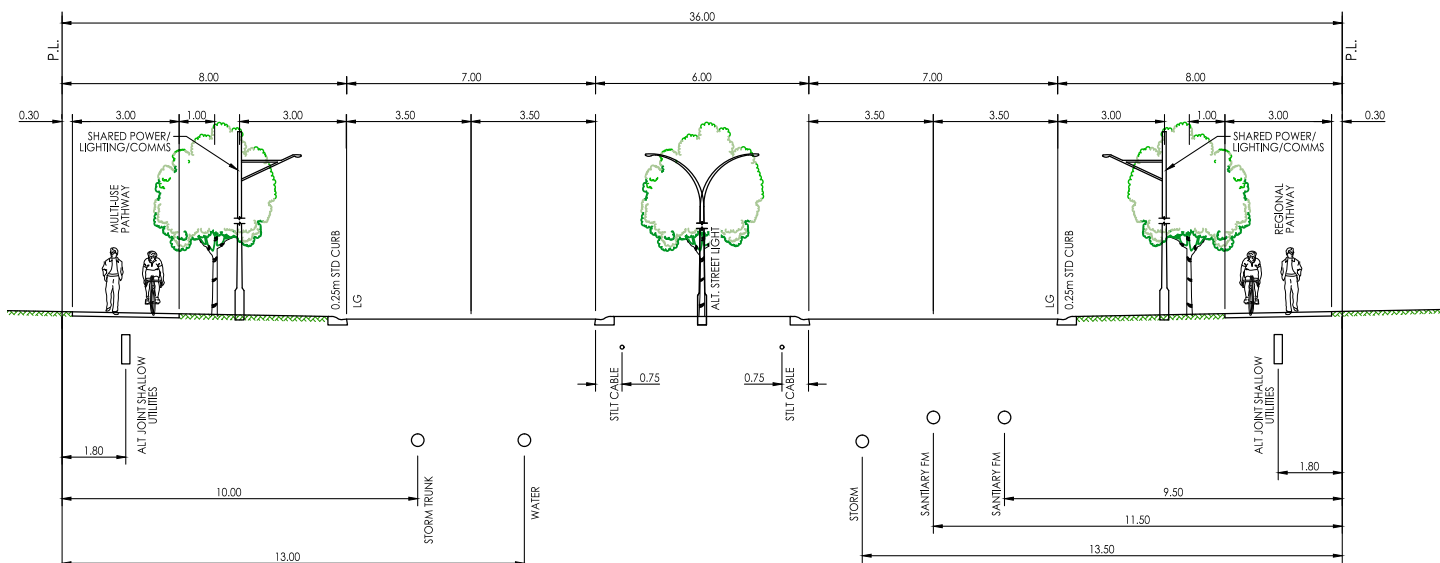
provides access to Stoney Trail (Highway 201). This standard aligns with the City's arterial roadway standard which will apply to 114 Ave.

The 36m arterial roadway accommodates 4 travel lanes (2 in each direction), each at a width of 3.5m (supporting transit service). It is divided by a 6m wide landscaped median with trees and streetlights. No parking is allowed. Standard curbs are provided along either side (4 total).

3m wide multi-use / regional pathways, for bicycles and pedestrians, are provided on both sides along with additional 4.5m wide landscaped boulevards with trees and combined power/lighting/communications poles. Underground utilities include sanitary, sanitary forcemains, storm, storm trunk, and water.

Township Road 232 is the centrepiece of the 232 Design Corridor Plan. As the gateway to the Shepard Logistics Centre and Rocky View County, this plan outlines enhanced site, building, and landscaping components within the lands on either side of Township Road 232.

Figure 18: 36 m Arterial





4.7.1 Emergency Access

Because of rail operations taking priority within the Plan Area, an internal road network with a number of no-through roads culminating in cul-de-sacs to facilitate turnaround movements for vehicles has been created. These cul-de-sacs are located immediately adjacent to the rail spur ROWs. To facilitate emergency access for each extended cul-de-sac occurrence within the Plan Area, an emergency access crossing will be designed and implemented in coordination with the third-party rail operator.

Policies

- 4.7.1.1** Prior to approval of the first phase of subdivision, the Developer shall provide, in coordination with CPKC or the Third-Party Rail Operator, detailed design of any required emergency access connections across rail spur rights-of-way, to the satisfaction of the County.
- 4.7.1.2** Building setbacks are dependent on Emergency Route Access and Emergency Response Plan.





4.8 Wetland Integration & Design

As identified in Section 2.4.2, two (2) waterbodies (in the form of wetlands) within the Plan Area have been crown-claimed through engagement and determination with Alberta Environment & Protected Areas (EPA). These wetlands are intended to be retained within the Development Concept, including sufficient buffers (minimum 30 metres), as Environmental Reserve (ER). Buffer areas surrounding the extents of the wetlands (bed and shore) varies from a minimum of 30 metres to potentially in excess of 50 metres in specific areas and will be designed to support the ecological function and value of these features.

In addition to ecological characteristics, the hydrology of these retained wetlands is being addressed in the stormwater management concept for the proposed development. Storm ponds in proximity to the wetlands are designed to support post-development hydrology that aligns with pre-development conditions. It is noted that achieving this intent presents challenges for the south-central wetland, which will be discussed with Alberta Environment & Parks (AEP) to support a practical solution. Details of the specific biophysical and stormwater design and recommendations for the retention of these wetlands is provided in the BIA and the SCMDP.

The retention of the north-western wetland offers an excellent opportunity for the development to provide a feature amenity at the gateway entrance to the development. This wetland is integrated with the adjacent storm pond and the 232 Design Corridor to provide a high-quality design outcome for the CS.

Policies

- 4.8.0.1** Landscaping drawings for the retained wetlands shall incorporate pathways that provide access to these environmental features and connect to the pathway network included within internal roads for convenient access.
- 4.8.0.2** Wetlands retained within the Plan Area shall incorporate a minimum 30m buffer from the bed and shore boundary of the wetland.
- 4.8.0.3** Design recommendations and requirements of the approved BIA supporting the CS shall be implemented as part of detailed design and construction of infrastructure and areas surrounding retained wetlands.
- 4.8.0.4** The Developer shall obtain all required Provincial and any municipal approvals relating to wetlands prior to construction of the relevant Subdivision phase commencing. This includes approval and compensation existing wetlands proposed to be removed within the Plan Area.



4.9 Private Landholdings

As noted in Section 2.2.1, a small collection of parcels along Township Road 232, identified as Existing Light Industrial on the Development Concept, are included in the CS boundary. No development or land use changes are currently proposed to these parcels (i.e. existing uses of these parcels can continue indefinitely under existing industrial land use). These parcels are, however, included in the CS to provide direction as to future development and integration with this proposed development. Specifically, should incorporation of these parcels be proposed as part of future subdivision resulting from this CS, the following policies will apply:

Policies

- 4.9.0.1** Consolidation of these parcels is preferred to support future development and subdivision;
- 4.9.0.2** Access to these parcels is preferred from the proposed internal road network. Access proposed from Township Road 232 should be consolidated into a single access point;
- 4.9.0.3** Requirements of the 232 Design Corridor are applicable to future development of these private landholdings;
- 4.9.0.4** Existing I-LHT land use is appropriate to support compatible industrial development within the Plan Area. The landowner(s) of these parcels may seek amendments to the DC District proposed as part of this CS if desired; and,
- 4.9.0.5** A CS amendment is not required to incorporate these parcels as part of the proposed development and subdivision. This includes any changes to the internal road network and stormwater infrastructure plan/layout to support design and efficiency outcomes.



4.10 Open Space & Pathways

The CS and associated Development Concept does not propose the provision of any Municipal Reserve (MR) lands within the Plan Area. Cash-in-lieu is therefore proposed in order to satisfy MR requirements in accordance with the *Municipal Government Act (MGA)*. A MR analysis and disposition is summarized in the below table.

Table 5 : Proposed Municipal Reserve (MR) Disposition

	Ac.	Ha.	%
Gross Area	1287.7	521.1	100%
Private Landholdings	19.1	7.7	1.5%
Environmental Reserve	38.4	15.5	3%
Gross Developable Area (GDA)	1230.20	497.9	95.5%
MR as cash in lieu	123.02	49.79	10%

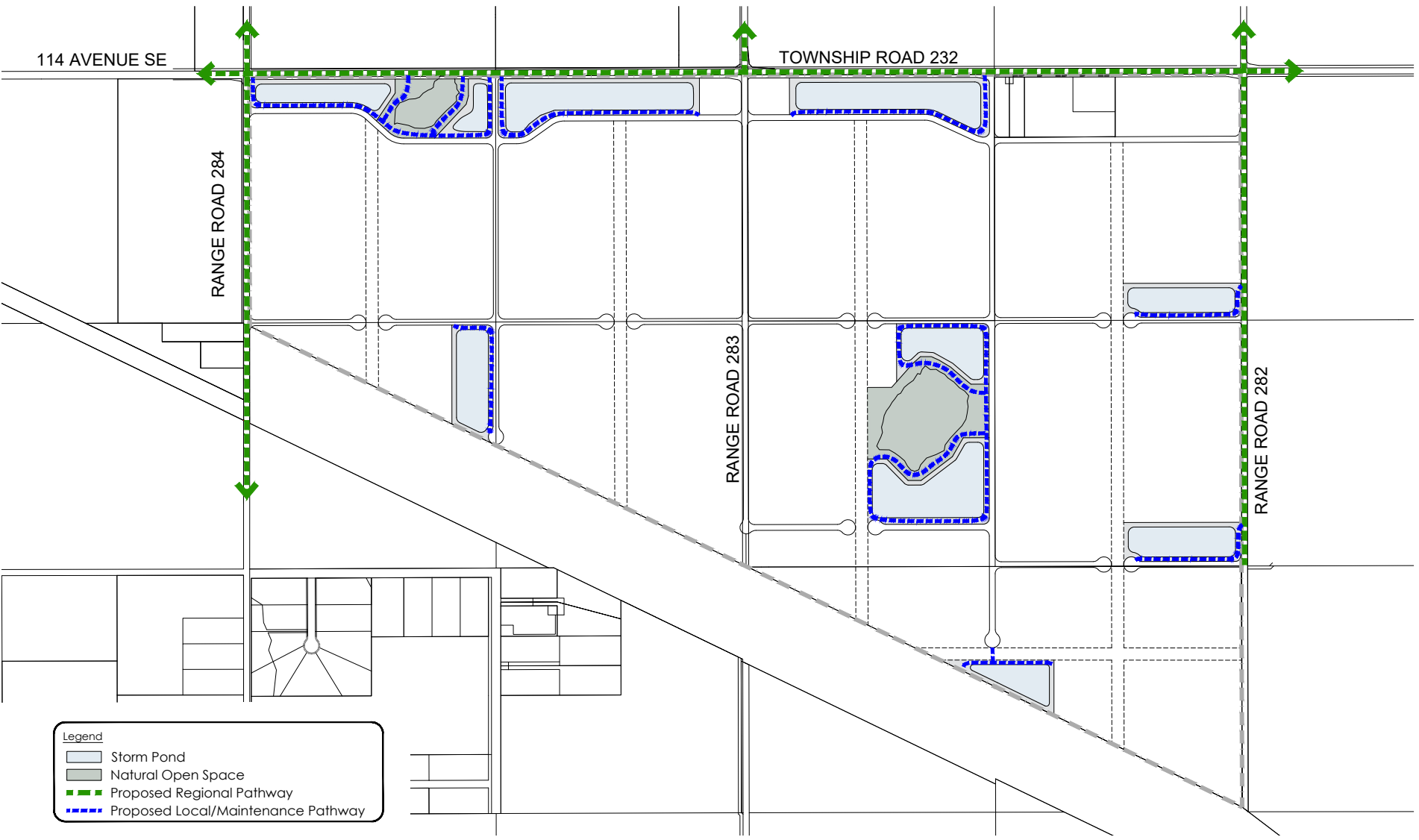
The Prairie Gateway ASP identifies a network of future regional pathways while also prioritizing safety in consideration of rail served and industrial uses. The pathway network, as depicted in **Figure 19: Open Space & Pathways**, has been carefully designed to ensure connectivity for pedestrians and cyclists in the public realm, while ensuring that public access and recreational opportunities into the Plan Area are limited for logistical and safety reasons.

Policies

- 4.10.0.1** Municipal Reserve (MR) shall be dedicated by the developer at the subdivision stage via “cash-in-lieu” payment in place of land dedication, pursuant to the provisions of the Municipal Government Act.
- 4.10.0.2** Environmental Reserve (MR) shall be dedicated by the developer at the subdivision stage, pursuant to the provisions of the Municipal Government Act.
- 4.10.0.3** Pathways shall not cross or interfere with rail spur rights-of-way or related rail infrastructure and operations.
- 4.10.0.4** Pathways within Environmental Reserve shall be designed as gravel pathways to reduce impact within these areas.



Figure 19: Open Space & Pathways







5 TRANSPORTATION

5.1 Introduction

A Traffic Impact Assessment (TIA) was completed by ISL as part of the Prairie Gateway ASP process. The purpose of this TIA is to evaluate the impacts of the proposed development on the existing road network and to outline areas that may require improvements and upgrades. These results are depicted in **Figure 20: Regional Transportation Network**.

5.2 Regional Transportation Network

The Regional Transportation Network proposed in this CS generally follows the network established in the Prairie Gateway ASP.





5.3 Transit Service Plan

Transit service opportunities for the Plan Area are achieved through the following:

- *Creation of road cross-sections (both Township Road 232 and internal roads) that provide minimum lane widths to support transit vehicles (i.e. 3.5m);*
- *Identification of a proposed transit route, focused along the 232 Design Corridor. As outlined within **Section 4.4.2**, selection of the preferred rail served option will impact the possible transit route within the Plan Area. Each rail served option allows for different areas of the Plan Area to be serviced within a 400m walking radius, due to the prioritization of rail operations (i.e. no crossings of rail spurs). Accordingly, the development is not able to provide transit service within a 400m walking radius for the entirety of the Plan Area. Given the nature of this development, this is not considered a concern for implementation or access to the development; and,*
- *Identification of potential bus stops along the transit route, focusing on provision of bus stops at key locations (e.g. along Township Road 232, and directly adjacent to the Supporting Commercial and Services development parcels).*

Given the presence of two Rail Served options (**Section 4.4.2**), options for transit routing and provision of bus stops has also been provided; see **Figure 21: Transit Service Plan Option 1** and **Figure 22: Transit Service Plan Option 2**.

Refinement and confirmation of a transit service is intended to be determined through ongoing coordination with the Developer, the County, and the City. The objective of the CS and the corresponding Development Concept has been to ensure a transit service can be effectively facilitated at the appropriate time.

Policies

- 5.3.0.1** The Developer, the County, and the City shall coordinate on the preparation and implementation of a transit service.
- 5.3.0.2** Transit stops should include shelter seating for pedestrians, where possible, and where determined appropriate by the transit provider and the County.
- 5.3.0.3** Transit routes and stops shall not create any conflicts with rail infrastructure and rail served development.
- 5.3.0.4** Following selection of a preferred Rail Shadow Plan, transit routes and bus stops should be implemented at time of Subdivision, in accordance with the corresponding Transit Service Plan Option.
- 5.3.0.5** Selection of final bus stop locations shall be determined at time of Subdivision, in coordination with the transit provider.
- 5.3.0.6** Regional/offsite transportation upgrades will be determined as part of each subdivision phase



Figure 20: Regional Transportation Network

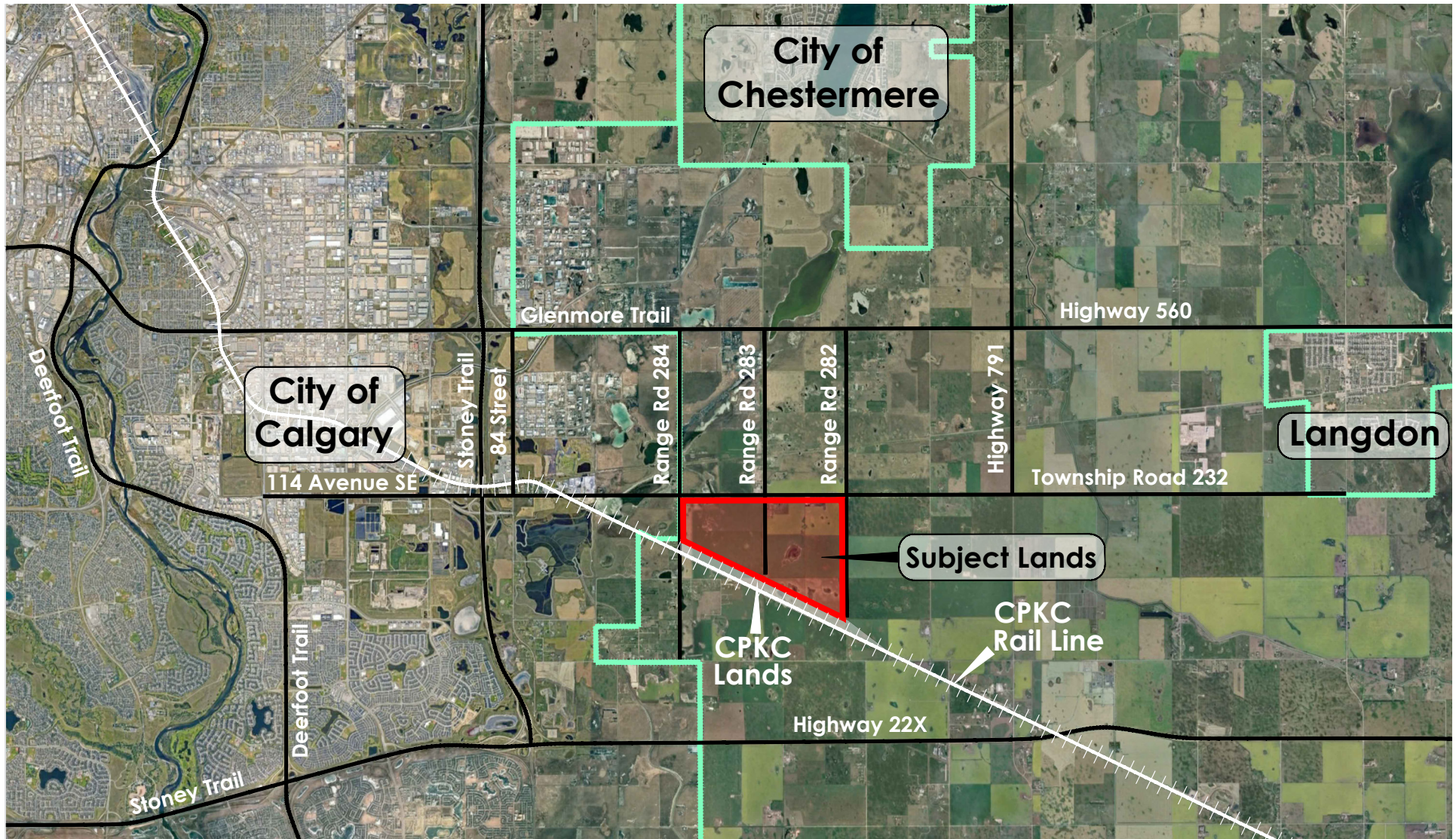




Figure 21: Transit Service Plan Option 1

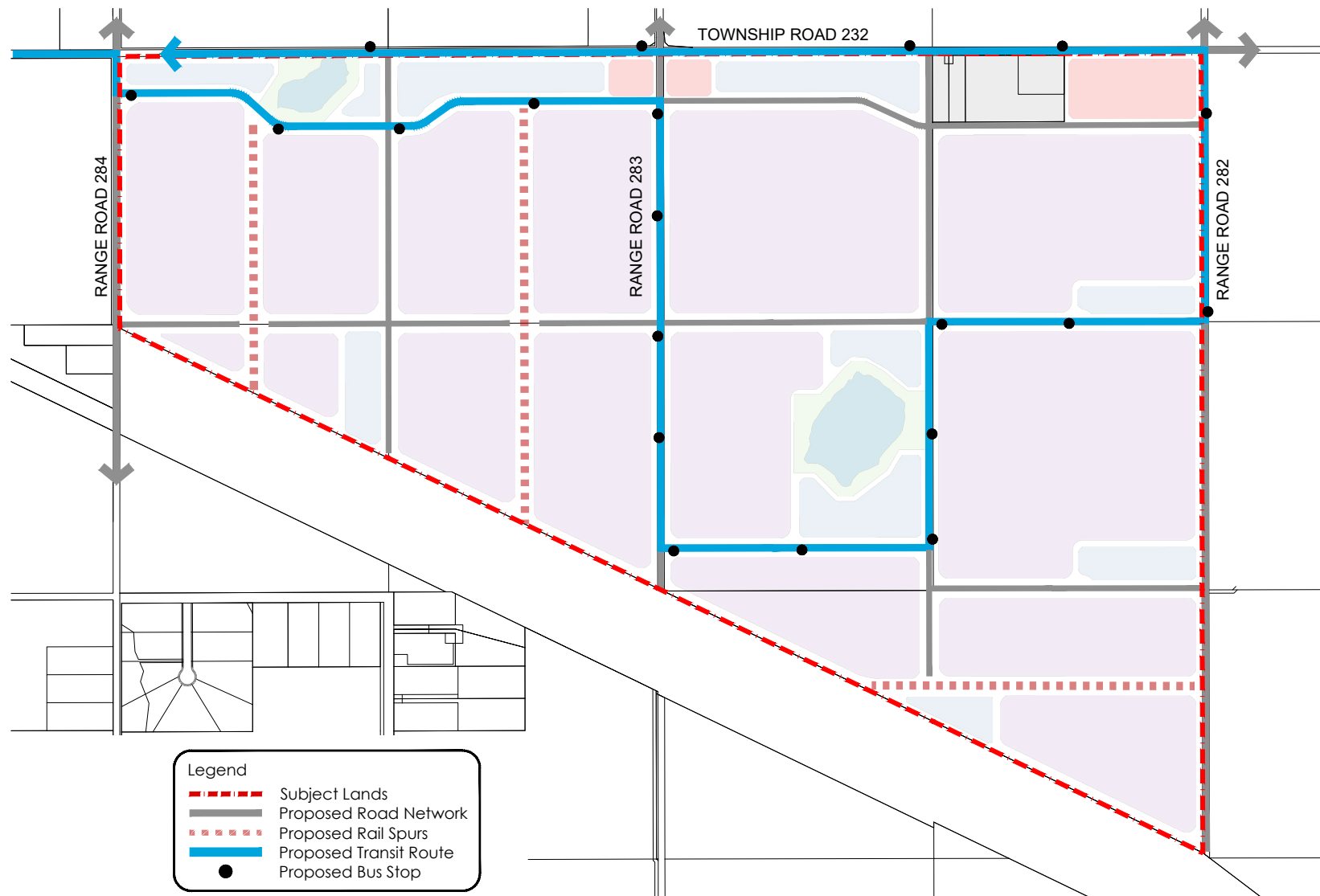
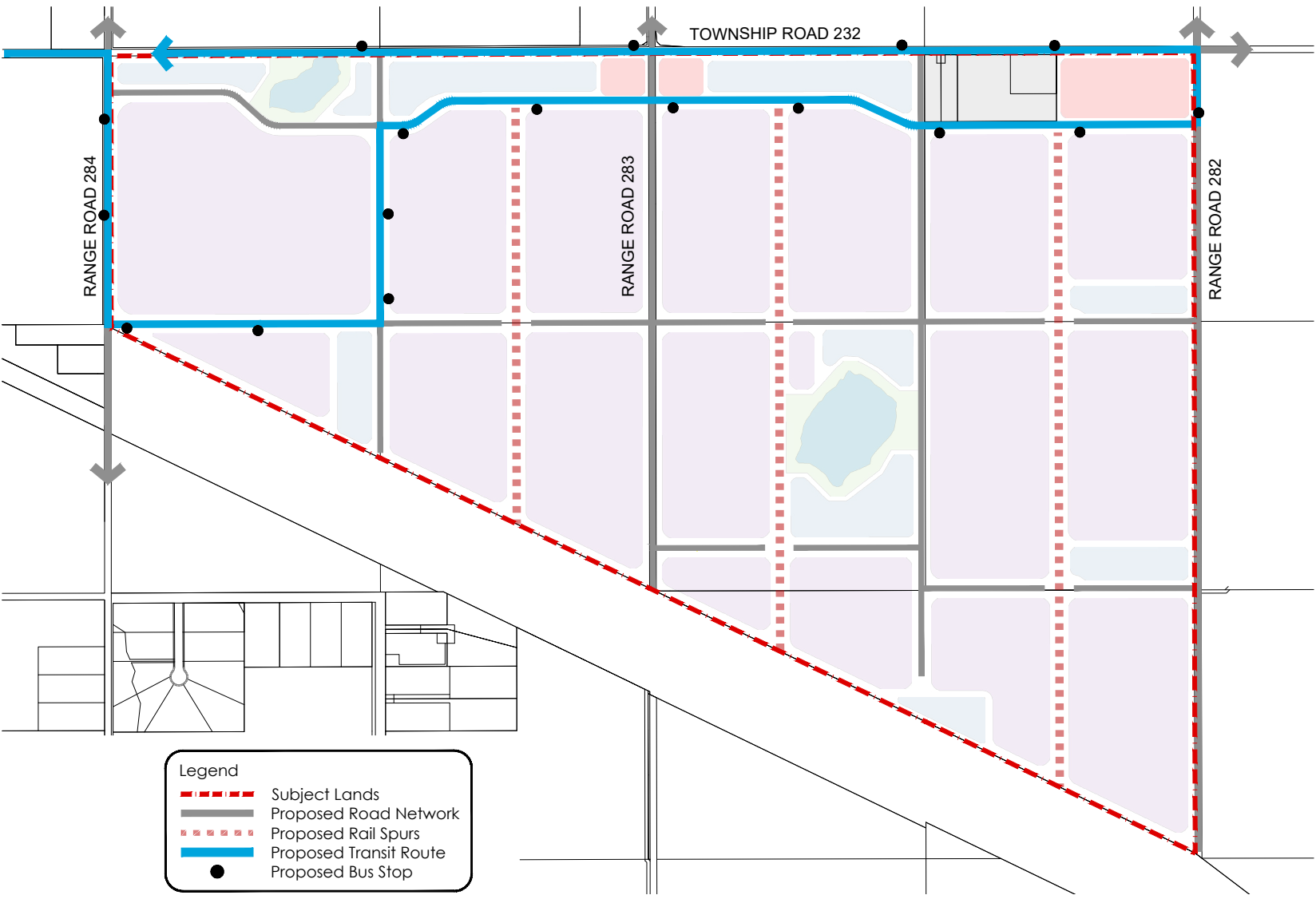




Figure 22: Transit Service Plan Option 2







6 SERVICING

A Servicing Strategy was prepared in coordination with the preparation of the Development Concept, and in alignment with the servicing plans/outcomes within the Prairie Gateway ASP. The Servicing Strategy was made feasible through a collaborative Deal Agreement between SDC, Rocky View County, and the City of Calgary. Each service is discussed in further detail throughout the following subsections.

6.1 Water

The provision, alignment, and capacity of the water distribution system is in general accordance with the Prairie Gateway ASP and ongoing water modeling refinement and coordination with the City of Calgary. The preliminary onsite water supply network/servicing plan, including sizing and location of required water mains and the water reservoir location, is outlined in **Figure 23: Water Servicing Plan**. The water reservoir proposed to service the Plan Area is located in the northwest corner (and proposed first phase) of the development, allowing for early construction and handling of water into the Plan Area (from the City). Design details and the amount of land required to accommodate the reservoir will be determined as part of the first phase of subdivision within the Plan Area.

Water supply is proposed to be provided via connection to the City water network. Specifically:

- *Initial phases are proposed to be serviced by a developer-funded 400mm watermain located on 114th Avenue SE.*
- *A 900mm water feedermain (Feedermain A) will then be constructed to support buildout, connecting to the existing 1200mm Glenmore feedermain (Glenmore PZ), located on Glenmore Trail. The feedermain connection will be aligned along 114th Avenue, and onto 100 Street SE, before connecting to Glenmore Trail*

- *A second 900mm water feedermain (Feedermain B) may be required, based on cumulative demands (etc. City, regional, and Prairie Gateway) that will be assessed at a future stage.*

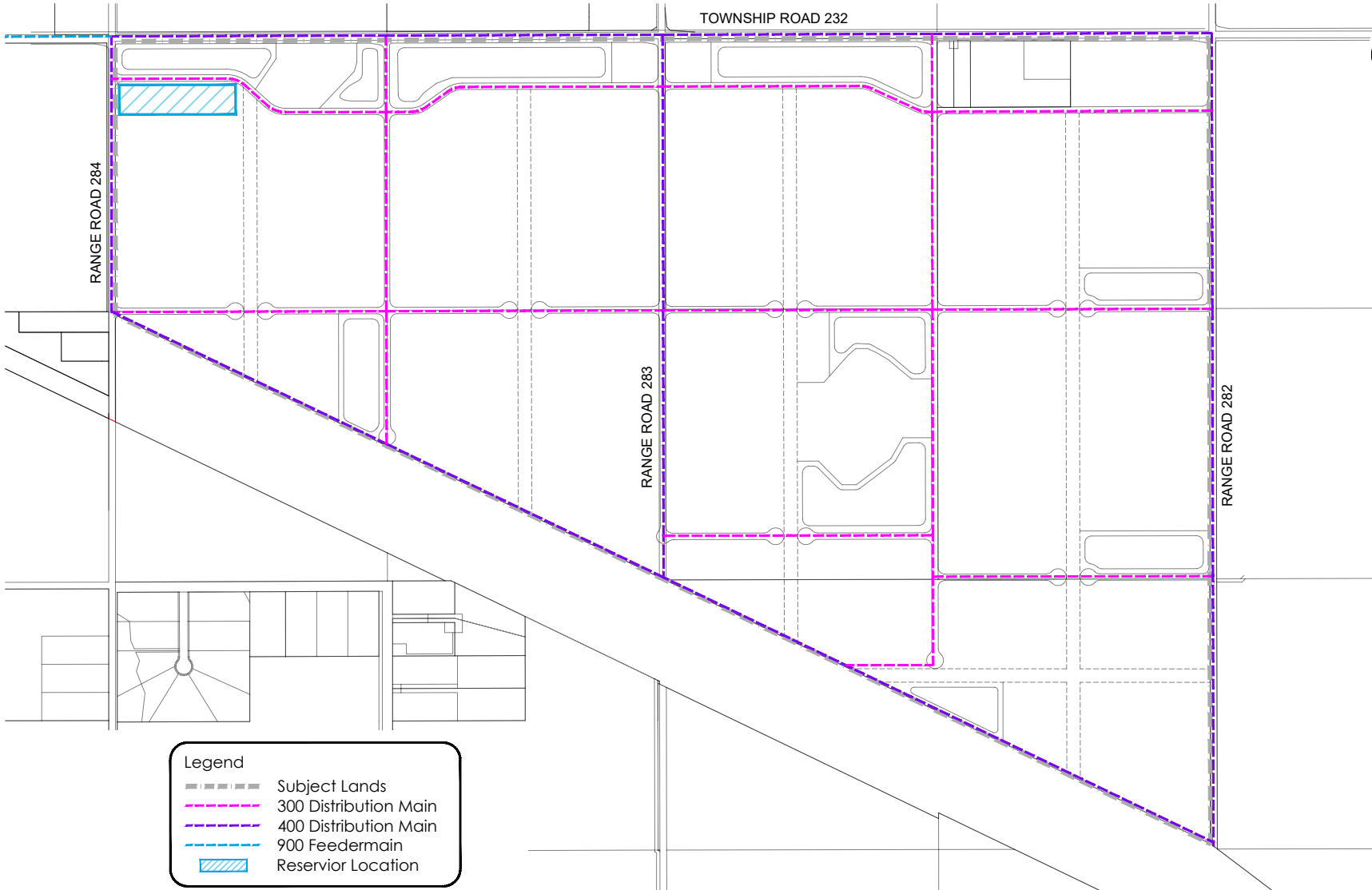
Ongoing coordination with City of Calgary is being undertaken to refine water demands on offsite specifics, and this section will be updated accordingly.

Policies

- 6.1.0.1** The proposed water servicing plan within the Conceptual Scheme Plan Area shall proceed generally as illustrated on **Figure 23: Water Servicing Plan**.
- 6.1.0.2** Utility rights-of-way and easements shall be provided to accommodate water servicing utilities at the subdivision and development permit stage, as deemed necessary.
- 6.1.0.3** Design specifications and land requirements/dedication for the water reservoir shall be determined as part of the first phase of subdivision.
- 6.1.0.4** Potable water shall not be used for irrigation within the Plan Area.
- 6.1.0.5** At Development Permit, the applicant shall provide water analysis that identifies the anticipated water generation of the proposed development to ensure water demands align with overall water supply capacity, in accordance with ASP and Deal Agreement provisions



Figure 23: Water Servicing Plan





6.2 Sanitary

The provision, alignment, and capacity of the sanitary system are in general accordance with the Prairie Gateway ASP and subsequent Determination of Sanitary Sewer Flow and Potable Water Demand Technical Memorandum. A summary of the proposed sanitary servicing network is outlined in **Figure 24: Sanitary Servicing Plan**.

Sanitary sewage from the Plan Area will flow to the City of Calgary's Fish Creek Treatment Plant. A single lift station and multiple force mains along 114th Avenue will convey sewage to a proposed regional lift station located at 100th Street SE and 114th Avenue, which will then direct flows to the Shepard Sanitary Trunk. The regional lift station, and downstream force main will be sized to accommodate both the Plan Area and additional City catchments.

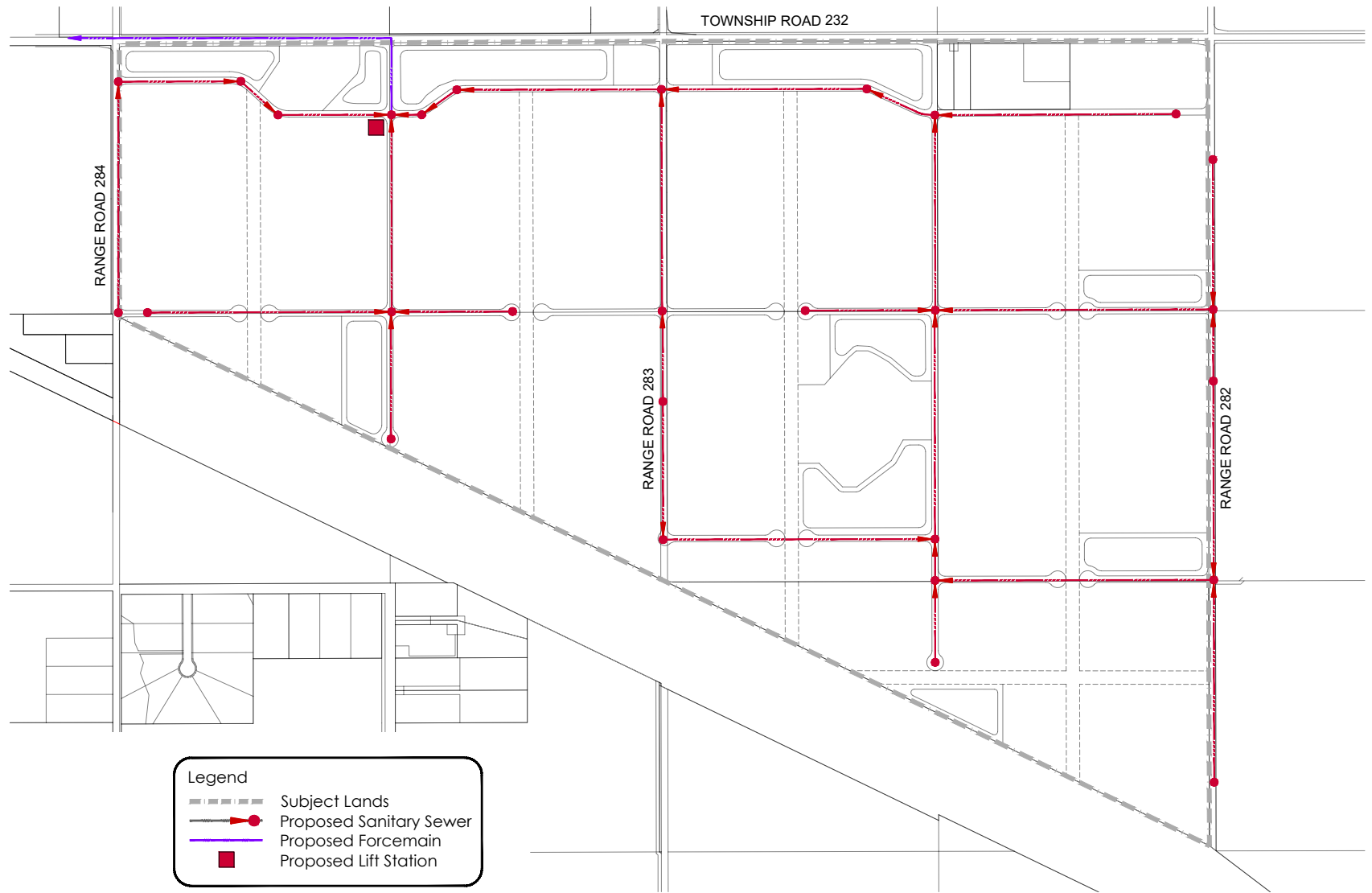
The sanitary sewer infrastructure will be comprised primarily of gravity sanitary sewers and will be located within proposed roadways throughout the Plan Area. A single sanitary lift station is also proposed in the northwest corner of the Plan Area, with sanitary force mains, which will connect to the City sanitary network. The force mains will be sized to accommodate the development staging and be utilized as build out of the development occurs. The intent of the onsite lift station is to convey flow from the Plan Area to the City of Calgary system. Design details and the amount of land required to accommodate the sanitary lift station will be determined as part of the first phase of subdivision within the Plan Area.

Policies

- 6.2.0.1** The proposed sanitary servicing plan within the Conceptual Scheme Plan Area shall proceed generally as illustrated on **Figure 24: Sanitary Servicing Plan**.
- 6.2.0.2** Utility rights-of-way and easements shall be provided to accommodate sanitary servicing utilities at the subdivision and development permit stage, as deemed necessary.
- 6.2.0.3** Design specifications and land requirements/dedication for the sanitary lift station shall be determined as part of the first phase of subdivision.
- 6.2.0.4** At Development Permit, the applicant shall provide sanitary analysis that identifies the anticipated sanitary demands generated by the proposed development to ensure sanitary demands align with overall sanitary capacity, in accordance with ASP and Deal Agreement provisions



Figure 24: Sanitary Servicing Plan





6.3 Stormwater

A Sub-Catchment Master Drainage Plan (SCMDP) has been prepared to support the CS and the Development Concept, in alignment with the Master Drainage Plan (MDP) that was prepared for the ASP. A summary of the stormwater management approach outlined within the SCMDP is provided below:

- The SCMDP proposes the construction of nine (9) stormwater management facilities (ponds) throughout the Plan Area. The pond locations have been developed in conjunction with preliminary grading and servicing, to support the provision of a balanced earthworks program that minimizes the amount of cut and fill required to develop the Plan Area.
- Pond locations represent low-points within the Plan Area and the earthworks program.
- A total of four (4) ponds are proposed along Township Road 232, supporting the provision of a naturalized interface and entry experience along the south side of this roadway, and softening the appearance of the industrial development (by ensuring industrial parcels and buildings are setback significantly from Township Road 232). These ponds will be supplemented with landscaping, trees, and pathways to support the provision of these ponds as amenities for the development, rather than only an infrastructure function.
- Three (3) ponds along Township Road 232 are hydraulically interconnected and provide post-development hydrology to the wetland being retained within the northwest corner. Details of how the storm system ensures matching pre and post-development hydrology for the wetland is outlined in the SCMDP.
- Five (5) other ponds are located throughout the Plan Area, including two ponds (4A and 4B) that are located immediately north and south of the central wetland that is intended to be retained within the Plan Area. Due to the size of the central wetland (~15 hectares), ensuring matching pre and post-development hydrology is challenging and requires a significant portion of stormwater runoff to be diverted to ponds 4A and 4B. Further details are provided in the SCMDP.
- All proposed ponds will be designed with a maintenance pathway around a portion of the pond boundary, which will also support pedestrian access and connectivity.
- Stormwater servicing is characterized by a stormwater servicing network in addition to a stormwater trunk network. Each of these networks are identified in **Figure 25: Stormwater Servicing Network** and **Figure 26: Stormwater Trunk Network**.

Policies

- 6.3.0.1** The proposed stormwater management system within the Conceptual Scheme Plan Area shall proceed generally as illustrated on **Figure 25: Stormwater Servicing Network**.
- 6.3.0.2** The Developer shall ensure the recommendations of the SCMDP are implemented through the subdivision, development, and building permitting stages in conformity with the EPA approvals.



6.3.0.3 All new stormwater management facilities shall be dedicated as Public Utility Lots.

6.3.0.4 Stormwater management facilities shall be designed and constructed in accordance with the approved Master Drainage Plan, Sub-Catchment Master Drainage Plan, CSMI, County Servicing Standards, County Policy and Provincial regulations.

6.3.0.5 As a condition of subdivision, the Developer must provide verification of related municipal, provincial and federal approvals for stormwater infrastructure (e.g. Water Act and EPEA approvals, as relevant).

6.3.0.6 Utility rights-of-way and easements shall be provided to accommodate stormwater utilities at the subdivision and development permit stage, as deemed necessary.

6.3.0.7 Stormwater management facilities located within the 232 Design Corridor shall be designed in accordance with the 232 Design Corridor Plan, including the provision of landscaping, trees, and access.

6.3.0.8 Sump pumps and stormwater drainage systems shall not connect to the wastewater system.





Figure 25: Stormwater Servicing Network

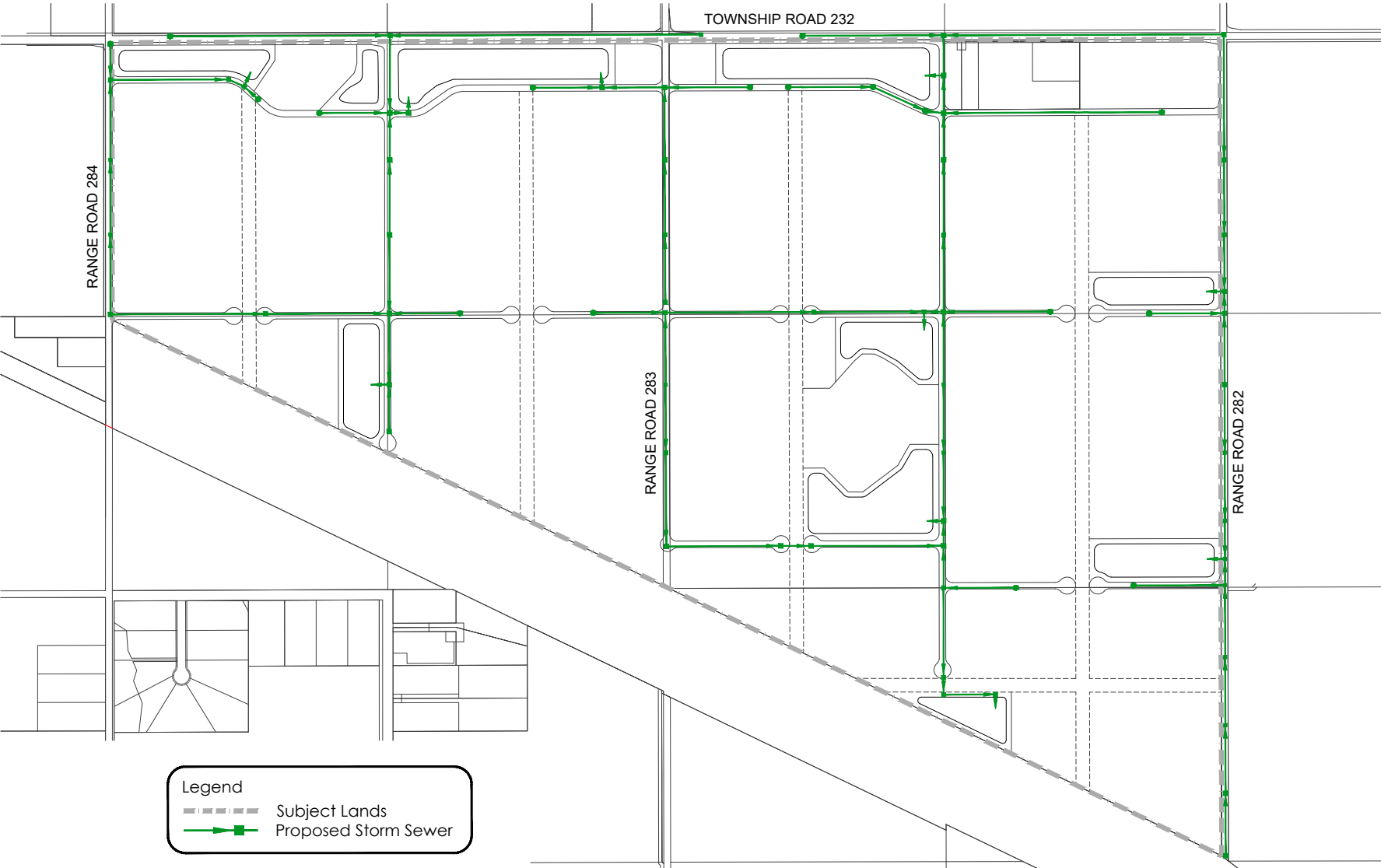
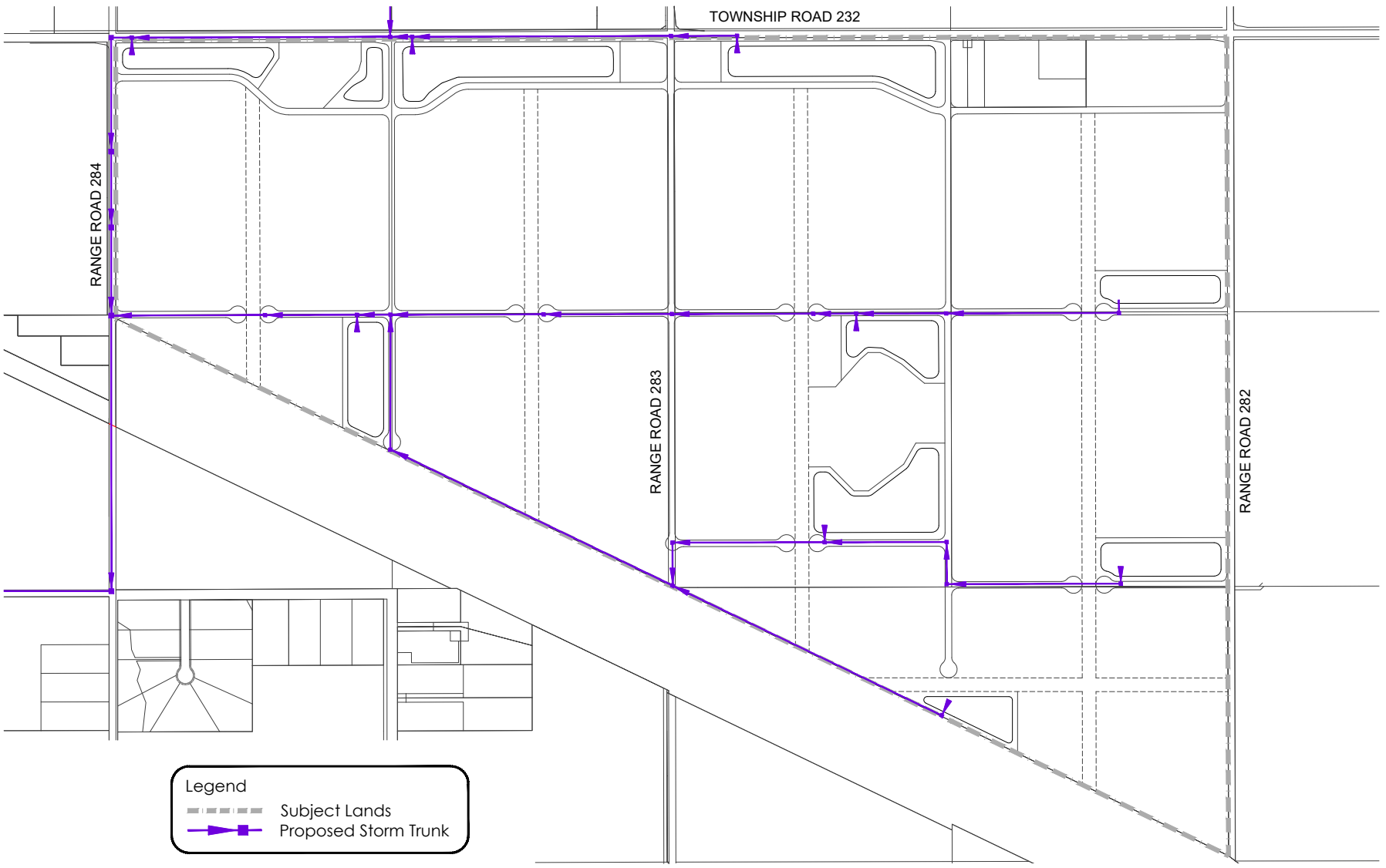




Figure 26: Stormwater Trunk Network





6.4 Shallow Utilities

Shallow utilities such as electrical, natural gas, telephone, and cable services will be extended into the Plan Area within the proposed road ROW with precise alignments to be determined at the subdivision stage.

Telecommunications and natural gas servicing to the development will be routed along the existing 114 Avenue SE from the community of Shepard near 89 Street SE.

ATCO Gas

ATCO will require approximately 5 km of gas line to be constructed for providing natural gas service. ATCO will provide the main service line to the development, then individual service connections will be required for the tenants thereafter. The proposed line assignment for the gas service is identified in proposed road cross-sections identified in **Section 4.7**.

Rogers Communications

Rogers will require approximately 3 km of fibre and associated civil infrastructure to be constructed for providing telecommunication service. Rogers has two options to service the Plan Area from the community of Shepard, both will require coordination with CPKC to cross the existing railway:

1. *New underground services installed in multi-party trench. Utilizing a multi-party trench will reduce both cost and construction timelines, when compared against a single party underground installation.*
2. *New above grade services from the community of Shepard, overhead pole mounted alignment along existing 114 Avenue SE.*

In line with the proposed road cross-sections identified in **Section 4.7**, Option 2 is the preferred servicing approach.

Telus Communications

Telus installation will require approximately 5 km of fibre and associated civil infrastructure to be constructed for providing telecommunication service, in addition to Rogers. Telus has two options to service the Plan Area from the community of Shepard, both will require coordination with CPKC to cross the existing railway:

1. *New underground services installed in multi-party trench. Utilizing a multi-party trench will reduce both cost and construction timelines, when compared against a single party underground installation.*
2. *New above grade services from the community of Shepard, overhead pole mounted alignment along existing 114 Avenue SE.*

In line with the proposed road cross-sections identified in **Section 4.7**, Option 2 is the preferred servicing approach.

Policies

- 6.4.0.1** The alignments for franchise utility installations shall be determined at the subdivision stage, in accordance with County Servicing Standards.
- 6.4.0.2** Communications utility line assignments should be located on the shared power/lighting/communication poles, as outlined in **Section 4.7**.
- 6.4.0.3** Shallow franchise utilities shall be installed and/or financed by the developer at the subdivision stage in consultation with the applicable utility providers.
- 6.4.0.4** Gas utility line assignments should be located within the boulevard of the road right-of-ways as outlined in **Section 4.7**.



6.5 Power Generation Facilities

The Developer is working with power utility providers regarding the provision of power servicing for the Plan Area. Onsite power infrastructure will involve provision of pad-mount transformers of a distribution scale and mounted on concrete pads on the ground (approximately 10x10 feet in size). The amount and location of these transformers will depend on final load requirements, and the final configuration of tenants in each subdivision phase.

A utility-scale transmission substation is required to support the provision of power to the Plan Area. The location of this substation is being confirmed in conjunction with the existing 240kV transmission line located approximately 1.0 kilometre to the north of Plan Area. The substation will be established by AltaLink, in consultation with Fortis, the Developer, and other stakeholders (i.e. the County and the City). Approval of the location and routing (if a transmission line is required) will be required from the Alberta Utilities Commission (AUC).

Policies

- 6.5.0.1** The location, size, and configuration of power infrastructure required within the Plan Area (e.g. pad mounted transformers) shall be determined at time of subdivision.
- 6.5.0.2** The Developer and the County shall coordinate with power utility providers on the provision and timing of power for implementation of the development.
- 6.5.0.3** For utility-scale power generation facilities, the Approving Authority may request additional technical studies and supporting information, including but not limited to, the following:
 - a. Development Impact Statement and Analysis to evaluate the impact of the proposal on adjacent sites from:
 - i. Noise;
 - ii. Visual appearance;
 - iii. Lighting;
 - iv. Odour; and/or
 - v. Dust impacts.
 - b. Impacts and mitigation of the anticipated vapour/steam by-products;
 - c. Biophysical Impact Assessment; and
 - d. Any additional studies to identify safety, health and/or nuisance impacts.



6.6 Protective Services

Police response will be provided by the RCMP as per the Provincial Police Service Agreement, until such time as another policing solution is required or prepared.

6.7 Fire Protection

Fire services will be provided by the County as the primary responder. The County may request the support of The City of Calgary Fire Department if required, as per the Secondary Emergency Response Fire Services Agreement between the County and the City. Primary response may change upon agreement between the County and the City.

6.8 Solid Waste & Recycling

The Developer/end users are responsible for solid waste management at various stages of development, with the Developer responsible for waste management during subdivision construction, and the end users/lot owners responsible for providing their own solid waste services to support their operations. The Langdon Transfer Site is identified as the appropriate waste collection station to service the Shepard Logistics Centre.

Policies

- 6.8.0.1** Solid waste management shall be guided by the County's Solid Waste Servicing Strategy.
- 6.8.0.2** The Developer shall be responsible for the management and disposal of solid waste generated through all stages of construction in accordance with County standards.
- 6.8.0.3** The Developer should encourage waste minimization and waste diversion practices in the Plan Area with a diversion target of 50%.
- 6.8.0.4** The Developer shall prepare a Waste Management Plan at the subdivision stage, to the satisfaction of the County.
- 6.8.0.5** Businesses shall be responsible for providing their own solid waste services.
- 6.8.0.6** Storage of garbage and waste material shall be in weatherproof and animal proof containers located within buildings or adjacent to the side or rear of buildings, and those areas shall be screened from view by all adjacent properties and public thoroughfares. This shall be enforced through the development permitting stages.





7 DEVELOPMENT GUIDELINES

This section outlines development guidelines for subsequent Subdivision, Development Permit (DP), and Building Permit (BP) applications submitted with the County within the Plan Area, to ensure an attractive and functional development. These guidelines are provided in addition to applicable County requirements, including the County's Land Use Bylaw, and the Commercial, Office, and Industrial Guidelines.

General policies are identified below, followed by policies for specific design components, including landscaping, interfaces, lighting, signage, fencing, and site and building design.

Policies

- 7.0.0.1** Prior to Subdivision approval, all aspects relating to landscaping, signage, lighting, and fencing or screening on publicly owned lands, including within road rights-of-way, shall be assessed to ensure they comply with the policies of this CS.
- 7.0.0.2** Prior to Development Permit and/or Building Permit approval, all aspects relating to landscaping, signage, parking, lighting, and fencing or screening on privately owned lands, such as within setbacks, will be evaluated to ensure they implement the policies of this Plan.
- 7.0.0.3** Where one or more of the policies within this Local Plan, the 232 Design Corridor Plan, or the DC District associated with this Plan apply and requirements conflict, the greater requirement shall apply.

7.1 Township Road 232 Design Corridor

The Prairie Gateway ASP identifies the intermunicipal entranceway of Township Road 232 as the 232 Design Corridor. Requirements for this corridor are addressed within the 232 Design Corridor Plan. This CS has been prepared in conjunction with the 232 Design Corridor Plan to ensure aligned outcomes and requirements for future development.

Policies

- 7.1.0.1** The policies of the 232 Design Corridor Plan shall apply to the land located within 200 metres south of the Township Road 232 right-of-way, as generally shown within the CS Development Concept.





7.2 Interfaces & Landscaping Plan

This section integrates identification and design response to the various interfaces within the Plan Area with a corresponding Landscape Plan. General policies are provided for types of interfaces (e.g. residential and agricultural), followed by specific guidelines for specific interfaces within the Plan Area, which are summarized in **Figure 27 – Plan Area Interfaces**.

General interfaces relevant to the Plan Area include an intermunicipal boundary (County-City) along the western border, as well as existing residential, agricultural, and industrial uses. The Plan Area also shares its southern boundary with the CPKC rail line and associated railway lands. Uses within interface area setbacks may include landscaping, landscaped stormwater ponds, and natural wetlands.

7.2.1 County-City

The intermunicipal boundary for the Plan Area is represented by Range Road 284, which will serve both the Plan Area as well as future industrial development within City lands to the west (facilitated by the Shepard Industrial ASP). Because the corresponding ASPs facilitate an industrial to industrial interface, significant design and interface guidelines are not considered necessary given the similar land use outcomes. With that said, development is required to comply with relevant IDC policies.

Policies

- 7.2.1.1** The Rocky View County/City of Calgary Intermunicipal Development Plan interface planning principles should be addressed in any Redesignation, Subdivision, or Development Permit application(s) along the intermunicipal boundary.



7.2.2 Residential

The Plan Area involves a residential interface for a small portion of Range Road 284. As outlined within the Prairie Gateway ASP, a 50 metre building setback is required for this residential use, which has been identified on the Development Concept in **Section 4.0**. In addition to the provision of this 50 metre setback, design guidelines are identified for building design and landscaping below.

7.2.3 Agricultural

Existing agricultural uses exist immediately east of the Plan Area, sharing a boundary with Range Road 282. The majority of these lands are designated Direct Control (DC) District 166, to support the development of solar farms. However, DC-166 only extends to the southwest corner of Section 11, creating an approximately 800 metre segment of industrial-agricultural interface along Range Road 282. Design guidelines for this interface are specifically addressed in **Section 7.2.5**, and specifically, Interface Condition 10.

Policies

7.2.2.1 Buildings on lands adjacent to existing residential uses shall be setback a minimum of 50 metres from the adjacent property line.

7.2.2.2 A minimum 6.0m landscaping buffer, including mass plantings and trees, shall be provided along the entirety of the residential interface to minimize the visual impact of the non-residential buildings. Screening/fencing should also be considered to support an appropriate interface to the existing residential use.



7.2.4 Natural Areas

As outlined within the Development Concept in **Section 4**, two wetlands will be retained as ER and incorporated into the development. Detailed outcomes and recommendations of these features is identified within the BIA and the SCMDP, with detailed design to include the preparation of landscaping plans. In addition to policies identified in **Section 4.8** the following policies are identified for these natural features.

Policies

7.2.4.1 At the Development Permit stage, Development sharing a boundary to an area dedicated as Environmental Reserve should:

- a. Provide a minimum 6.0 metre landscaped setback.
- b. Provide direct pedestrian connections to pathways within these ER areas, where practical.
- c. Design outdoor amenity space for employees to be oriented and connected to these ER areas.
- d. Limit the amount of parking and storage areas located along the shared boundary.
- e. Ensure stormwater runoff is appropriately controlled to avoid excess discharge into the ER area.
- f. Limit light pollution along shared boundaries to ER areas through lighting design/controls.

7.2.4.2 Roadways sharing a boundary to an area dedicated as Environmental Reserve should provide connections from pathways included within boulevard rights-of-way to pathways within these reserve areas, where practical.





7.2.5 Specific Interfaces & Landscape Plan

The Specific Interfaces and Landscape Plan identifies the proposed design and landscaping treatments for the Plan Area, including specific interfaces within the Plan Area, identified in **Figure 27: Plan Area Interfaces**. Landscaping will contribute to the overall character, amenity, and implementation of the development.

In meeting the requirements of ASP Policies 12.09 and 12.20, the Landscaping Plan for this CS consists of a Landscape Intent Statement that communicates the vision, goals and guidelines for landscape design for the development, followed by landscaping requirements for specific Plan Area Interfaces. To support the implementation of this Landscaping Plan, a Landscape Design and Implementation Plan is proposed to be prepared and submitted with the County as part of the first phase of subdivision for the Plan Area.

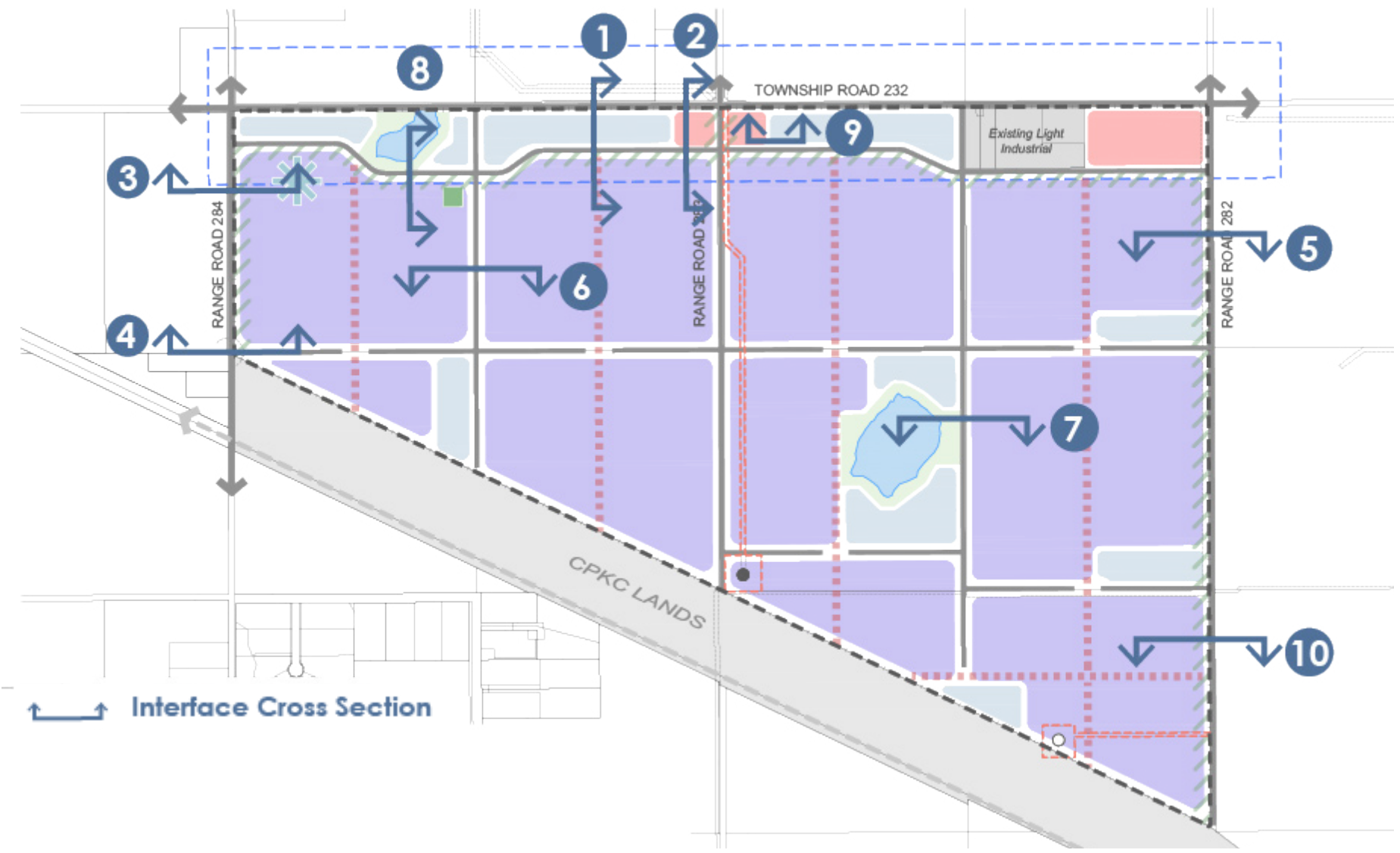
Landscape Intent

The landscape provision and design for the development aims to create a practical, aesthetic, and sustainable environment that integrates functional landscape elements with aesthetically pleasing streetscapes and public spaces. By prioritizing native and low-maintenance plant species, tailored landscaping requirements, and seamless transitions to natural areas, the plan supports practical design and maintenance outcomes.





Figure 27: Plan Area Interfaces





Key objectives of the Landscape Plan are as follows:



Public vs Private Landscaping

Landscaping requirements are balanced between public and private areas, with landscaping in public areas focused on stormwater facilities (ponds) and Environmental Reserves (ER) required for the retained wetlands. These areas will be designed to support ecological functions, provide habitat for local wildlife, and offer aesthetic value to the Plan Area.



Enhanced Landscaping for Compatibility

Enhanced landscaping will be applied at specific interfaces to support compatibility and soften the appearance of industrial uses. This includes using planting and fencing/screening to create visual and noise buffers between industrial activities and adjacent properties or natural areas.



Stormwater Facilities

Stormwater facilities will be designed within the 232 Design Corridor to serve as amenities, not just utilities. This includes incorporating naturalized planting, walking paths, and seating areas (where practical) around ponds to create attractive, multi-functional spaces that manage stormwater while providing recreational opportunities.



232 Design Corridor

Enhanced landscaping will be implemented within the 232 Design Corridor to create a visually appealing and environmentally sensitive transition area. This will involve the use of native plants, decorative elements, and strategic planting to enhance the corridor's aesthetic and ecological value.



Roadway Landscaping

Landscaping along roadways will be provided within the front setbacks of private parcels. This approach minimizes maintenance requirements for the County and ensures a continuous landscaped interface that is not disrupted by utility lines within public boulevards. The design will include a mix of trees, shrubs, and ground cover to create a continuous streetscape outcome.



Transition to Natural Areas

Ensure a seamless transition from industrial parcels to natural areas by using gradual changes in planting density and species composition. This approach will support wildlife movement and create a more natural, integrated landscape.



Native Plant/Low Maintenance Species

Use native and low-maintenance plants in all landscaping efforts to support local ecosystems, reduce maintenance requirements, and enhance the landscape's resilience to local climatic conditions. Native plants will be selected for their adaptability, ecological benefits, and low water requirements.

The following design guidelines apply in supporting the implementation of Landscape Plan objectives identified above:

A. Landscape requirements shall meet the minimum landscaping requirements of the County's Land Use Bylaw, or greater, where specifically identified in this CS.

B. Stormwater ponds within the 232 Design Corridor should be designed with naturalized edges and use native plants.

C. Passive recreation opportunities should be incorporated into stormwater ponds within the 232 Design Corridor, through the provision of walking paths and seating areas around a portion of these facilities.

D. A selection of native, low-maintenance trees, plants, shrubs, and ground covers should be identified prior to approval of the first phase of subdivision, in order to create consistency in the provision of landscaping throughout the Plan Area. These native, low-maintenance landscaping species should be drought-tolerant and adapted to local soil conditions.

E. A higher density of landscaping (trees and shrubs) should be implemented within the 232 Design Corridor, within the setback to the existing residential interface (along a portion of Range Road 284), and along the interface to agricultural uses (along a portion of Range Road 282).

F. Landscaping along the 232 Design Corridor should incorporate decorative elements such as boulders, mulch, and entry features to enhance the aesthetic value.

G. Landscaping should gradually transition from formal landscaping near buildings to more naturalized planting within Environmental Reserve.

H. Detailed design drawings for stormwater ponds and Environmental Reserve areas should designate clear, safe pathways for public access that connect adjacent development parcels with these features.

I. Detailed landscape plans for areas of public dedication (e.g. stormwater ponds and Environmental Reserve areas) that are provided at the time of subdivision should be supported with a maintenance schedule that identifies maintenance requirements, including inspections, watering, soil management, hardscape maintenance, and seasonal requirements.

Plan Area interfaces are specifically identified and addressed below in **Sections 7.2.5.1 to 7.2.5.10** of this report.



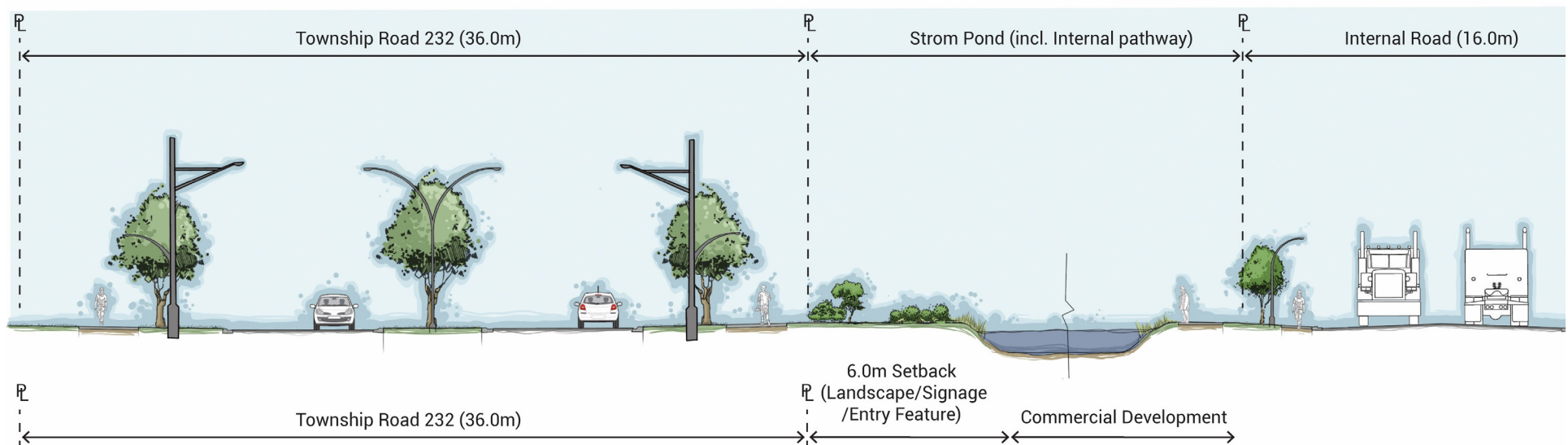


7.2.5.1 Interface Condition 1: 232 Design Corridor – Storm Pond and Township Road 232

Landscaping will be provided along the entirety of Township Road 232, both within the road right-of-way itself (i.e. street tree line assignments and stormwater facilities) and within development parcels, in compliance with the 232 Design Corridor Plan. This specific condition addresses the interface between Township Road 232 and the adjacent stormwater facilities.

Trees will be provided within the boulevard of the road right-of-way, with the adjacent stormwater facilities providing sufficient room (between the pond high water level and the road right-of-way) for sod and selected plantings and shrubs. Detailed design drawings for the stormwater facilities will include a detailed landscape plan, including areas for group plantings. No landscape berms are proposed/considered necessary for this interface condition. Given the presence of a Regional Pathway along Township Road 232, a separate pathway along the northern side of the stormwater facilities is not proposed. Furthermore, it is proposed that no signage be incorporated within this interface.

Figure 28: Cross-section for Interface Condition 1



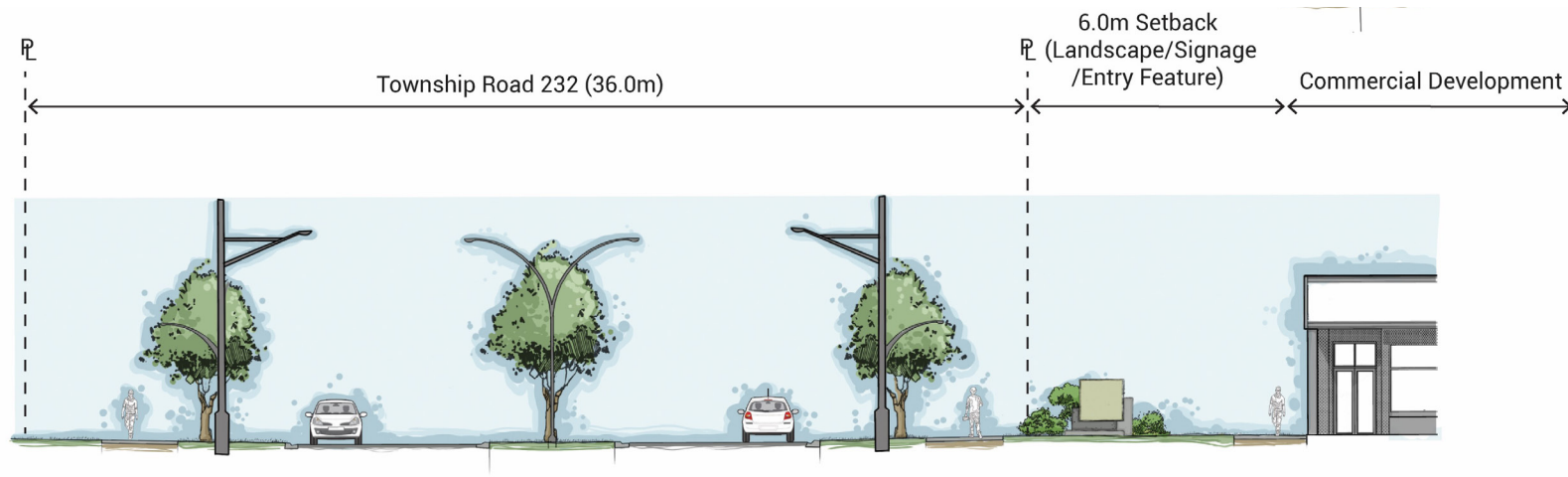


7.2.5.2 Interface Condition 2: 232 Design Corridor – Supporting Commercial and Services Development Parcels and Township Road 232

The other predominant interface along Township Road 232 is with the Supporting Commercial & Services development parcels. These parcels will have a minimum 6.0 metre setback and a maximum 19.0 metre setback.

The 6.0 metre setback will be landscaped and also allow for the provision of signage and entry features. A maximum setback is proposed to ensure buildings address the 232 Design Corridor and avoid large

Figure 29: Cross-section for Condition 2



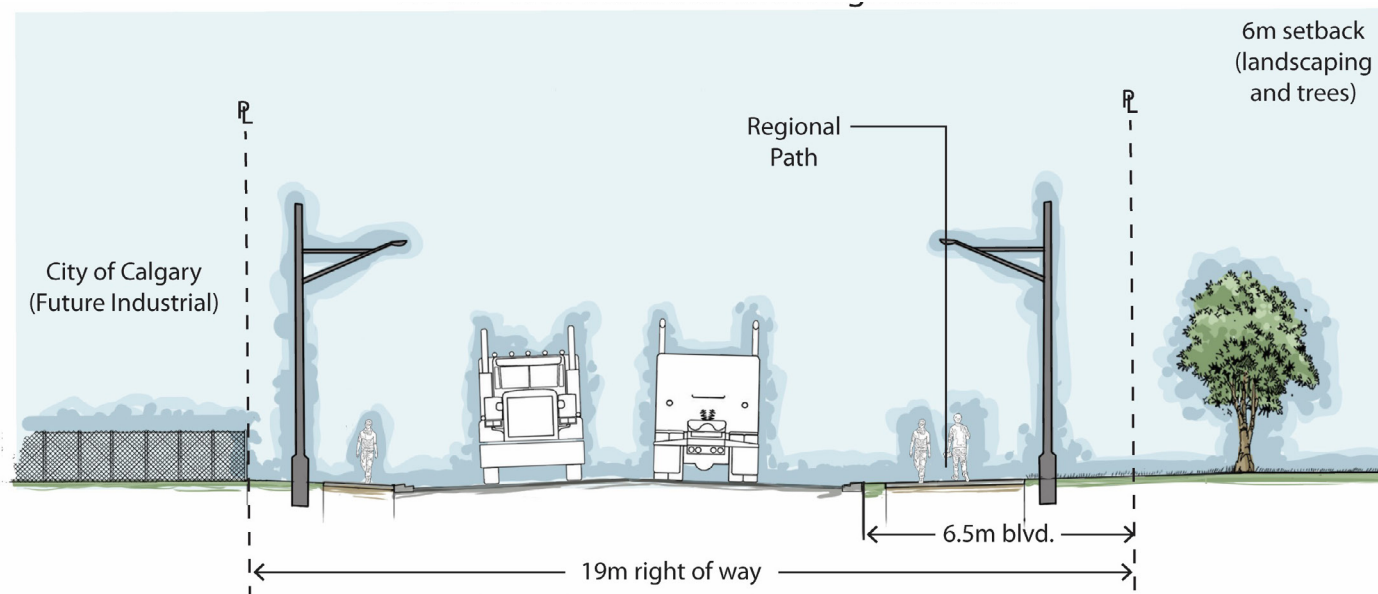


7.2.5.3 Interface Condition 3: Range Road 284 (Future City of Calgary Industrial) and the Plan Area

This interface is along the majority of the western boundary of the Plan Area (Range Road 284), adjacent to a future industrial area within the City. Given the compatibility of uses along this roadway, significant design and interface requirements are not considered necessary. A 19.0 metre road ROW cross-section is proposed, with Regional Pathway located within the eastern boulevard. A 6.0 metre landscape buffer is proposed within the front setback of the development parcels within the Plan Area, with trees to be provided within that landscape buffer at the minimum rate specified within the County LUB.

To soften the appearance of industrial buildings along this roadway, a tiered building setback is proposed, with portions of buildings higher than 20.0 metres requiring an additional 15 metre setback from the front boundary. Should areas of the development parcel be used for storage, appropriate screening, in the form of structures, fencing, additional landscaping, or a combination of these, must be installed.

Figure 30: Cross-section for Interface Condition 3



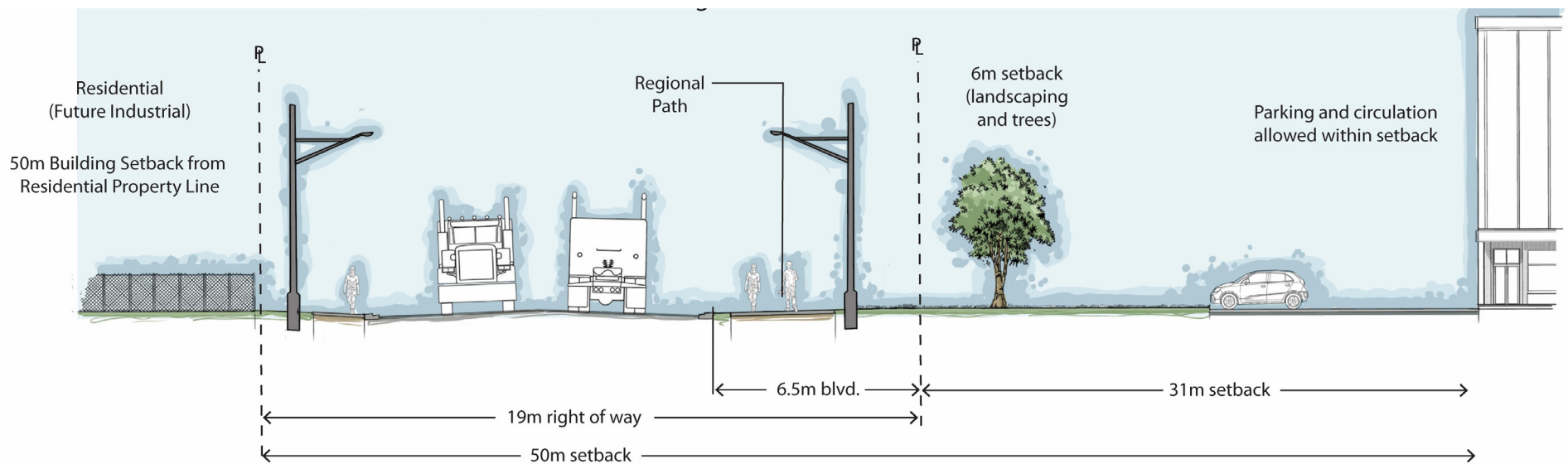


7.2.5.4 Interface Condition 4: Range Road 284 (Existing Residential) and the Plan Area

The southwest corner of the plan area along Range Road 284 is adjacent to existing residential uses. In accordance with the Prairie Gateway ASP, a 50 m setback applies to this interface condition, measured from the non-residential development to the non-residential property line.

Higher intensity landscaping (exceeding the County LUB minimum), within a 6.0m setback is proposed along this Interface Condition, measured from the non-residential development to the non-residential property line. No landscaping berms are considered necessary given the extent of the building setback. For the remainder of the 50 metre setback, only parking and circulation areas are allowed (i.e. no outdoor storage areas).

Figure 31: Cross-section for Interface Condition 4





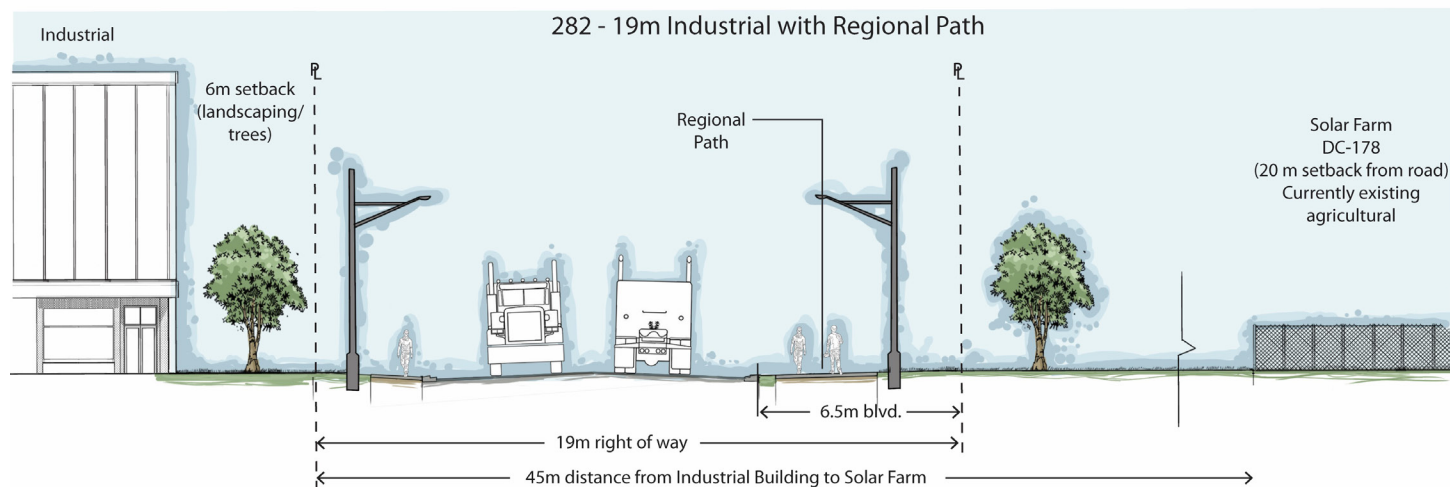
7.2.5.5 Interface Condition 5: Range Road 282 (DC-166 Future Solar Farm) and the Plan Area

This interface is along the majority of the eastern boundary of the Plan Area (Range Road 282), adjacent to a future solar farm. Given the compatibility of uses along this roadway, and the minimum 20.0 metre setback identified within DC-166 for the solar farm, significant design and interface requirements are not considered necessary. A 19.0 metre road ROW cross-section is proposed, with a Regional Pathway located within the eastern boulevard. This Regional Pathway is not intended to be constructed upfront with the construction of Range Road 282. Instead, the Developer proposes that the Regional Pathway be provided in conjunction with the timing of the solar farm development (or alternative development of the DC-166 lands).

A 6.0 metre landscape buffer is proposed within the front setback of the development parcels within the Plan Area, with trees to be provided within that landscape buffer at the minimum rate specified within the County LUB.

To soften the appearance of industrial buildings along this roadway, a tiered building setback is proposed, with portions of buildings higher than 20.0 metres requiring an additional 15 metre setback from the front boundary. Should areas of the development parcel be used for storage, appropriate screening, in the form of structures, fencing, additional landscaping, or a combination of these, must be installed.

Figure 32: Cross-section for Interface Condition 5





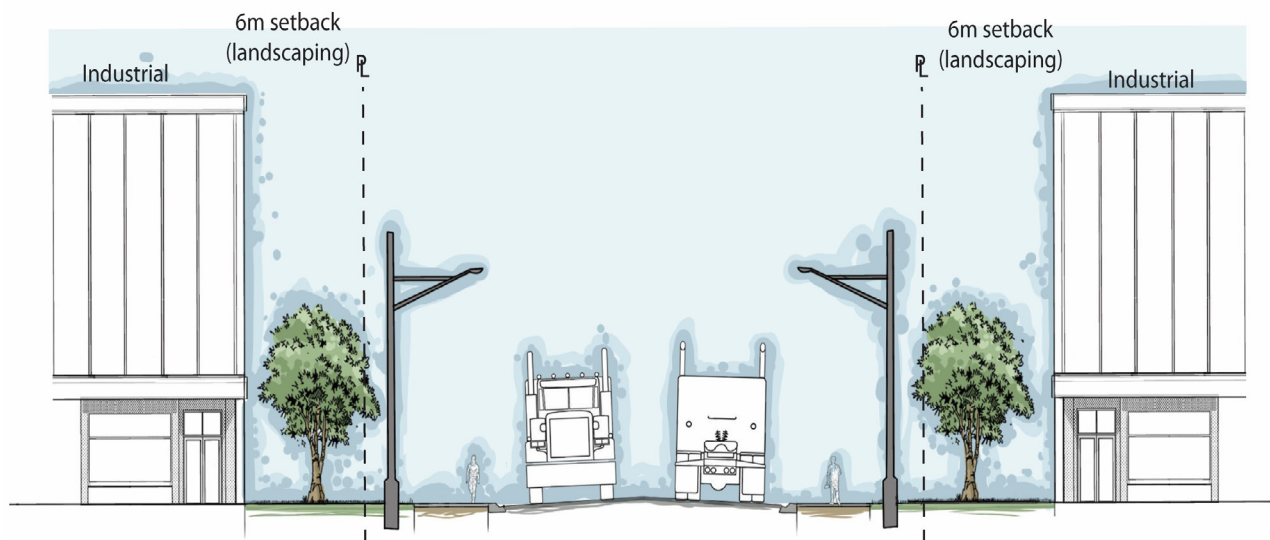
7.2.5.6 Interface Condition 6: Internal Roadways for Rail Served and Non-rail Served Development

Both, rail and non-rail served development are proposed to provide 6.0 metre landscaped setbacks along all internal roadways, including trees at a minimum intensity in compliance with the County LUB.

A tiered front building setback is proposed to soften the appearance of buildings from these internal roadways, with portions of buildings higher than 20.0 metres requiring additional 15 metre setbacks from the front boundary.

Should areas of the development parcel be used for storage, appropriate screening, in the form of structures, fencing, additional landscaping, or a combination of these, must be installed.

Figure 33: Cross-section for Interface Condition 6



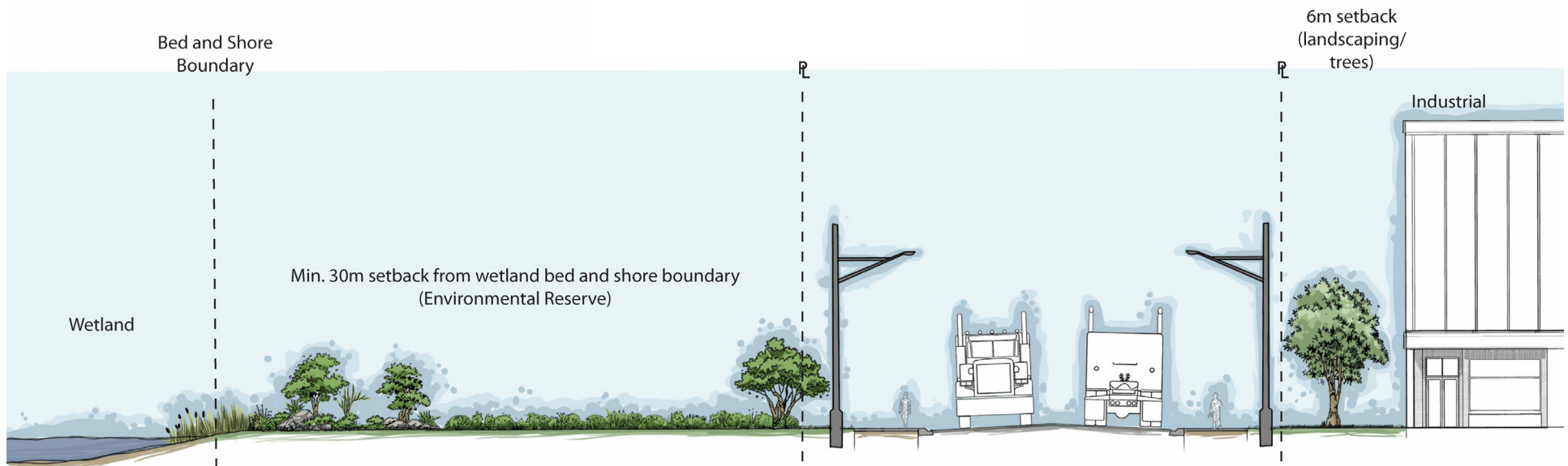


7.2.5.7 Interface Condition 7: Internal Roadways and Retained Wetlands

The Development Concept involves two separate instances of retained wetlands adjacent to an internal roadway. The wetlands being retained require a minimum 30 metre setback from the wetland bed and shore boundary, which will be supplemented with naturalized landscaping to support and protect ecological function.

Pathways may be incorporated within the 30 metre setback to provide passive recreation opportunities. No additional design and interface conditions are proposed for industrial development on the other side of the road, outside of the standard 6.0 metre landscape front setback.

Figure 34: Cross-section for Interface Condition 7





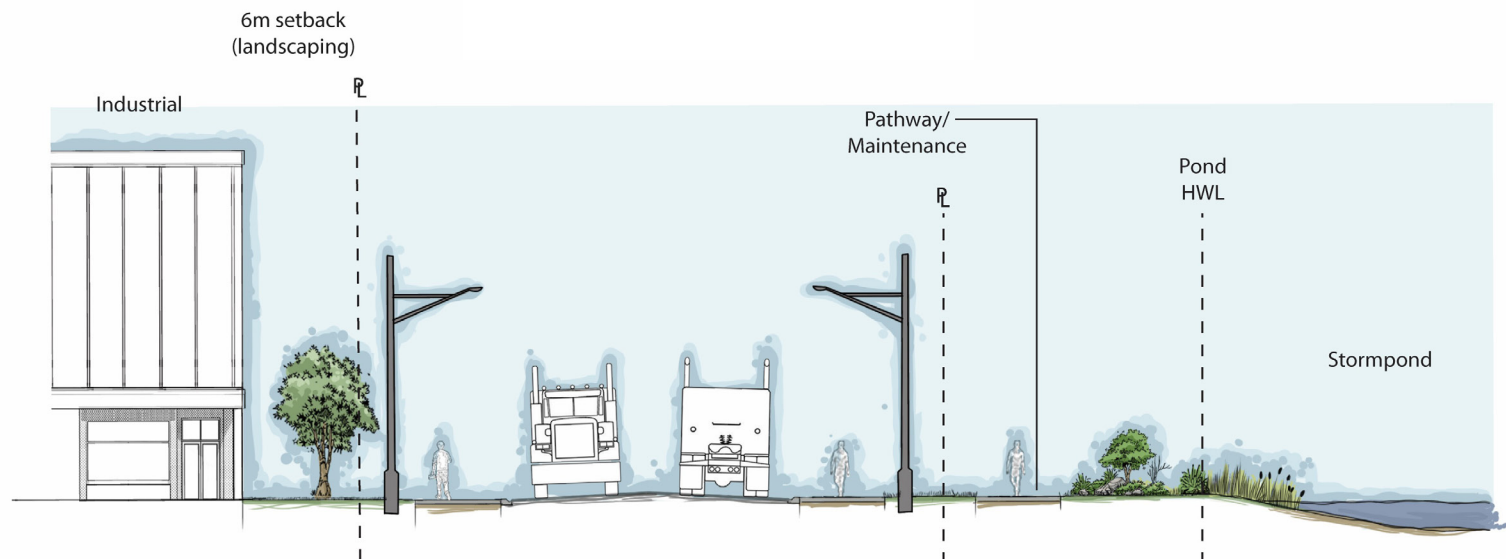
7.2.5.8 Interface Condition 8: Storm Pond and Internal Roadway

The Development Concept involves several instances of this interface with stormwater ponds adjacent to internal roadways. Stormwater ponds are designed to provide sufficient room (between the pond high water level and the road right-of-way) for sod and selected plantings and shrubs, as well as an internal pathway that supports maintenance and pedestrian access.

Detailed design drawings for the stormwater facilities will include a detailed landscape plan, including areas for group plantings, and the internal pathway (including proposed connections to the sidewalk) within the adjacent road right-of-way. No landscape berms are proposed/considered necessary for this interface condition.

No additional design and interface conditions are proposed for industrial development on the other side of the road, outside of the standard 6.0 metre landscape front setback.

Figure 35: Cross-section for Interface Condition 8



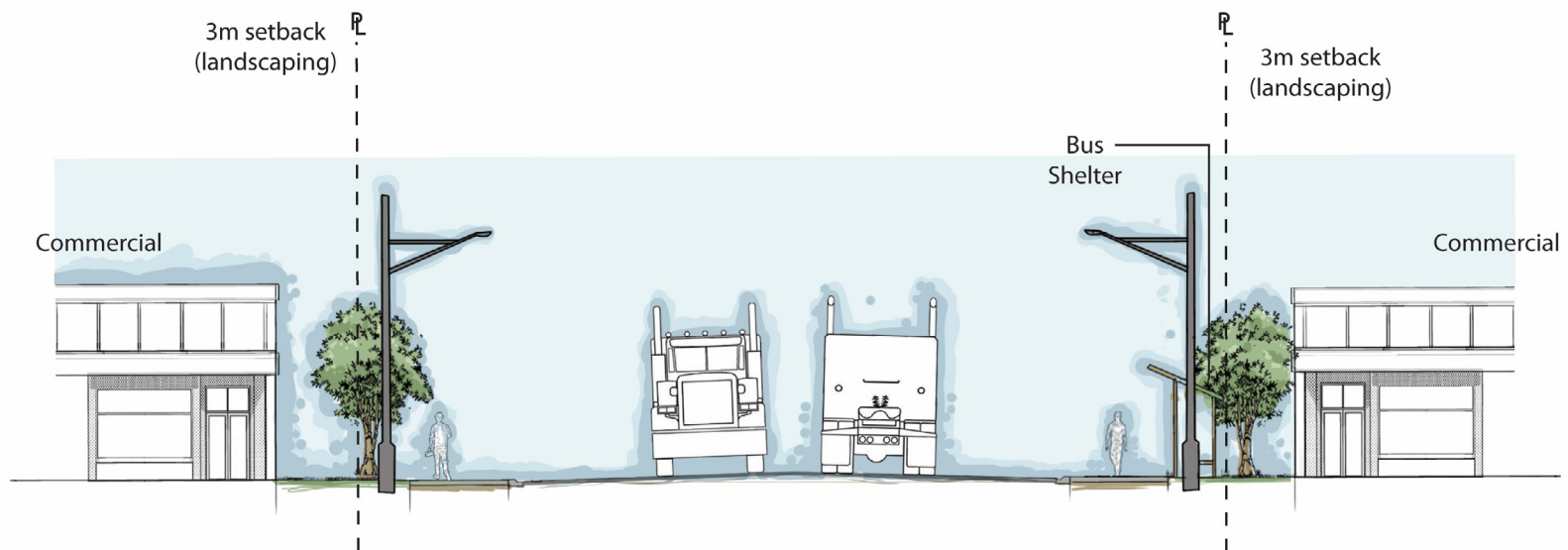


7.2.5.9 Interface Condition 9: 22m Modified Industrial

This interface is located at the main entry point to the development, along Range Road 283, and between the two proposed Supporting Commercial & Services development parcels.

The proposed road cross-section involves four lanes (two for each direction), with adjacent commercial development on both sides of the road intended to address this roadway, through a combination of landscaping and a reduced 3.0 metre setback (creating a built form edge).

Figure 36: Cross-section for Interface Condition 9



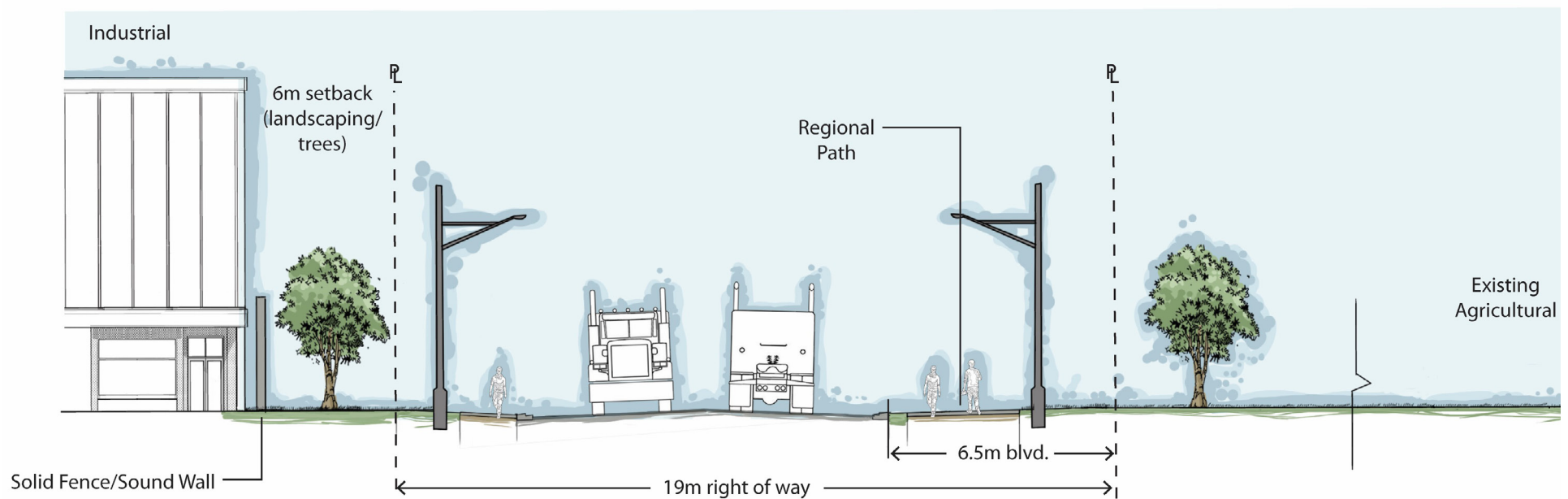


7.2.5.10 Interface Condition 10: Agriculture

This interface represents a ~800m segment at the southeast corner of the Plan Area, along Range Road 282. The proposed Interface Condition maintains the same road cross-section and landscaping as the other segment of Range Road 282 (Condition #5), although adds the provision of a solid fence/acoustic barrier between proposed building(s) and the landscape buffer).

In addition to the physical design outcomes, the Conceptual Scheme also proposes policies on lighting (7.2.5.8) and uses (7.2.5.9) to facilitate a compatible use interface.

Figure 37: Cross-section for Interface Condition 10





Policies

7.2.5.1 A Landscape Design and Implementation Plan shall be prepared and submitted with the County as part of the first phase of subdivision for the Plan Area, in accordance with this Landscape Plan. This Plan shall include:

- a. Detailed landscape design drawings for all areas of public dedication within the phase
- b. Specification for plant species to be incorporated within the Plan Area;
- c. Selected species should have low or no maintenance requirements, and be drought tolerant;
- d. Identification of the methods of irrigation and maintenance for landscaped areas;
- e. Detailed design for areas of mass plantings: and
- f. Include details of hardscape landscaping items, including non-plant elements, such as pathways, patios, retaining walls, entry features, water features, and other structures. This section should describe the materials, dimensions, and placement of these features

g. A Maintenance Strategy that identifies requirements for the ongoing care and maintenance of the landscape, including pruning, fertilization, pest management, and irrigation schedules (where relevant)

h. An estimated budget for the landscape project, including costs for plants, materials, labor, and any additional expenses

7.2.5.2 At time of subdivision, landscaping within areas of public dedication shall be provided in compliance with this Landscaping Plan and the 232 Design Corridor Plan (where relevant).

7.2.5.3 At time of Development Permit, landscaping within private development parcels shall be provided in compliance with this Landscaping Plan and the 232 Design Corridor Plan (where relevant).

7.2.5.4 All landscaping and maintenance within private development parcels shall be the responsibility of the owner/developer.

7.2.5.5 All maintenance of landscaping within public boulevards (i.e. sod) shall be the responsibility of the adjacent owner/developer of the parcel.

7.2.5.6 All landscaping areas requiring higher intensity landscaping shall provide a minimum of one tree for every 30 m² and one shrub for every 40 m² of landscaped area shall be provided.



7.2.5.7 Where applicable, deciduous trees shall be a minimum 63 mm caliper measured 450 mm from ground level and coniferous trees shall be 2.5 metres in height, as per the County's Land Use Bylaw.

7.2.5.8 Prior to Development Permit approval, and in addition to Policy 7.3.4.4, the developer of parcels designated as Development Lands, which will include both Rail Served and Non-Rail Served Industrial Development, shall provide a Landscape Plan that, as per the County's Land use Bylaw:

- a. Includes a minimum of 6 m landscaped yard adjacent to any public roadway; and
- b. One shrub for every 80 m² of landscaped area shall be provided, to a minimum of six shrubs.

7.2.5.9 At the time of Development Permit application, a lighting plan shall be submitted for any development adjacent to Interface Condition 10. This plan should demonstrate measures to limit lighting along the shared boundary, minimizing impacts on adjacent agricultural land.

7.2.5.10 Heavy industrial uses that may have an effect on the safety, use, amenity, or enjoyment of adjacent or nearby sites due to appearance, noise, odour, emission of contaminants, fire or explosive hazards, or dangerous

goods, are discouraged where they share a boundary with Interface Condition 10. Additional details and studies may be requested by the County Development Officer at time of Development Permit to ensure development does not create a Nuisance on adjacent agricultural land.

7.2.5.11 Applications for non-agricultural development adjacent to agricultural lands should adhere to the County's Agricultural Boundary Design Guidelines.

7.2.5.12 The proposed Regional Pathway along Range Road 282 shall not be constructed until parcels east of the roadway are developed for non-agricultural uses.



7.3 Lighting, Signage, & Fencing

The Shepard Logistics Centre CS intends to integrate with existing and future adjacent developments. While ensuring safety in operations will be a priority, particularly for Rail Served Development, development is expected to establish and maintain lighting, signage, and fencing standards that are aligned with the County's Land Use Bylaw and the County's Commercial, Office and Industrial Design Guidelines.

Policies

7.3.0.1 Prior to Subdivision and/or Development Permit approval, the developer(s) shall prepare a lighting plan that implements the following, to the satisfaction of the Development Authority, for all private lighting:

- a. Ensures safe and well-lit pedestrian areas, including parking areas and building entrances;
- b. Should be concentrated on the buildings and parking lots;
- c. Should be located within key landscaped areas or along trails;
- d. Must not interfere with adjacent highways and roadways;
- e. Should minimize light trespass onto wetlands;
- f. Should be designed to direct downward, conserve energy, reduce glare, and minimize light trespass onto surrounding properties;

- g. Limits off-site light pollution;
- h. When not attached to a building, lighting should be solar powered where possible.

7.3.0.2 In addition to the requirements listed above in Policy 7.3.0.1, developer(s) should apply industry best practice dark sky principles to mitigate light pollution, including the following considerations:

- a. A luminaire backlight, uplight and glare value of 0 should be used for public and rail infrastructure;
- b. Post-top lighting, column lighting, in-pavement lighting and specialty lighting should not be used due to glare, backlight, and other light pollution concerns; and
- c. Development should implement time of day restrictions and other best dark sky practices to ensure light spill into adjacent properties or the surrounding environment is minimized.



7.3.0.3 Prior to Subdivision and Development Permit approval, the developer(s) shall prepare a signage plan that implements the following to the satisfaction of the Development Authority:

- a. Includes appropriate locations setback 3 metres from the road right-of-way;
- b. Includes types of signs or features(s); and
- c. Complies with the County's Land Use Bylaw. If there is a conflict between a requirement in the Land Use Bylaw and the guidelines in this document, the Land Use Bylaw will take precedence.

7.3.0.4 Fencing shall comply with the County's Land Use Bylaw, with maintenance being the responsibility of the developer or owner.





7.4 Site & Building Design

As development proceeds within the Plan Area, the developer(s) shall provide detailed site and building design at the Development Permit stage in accordance with the policies below.

Policies

General Policies

7.4.0.1 Detailed site and building design shall be prepared at the Development Permit stage.

Building Form & Design

7.4.0.2 All buildings shall provide fire suppression systems that are in compliance with the County's Fire Suppression Bylaw and the Alberta Building Code.

7.4.0.3 Crime Prevention Through Environmental Design (CPTED) features should be considered and incorporated into the design and construction of all new development, wherever possible.

7.4.0.4 Where buildings exceed 20m in height and face residential areas or roadways, building and site design shall incorporate tools to promote transition in scale between buildings and protecting access to sunlight and sky views. This could include but is not limited to angular planes, step-backs, or landscape features.

7.4.0.5 Facades of buildings facing existing residential uses, as identified in the Plan's Development Concept, shall include at least three of the following architectural elements to the satisfaction of the Development Authority:

- a. Colour change;
- b. Texture change;
- c. Material modular change; and/or
- d. Expression of an architectural bay through a change in plane such as an offset, reveal, or projecting rib.

7.4.0.6 Rooftop apparatus should be located and concealed to reduce or eliminate public view from adjacent roads or homes.

7.4.0.7 To the satisfaction of the Development Authority, all buildings and structures shall:

- a. Treat the walls of the primary entrance and walls visible from public roadways with variations in façade, colour, articulations, and architectural elements;



- b. Be constructed of High-Quality Building Materials;
- c. Consider rooftop solar system for the purposes of microgeneration; and
- d. Be oriented to ensure the rear of buildings is not facing a public roadway.

7.4.0.8 At the Development Permit stage, development should consider the inclusion of green building techniques and energy efficient designs. This could include, but is not limited to, the use of recyclable materials for buildings and/or rail infrastructure and the implementation of automated monitoring systems to reduce emissions and improve efficiency.

Storage

7.4.0.9 All outdoor storage areas, truck bays, loading areas, waste and recycling receptacles, and other areas that have adverse visual impacts to the public shall be screened to the satisfaction of the Development Authority. Screening can include, but is not limited to: landscaping, fencing, louvered panels, mesh screens, green walls or other decorative screens, or a combination thereof.

Parking

7.4.0.10 Prior to Development Permit approval, the developer(s) shall prepare a parking plan that implements the following to the satisfaction of the Development Authority:

- a. Ensures storage areas, truck bays, and loading areas are not located in front yards of properties abutting public roads;
- b. Includes landscaping buffers within any parking area between a road and the primary entrance;
- c. Clearly differentiates visitor parking from staff parking areas;
- d. Includes pedestrian connections to nearby transit stops and planned open spaces, pathways, and trails; and
- e. Considers electric vehicle ready charging stations for all vehicles.

Commercial / Supporting Services Cells

7.4.0.11 Development within cells identified as Commercial / Supporting Services should:

- a. Identify a hierarchy of pedestrian routes that connect destinations on the site;
- b. Locate commercial uses along higher activity public streets or internal publicly accessible private streets;
- c. Position buildings to face public streets or internal publicly accessible private streets;
- d. Provide on-site pedestrian routes to minimize conflicts with vehicles, particularly near access and service areas;



- e. Locate service areas away from public streets and screen with landscaped areas where possible;
- f. Provide well-marked, individual entrances for units which face a public street or internal publicly accessible private street;
- g. Use building articulation to provide a well-defined, continuous frontage and improve the pedestrian experience using varied textures, high quality building materials and setbacks; and
- h. Position landscaped areas to enhance and complement the interface between the building and pedestrian routes.

7.4.0.12 Development within cells identified as Commercial/Supporting Services with office or light industrial uses located on the ground floor facing a public street or internal publicly accessible private street should provide:

- a. Windows with views to the street and access to natural light;
- b. Amenity space that could be used for daily activity or seasonal programming; and
- c. Lobbies that have well-marked entrances and allow for clear sight lines to and from the building.

7.4.0.13 Considering the inclusion of vehicle-oriented Commercial / Supporting Services uses along Township Road 232, development should be designed to:

- a. Minimize the number of locations where vehicles cross the sidewalk;
- b. Locate driveways to internal roadways, minimizing access from Township Road 232;
- c. Incorporate landscaped areas;
- d. Prioritize and provide direct, well-defined pedestrian routes to transit stops; and
- e. Provide on-site pedestrian routes to minimize conflicts with vehicles, particularly near access and service areas.

7.4.0.14 Commercial developments shall include bicycle racks.

7.4.0.15 Light industrial uses located on the same parcel as commercial development should be fully enclosed within a building.



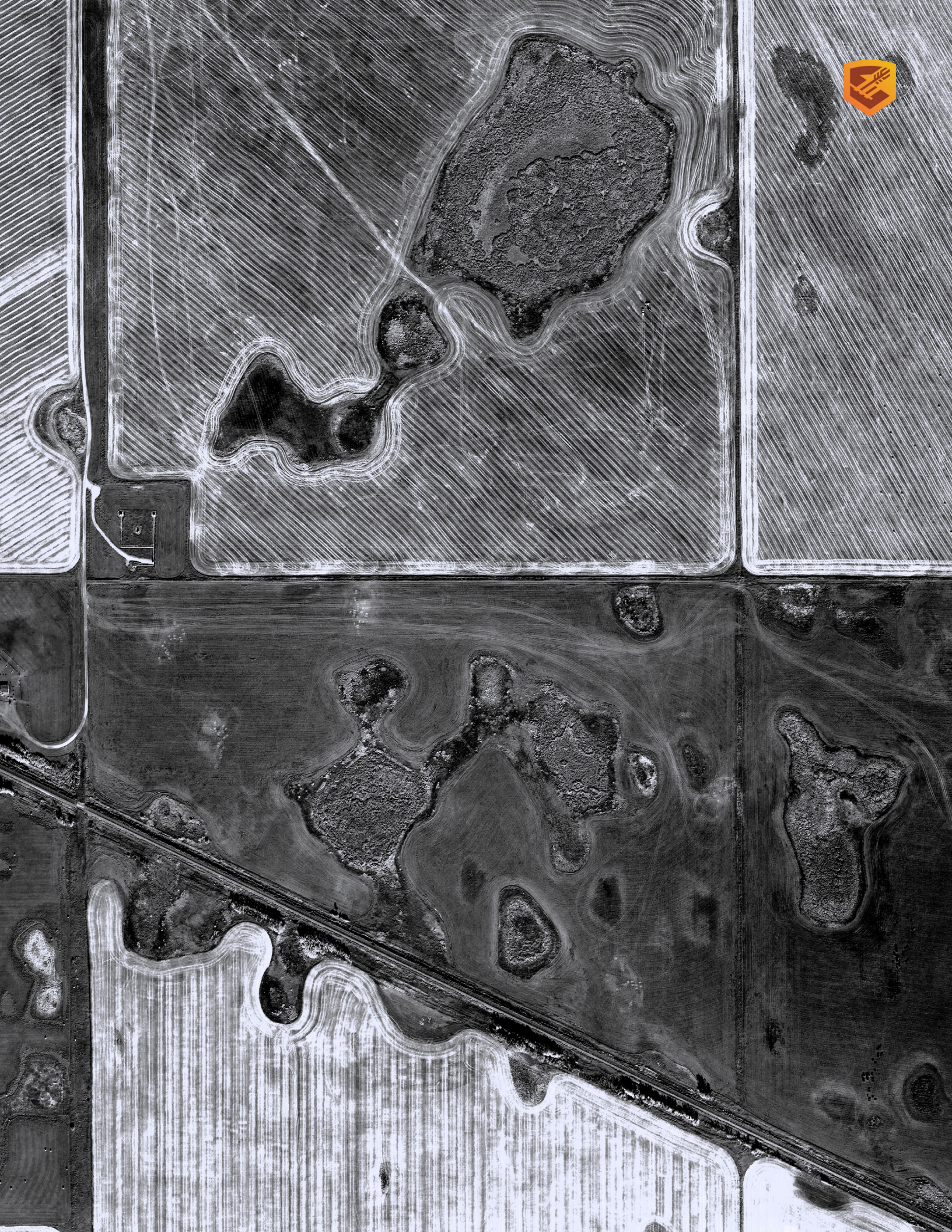
Development Lands Cells

7.4.0.16 To the satisfaction of the Development Authority, all buildings and structures within the Development Lands cells, which will include both Rail Served and Non-Rail Served Industrial uses, shall evaluate rooftop solar systems for the purposes of microgeneration.

7.4.0.17 At the Development Permit stage, development within the Development Lands cells should:

- a. Incorporate opportunities for on-site renewable energy generation;
- b. Consider waste heat recovery and re-use; and
- c. Provide landscaping and passive amenities to workers and visitors to the area.







8 PUBLIC CONSULTATION

8.1 Purpose of Consultation

The Developer of Shepard Logistics Centre is committed to continuing consultation with adjacent landowners and key stakeholders from the broader community to ensure that details relative to the proposed development are communicated openly and transparently.

8.2 Open Houses

Through the Prairie Gateway ASP process, the County hosted two Open Houses, occurring on January 30, 2024, and May 28, 2024. The Shepard Logistics Centre project team, including the Developer (SDC) and consultants from Stantec and ISL, attended both Open Houses to answer questions and receive feedback. Concerns were primarily surrounding:

- *Existing traffic conditions and anticipated impacts of development;*
- *Noise, light, and air pollution;*
- *Impacts on wildlife; and*
- *Servicing, including flooding/drainage issues.*

Following the second Open House, the majority of respondents were supportive of the proposed land use strategy in general while the primary concern was in regards to transportation infrastructure upgrades within and around the ASP boundary. Further details on these findings can be found on the County's webpage under the Prairie Gateway Area Structure Plan project page.

8.3 Shepard Community Association

On June 19, 2024, the Shepard Logistics Centre project team, including the Developer and Consultants from Stantec and ISL, presented the ongoing work to the Shepard Community Association. This presentation included the opportunity for Q&A, as well as an FAQ document that was distributed after the meeting.

This presentation allowed the Developer and the Consultant team to directly address many of the concerns raised through the Open Houses, as summarized in Section 8.2. In addition, the Developer and consultant team had the opportunity to respond to questions surrounding timelines, specific servicing alignments and transportation upgrades, and rail site operations.

Throughout this session, the Consultant team clarified that many of the transportation related concerns will be mitigated through the upgrades included in the TIA submitted with the Prairie Gateway ASP.

Additional consultation with the Shepard Community Association is intended to be undertaken following submission of the CS.



Prairie Gateway ASP Open House held on May 28, 2024





9 IMPLEMENTATION

9.1 Process

Land use within the Shepard Logistics Centre CS Plan Area will be approved by County Council in accordance with the Rocky View County Land Use Bylaw (C-8000-2000) and the Prairie Gateway ASP, as generally illustrated on *Figure 12: Land Use Strategy*. A Direct Control (DC) District is proposed to incorporate and regulate the unique requirements of a rail served development. Specifically, the proposed DC District:

- *Defines specific land uses associated with rail served and large-scale industrial development that are not captured by land use definitions within the County LUB;*
- *Defines specific planning and design items associated with rail served and large-scale industrial development that are not captured by land use definitions within the County LUB;*
- *Prepares separate LUB regulations for the three (3) distinct land use categories identified in Section 4.5 (Rail Served Development, Non Rail Served Development, Supporting Commercial & Services);*
- *Incorporates public utilities within the proposed DC District, to provide flexibility for their design and siting;*
- *Allows parking requirements to be determined on a site-by-site basis, ensuring tailored provision of parking for a wide range of potential uses within the Plan Area;*
- *Ensures rail served development parcels are utilized for rail served purposes, including a mechanism to maintain rail served utilized with change on future uses/tenants;*
- *Provides sufficient building height controls to incorporate a wide range of rail served uses. It is acknowledged that some rail served and large-scale industrial uses, such as cold storage*

developments, require heights up to 150 ft (~45m). Tailored setbacks should be identified to ensure appropriate location and tiering of these higher buildings;

- *Identifies appropriate regulations and Permitted/Discretionary uses for parcels within the 232 Design Corridor; and*
- *The Public Utility Lots (PUL) dedicated to contain the water, sanitary, and stormwater management facilities will also be permitted and regulated by the DC District.*

Policies

- 9.1.0.1** A Land Use Redesignation application shall be prepared concurrently with this Conceptual Scheme, and in accordance with Figure 12: Land Use Strategy.
- 9.1.0.2** Applications for Redesignation and Subdivision shall require the concurrent or prior adoption of a Local Plan, unless otherwise directed by the County.
- 9.1.0.3** Redesignation, Subdivision, and/or Development Permit applications shall address the requirements of this Plan and the policies of the Prairie Gateway ASP.



9.1.0.4 Applications for Redesignation, Subdivision, and Development Permit should comply with the policies and requirements of the following master plans and servicing standards, as amended or replaced, unless otherwise directed by the policies of this Plan:

- a. Prairie Gateway Master Drainage Plan;
- b. Active Transportation Plan: South County;
- c. Recreation and Parks Master Plan;
- d. Rocky View County Solid Waste Master Plan;
- e. Rocky View County Servicing Standards; and
- f. Fire Services Master Plan.

9.1.0.5 At the time of Subdivision, Infrastructure costs and levies shall be paid in accordance with the approved Deal Agreement and related policy.

9.1.0.6 Agricultural operations should continue, where appropriate, until development of those lands occurs in accordance with this CS

9.2 Anticipated Phasing

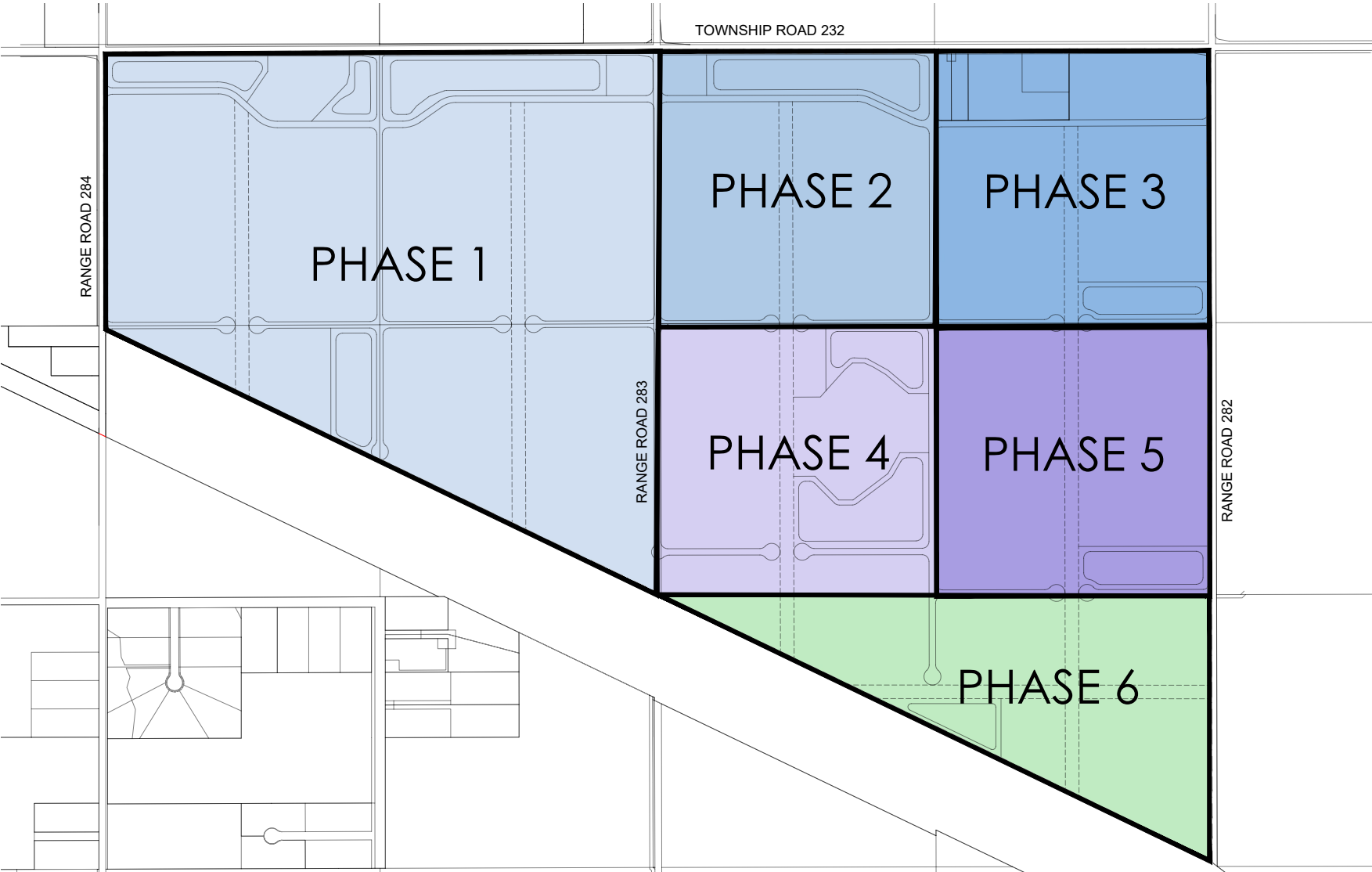
The anticipated phasing, illustrated in **Figure 38: Anticipated Phasing**, has been determined in a logical and cost-effective manner, guided by the availability of efficient and cost-effective utility services from the City (to the west).

Policies

9.2.0.1 The ultimate phasing shall be determined at the Subdivision stage.



Figure 38: Anticipated Phasing





APPENDICES











APPENDIX A: POLICY SUMMARY



Prairie Gateway ASP Policy Summary for Shepard Logistics Centre Conceptual Scheme

Policy Reference Key






-  Applicable and addressed in Conceptual Scheme / Land Use
-  Applicable at Subdivision
-  Applicable at Development Permit
-  Applicable for all development stages
-  Applicable for site operations / implementation
-  Not Applicable (NA)

ASP Summary












ASP Section 6: Land Use Strategy

Policy #	Policy Statement	Applicability
6.01	To provide a holistic, efficient, and thorough approach to development, Local Plans (conceptual schemes and master site development plans) must be prepared in accordance with Section 25 of this Plan. Conceptual schemes and master site development plans within the Plan area are not intended to be adopted by bylaw and appended to the Plan.	
6.02	A lot owner's association or similar body may be established to assume responsibility for common amenities and maintenance, and to manage items including but not limited to pathways, and infrastructure. a. A lot owners' association or similar body may be registered on title, at the subdivision stage, and enforce architectural controls that are in alignment with this Plan, Appendix B, and Local Plans. b. Where a lot owners' association or similar body is established, there should be one single body established for the whole Plan area, or rationale should be provided that justifies a portion of the Plan area.	





ASP Section 7: Agriculture











Policy #	Policy Statement	Applicability
7.01	Existing agricultural operations within the Plan area are encouraged to continue until development of those lands to another use is deemed desirable and that use is determined to be in accordance with the policies of this Plan.	
7.02	First Parcel Out shall be situated in a manner that minimizes the impact on future industrial development of the Plan area. First parcels out: a. shall meet the site requirements of the Municipal Development Plan and shall be no larger than is necessary to encompass the residence, associated buildings, landscape improvements, and access; b. shall meet the County's access management standards; and c. should be located on the corners of the quarter section.	
7.03	Redesignation and subdivision of land for agricultural purposes should not be supported.	
7.04	Confined Feeding Operation development shall not be permitted in the Plan area.	
7.05	A Local Plan is not required where the subdivision meets Policy 7.02 and the criteria for a first parcel out or agricultural use in accordance with the Municipal Development Plan.	

ASP Section 8: Commercial




Policy #	Policy Statement	Applicability
8.01	Commercial development should be located within the 232 Design Corridor (Map 7) or on Range Road 283, and not within areas identified as Rail Served Development or where it interferes with the industrial nature of the Plan.	
8.02	Commercial development should be located within 400 metres of a planned transit stop.	
8.03	Commercial and other business uses that are compatible with industrial uses and do not impact Rail Served Development opportunities may be appropriate within an industrial area.	
8.04	Regional or large-scale commercial uses shall not be located within the Plan area.	
8.05	Commercial development shall not include recreation or institutional uses.	
8.06	Commercial developments may include: a. stand-alone or mixed-use buildings; b. offices; and c. light industrial uses.	
8.07	Electric Vehicle ready charging stations should be included for fleet and public vehicles.	
8.08	A Local Plan shall be required to support applications for commercial development. The Local Plan shall: a. ensure that the types of uses are consistent with the policies of this Plan and are compatible with adjacent industrial uses; and b. where necessary or required: i. provide a strategy to mitigate lighting as per Section 13; and ii. address the policies of this plan regarding Non-Residential/Residential Interface areas.	
8.09	A Local Plan shall incorporate policies that provide for green building techniques and energy efficient design.	
8.10	The Local Plan shall address 'Other Commercial Areas' requirements of Appendix B , and will be used to provide site, building and landscaping direction that guides applicant subdivision, development permit and building permit submissions, and approvals by the Approving Authority.	
8.11	Local Plans must demonstrate that commercial development could be serviced by transit and should plan for: a. one or more transit bus stop(s) within 400 metres; b. bicycle racks and bicycle and pedestrian connections; and c. transit service amenities, such as an attractive shelter seating for pedestrians, where determined appropriate by the transit provider and the County.	









ASP Section 9: Industrial







Policy #	Policy Statement	Applicability
9.01	Industrial development shall be generally located as shown on Map 5 .	
9.02	Industrial uses such as distribution logistics, warehousing, transportation, industrial services, construction, manufacturing, services (business, petroleum, professional, scientific, and technical), and industrial storage that do not have significant offsite nuisance factors shall, subject to the availability of servicing, be supported within the industrial area.	
9.03	Outside storage as a primary use of a site shall not be in the Plan area.	
9.04	Heavy industrial uses with the potential for offsite impacts such as unsightly appearance, noise, odour, emission of contaminants, fire or explosive hazards, or dangerous goods should:	

	a. be located in the Rail Served Policy Area as shown on Map 5 and Map 6 ; b. be located in areas close to, or adjacent to, hazardous goods routes, railway lines, or other means of access suitable for the transportation of raw materials and goods; c. mitigate off-site impacts where possible; and d. provide mitigating landscaping where appropriate.	
9.05	Development shall be in accordance with the County's Commercial, Office, and Industrial Design Guidelines.	
9.06	Recreational development, institutional development, and private school uses are not compatible with the goals of this Plan and shall not be permitted.	
9.07	Industrial development within the Plan area should: a. include opportunities for on-site renewable energy generation; b. consider waste heat recovery and re-use; and c. provide landscaping and passive amenities to workers and visitors to the area.	
9.08	Electric Vehicle ready charging stations should be included for fleet and public vehicles.	
9.09	Industrial uses located adjacent to existing or future residential, or agricultural land uses shall follow the Interface policies in Section 12 of this Plan.	
9.10	The County may require additional development setbacks for heavy industrial developments where offsite impacts (noise, odour, dust, vibration, emissions) could negatively impact adjacent properties.	
9.11	A Local Plan shall be required to support applications for industrial development. The Local Plan shall: a. ensure that the type of uses for the industrial area are consistent with Policies 9.02 to 9.10 ; b. within the Rail Served Policy Area, conform to the direction identified in Section 10 , including the Rail Design Shadow Plan; and c. where necessary, in the opinion of the Approving Authority: i. provide a strategy to mitigate offsite impacts; including noise reduction due to operations and lighting as per Section 13 ; and ii. address the policies of this Plan regarding Non-Residential / Residential Interface areas, where required.	
9.12	A Local Plan may allow for a range of building heights, building sizes, floor plate areas, and ceiling height dependent on the needs of the users and in compliance with the <i>Land Use Bylaw</i> .	
9.13	A Local Plan shall incorporate policies that support green building techniques and energy efficient design.	
9.14	The Local Plan shall address 'Industrial Areas' requirements of Appendix B , and will be used to provide site, building and landscaping direction that guides applicant subdivision, development permit and building permit submissions, and approvals by the Approving Authority.	



ASP Section 10: Rail Served Policy Area












Policy #	Policy Statement	Applicability
10.01	The Rail Served Policy Area shall develop into an efficient industrial park focusing on Rail Served Development.	
10.02	Rail Served Development shall only be located within the Rail Served Policy Area.	
10.03	Except in the 232 Design Corridor, commercial development is discouraged within any area where Rail Served Development is identified.	

10.04	Rail served buildings shall be located in a manner that allows safe and efficient use of the rail infrastructure.	
10.05	Development adjacent to the CPKC Mainline should consider best practices for development in proximity to rail infrastructure.	
10.06	The highest percentage of the Rail Served Policy Area as possible should be planned for and developed as Rail Served Development.	
10.07	<p>A minimum of 50 per cent of the Net Rail Served Policy Area shall be Rail Served Development.</p> <p>a. Notwithstanding Policy 10.07, the Approving Authority may consider a lower percentage in cases where the Local Plan cannot achieve the minimum. The Local Plan application must provide rationale that meets the following criteria for consideration of a lower percentage:</p> <ul style="list-style-type: none"> i. the application meets the vision and goals of the Plan; ii. physical barriers, such as pipelines or natural features, making the development of an area unfeasible or unrealistic to develop with rail infrastructure; iii. operational efficiencies or track geometry limits a higher percentage of Rail Served Development from being feasible or reasonable; and/or iv. other circumstances that limit Rail Served Development. <p>b. Where Policy 10.07(a) is granted by the Approving Authority, the applicant shall update the Rail Design Shadow Plan to reflect the revised percentage and explore alternative areas where Rail Served Development could occur.</p>	
10.08	<p>The road network within the Rail Served Policy Area shall:</p> <ul style="list-style-type: none"> a. not include privately owned rail infrastructure crossings by a public road, Regional Pathway, or sidewalk; i. Notwithstanding Policy 10.08(a), crossings for the sole purpose of emergency services may be permitted at the discretion of the Approving Authority. b. not include Regional Pathways within 8.0 metres measured from the centre of any rail line unless safety provisions are satisfactory to the Approving Authority; c. not include any road right-of-way within 8.0 metres measured from the centre of any rail line; d. restrict public access and interactions with rail infrastructure and the CPKC Mainline; e. not include new public road crossings of the CPKC Mainline; and f. Local Plan provide for public roads that allow for access while ensuring emergency entry and egress. 	
10.09	<p>Any Local Plan within the Rail Served Policy Area shall:</p> <ul style="list-style-type: none"> a. identify the location of rail served areas and non-rail served areas with the goal of maximizing the Rail Served Development area; b. be designed to maximize the operational efficiency of the rail served areas; c. prioritize rail served uses over all other uses; d. recognize the physical layout of the rail served area may take precedence over other land uses and natural features; e. integrate and achieve the objectives of the 232 Design Corridor Design Plan outlined in Appendix B, where applicable; and f. propose ways to discourage the change in use from Rail Served Development to non-Rail Served Development. 	
10.10	Local Plans shall align with the general design and restrictions of the Rail Design Shadow Plan.	
10.11	With the submission of the first Local Plan that encompasses any portion of the Rail Served Policy Area, a Rail Design Shadow Plan must be	



	submitted and approved by the County that provides a comprehensive approach to the entire Rail Served Policy Area. a. All subsequent Local Plans that encompass any portion of the Rail Served Policy Area shall align with the Rail Design Shadow Plan.	
10.12	The Rail Design Shadow Plan shall: a. address continuity, protecting rail served access, and integration of the proposed development with adjacent lands; b. guide subsequent Local Plan applications; c. outline future rail service on remaining lands by identifying development constraints and conceptual rail locations; d. meet Policy 10.09(b); and e. be updated with each Local Plan submission.	
10.13	Local Plans shall demonstrate how the design does not impede the planning and development of the remainder of the Rail Served Policy Area in accordance with the policies of this Plan.	
10.14	Until such time as the railway facility lands are identified, the policies of this Plan shall prevail for all non-federally regulated Railway Land.	
10.15	The identification of the railway facility does not require an amendment to this Plan.	
10.16	In preparation of a Local Plan that requires the use of or access to CPKC owned Railway Lands, the applicant shall consult CPKC regarding the use of CPKC land.	
10.17	Upon identification of the railway facility, the following shall be submitted to the County: a. A Local Plan showing the proposed development and indicating how impacts to residential lands in proximity to the railway facility will be mitigated; and b. The Local Plan should provide the following: i. a map and general description of the uses and infrastructure within the railway facility; ii. the proposed use and alignment, if any, of the services provided to the Plan area, which include transportation access, water, sanitary, stormwater, electrical, natural gas, and telecommunication; iii. identification of servicing needs and consideration of levy requirements; iv. identification of any private services internal to the railway facility, such as a stormwater pond; v. confirmation that utility right-of-way plans and agreements for specific access and utility servicing purposes will be provided; vi. confirmation that compensation for disturbed wetlands, if any, within the railway facility will be provided, as per Environment Canada Guidelines; vii. consideration of off-site impacts to the County, such as impacts to road operations on Range Road 284; viii. consideration of the Plan lighting policies; ix. consideration of the 'Industrial Areas' and 'Other Commercial Areas' site and building guidelines in Appendix B of the Plan; and x. a commitment to discuss Emergency Service provision with the County and other emergency service providers.	
















ASP Section 11: Township Road 232 Design Corridor











Policy #	Policy Statement	Applicability
11.01	The policies of the 232 Design Corridor shall apply to land located within 200 metres of the Township Road 232 right of-way, as generally shown on Map 7 .	
11.02	Proposed commercial development should be located at intersections to create an attractive streetscape.	


11.03	Office and light industrial uses should be located within the 232 Design Corridor and: a. are encouraged to occur in conjunction with commercial and other compatible uses; or b. may be allowed in the form of a stand-alone office building provided that the use is compatible with the character of the area.	
11.04	Road and sidewalk standards along the 232 Design Corridor should match the entryway to The City of Calgary's Shepard Industrial Area Structure Plan.	
11.05	Stormwater ponds within the 232 Design Corridor area shall provide attractive high quality landscaping that implements the requirements of Appendix B .	
11.06	Parcels along Township Road 232 should have vehicular access to local roads with direct access to Township Road 232 limited to major intersections. Spacing and the number of intersections will be determined through a Traffic Impact Assessment.	
11.07	Local Plan, subdivision, and development permit applications shall meet the requirements of Appendix B and the 232 Design Corridor Design Plan.	
11.08	Prior to development permit approval for structures, a signage plan that implements the requirements of Appendix B and the 232 Design Corridor Design Plan shall be prepared to the satisfaction of the County. This plan must show the location and type of freestanding signs.	
11.09	Prior to development permit approval for structures, a lighting plan that implements the requirements of Appendix B and the 232 Design Corridor Design Plan shall be prepared to the satisfaction of the County.	
11.10	Prior to development permit approval for structures, a parking plan that implements the requirements of Appendix B and the 232 Design Corridor Design Plan shall be prepared to the satisfaction of the County.	
11.11	With the submission of the first Local Plan that encompasses any portion of the 232 Design Corridor, a comprehensive Design Plan must be submitted that provides a cohesive approach to the entire 232 Design Corridor area. The 232 Design Corridor Design Plan shall: a. achieve the 232 Design Corridor objectives; b. provide site, building and landscaping direction that guides applicant subdivision, development permit, and building permit applications; c. implement the requirements of Appendix B; d. be to the satisfaction of and approved by the Approving Authority; and e. be appended to any applicable Local Plan.	
11.12	Local Plans located within the 232 Design Corridor shall include policies to ensure a comprehensive design character and require individual buildings to use a variety of High Quality Building Materials, and a variety of design and architectural elements in alignment with Appendix B , that are implemented through conditions at subdivision and development permit stages.	
11.13	Local Plans shall demonstrate how they achieve the goals of the 232 Design Corridor Design Plan, to the satisfaction of the Approving Authority.	

ASP Section 12: Interfaces







Policy #	Policy Statement	Applicability
12.01	Interface policies apply to those areas identified on Map 7 .	
12.02	Local Plans for non-residential uses adjacent to the areas identified on Map 7 shall include an interface strategy that addresses the policies of this section.	

12.03	The interface strategy shall seek to mitigate impacts to adjacent residential areas with particular emphasis on protecting residents from noise, light, visual, privacy intrusions, and other forms of nuisance.	
12.04	The County will work with CPKC or its assignee to identify opportunities to mitigate impacts from the Railway Land to adjacent residential land.	
12.05	The Non-Residential / Residential Interface area shall separate, minimize and/or buffer adjacent residential land from impacts such as noise, light, visual, and privacy intrusions. As shown on Figure 2 , this may include but is not limited to setbacks, berming, fencing with screening, and/or landscaped screening to the satisfaction of the Approving Authority.	
12.06	Non-residential developments located adjacent to a residential area should comply with the following: a. land uses, whether outside or inside of a building, which have significant nuisances (noise, dust, smell, and vibration) should not be permitted; and b. overnight trucking or automotive-related activities including parking, loading, storage, or delivery are not desirable uses in the Non-Residential / Residential Interface area and should be located within the areas where off-site impacts can be appropriately mitigated.	
12.07	Where non-residential development is on lands directly adjacent to or across a road from a residential area, the non-residential development shall be setback a minimum of 50 metres from the non-residential property line for the length of the residential boundary.	
12.08	Uses within the 50 metre Non-Residential/Residential Interface area building setback may include: a. landscaping, berms, landscaped stormwater ponds, and natural wetlands; and b. surface parking (up to 10 metres in width) where the parking is hidden from view by screening such as berms, fencing, and/or landscaping.	
12.09	High quality landscaping should be emphasized in the setback area. A landscape plan shall be prepared for the setback as part of a Local Plan that addresses the County's Land Use Bylaw, Appendix B , and any applicable design guidelines.	
12.10	Mass plantings and/or berms shall be required to minimize the visual impact of the non-residential buildings. The plantings and earth berms should incorporate natural contours and variations in height to achieve a natural landscaped appearance.	
12.11	Outside storage is not an acceptable use in Non-Residential / Residential Interface areas.	
12.12	High quality building appearance should be emphasized where non-residential buildings face residential areas. Building design shall address the design guidelines in Appendix B of this Plan, the County's Commercial, Office, and Industrial Design Guidelines, and the design principles established within any Local Plan adopted by the County.	
12.13	Garbage storage, loading bays, loading doors, or other activities creating heavy truck movements on lots adjacent to a residential area shall not face the residential area.	
12.14	A Local Plan adjacent to a residential land use shall appropriately plan and design an interface that minimizes conflict between incompatible land uses.	
12.15	The Local Plan shall provide illustrations (e.g., cross-section) and graphics to show the proposed interface design.	
12.16	Where industrial development is adjacent to a residential land use, Local Plans shall address building height within the setback area described in Section 12, Map 7, and Appendix B .	
12.17	High quality landscaping should be emphasized in interface areas.	












12.18	<p>The Rocky View County / City of Calgary Intermunicipal Development Plan, interface planning principles should be addressed in any Local Plan, redesignation, subdivision, or development permit application adjacent to The City of Calgary.</p> <p>a. Where industrial uses adjacent to The City of Calgary are proposed, they should be compatible in use with the Shepard Industrial ASP.</p>	
12.19	<p>Any Local Plan adjacent to the City of Calgary shall demonstrate how it integrates with the planned industrial development in the City of Calgary (Shepard Industrial ASP).</p> <p>a. The interface shall be appropriately planned and designed to minimize conflict between incompatible land uses.</p> <p>b. The Local Plan shall provide illustrations (e.g., cross-section) and graphics to show the proposed interface design.</p>	
12.20	<p>As part of a Local Plan application, a landscape plan shall be prepared for any development in the County-City Interface area and shall address the design guidelines in Appendix B of this Plan, the County's Land Use Bylaw, and the County's Commercial, Office and Industrial Design Guidelines.</p>	
12.21	<p>Applications for non-agricultural development adjacent to agricultural lands should adhere to the County's Agricultural Boundary Design Guidelines.</p>	
12.22	<p>Proposals for non-agricultural development adjacent to agricultural lands located within the Plan boundary shall incorporate buffering, siting, and design techniques to minimize negative impacts on agricultural lands.</p>	
12.23	<p>Agricultural buffering techniques may include a combination of the following:</p> <p>a. Barrier fencing to prevent access and catch debris;</p> <p>b. Vegetated berms;</p> <p>c. Stormwater management facilities;</p> <p>d. Ecological / vegetative buffers;</p> <p>e. Use of topographic barriers such as slopes, roads, watercourses, or wetlands; and</p> <p>f. Increased setbacks for housing and other buildings.</p>	
12.24	<p>Public access such as trails, pathways, and parks should be discouraged adjacent to agricultural lands unless supported by the open space and pathway plan (Map 8).</p>	
12.25	<p>All development shall address the County's Commercial, Office, and Industrial Design Guidelines with respect to the retention and enhancement of natural areas.</p>	
12.26	<p>Design of industrial uses adjacent to retained natural areas shall:</p> <p>a. minimize impact on the natural area;</p> <p>b. provide appropriate landscaping to provide a buffer between parking areas and private roads or driveways and adjacent natural areas;</p> <p>c. locate surface parking and loading areas away from the adjacent edge of the natural area; and</p> <p>d. integrate pathways and trails with the natural areas.</p>	
12.27	<p>Design of commercial uses adjacent to natural areas shall:</p> <p>a. minimize impact on the natural area;</p> <p>b. provide natural surveillance between the built form and the natural areas;</p> <p>c. integrate pathways and trails with the natural areas;</p> <p>d. locate surface parking and loading areas away from the adjacent edge of the natural area; and</p> <p>e. allow direct pedestrian connection from the commercial uses to the pedestrian pathways within the natural areas.</p>	











12.28	The placement of the buildings should reinforce the sense of entry and provide visual connections to natural areas.	
-------	---------------------------------------------------------------------------------------------------------------------	-------------------------------------------------------------------------------------

ASP Section 13: Lighting


Policy #	Policy Statement	Applicability
13.01	All private lighting, including security and parking area lighting, shall be downward directed, designed to conserve energy, reduce glare, and minimize light trespass onto surrounding properties.	
13.02	All development within the Plan area should apply industry best practice dark sky principles to mitigate light pollution, including the following considerations: a. A luminaire backlight, uplight and glare value of 0 should be used for public and rail infrastructure; b. Post-top lighting, column lighting, in-pavement lighting and specialty lighting should not be used due to glare, backlight, and other light pollution concerns; and c. Development should implement time of day restrictions and other best dark sky practices to ensure light spill into adjacent properties or the surrounding environment is minimized.	
13.03	Site and building lighting should ensure safe and well-lit pedestrian areas, including parking areas and building entrances.	
13.04	Lighting should be located within key landscape areas or along trails while minimizing light trespass onto Environmentally Sensitive Areas.	
13.05	Light trespass onto properties outside of the Plan area shall be minimized to the greatest extent possible.	
13.06	Lighting not attached to a building should be solar powered.	















ASP Section 14: Natural and Historic Environment



Policy #	Policy Statement	Applicability
14.01	Wetland protection shall be guided by County, regional, and Provincial policy.	
14.02	Wetland classification and relative value shall be determined using the Alberta Wetland Classification System.	
14.03	Wetlands within the Plan area north of Township Road 232 that are not claimed by the Crown and have a high relative value should be protected.	
14.04	Wetlands that form part of a stormwater management system shall be retained where possible in accordance with the Master Drainage Plan. Retention, maintenance, and/or removal of other wetlands will be in accordance with the direction of the Master Drainage Plan.	
14.05	Where wetlands are not retained, developers shall provide for appropriate replacement or compensation, in accordance with provincial policy.	
14.06	Riparian area protection shall be guided by County and Provincial policy and regulation.	
14.07	Building and development in the riparian setback area shall be in accordance with the County's Land Use Bylaw and Riparian Setback policy.	
14.08	The riparian setback area uses may include natural trails that are designed and programmed to have low environmental impacts, this may include supporting activities such as walking, hiking, and cycling.	
14.09	The riparian protection area shall remain vegetated and development proponents are strongly encouraged to maintain the natural riparian function through the use of native plant species.	
14.10	Existing tree stands and related habitat should be retained where possible.	
14.11	North of Township Road 232, contiguous corridors should be retained for connectivity of existing tree stands, wetlands, creeks, streams, and	

	drainages to allow for wildlife movement and possible stormwater integration with the Shepard Wetland Complex.	
14.12	Provincial guidelines should be followed to determine whether any Historical Resources Application is required under the Historic Resources Act: a. Any required avoidance or mitigation measures shall be incorporated within the development proposal and detailed within the Local Plan.	
14.13	Names of new developments and/or roads should incorporate traditional knowledge, commemorations significant to Indigenous Peoples, the names of local settlement families, historical events, topographical features, or locations. a. Where names reflect indigenous culture, the Nations should be consulted.	
14.14	Conservation should be considered for Historic Resources (specifically archaeological resources) discovered within the Plan area.	
14.15	At the time of Local Plan preparation, a Biophysical Impact Assessment (BIA) to evaluate impact on wetlands, wildlife, vegetation, historical resources, and Environmentally Sensitive Areas shall be submitted in accordance with the County Servicing Standards.	
14.16	Local Plans shall identify the classification and value of wetlands within the Local Plan area boundary. This shall be done as part of a wetland assessment, to be provided at the Local Plan preparation stage.	
14.17	Local Plans shall determine, through consultation with the Province and County, whether wetland assessment for Crown-claimed wetlands is complete.	
14.18	The Local Plan shall demonstrate the connectivity and function of all retained natural features.	
14.19	At the time of Local Plan preparation, the riparian setback area from a protected watercourse shall be determined using the Province's "Stepping Back from the Water: A Beneficial Management Practices Guide for New Development Near Water Bodies in Alberta's Settled Region", or a similar provincial document that may replace this document.	
14.20	Where a road is proposed to cross Environmentally Sensitive Areas: a. applicable Provincial approval shall be obtained; b. studies shall be done to ensure that any potential changes to existing wetland boundaries are minimized; c. studies should consider the most appropriate environmentally beneficial technique to maintain the ecological quality of the area; d. mitigation measures to protect the Environmentally Sensitive Areas shall be designed and implemented during construction; and e. the applicant shall demonstrate why another location is not feasible.	
14.21	In preparation of a Local Plan, the applicant shall provide documentation that the Historical Resources Act requirements for the property have been completed to the satisfaction of the Province of Alberta (Historic Resource Management Branch).	














ASP Section 15: Open Space, Parks, and Pathways

Policy #	Policy Statement	Applicability
15.01	Open space shall be provided in the Plan area through such means as: a. the dedication of municipal reserves, environmental reserves, and public utility lots; b. government lands for public use; c. privately owned land that is accessible to the public; d. publicly owned stormwater conveyance systems; e. land purchases, endowment funds, land swaps, and donations; and/or	

	f. other mechanisms approved by the County.	
15.02	Linear open spaces should be designed to allow access to people of all ages and abilities and provide opportunities for passive recreation.	
15.03	Open space shall be planned and integrated into the Plan area so that the function of each space will provide a positive and safe social, ecological, cultural, and/or recreational experience.	
15.04	Where historic resources are identified within open space, they should remain undisturbed where possible. When not retained, opportunities for celebration must be considered. a. For any Indigenous historic resources, the applicant shall consult Nations: i. prior to any historic resource removal; and ii. for consideration of opportunities for celebration.	
15.05	An interconnected linear system of trails and pathways shall be provided, which connect to existing or proposed active transportation networks in general accordance with Map 8 .	
15.06	The network of pathways, trails, and sidewalks should: a. promote walking and cycling; and b. provide safe and efficient connections between commercial and industrial areas.	
15.07	The design and construction of parks, pathways, trails, and associated amenities shall be of high quality and adhere to the construction and design standards, including but not limited to: a. the Geometric Design Guide for Canadian Roads; b. the County's servicing standards; and c. the Parks and Pathways: Planning, Development and Operational Guidelines.	
15.08	Regional pathways should be provided within planned road rights-of-way in general accordance with Map 8 .	
15.09	Within road rights-of-way, pathways should be located on: a. the north side of Township Road 232; b. the west side of Range Road 284; c. the east side of Range Road 283; and d. the west side of Range Road 282.	
15.10	Regional Pathways and natural area corridors should connect to or support critical linkages between Calgary and Rocky View County.	
15.11	Maintenance roads located around the perimeter of any stormwater pond should be used as a pathway and connect to the greater Regional Pathway network of the Plan area. a. Notwithstanding Policy 15.11 , if a stormwater pond is within the Rail Served Area, a Regional Pathway may not be required.	
15.12	Where wetlands are retained, an adjacent pathway should be developed around all or a part of the wetland in a manner that minimizes disturbance to the wetland and riparian area.	
15.13	Pathways should not be located within 8.0 metres of identified rail infrastructure.	
15.14	The Local Plan shall demonstrate how the pathway is connected to the open space, trails and pathways network as generally shown on Map 8 .	
15.15	Local Plans should incorporate the goals and policies of the Parks and Open Space Master Plan and the Active Transportation Plan: South County. In doing so, Local Plans should: a. provide connections within, and external to, the Local Plan area; b. wherever possible, be located within or align with a park, wetland, stormwater conveyance system, natural water course, riparian area, or natural area;	










	c. incorporate crime prevention through environmental design (CPTED) features; and d. contribute to the regional trail and pathway system and, where possible, connect with other municipalities' pedestrian networks.	
15.16	Pathway alignment, as identified on Map 8 , may be refined at the Local Plan application stage.	
15.17	At the Local Plan stage, Industrial and Commercial uses adjacent to natural areas should: a. allow for outdoor amenity space to be integrated with the natural areas; b. allow direct pedestrian connection to the pedestrian pathways within the natural area; and c. coordinate landscape components, where appropriate, to reflect the nature of the adjacent open space.	

ASP Section 16: Reserves






Policy #	Policy Statement	Applicability
16.01	Voluntary dedication of reserve land beyond the maximum amount allowed by the <i>Municipal Government Act</i> may be considered if it is demonstrated that the additional reserve will benefit the County and result in no additional acquisition costs to the County.	
16.02	The acquisition and disposal of reserve land, and the use of money in place of reserve land, shall adhere to County policy, agreements with local school boards, and the requirements of the <i>Municipal Government Act</i> .	
16.03	Provision and allocation of reserves shall be determined at the time of subdivision by the subdivision Approving Authority.	
16.04	The amount, type, location, and shape of reserve land shall be suitable for public use and readily accessible to the public.	
16.05	The dedication of reserves should meet the present or future needs of the Plan area by considering the recommendations of this Plan, the County's Parks and Open Space Master Plan, County Active Transportation Plan, a Local Plan, school boards, and any other relevant policies or agreements.	
16.06	Reserves owing on a parcel of land should be provided as land to achieve the local pathway network or cash-in-lieu as determined by the County.	
16.07	Municipal reserve, school reserve, or municipal and school reserve shall be provided through the subdivision process to the maximum percentage allowed by the <i>Municipal Government Act</i> .	
16.08	Lands that qualify as environmental reserve should be dedicated as environmental reserve land at the time of subdivision, as per the <i>Municipal Government Act</i> .	
16.09	Lands that are determined to be of environmental significance but do not qualify as environmental reserve should be protected in their natural state through alternative means as determined by the County.	
16.10	Environmental reserves should be determined by conducting: a. a Biophysical Impact Assessment report; b. a geotechnical analysis; and/or c. other assessments acceptable to the County	
16.11	Within a Local Plan boundary, reserve lands may be deferred by registering a deferred reserve caveat to a future subdivision.	
16.12	A reserve analysis shall be required with the preparation of a Local Plan to determine the amount, type, and use of reserves owing within the Local Plan area.	
16.13	The reserve analysis shall include a determination of: a. the total gross area of the Local Plan; b. the type and use of reserves to be provided within the Local Plan area; c. other reserves owing on an ownership basis;	












	d. the location of the reserve types and amounts in relation to the Local Plan area's overall open space system, with this information to be shown on a map; and e. the amount of residual reserves to be taken as money in place of land.	
--	-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--

ASP Section 17: Emergency Services














Policy #	Policy Statement	Applicability
17.01	In association with County Fire Services, the RCMP, and other emergency service providers, an adequate level of service shall be provided to the Plan area.	
17.02	Policing will be provided by the RCMP as per the Provincial Police Service Agreement, until such time as another policing solution is required or sought out.	
17.03	Fire services will be provided by the County as the primary responder. The County may request the support of The City of Calgary Fire Department if required, as per the Secondary Emergency Response Fire Services Agreement between the County and The City. a. Notwithstanding Policy 17.03 , primary response may change upon agreement between the County and The City.	
17.04	Community Service Reserve land may be used to locate an Emergency Response Station within the Plan area if the land is declared surplus to school needs.	
17.05	Proposed development within the Plan area will be reviewed by County Fire Services and the Approving Authority to ensure appropriate Fire Protection measures are incorporated.	
17.06	All industrial and commercial buildings shall provide fire suppression systems that are in compliance with the County's Fire Suppression Bylaw and the Alberta Building Code.	
17.07	Local Plans shall address fire and protection response measures and on-site firefighting requirements through consideration of such factors as uses, building heights and design, efficient road design, safe and efficient access for emergency service vehicles, wildland fire protection, and fire control measures.	
17.08	Crime Prevention Through Environmental Design (CPTED) features should be considered and incorporated into the design and construction of all new development, wherever possible.	
17.09	The County shall collaborate with The City of Calgary, CPKC, and any third-party site operator to develop an Emergency Response Plan to mitigate any risks related to Railway Lands and train movements.	













ASP Section 18: Transportation

Policy #	Policy Statement	Applicability
18.01	The regional transportation system should be developed in general accordance with Map 9 and the Transportation Impact Assessment. The classifications of the grid transportation network will be refined through further transportation analysis and/or at the Local Plan stage.	
18.02	Rocky View County shall work collaboratively with The City of Calgary and Alberta Transportation on the required transportation upgrades, and connections to Stoney Trail and Highway 560 (Glenmore Trail).	
18.03	The local transportation network should be generally located as depicted on Map 9 .	
18.04	Local roads shall be designed in accordance with the County's Servicing Standards.	
18.05	To efficiently move traffic, reduce emissions, and improve safety, the use of roundabouts at major intersections should be evaluated for feasibility.	



18.06	The cross section for Township Road 232 should match the planned cross section for 114 Avenue SE within The City of Calgary.	
18.07	The transportation network shall be designed and built to accommodate a future transit service, while accommodating the design of a rail served industrial park. a. When developed, the following roads shall be developed as transit ready routes: i. Township Road 232; ii. Range Road 284; iii. Range Road 283 north of Township Road 232; and iv. Range Road 282. b. Notwithstanding Policy 18.07(a) , transit ready routes may be refined at the Local Plan stage through the Transit Service Plan if refinements would result in more effective transit provision or to avoid conflicts with the policies of the Rail Served Area.	
18.08	Transit design should plan routes that minimize the number of turns while providing maximum coverage.	
18.09	At the subdivision stage, based on further analysis and lot design, the locations of bus stops identified in the Transit Service Plan will be refined to accommodate the subdivision design.	
18.10	All streets accommodating temporary phased or permanent transit service shall be a collector or higher-order street classification.	
18.11	A Traffic Impact Assessment shall be required as part of the Local Plan preparation and/or subdivision application process and shall confirm road classifications, intersection spacing, and intersection treatments.	
18.12	As part of the Local Plan preparation, the designation, design, and construction of the local network roads, including classification, street sizing, number, and intersection/access spacing, shall be determined.	
18.13	Local Plan transportation network design should be a grid network where possible.	
18.14	Local roads shall be designed in accordance with the urban requirements of the County's Servicing Standards. a. Notwithstanding Policy 18.14 , the rural road requirements of the County's Servicing Standards may be considered where technical constraints make the urban requirements unfeasible.	
18.15	Roads shown on Map 9 as solid lines should be part of a Local Plan. a. Roads shown on Map 9 as dashed lines are conceptual and may be altered as part of the Local Plan submission, subject to the following: i. a Transportation Impact Assessment determines that the change would not negatively affect the network; and ii. the change will not negatively affect the ability to provide fire and emergency services.	
18.16	With the submission of a Local Plan, a Transit Service Plan prepared in collaboration with a public transit service provider shall be submitted. The Transit Service Plan: a. should identify, within the Rail Served Policy Area, transit routes that maximize transit coverage while minimizing conflicts with Rail Served Development; b. for the Rail Served Policy Area, shall accompany the Rail Design Shadow Plan and support the objectives and policies of the Rail Served Policy Area; c. shall identify other potential transit routes; d. shall provide guidance on the spacing of bus stops, transit amenities, and street classification; and e. shall include a conceptual map.	

ASP Section 19: Utility Services

Policy #	Policy Statement	Applicability
19.01	Water, wastewater, and shallow utility services shall be provided to the entire Plan area.	
19.02	The location of regional and local infrastructure corridors, utility rights-of-way and easements, and related line assignments are identified on Map 3 . Local Plans in proximity to a regionally significant corridor should identify and protect the corridor to the satisfaction of the County, utility company, and easement holder.	
19.03	Proposed land use, employment (flow) forecast, or transportation network changes to the Plan may require a re-evaluation or modification of the proposed and existing utility infrastructure at the regional level.	
19.04	If a District Energy System is available or planned for within the Plan area, development should connect to that District Energy System.	
19.05	Upon The City of Calgary request, water and wastewater design reports and drawings that impact The City water and wastewater infrastructure shall be circulated to The City for review and approval, prior to Local Plan approval.	
19.06	The provision, alignment, and capacity of the water distribution system shall be in general accordance with Map 10 and Determination of Sanitary Sewer Flow and Potable Water Demand Technical Memorandum. These alignments are conceptual and will be further identified as part of the Local Plan preparation.	
19.07	Water servicing for the Plan area will be provided to the County boundary by The City of Calgary, subject to City Council approval of a Master Servicing Agreement. a. Notwithstanding Policy 19.07 , a supplemental non-potable water or potable water system for a high demand user beyond the regional water network's capacity may be considered, subject to technical review and an update of the Sanitary Sewer Flow and Potable Water Demand Technical Memorandum, and subject to approval of the alternative option by the applicable approving authorities.	
19.08	Design and construction of the water distribution system within the Plan area shall be to the County servicing standards.	
19.09	Where the Plan area water distribution system connects to The City of Calgary owned regional distribution system, the connection point(s) shall be to City standards.	
19.10	All water systems serving developments within the Plan area shall be designed to provide adequate water pressure to combat fires.	
19.11	Within the Plan boundary, infrastructure and land related to the distribution of water shall be identified prior to the approval of a Local Plan application and dedicated to the County as per the requirements of the Development Agreement.	
19.12	Reduction and reuse of water is encouraged in accordance with Provincial laws and regulations. a. Where possible, the reuse of water is encouraged for energy generation, industrial processing, and other uses allowed for by the province. b. Potable water shall not be used for irrigation.	
19.13	The provision, alignment, and capacity of the wastewater system shall be in general accordance with Map 11 and Determination of Sanitary Sewer Flow and Potable Water Demand Technical Memorandum. These alignments are conceptual and will be further identified as part of the Local Plan preparation.	














19.14	Wastewater servicing for the Plan area will be provided to the County by The City of Calgary, subject to City Council approval of a Master Servicing Agreement.	
19.15	Where the Plan area wastewater distribution system connects to The City of Calgary owned regional collection system, the connection point and effluent standards shall be to City standards.	
19.16	Design and construction of the wastewater distribution system within the Plan area shall be to the County standards.	
19.17	Within the Plan boundary, infrastructure and land related to the collection and conveyance of wastewater shall be identified prior to the approval of a Local Plan application and dedicated to the County at the subdivision stage.	
19.18	If supplemental non-potable water or potable water from a high demand user is returned to the wastewater system, updates to the Technical Memorandum and Master Servicing Agreement may be required, as deemed necessary by the County and The City of Calgary.	
19.19	Sump pumps and stormwater drainage systems shall not connect to the wastewater system.	
19.20	Shallow utility alignment should be: a. identified at the Local Plan stage and determined at the subdivision stage; b. located within a utility right-of-way and not within the road allowance or under sidewalks or pathways; and c. located to avoid identified natural areas, tree plantings, and open spaces, and minimize the impact on natural features.	
19.21	Wherever possible, utility easements should be utilized to ensure their location, identification, and maintenance can be made with ease and without service disruption.	
19.22	Shallow utility rights-of-way, public utility lots, and easements shall be provided at the subdivision or development permit stage, as deemed necessary by the utility provider.	
19.23	Local Plan, redesignations, subdivision, and/or development permit applications shall be in alignment with the wastewater servicing plan and water servicing plan.	
19.24	As part of the Local Plan preparation, the developer shall consult with The City of Calgary and the County to identify: a. any downstream trunk, wastewater treatment plant upgrades, or other infrastructure required to provide wastewater servicing; and b. any upstream mains, water treatment plant upgrades or other infrastructure required to provide water servicing.	
19.25	The location and size of utility rights-of-way and easements, and related line assignments, should be determined at the Local Plan stage to the mutual satisfaction of the County, the developer, and the utility companies.	






ASP Section 20: Power Generation Facilities

Policy #	Policy Statement	Applicability
20.01	The operator of any power generation facility shall obtain all relevant provincial approvals and adhere to the technical development requirements of the Local Plan.	
20.02	For utility-scale power generation facilities, the Approving Authority may request additional technical studies and supporting information, including but not limited to, the following: a. Development Impact Statement and Analysis to evaluate the impact of the proposal on adjacent sites from: i. noise;	








	ii. visual appearance; iii. lighting; iv. odour; and/or v. dust impacts. b. impacts and mitigation of the anticipated vapour / steam by-products; c. Biophysical Impact Assessment; and d. any additional studies to identify safety, health and/or nuisance impacts.	
--	-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--

ASP Section 21: Stormwater




Policy #	Policy Statement	Applicability
21.01	The stormwater system alignment shall be in general accordance with Map 12 , and capacity in general accordance with the Master Drainage Plan. These alignments are conceptual and will be refined as part of the Local Plan preparation and subsequent lower tier stormwater reports.	
21.02	All new development shall conform to the recommendations outlined in the Master Drainage Plan, Sub-Catchment Master Drainage Plan, and subsequent required stormwater reports regarding release rates, volume control targets, water quality, and assessment of downstream drainage constraints.	
21.03	Stormwater ponds should be enhanced with bio-engineering techniques, wherever possible, to promote volume control and improved water quality.	
21.04	Natural wetlands and/or natural drainage courses that are retained should receive treated stormwater through direct or indirect flow in order to maintain the integrity of the wetland and the drainage course.	
21.05	All new development shall be required to connect to the stormwater system.	
21.06	Mitigation of potential negative impacts of development to watercourses, waterbodies, and adjacent landowners must be identified and addressed in the Master Drainage Plan and subsequent required stormwater reports.	
21.07	All stormwater design reports and drawings that discharge to The City of Calgary stormwater infrastructure shall be circulated to The City for review and approval, prior to Local Plan approval.	
21.08	Regional stormwater treatment will be provided by The City of Calgary, subject to City Council approval of a Master Servicing Agreement.	
21.09	Stormwater flows (quantity and quality) and infrastructure connecting to The City of Calgary's stormwater treatment system shall meet City standards.	
21.10	The Master Drainage Plan, subsequent plans, and stormwater management facilities shall align with the East Calgary Regional Stormwater Plan.	
21.11	Stormwater infrastructure within the Plan area shall be constructed, operated, and maintained in accordance with County servicing standards, County policy, the Master Drainage Plan, and provincial regulations.	
21.12	Where the Plan area stormwater system connects to The City of Calgary owned stormwater system, the connection point and stormwater quality standards shall be to City standards.	
21.13	The County supports best management practices that reduce impervious surfaces, clean or filter runoff, and allow for reuse of stormwater for non-potable purposes. Reduction in quantity and improvement in quality can be achieved by the: a. design of source control practices in order to reduce the amount of water moving downstream and the need for end-of-pipe stormwater treatment solutions; b. use of low impact development methods; c. reduction of impermeable surface runoff; and	













	d. reuse of stormwater for irrigation or other non-potable purposes.	
21.14	The County supports the reuse of stormwater in accordance with provincial requirements.	
21.15	Within the Plan boundary, infrastructure and land related to the stormwater treatment and conveyance system shall be identified prior to the approval of a Local Plan application and dedicated to the County at the subdivision stage.	
21.16	As part of a Local Plan application, a Stormwater Management Report that is consistent with the approved Master Drainage Plan, or any subsequent stormwater plan shall be submitted.	
21.17	Local Plans and subdivisions shall adhere to the approved Master Drainage Plan and subsequent required stormwater reports.	
21.18	The Local Plan shall adhere to the monitoring requirements of the approved Master Drainage Plan and subsequent required stormwater reports.	








ASP Section 22: Solid Waste and Recycling

Policy #	Policy Statement	Applicability
22.01	Solid waste management shall be guided by Rocky View County's Solid Waste Servicing Strategy.	
22.02	The developer shall be responsible for the management and disposal of solid waste generated through all stages of construction in accordance with County standards.	
22.03	Waste minimization and waste diversion practices are encouraged in the Plan area and should have a diversion target of 50 per cent.	
22.04	A Local Plan should: a. address solid waste management through all stages of development, including occupancy; b. identify the appropriate waste collection stations that serve the Local Plan area; c. conform to the policies of the County's Solid Waste Master Plan; and d. set a solid waste diversion target to inform the subdivision construction management plan.	
22.05	Businesses shall be responsible for providing their own solid waste services.	
22.06	Solid waste management shall be the responsibility of property owners in country residential and agriculture areas.	
22.07	Waste collection stations should be used for the disposal of solid waste and recyclable materials.	







ASP Section 23: Oil and Gas











Policy #	Policy Statement	Applicability
23.01	Applicants proposing to develop land in the vicinity of oil and gas facilities and wells shall adhere to the setback requirements and policies of this Plan, and the Directives and Bulletins of the Alberta Energy Regulator.	
23.02	At the time of subdivision or development permit approval, a restrictive covenant shall be registered that prevents the construction of any building within the setback area associated with an active, suspended, reclaimed, or abandoned well.	
23.03	Prior to the preparation of a Local Plan to develop lands within 1.5 kilometres of a petroleum facility that is situated within an Emergency Planning Zone, the developer shall consult with the County and the operator of the facility to determine how an Emergency Response Plan will be prepared, updated, or replaced.	


	a. Where the Emergency Planning Zone includes any land in the City of Calgary, the County shall consult with The City.	
23.04	The location, development setbacks, Emergency Planning Zones, and emergency response planning regarding all petroleum facilities shall be identified in the Local Plan and included in any marketing information and other public communication materials.	
23.05	Prior to the preparation of a Local Plan to develop lands with identified oil and gas wells and/or pipelines, the developer shall consult with the County and the affected operator of the facility to discuss development planning and implementation.	
23.06	All buildings located in proximity to an abandoned well site shall comply with the Alberta Matters Related to Subdivision and Development Regulation and Alberta Energy Regulator setback requirements or provide a minimum building setback as required by the operator(s), whichever is greater.	
23.07	Vehicular access to an abandoned well site shall: a. be identified in the Local Plan; and b. if required, be protected by easements in favour of the County at the time of subdivision or development approval.	
23.08	In conjunction with the preparation of a Local Plan, a subdivision, or development permit application for any parcel containing an abandoned well, the applicant shall provide: a. surveyed locations and depth, if known, of abandoned wells and pipelines and confirmation from the Alberta Energy Regulator of any setbacks; b. a Phase I Environmental Site Assessment specific to the abandoned well; c. a Phase II Environmental Site Assessment specific to the abandoned well as deemed appropriate by the Approving Authority; d. an evaluation of the integrity of the well abandonment; and e. a reclamation certificate for the well, if possible.	
23.09	Pending the results of a Risk Assessment, lands with abandoned wells may be part of the Municipal Reserve dedication if they are compatible with a park or trail plan, at the discretion of the Approving Authority.	
23.10	Roads shall not be located over abandoned wells.	
23.11	During land development, all abandoned well sites shall be marked with temporary signage identifying the location and depth, if known, of the abandoned well and providing contact information for the Alberta Energy Regulator. Such signage, as well as adequate fencing and any other necessary protective measures, shall be in place during the development process to prevent damage to the abandoned well bore.	
23.12	All land uses on pipeline rights-of-way shall have regard for the safe, ongoing operations of these facilities.	
23.13	If applicable, crossing and access agreements shall be in place prior to conditional subdivision approval over lands encumbered by a pipeline right-of-way.	
23.14	Pathways and other recreational uses may be permitted on pipeline right-of-way with the consent of the easement holder and at the discretion of the Approving Authority.	
23.15	Intersections of water, wastewater or stormwater utility lines, roads, and parcels where new building construction will take place shall not be co-located with abandoned pipelines. At the discretion of the Approving Authority, an environmental assessment of a pipeline right-of-way where the pipeline has been removed may be required to demonstrate that land is suitable for the intended use.	

23.16	A discontinued pipeline is a temporarily deactivated pipeline that may go back into service in the future, and therefore, the setback requirements shall remain as if the pipeline was operating and in compliance with provincial regulations.	
23.17	An abandoned pipeline is one which will not be reactivated for service, and therefore, the minimum setback for an abandoned pipeline is the edge of the pipeline right-of-way, unless the pipeline has been removed.	
23.18	Where feasible and as negotiated between the operator and developer, removal of abandoned pipelines is strongly encouraged as part of area development.	
23.19	As part of a Local Plan preparation process, applicants shall obtain a Land Development Information package from the Alberta Energy Regulator and identify the locations of all petroleum wells and pipelines (abandoned and operating) in the Local Plan area. In addition, the applicant must determine if an Emergency Planning Zone has been established around a sour gas facility or well.	
23.20	A Risk Assessment shall be required prior to, or in conjunction with, a Local Plan application for land on which oil and gas facilities and their associated setbacks are present. Terms of Reference for this Assessment are to be developed in consultation with the Approving Authority. This Assessment shall be used by the Approving Authority to determine whether the proposed development should be subjected to a greater setback distance and whether additional mitigation measures should be integrated at the time of development.	
23.21	With each Local Plan application, the applicant shall update the Risk Assessment with any changes to oil and gas infrastructure to ensure that it is current.	
23.22	Prior to a Local Plan application with lands in a setback area of oil and gas infrastructure, the applicant must consult with the operator and Alberta Energy Regulator.	










ASP Section 24: Implementation

Policy #	Policy Statement	Applicability
24.01	All costs associated with the construction and installation of transportation, water, wastewater, and shallow utility infrastructure within the Plan area (onsite) are the developer's responsibility. a. Where a developer has oversized infrastructure, they may be eligible for a cost recovery agreement.	
24.02	Offsite water, wastewater, and stormwater infrastructure construction costs that benefit the Plan area or other areas will be recovered by the appropriate levy or other cost recovery mechanisms.	
24.03	Development shall be required to pay the Rocky View County: a. Water and Wastewater Off-Site Levy; b. Stormwater Off-Site Levy; c. Transportation Off-Site Levy; and d. Any other new levy applicable to the development.	
24.04	Local Plans shall address and adhere to the requirements of the Prairie Gateway Area Structure Plan. In support of Local Plans and redesignation applications, the developer will be required to submit a rationale showing how their proposal is consistent with the vision and policies of the Prairie Gateway Area Structure Plan and supporting technical studies.	
24.05	Local Plans are to be prepared as per the policies of this Plan.	
24.06	As part of the Local Plan process, the identification, timing, and funding of any required improvements is required. Improvements that are:	

	<p>a. internal to the Plan area will be determined to the satisfaction of the County; and</p> <p>b. external to the Plan area, including provincial or The City of Calgary infrastructure, will be determined to the satisfaction of the County, in collaboration with The City of Calgary, and/or province.</p>	
24.07	Applications for redesignation and subdivision shall require the concurrent or prior adoption of a Local Plan, unless otherwise directed by the County.	
24.08	Subdivision applications shall address and adhere to the requirements of the supporting Local Plan and the policies of this Plan.	
24.09	The boundary of a Local Plan shall be determined in consultation with the County. Council shall have the discretion to consider alternative Local Plan boundaries.	
24.10	Where a Local Plan is not required, or is silent on a subject, the relevant policies of the Prairie Gateway Plan and Municipal Development Plan shall apply to redesignation and subdivision applications.	
24.11	<p>Applications for redesignation, subdivision, development, and Local Plans shall comply with the policies and requirements of the following master plans and servicing standards, as amended or replaced, unless otherwise directed by the policies of this Plan:</p> <p>a. Prairie Gateway Master Drainage Plan;</p> <p>b. Active Transportation Plan: South County;</p> <p>c. Recreation and Parks Master Plan;</p> <p>d. Rocky View County Solid Waste Master Plan;</p> <p>e. Rocky View County Servicing Standards; and</p> <p>f. Fire Services Master Plan.</p>	
24.12	Phasing of development within the Prairie Gateway Plan should be done in a logical and cost-effective manner guided by the availability of efficient and cost-effective utility services, Local Plan(s), and in lands in the Rail Served Policy Area, a Rail Design Shadow Plan.	
24.13	Development of industrial uses should proceed in an orderly manner, when serviced by existing or upgraded infrastructure and transportation networks.	
24.14	With the exception of Policy 24.15 and Policy 24.16 , subdivision approval requires confirmation that the regional utilities infrastructure required to service the subdivision are approved for construction or constructed.	
24.15	<p>Notwithstanding Policy 19.08 and Policy 19.16, no more than 160 contiguous acres of the gross developable Plan area may be permitted to subdivide using a temporary servicing solution in accordance with County policy if the following conditions are met:</p> <p>a. a potable regional water and wastewater system is not available at the Plan area boundary;</p> <p>b. a piped water and wastewater system that services the Plan area and can connect to a regional water and wastewater system is constructed;</p> <p>c. the developer enters into a deferred services agreement and connects to services when available;</p> <p>d. fire protection in accordance with all the applicable bylaws and codes is provided;</p> <p>e. no compensation will be provided for the costs incurred for the construction, decommissioning, and subsequent connection to a piped water and wastewater system; and</p> <p>f. the proposed temporary solution meets provincial regulations.</p>	
24.16	Notwithstanding Policy 21.05 , no more than 160 contiguous acres of the gross developable Plan area may be permitted temporary stormwater treatment ponds in accordance with County policy if the following conditions are met:	

	<p>a. external connections to the offsite stormwater management system are not available to the Plan area;</p> <p>b. a local stormwater treatment and conveyance system that services the Plan area and can connect to the main stormwater pipe is constructed;</p> <p>c. the developer enters into a deferred services agreement and connects to the stormwater system when available; and</p> <p>d. no compensation will be provided for the costs incurred for the construction, decommissioning, and subsequent connection to the piped stormwater system.</p>	
24.17	The Prairie Gateway ASP shall be subject to an assessment and possible full review every 10 years, or in accordance with the Municipal Development Plan, County policies, and the <i>Municipal Government Act</i> .	

ASP Section 25: Intermunicipal Collaboration and Cooperation

Policy #	Policy Statement	Applicability
25.01	This Plan shall be submitted to the Calgary Metropolitan Region Board Growth Plan for review and approval.	
25.02	The County shall consult and cooperate with The City of Calgary on planning, transportation and servicing matters that may arise within the Plan area in order to achieve a cooperative and coordinated outcome.	
25.03	All proposed Plan amendments and applications for redesignation, subdivision, development permit, including development permit renewals, shall be circulated to The City of Calgary in accordance with current IDP policy or as otherwise agreed to by the municipalities.	
25.04	Local Plans are to be consistent with the framework provided by this Plan. In addition to the following policies, the standard technical requirements of a conceptual scheme or master site development plan, as identified by the Municipal Development Plan, shall be applied.	
25.05	Prior to approval of Local Plan and land use applications, the County shall consider the use of appropriate mechanisms, such as joint studies and infrastructure cost sharing agreements, to address cross boundary impacts.	
25.06	Any Local Plan or land use applications located within the Plan area, together with all relevant supporting technical documents, shall be circulated to The City of Calgary. Collaboration on such applications shall begin at an early stage to allow sufficient time to identify and address potential impacts on The City of Calgary infrastructure and interests.	
25.07	The County shall work with The City of Calgary to explore intraregional transit options with connections to the Plan area, should they become viable.	
25.08	Prior to approval of a Local Plan or subdivision application adjacent to The City of Calgary, the County and The City of Calgary shall ensure that material cross boundary impacts are identified and addressed through Local Plan policy and/or subdivision conditions.	
25.09	<p>Rocky View County shall ensure that Local Plans and applications for redesignation and subdivision of lands in areas adjacent to The City of Calgary address:</p> <p>a. regional drainage to ensure the protection of required drainage corridors;</p> <p>b. alignment and connectivity of pathways, roadways, and utilities with the adjacent municipality; and</p> <p>c. other appropriate policies of this Plan.</p>	

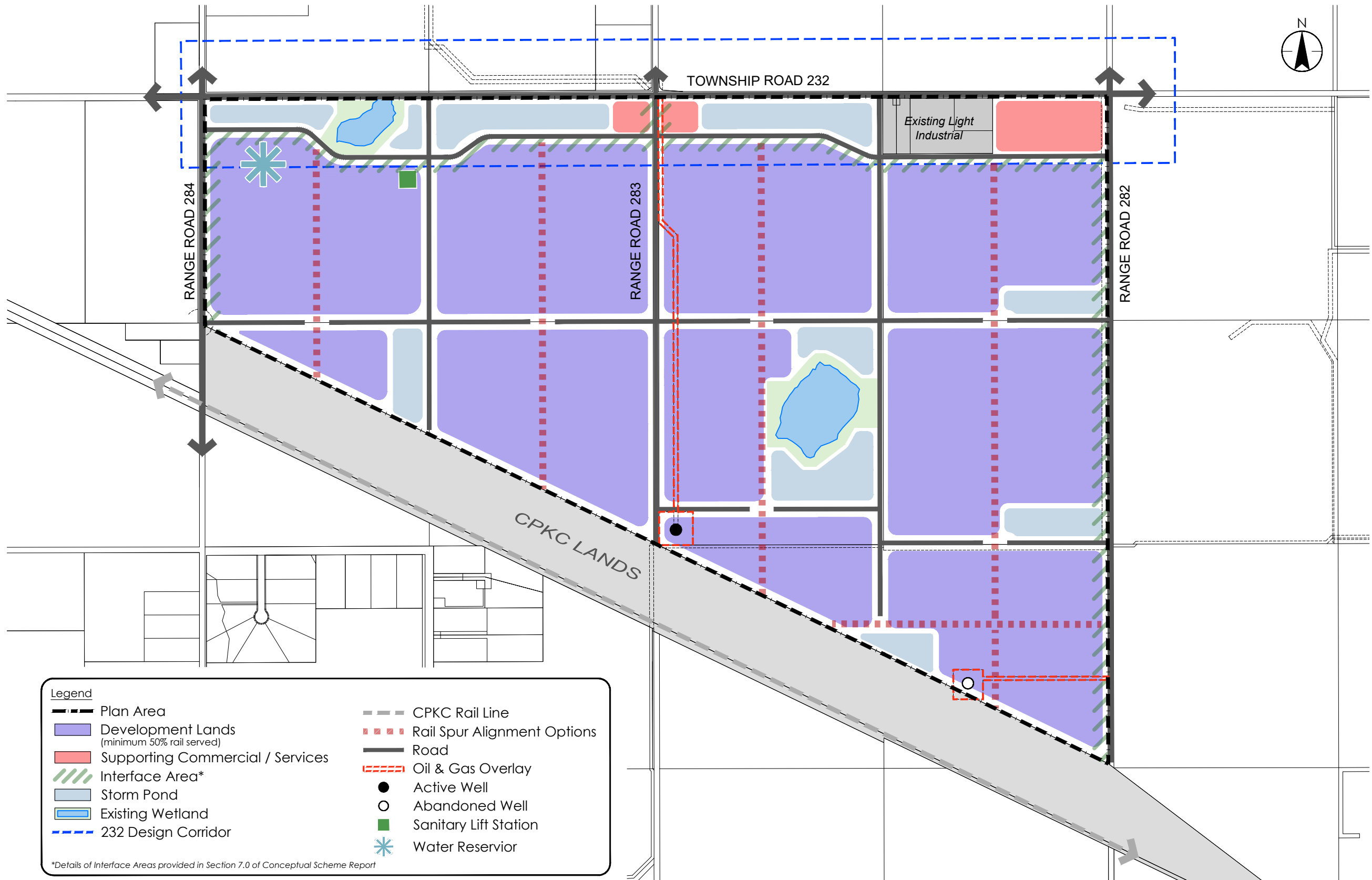


APPENDIX B: RISK ASSESSMENT

The Risk Assessment has been provided under a separate cover.



APPENDIX C: DEVELOPMENT CONCEPT



V:\1165\active\116536040\200_planning\300_outline_plan\00_cad\figures\116536040_fig-devconc.dwg Layout: dev conc

SHEPARD LOGISTICS CENTRE

Development Concept



Feb 2025

CONCEPT ONLY
THIS DRAWING IS AN ARTISTIC REPRESENTATION OF
DESIGNS PREPARED BY STANTEC CONSULTING LTD.
IT IS CONCEPTUAL IN NATURE AND SUBJECT TO CHANGE.
COPYRIGHT RESERVED.

116536040