

# Township Road 250 Conceptual Scheme



Approved – June 25, 2019



ROCKY VIEW COUNTY  
Cultivating Communities



## BYLAW C-7893-2019

### A Bylaw of Rocky View County known as the Township Road 250 Conceptual Scheme

The Council of Rocky View County enacts as follows:

#### PART 1 – TITLE

This bylaw shall be known as Bylaw C-7893-2019.

#### PART 2 – DEFINITIONS

In this bylaw, the definitions and terms shall have the meanings given to them in Land Use Bylaw (C-4841-97); Conrich Area Structure Plan (Bylaw C-7468-2015) and the *Municipal Government Act*.

#### PART 3 – EFFECT OF BYLAW

**THAT** Bylaw C-7893-201, known as the "Township Road 250 Conceptual Scheme", affecting a portion of NW-33-24-28-W04M and a portion of NE-33-24-28-W04M consisting of 54.26 hectares (134.09 acres) of land, be adopted as contained in Schedule "A", attached to and forming part of this Bylaw.

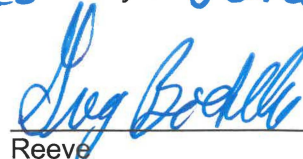
#### PART 4 – TRANSITIONAL

Bylaw C-7893-2019 is passed when it receives third reading, and is signed by the Reeve/Deputy Reeve and the Municipal Clerk, as per Section 189 of the *Municipal Government Act*.

#### Division: 5

**File:** 04333056, 04333032, 04333025, 04333046, 04333053, 04333029, 04333060, 04333048, 04333024, 04333057, 04333003, 04333026, 04333045, 04333059, 04333047, 04333054, 04333023, 04333042 – **PL20180130**

PUBLIC HEARING WAS HELD IN COUNCIL this 25<sup>th</sup> day of June, 2019  
 READ A FIRST TIME IN COUNCIL this 25<sup>th</sup> day of June, 2019  
 READ A SECOND TIME IN COUNCIL this 25<sup>th</sup> day of June, 2019  
 UNANIMOUS PERMISSION FOR THIRD READING 25<sup>th</sup> day of June, 2019  
 READ A THIRD TIME IN COUNCIL this 25<sup>th</sup> day of June, 2019

  
 \_\_\_\_\_  
 Reeve

  
 \_\_\_\_\_  
 CAO or Designate

  
 \_\_\_\_\_  
 Date Bylaw Signed



**ROCKY VIEW COUNTY**  
Cultivating Communities

**SCHEDULE 'A'**  
**FORMING PART OF BYLAW C-7893-2019**

A Conceptual Scheme affecting a portion of NW-33-24-28-W04M and a portion of NE-33-24-28-W04M consisting of 54.26 hectares (134.09 acres) of land herein referred to as the "Township Road 250 Conceptual Scheme".

# Township Road 250

## Conceptual Scheme

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# Township Road 250

## Conceptual Scheme

### Supporting technical studies (under separate cover):

1. Township Road 250 Traffic Impact Assessment. March 9, 2018. Watt Consulting Group
2. Conrich Stormwater Servicing Option Development. April 19, 2018. ISL Engineering and Land Services
3. Limited Phase 1 Environmental. April 2018. Tannas Conservation Services Ltd.
4. Biophysical Overview for Conceptual Scheme for Township 250 Transition Area. April 2018. Tannas Conservation Services Ltd.





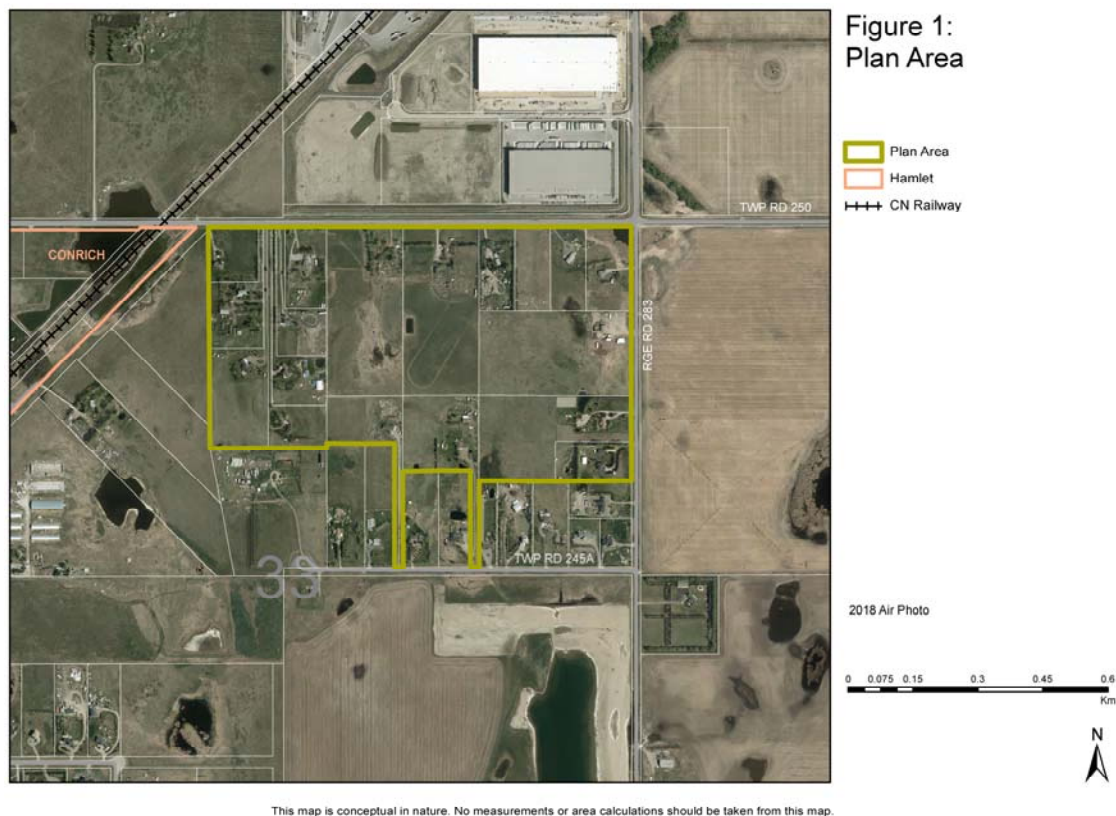
# 1

## Introduction

### Plan Area

The Township Road 250 Conceptual Scheme has been created as a collaborative effort between Rocky View County and the landowners in the Plan Area. The preparation of a Conceptual Scheme (CS) was the result of the Conrich Area Structure Plan (ASP) approved in 2015, which identified the CS as an action item to address the complex land use and design issues in this area. The CS responds to the changing nature of the Conrich area, and to recent adjacent industrial development to the north, providing a transition between the industrial developments and the residential and agricultural developments located to the south.

The Township Road 250 Conceptual Scheme applies to the area within the defined boundary as shown on [Figure 1: Plan Area Location](#). The Plan Area contains 18 parcels (see Table 1) ranging in size from 1.48 hectares (3.68 acres) to 6.90 hectares (17.06 acres), totaling approximately 54 hectares (134 acres), and is bounded by Township Road 250 to the north, Range Road 283 to the east, and panhandle parcels that touch Township Road 245A to the south.



**Table 1: Legal Parcels in Plan Area**


Roll Number	Lot Block Plan	Acres	Hectares
04333003	Lot 2, Block1, Plan 9411221	5.04	2.040
04333023	Block 4, Plan 731369	17.06	6.904
04333024	Block 3, Plan 731369	17.05	6.900
04333025	Block 6, Plan 1657 LK	17.06	6.904
04333026	Lot 1, Block 5, Plan 9012115	4.00	1.619
04333029	Lot 3, Block 2, Plan 9210427	4.47	1.809
04333032	Lot 11, Plan 9311225	13.05	5.281
04333042	Lot 6, Block 2, Plan 9210380	4.28	1.732

Roll Number	Lot Block Plan	Acres	Hectares
04333045	Lot 2, Block 5, Plan 9012115	4.00	1.619
04333046	Lot 3, Block 5, Plan 9012115	4.00	1.619
04333047	Lot 4, Block 5, Plan 9012115	5.07	2.052
04333048	Lot 5, Block 9, Plan 9111682	8.53	3.452
04333053	Lot 4, Block 2, Plan 9210427	4.08	1.651
04333054	Lot 5, Block 2, Plan 9210380	4.28	1.732
04333056	Lot 3, Plan 9212084	6.06	2.452
04333057	Lot 12, Plan 9311225	4.02	1.627
04333059	Lot 1, Block 1, Plan 9411221	3.68	1.489
04333060	Lot 3, Block 1, Plan 9411221	8.36	3.383
<b>TOTAL</b>		<b>134.09</b>	<b>54.176</b>

## Local Context

The Plan Area is located in close proximity to the hamlet of Conrich, which is a triangular-shaped area west of the Plan Area at the intersection of Conrich Road and Township Road 250. The hamlet includes several residences, a private school, a service station, wetlands/water bodies, and some industrial uses. Residential development in Conrich is located south and west of the Plan Area in the form of low density and country residential development.

Located directly north is Canadian National Railway's (CN's) Calgary Logistics Park and intermodal terminal that includes large industrial buildings and associated infrastructure adjacent to the rail line. The Logistics Park brings a hub of economic activity along with traffic, 24-hour lighting, noise, and other off-site impacts. Further development to the northeast of the Plan Area is anticipated as per the approved Conrich Station Conceptual Scheme, which will likely trigger the eventual upgrade of Township Road 250 to a full 50 metre right-of-way with limited access.



# 2

## Existing Site Conditions

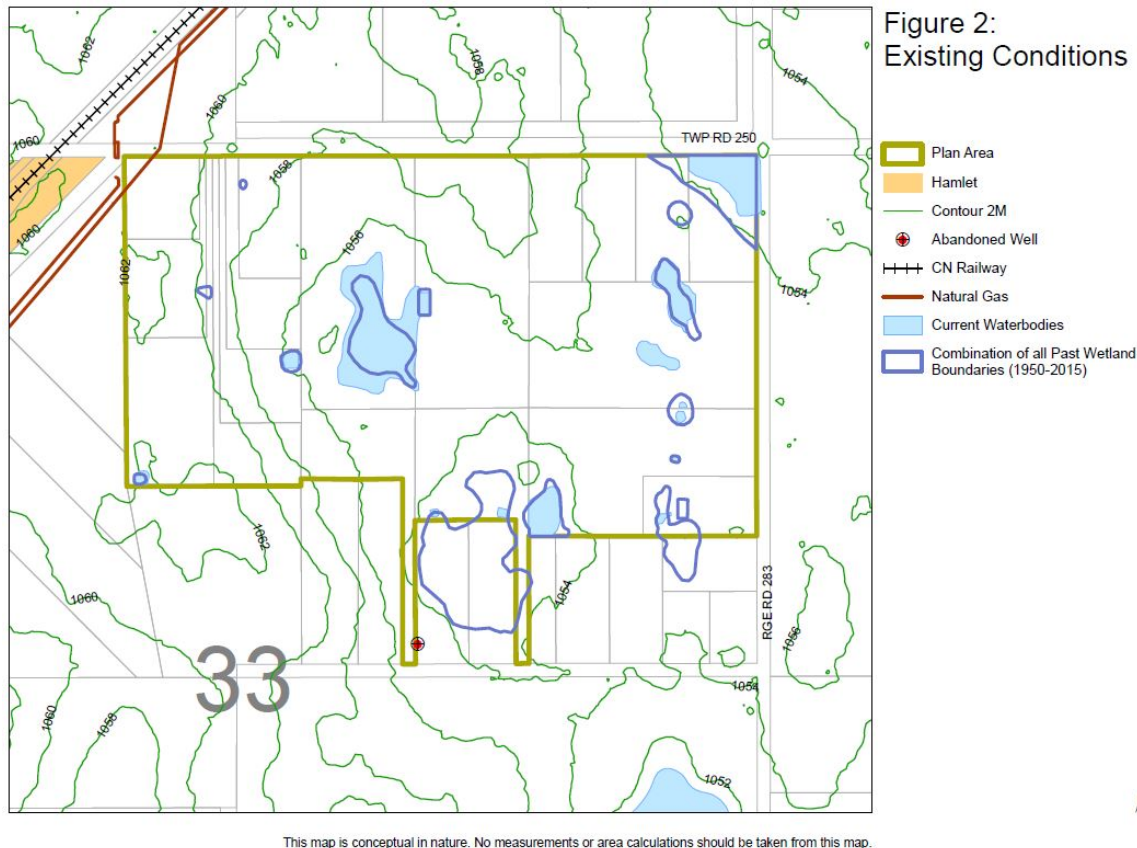
The following section documents the existing conditions that may impact future development of the Plan Area. The Plan Area currently exists as pasture, wetlands, and country residential development. Residences are generally widely spaced, with generous, landscaped yards, and include some keeping of livestock, such as horses and other animals. Crop production is minimal, with the majority of land either landscaped or left in a natural state.

### Biophysical Considerations

The Plan Area is generally flat, with the lands sloping across the site from the northwest to the south and east. Figure 2: Existing Conditions highlights the topography and surface drainage found within the Plan Area.







*Tannas Conservation Services Limited* completed a Biophysical Overview in support of this Conceptual Scheme to determine the existing environmental conditions of the Plan Area and to assess the potential environmental impacts as a result of disturbance. Based on review of multiple databases, neither the site nor surrounding areas contained any species of concern, Environmentally Significant Areas (ESAs), or protected areas.

Vegetation within the Plan Area is exclusively non-native forage crops (hay and residential lawns) as a result of the residential development on site. The likelihood of requiring vegetation surveys is therefore limited, as many existing wetlands have been developed; undeveloped wetlands may be subject to surveys. No rare plants have been documented within the Plan Area, making them unlikely to exist; although, without a physical survey, it cannot be confirmed if they are present on site.

Additionally, within five kilometers of the Plan Area, several wildlife species listed as “endangered”, “threatened”, or of “special concern” are listed in provincial and/or federal databases as occurring.

The hydrology of the Plan Area exists within the Shepard sub-basin, which flows south into the Bow River, and contains 13 wetlands and/or dugouts. Generally, the wetlands range between temporary (surface water is present for up to a couple of weeks during the average year) to

seasonal (surface water is present for up to a couple of months of the average year). There are no permanent or semi-permanent wetlands contained within the Plan Area. The dugouts and ponds that have been created show open water, but all appear to be human-made or wetlands that have been manipulated by human activity (refer to [Figure 2: Existing Conditions](#), which highlights the general location of the wetlands and the current and historical boundaries as determined by the Biophysical Overview).

Due to the potential for rare plants in wetland areas, rare plant surveys may need to be completed in spring/summer during appropriate survey windows. Impacts to wildlife will likely be minimal, but a wildlife survey may need to be conducted due to the presence of wetlands and the potential for nine wildlife species of concern being found within five kilometers of the site.

### Policies

- 2.1.1** A Biophysical Impact Assessment (BIA) and/or Wetland Assessment and Impact Report (WAIR) may be required in support of a redesignation, subdivision, or development application if determined necessary by the County and/or Alberta Environment & Parks.

## Historical Resources

A Historical Resource Overview (HRO) was completed by the firm The Archaeology Group in January 2018. The HRO concludes that no further Historical Resources work is recommended for the Plan Area, as there is little potential for finding undisturbed historical resource due to the previously disturbed nature, and the lack of significant topography or hydrologic features. Where sites of archaeological or historical importance are identified during the subdivision approval stage, it is important that these sites are addressed prior to development of the Planning Area, and that clearance is obtained from Alberta Culture pursuant to the Historical Resources Act. A Historical Resources Clearance must accompany subsequent plans in order to ensure that potential archaeological or historical resources are not compromised.

### Policies

- 2.2.1** The developer shall provide Historical Resource Clearance evidence of compliance with Alberta Culture and Tourism prior to subdivision.

## Transportation

Three existing roadways provide access to the Plan Area (See [Figure 1](#)). The roads include:

- **Township Road 250:** A two-lane, east-west, undivided, Secondary Highway with a paved surface treatment, connecting the Plan Area to Stoney Trail and the city of Calgary.



- **Range Road 283:** A two-lane, north-south, gravel roadway; the intersection and portions north of Township Road 250 were recently upgraded by CN in support of the construction of the Calgary Logistics Park at Conrich. Range Road 283 provides a direct connection to the Trans-Canada Highway to the south.
- **Township Road 245A:** A two-lane, east-west, gravel roadway with a cul-de-sac that provides access to the southern portion of the Plan Area via two panhandle driveways. Township Road 245A only accommodates local traffic.

**Driveway Access:**

There are currently 18 direct driveway access points to the Plan Area based on the existing lot ownership pattern. Some of these driveway access points are from panhandle lot configurations.

## Pipelines & Well Sites

There is an existing high pressure natural gas pipeline located just inside the northwest boundary of the Plan Area. There is also an abandoned well (Nexen Energy) located at 10-33-024-28-W4M along Township Road 245A, which appears to be located on Lot 3, Plan 9212084 within the Plan Area (see [Figure 2: Existing Conditions](#)).

Future development must comply with all regulatory setbacks pertaining to the existing abandoned well site, and consultation with the owners/operators of each of these infrastructure components is expected to occur at each development stage as contemplated by this Plan.

### Policies

- 2.4.1** Appropriate development setbacks for the well and the pipeline apply and shall be respected until such time the well is abandoned and reclaimed to the standards required by the Alberta Energy Regulator (AER).

# 3

## Policy Context

Preparation of the Township Road 250 Conceptual Scheme was identified as a key action item within the ASP. The impact of industrial activity adjacent to the Plan Area has resulted in the need to provide a transition area for the Plan Area that better anticipates future development and reflects the market opportunities available to landowners, while ensuring adequate screening and buffering of different land uses. The preparation of the Conceptual Scheme included review of the Area Structure Plan and the County's Land Use Bylaw.

### Conrich Area Structure Plan

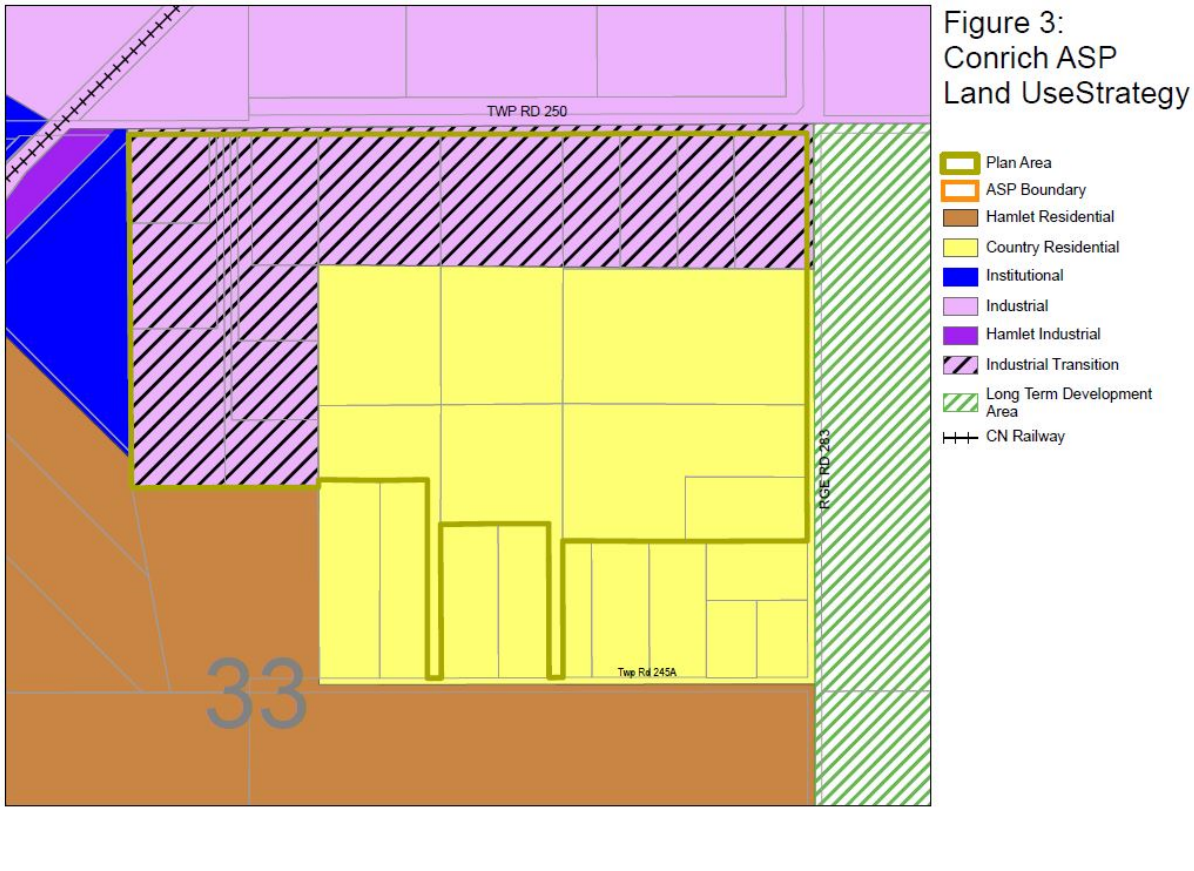
The Conrich Area Structure Plan (Bylaw C-7468-2015) applies to approximately 4,402 hectares (10,877.58 acres) and provides an intermediary step in the planning process that offers policy guidance to local plans. The ASP establishes development direction for the Plan Area, in alignment with the County Plan's Regional Business Centre designation. As shown on [Figure 3](#), the ASP's Land Use Plan identifies the following two land uses within the Township Road 250 Plan Area:

- 1) Industrial Transition (north portion); and
- 2) Country Residential (south portion).

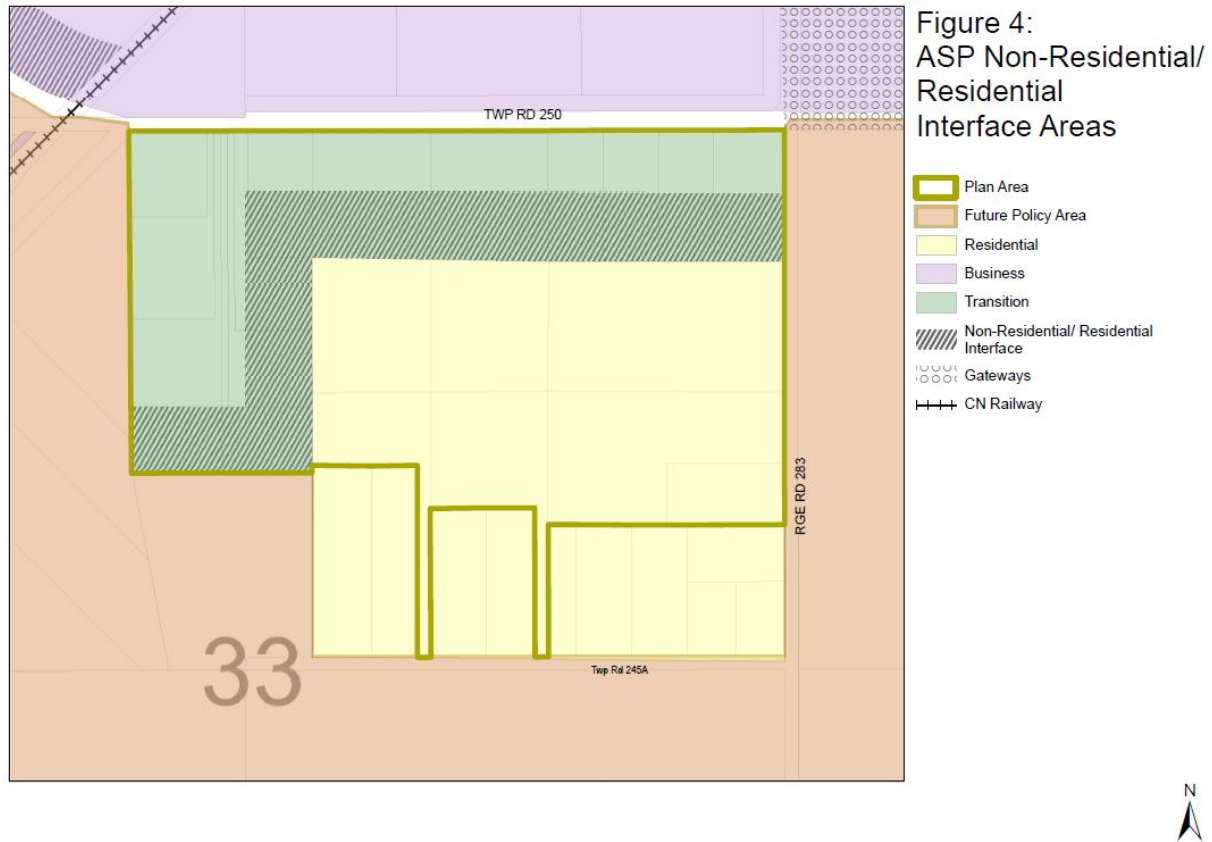
The ASP supports both the existing Country Residential uses within the south portion and the transition to industrial or commercial uses in the north of the Township Road 250 Plan Area, as well as the ability for live/work in the industrial transition area.

A key aspect of the ASP is a need to appropriately mitigate the interface between the Industrial Transition and Country Residential uses (see [Figure 3](#)). The ASP highlights several prescriptive policies regarding interface mitigation that specifically apply to the Plan Area.

This conceptual scheme (a local plan) is based on a predetermined boundary outlined in the ASP. As such, the general concept of this plan aligns with the policy framework and development intent of the ASP.



This map is conceptual in nature. No measurements or area calculations should be taken from this map.



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## Existing Land Use Designations

The Land Use Bylaw identifies the entire Plan Area as a Residential Two (R-2) District. The purpose of the R-2 District is to provide a residential use on a small parcel of land that accommodates minor agricultural pursuits and accessory buildings. Figure 5: Existing Land Use Designations highlights the existing and adjacent land use designations.

The Plan Area is adjacent to R-1 and R-2 lands in the south and west, which are also residential in nature. Agricultural uses such as Ranch and Farm (RF) and Farmstead (F) exist to the east and southeast of the site, while Industrial – Industrial Activity (I-IA) exists to the north and northeast. Lands south of Township Road 245A are a Direct Control District (DC-134), which provides for a comprehensive mixed use development as part of the Buffalo Hills Conceptual Scheme.

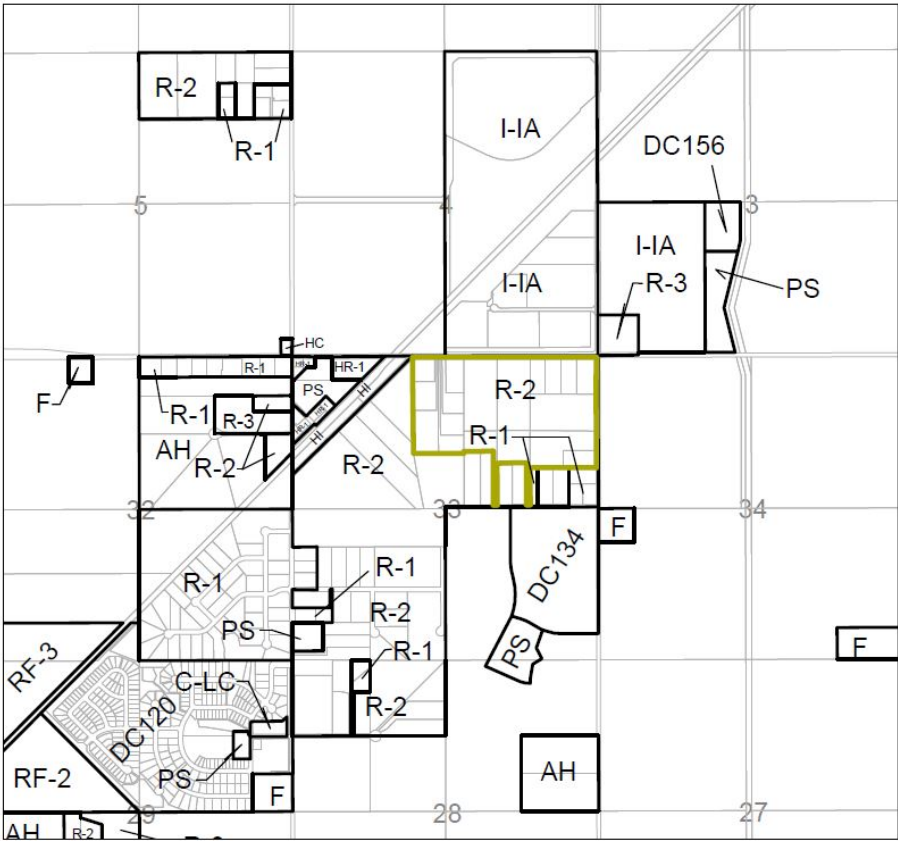
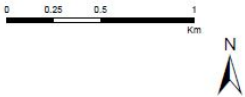


Figure 5  
Existing Land Use  
Designations

Plan Area



This map is conceptual in nature. No measurements or area calculations should be taken from this map.



## 4

## Community Engagement

Community engagement was a key aspect in the development of this Conceptual Scheme and included a four-phased approach. A summary report was produced after each event and posted on the County's website. The first three phases played an important role in developing the Conceptual Scheme design concept and associated policies, while the last phase communicated the results. Table 2: Engagement Summary identifies the purpose and the result of each phase of engagement.

**Table 2: Engagement Summary**

Phase	Purpose	Feedback Summary
<b>Phase 1: Project Initiation</b>	<ul style="list-style-type: none"> <li>Inform residents and landowners about the project timeframes and existing ASP policies affecting the subject area;</li> </ul>	<ul style="list-style-type: none"> <li>General feedback from attendees at the end of the evening was positive and reflected an increased understanding of the Conrich ASP policies and the project.</li> </ul>
<b>Open House #1 October 30, 2017</b>	<ul style="list-style-type: none"> <li>Answer questions and receive feedback on the proposed project and related policies and issues; and</li> <li>Understand from residents/landowners the existing level of knowledge about the Conrich ASP, level of development interest and initial preferences of types of industrial development in the industrial transition area.</li> </ul>	<ul style="list-style-type: none"> <li>Stakeholders are generally well-established in this area, desire live/work or light industrial lands uses, and have a broad spectrum of development intentions for their land.</li> <li>Residents stated a lack of prior knowledge of the Conrich ASP.</li> </ul>



Phase	Purpose	Feedback Summary
<b>Phase 2:</b> <b>Setting the Direction</b> <b>Stakeholder Meeting</b> <b>November 29, 2017</b>	<ul style="list-style-type: none"> <li>Share results of first open house; and</li> <li>Collaborate, design, and discuss potential concept ideas for the Township Road 250 Conceptual Scheme.</li> </ul>	<ul style="list-style-type: none"> <li>The general consensus from the group was that compatible developments in the industrial transition area should be low-impact and aesthetically pleasing.</li> <li>A list of appropriate land uses and lot sizes was finalized.</li> <li>Two road options emerged.</li> <li>Consensus was reached regarding the appropriate buffering technique.</li> </ul>
<b>Phase 3: Draft Design &amp; Policies</b>  <b>Open House #3</b> <b>January 30, 2018</b>	<ul style="list-style-type: none"> <li>Receive feedback from landowners and stakeholders on the proposed conceptual scheme design options, project objectives, and policy directions.</li> <li>Provide feedback on preferred Conceptual Scheme Designs and related policies.</li> </ul>	<ul style="list-style-type: none"> <li>Stakeholders selected a preferred design.</li> <li>Current residents strongly desired a live-work overlay.</li> <li>Business-commercial land uses with smaller lots are preferred to light industrial uses.</li> <li>Business hours, traffic, noise, and other operation aspects must be controlled to minimize the impact on existing residents.</li> <li>Existing landowners should not have to apply for live-work zoning.</li> <li>15 m dedication of land on the south side of Township Road 250 should not be required.</li> </ul>
<b>Phase 4:</b> <b>Completion and Adoption</b> <b>Open House #4</b> <b>November 14, 2018</b>	<ul style="list-style-type: none"> <li>Receive feedback from landowners and stakeholders on the proposed conceptual scheme design options, project objectives, and policy directions.</li> </ul>	<ul style="list-style-type: none"> <li>Stakeholders reviewed the draft plan.</li> <li>Some residents were happy with the plan, and a few were unhappy with it.</li> <li>Questions were raised regarding whether off-site levies were required, and how the boundary of the plan area was determined.</li> <li>Preference that road dedication not be required.</li> <li>A landowner seeking to develop wants to meet with their immediate neighbours in the plan area to see about working together</li> </ul>

# 5

## DEVELOPMENT CONCEPT

The Plan Area is envisioned as a transitional business commercial and country residential area. Business commercial developments will primarily serve local residents and those employed in the Conrich area. The character of the Plan Area will create a suitable transition from the large-format industrial development to the north and the residential character to the south.

The design of the Plan Area as shown on [Figure 6: Development Concept](#) displays a grid network design that can be developed logically over time and strategically utilizes road rights-of-way to separate land uses where possible, with the addition of landscape buffering to further reduce land use conflicts. The overall design seeks to provide a framework for the transition from residential to limited business commercial that respects the adjacent residential development. This is accomplished in two phases:

Phase 1 – Transition: Residential with Live-Work Overlay (no land use redesignation required – land use rights established through amendment of the Land Use Bylaw to incorporate the live-work overlay)

Phase 2 – Final Configuration: Limited Business Commercial (land use redesignation required)

Phase 1, the transitional phase, will apply to the area shown as Live-Work Overlay on [Figure 6: Development Concept](#), allowing the northern most existing R-2 residential area to take on transitional business commercial opportunities of limited scope. Over time, it is anticipated these areas will seek to develop into more formal business commercial uses that will require redesignation to achieve. It is then that the final configuration, or Phase 2, will have been achieved. By requiring the use of design elements, such as landscape buffers, the relationship between this transition area and the relatively more permanent Country Residential area is managed. Separately, the R-2 residential areas in the southern most plan area have been provided a policy framework for developing to R-1 residential uses, but without an overlay. Redesignation will be required to change from R-2 to R-1 to accommodate smaller parcels.





Within the Plan Area, residential/non-residential interfaces have been carefully considered through both policy and design. Where the residential/non-residential interface is separated by a road, the right-of-way has been expanded to 30 metres to accommodate a pathway and additional landscaping. Where the interface is simply a property line, additional landscaping requirements will be required on the non-residential parcels to ensure any negative impacts to the residential area are minimized. In consideration of this transitional live-work overlay, a series of requirements provides for design elements such as: prohibiting RV storage and any uses that may cause off-site impacts, landscaping requirements to buffer between business and residential uses, and proposed separation of roadways based on uses (where truck roadways would be separated from residential roadways).

Full build-out of the Plan Area, including complete stormwater, water and waste water servicing, will be challenging given the current divided land ownership. Consolidation of existing parcels may be required to facilitate viable, fully-serviced business commercial parcels, and to create the economies of scale necessary to fund the infrastructure investment required.

## Development Concept

The Development Concept has been prepared in accordance with the Conrich Area Structure Plan. [Figure 6: Development Concept](#) illustrates the land uses and associated road network anticipated for the Plan Area. The Development Concept divides the Plan Area into two distinct land use areas and an overlay policy area. The land use areas include:

- Transitional Area:
  - Live-Work Overlay (Phase 1, transition);
  - Limited Business Commercial designation (Phase 2, final configuration);
- Country Residential.

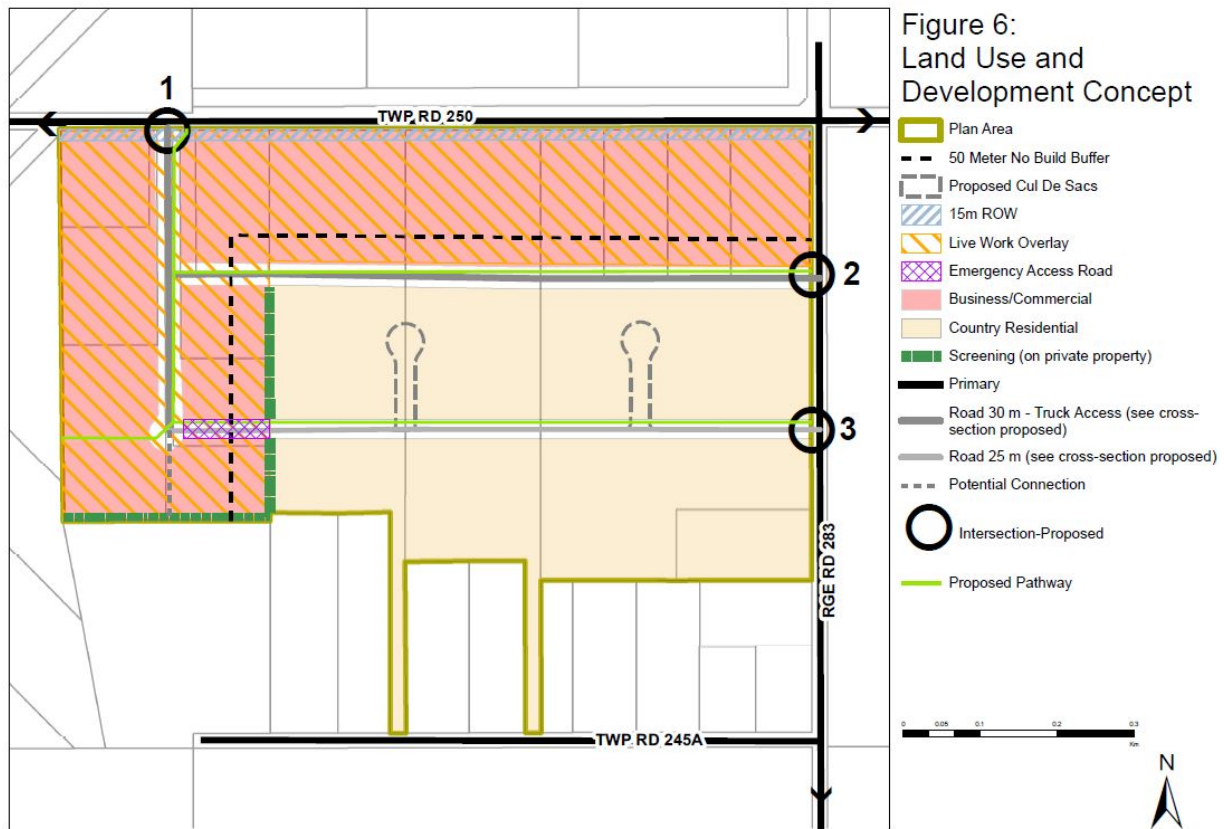
## Development Statistics

A summary of the land use policy areas proposed for the Plan Area, including the area and proportion of each use, is summarized in [Table 3: Plan Area Statistics](#).

Table 3: Plan Area Statistics

Land Use	+/- Acres	+/- Hectares	Proportion of Plan Area (%)
<b>Transitional Area (includes Live-Work overlay)</b>	56.2	22.743	42.0%
<b>Country Residential</b>	57.9	23.431	43.2%
<b>Internal Rights-of-Way (roads, pathways, landscaping)</b>			
a) 30 m right of way	9.1	3.683	6.8%
b) 25 m right of way	5.0	2.023	3.7%
<b>Township Road 250 road widening (15 metres)</b>	3.6	1.457	2.7%
<b>Range Road 283 road widening (15 m)</b>	2.1	0.850	1.6%
<b>TOTAL</b>	<b>134.09</b>	<b>54.187</b>	<b>100%</b>





## Transitional Area: Phase 1– Live-Work Overlay

The Live-Work Overlay provides an additional layer of policy for the lands identified as Business Commercial. The Live-Work Overlay responds directly to a desire of the Plan Area's existing landowners to allow larger home-based businesses on existing residential parcels. This development opportunity allows for applications of home-based businesses of a greater size and intensity than is presently permitted by the County, and provides flexibility as the area transitions to a fully serviced Business Commercial area. This additional policy layer no longer applies once a parcel of land is redesignated to a Business Commercial district.

**Table 4: Policy Summary – Live-Work Overlay**

Policy & Design Criteria	Live Work
<b>Predominant uses (temporary)</b>	<ul style="list-style-type: none"> <li>• Single detached residence (must remain)</li> <li>• Home-based business</li> </ul>
<b>Prohibited uses</b>	<ul style="list-style-type: none"> <li>• Outdoor RV storage</li> <li>• Any uses with significant off-site impacts</li> </ul>
<b>Minimum Lot Size</b>	<ul style="list-style-type: none"> <li>• Existing lot size (<math>\pm</math> 3.95 acres)</li> </ul>
<b>Maximum Building Height</b>	<ul style="list-style-type: none"> <li>• 10 m (32.81 ft.) for all buildings</li> </ul>
<b>Maximum Building Footprint</b>	<ul style="list-style-type: none"> <li>• Principal building – as per land use district</li> <li>• Maximum footprint for a single accessory building is 500 m<sup>2</sup> (5,382 sq. ft.)</li> <li>• Maximum area for all accessory building(s) is: 500 m<sup>2</sup> (5,382 sq. ft.)</li> </ul>
<b>Water &amp; Waste Water Servicing</b>	<ul style="list-style-type: none"> <li>• Home Based Businesses may potentially be able to utilize existing on-site services if the on-site system has capacity. Once lands redesignate to Business Commercial, tie-in to both water and waste water will be required.</li> </ul>
<b>Stormwater Servicing</b>	<ul style="list-style-type: none"> <li>• On-site stormwater management required until piped stormwater servicing is available</li> </ul>
<b>Building Exterior</b>	<ul style="list-style-type: none"> <li>• Exterior of accessory / home-business building(s) should match or complement the residence</li> </ul>

## Objectives

To create an interim opportunity for home-based businesses of greater size and intensity than is generally permitted within a country residential area to assist with the transition of the area to a fully serviced Business Commercial area.

## Policies

The Live-Work Overlay policies shall only apply to existing residential lots identified as Transitional Area until such time as a lot is redesignated to a non-residential district.

### Policies - Land Uses

**5.3.1** The Live-Work Overlay should be implemented in the County's Land Use Bylaw.

### Policies - Architecture & Site Design

**5.3.2** The Live-Work Overlay area is intended for home-based businesses operated by an owner or resident operator. Home based businesses in the Live-Work area shall:

- a. require that the residence remain;
- b. be an appropriate size and scale for the residential character of the area;
- c. require a Development Permit;
- d. notwithstanding ASP policies for water and wastewater, may be serviced from existing on-site water and waste water services;
- e. mitigate any potential off-site impacts (e.g. noise, traffic, dust);
- f. may require screening or landscape buffering for outside storage as per the land use bylaw (per home-based business development);
- g. Ensure building placement so as not to be placed within the required 50 m landscape buffer;
- h. contain not more than six (6) non-resident employees;
- i. for those activities occurring outside of an enclosed building, limit the hours of operation to 8:00 a.m. and 7:00 p.m.;

**5.3.3** The maximum area for all live-work buildings shall not exceed 500 m<sup>2</sup> (5,382 sq. ft.).

**5.3.4** The maximum height for all live-work buildings shall not exceed 10 m (32.81 ft.).

**5.3.5** Development applications for the Live-Work Overlay area may be required to provide, in addition to the County's standard application requirements, the following:

- a. Technical studies addressing drainage and storm water management.
- b. Documentation of existing site conditions including any environmental features (e.g. wetlands) and how the proposed development will address these existing features.

## Transitional Area: Phase 2 – Business Commercial

After the Live-Work transition, the Business Commercial area is the final configuration that will allow for a mix of light industrial, commercial, office, small to medium retail, and institutional developments that benefit from transportation access, adjacent industrial developments, and exposure to Township Road 250. The Business Commercial area will accommodate developments that have minimal off-site impacts and are compatible with the existing country residential character of the area. Any parcels located within the Plan Area that seek to develop into the business commercial uses will require redesignation.

An attractive built form is envisioned through the implementation of architectural standards and landscaping requirements. Buffering between the Business Commercial and Country Residential areas will be accomplished through either an expanded road right-of-way that includes a landscaped pathway, or, where a road does not separate these uses, through a landscape buffer. The intent of the northern east-west roadway is to exclusively serve truck traffic related to the business uses, and to separate the roadway from the Country Residential uses adjacent to, and south of, the transitional area.

### Objectives

To attract business development by identifying and defining the appropriate light industrial, business, and commercial uses for the Township Road 250 Transition Area that balances the intent of the higher order statutory plans with the needs of the existing community.

To demonstrate sensitivity and respect for adjacent residential properties through appropriate design measures, interface transitions, and buffering.

### Policies – Land Uses

**5.4.1** Development of the Business Commercial area shall comply with Figure 6: Development Concept.

**5.4.2** Developments within the Business Commercial area shall be small to medium in scale, accommodate low-impact development that occurs within enclosed

buildings or properly screen outside areas, and may include such uses as restaurants, commercial, office, religious assembly, institutional uses, or buildings, greenhouses, small-scale industrial, light manufacturing, and artist studios.

**Table 5: Policy Summary – Business Commercial**

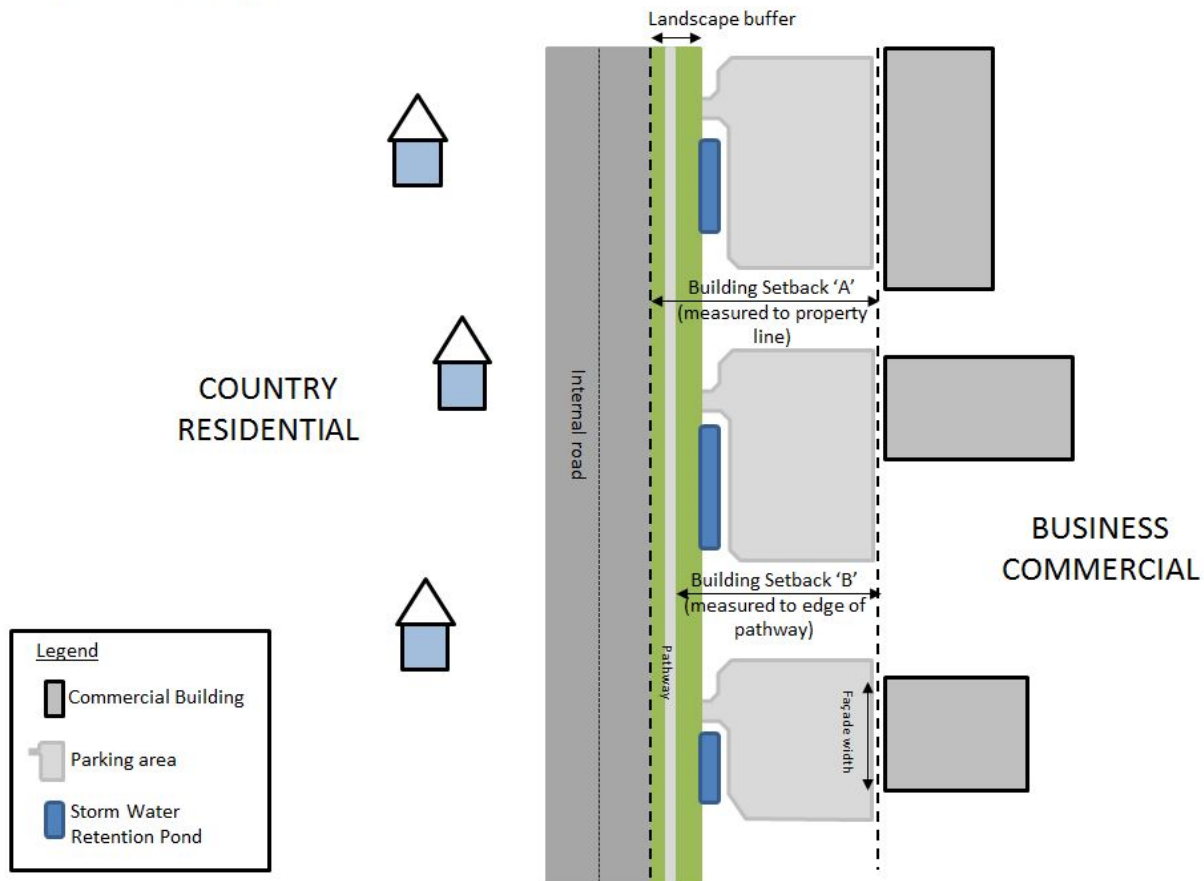
Policy & Design Criteria	Business Commercial	
<b>Predominant uses</b>	<ul style="list-style-type: none"> <li>• Retail; small to medium format</li> <li>• Office, Commercial</li> <li>• Institutional, Religious assembly</li> <li>• Greenhouses / horticulture</li> </ul>	<ul style="list-style-type: none"> <li>• Small or medium manufacturing</li> <li>• Light industrial</li> <li>• Artist studios</li> </ul>
<b>Temporary Uses</b>	<ul style="list-style-type: none"> <li>• Live-work / home-based businesses</li> </ul>	
<b>Prohibited uses</b>	<ul style="list-style-type: none"> <li>• Large format warehouse or retail, heavy industry</li> </ul>	
<b>Minimum Lot Size</b>	<ul style="list-style-type: none"> <li>• 1.01 hectares (2.5 acres)</li> </ul>	
<b>Maximum Building Height</b>	<ul style="list-style-type: none"> <li>• 12.5 m</li> </ul>	
<b>Maximum Building Footprint</b>	<ul style="list-style-type: none"> <li>• 4,000 m<sup>2</sup> (43,055 ft<sup>2</sup>)</li> </ul>	
<b>Lot Coverage</b>	<ul style="list-style-type: none"> <li>• 50% for lots <i>adjacent to the Country Residential area</i><sup>1</sup></li> <li>• For lots not <i>adjacent to the Country Residential area</i><sup>1</sup>, lot coverage shall be as per the Land Use Bylaw.</li> </ul>	
<b>Façade Width</b>	<ul style="list-style-type: none"> <li>• Façade widths that exceed 30 m shall incorporate wall plane projections or recesses of at least 3% of the width of the façade that extend at least 20% of the façade width.</li> </ul>	
<b>Minimum frontage building setback</b>	<ul style="list-style-type: none"> <li>• For Business Commercial lots <i>adjacent to the Country Residential area</i><sup>1</sup>, including those separated by a road, the non-residential building must be setback a minimum of 50 m from the non-residential property line (Setback 'A' on <a href="#">Figure 11</a>), or setback 50 m from the far edge of pathway where present (Setback 'B' as on <a href="#">Figure 11</a>).</li> <li>• For lots not <i>adjacent to the Country Residential area</i>, the setbacks are as per the Land Use Bylaw.</li> </ul>	

Policy & Design Criteria	Business Commercial
	<ul style="list-style-type: none"> <li>Landscaping and screening shall meet Land Use Bylaw requirements for visual impacts and screening, with reference to outdoor storage areas, or landscape buffer requirements in this conceptual scheme.</li> </ul>
<b>Water &amp; Waste Water Servicing</b>	<ul style="list-style-type: none"> <li>Municipal piped water and waste water servicing required.</li> </ul>
<b>Stormwater Servicing</b>	<ul style="list-style-type: none"> <li>Zero discharge and on-site evaporation ponds required until a stormwater conveyance system is available.</li> </ul>

<sup>1</sup> **Adjacent to the Country Residential area** means any lot sharing a property line with the Country Residential area or a lot that is located directly across a road from the Country Residential area.



Figure 7: Site Design Diagram



### Policies – Servicing

- 5.4.3** All Business Commercial development shall be serviced with piped municipal water and waste water.
- 5.4.4** All Business Commercial development shall adhere to County standards for stormwater management in accordance with the Conrich Stormwater Servicing Option Development, the Conrich Master Drainage Plan, and Rocky View County Standards, including on-site evaporation ponds and zero discharge until a stormwater conveyance system is available.
- 5.4.5** Where upgrades to regional transportation infrastructure or municipal servicing provided by a developer are demonstrated to benefit lands outside the Conceptual Scheme area, the County shall facilitate recovery of costs in accordance with the Infrastructure Cost Recovery Policy.

## Policies – Roads

- 5.4.6** The road network within the Business Commercial area shall adhere to Figure 6: Development Concept. Additional roads in the Plan Area beyond those identified in Figure 6 are discouraged.

## Policies – Architecture & Site Design

- 5.4.7** The *minimum parcel size* in the Business Commercial area shall be 1.01 hectares (2.5 acres).
- 5.4.8** The *maximum building height* in the Business Commercial area shall be 12.5 m, or lower where required by the County's Land Use Bylaw.
- 5.4.9** Developments in the Business Commercial area shall be limited to small to medium sized developments with a *maximum building footprint size* of 4,000 m<sup>2</sup> (43,055 ft<sup>2</sup>).
- 5.4.10** Building *façade widths* that exceed 30 m shall incorporate wall plane projections or recesses of at least 3% of the width of the façade that extend at least 20% of the width.

### Setbacks

- 5.4.11** Setbacks for development shall be per the Land Use Bylaw, except where specific policies in this Conceptual Scheme require greater setbacks.
- 5.4.12** For Business Commercial lots adjacent to the Country Residential area, including those separated by a road, the non-residential building must be setback a minimum of 50 m from the non-residential property line (Setback 'A' on Figure 11), or setback 50 m from the far edge of pathway where a pathway is present (Setback 'B' as on Figure 11).

*For policy 5.4.12 (above), “adjacent to the Country Residential area” means any lot sharing a property line with the Country Residential area or a lot that is located directly across a road from the Country Residential area.*

- 5.4.13** Business uses located in the non-residential/residential interface (Figure 6) shall comply with the Conrich ASP Non-Residential/Residential Interface policies.

### Architectural Design

- 5.4.14** All development applications in the Business Commercial area shall be of high-quality building design, address compatibility with adjacent development and



non-residential – residential interfaces, and conformance with the County's **Commercial, Office and Industrial Design Guidelines**;

- 5.4.15** All private lighting, including security and parking area lighting, shall be designed to respect the County's "dark sky" Land Use Bylaw requirements, conserve energy, reduce glare, and minimize light trespass onto surrounding properties.

## Landscape Buffer

This Conceptual Scheme identifies, in the southwest portion of the Plan Area, a Country Residential and Business Commercial interface area that is not buffered by a public right of way. Figure 8: Landscape Buffer identifies this area with a green hatched line titled "landscape buffer." The intent of this area is to provide a landscape buffer on the private Business Commercial lands in order to appropriately mitigate any potential negative visual impacts associated with Business Commercial developments. Figure 8 conceptually illustrates how a 10 metre green buffer, including landscaping and a screening fence, can be used to provide a sufficient separation between non-residential and residential developments.

**Figure 8: Landscape Buffer**



## Policies – Landscaping

- 5.5.1** The developer of a Business Commercial lot shall be responsible for providing the appropriate level of screening and landscaping between the Business Commercial and Country Residential areas in alignment with Figure 8 and the County **Landscaping Guidelines**.

**5.5.2** Screening should generally align with Figure 8 in order to appropriately mitigate any negative impacts associated with Business Commercial developments.

**5.5.3** The developer shall comply with the County's Land Use Bylaw landscaping and screening requirements and the **County's Commercial, Office, and Industrial Design Guidelines** and document how the local plan meets those requirements and guidelines.

## Country Residential

The County Residential Area is intended to support the continuation of rural living and existing country residential development while allowing for the re-subdivision of larger parcels in compliance with this Conceptual Scheme. Country Residential lots will be required to connect to water and waste water services once available.

### Objective

To promote the continuation of rural living and existing country residential development while allowing for the re-subdivision of larger parcels in compliance with this Conceptual Scheme.

**Table 6: Policy Summary – Country Residential**

Policy & Design Criteria	Country Residential
<b>Predominant uses</b>	<ul style="list-style-type: none"> <li>Single detached residence</li> </ul>
<b>Minimum Lot Size</b>	<ul style="list-style-type: none"> <li>As per R-1 land use district</li> </ul>
<b>Maximum Building Height</b>	<ul style="list-style-type: none"> <li>As per R-1 land use district</li> </ul>
<b>Water Servicing</b>	<ul style="list-style-type: none"> <li>Single lot subdivisions – on-site (well and septic) servicing may remain; newly created parcels will be required to connect to water services</li> <li>Multi-lot subdivisions – piped water servicing</li> </ul>
<b>Waste Water Servicing</b>	<ul style="list-style-type: none"> <li>On-site servicing until servicing is available; Deferred Services Agreement will be required once wastewater systems available</li> </ul>
<b>Storm Water Servicing</b>	<ul style="list-style-type: none"> <li>On-site storm water management required until storm water conveyance system is available</li> </ul>

## Policies

- 5.6.1** Residential development shall be supported in the area identified as Country Residential on Figure 6: Development Concept.
- 5.6.2** Lot sizes within the Country Residential area shall be a minimum of 0.8 hectares (2 acres) in area.
- 5.6.3** The road network within the Country Residential area shall adhere to Figure 6: Development Concept. Additional roads in the Plan Area beyond those identified in Figure 6 are discouraged.
- 5.6.4** Multi-lot country residential subdivisions shall be required to connect to piped water servicing.
- 5.6.5** Multi-lot country residential subdivisions shall be required to connect to piped waste water servicing if available.
- 5.6.6** On-site servicing (water well and septic system) may be permitted for single-lot subdivisions based on verification from supporting studies (e.g. water well / groundwater study, private septic system treatment study).
  - a. If on-site servicing is permitted, the applicant/owner shall be required to enter into a Deferred Servicing Agreement with the County to connect to piped water and waste water servicing once available.
- 5.6.7** Country residential subdivisions (single or multi-lot) may be required to consolidate existing panhandle access roads into County standard public roadways.

# 6

## Road Network & Interfaces

This Conceptual Scheme identifies an internal road network to accommodate not only vehicular mobility, but also the inclusion of landscaped areas and a greenway to promote walkability and provide a buffer between the Business Commercial and Country Residential areas. This mobility network enables connections between the various uses internal and external to the Plan Area while providing a spatial separation between non-residential and residential uses. This Conceptual Scheme aligns with the Conrich ASP, with intent to promote, conserve, and enhance an interconnected open space system, while providing the appropriate interface between future business and residential areas. The Plan also has regard to the Active Transportation Plan adopted by the County in October 2018, which seeks to provide a backbone active transportation network.

The internal road network is expected to conform to [Figure 6](#) – Development Concept. Access will be provided to the site in three locations:

1. Access #1 / north-south road: From Township Road 250, by consolidating the existing pan-handles within the northwest section of the Plan Area;
2. Access #2 / northern east-west road: A 30 metre road standard that serves both the Business Commercial and Country Residential areas (see [Figure 12](#) and [Figure 13](#) for cross-section details); and
3. Access #3 / southern east-west road: A 25 metre road standard that primarily serves the Country Residential Area (see [Figure 14](#) and [Figure 15](#) for cross-section details).

### External Road Network

In 2018, Watt Consulting Group completed a Traffic Impact Assessment (TIA) in support of this Conceptual Scheme and the Development Concept as shown in [Figure 6](#). The TIA forecasts traffic volumes (using 2017 County traffic model data) and assesses the operational capacity of the access intersections using background and post-development scenarios. The TIA analyzed the future transportation network, cumulative impacts to the regional and internal road networks,

and intersection spacing, roundabout analysis, and potential mitigation measures associated with the traffic expected to be generated by development within the Plan Area.

The TIA assumed that by 2030 the lands along Township Road 250 will develop to accommodate live/work development on the properties. This would allow the homeowners to operate a shop at the same location as their residence. In the longer term, the Plan Area anticipates these lands will be redesignated to business-commercial land uses by the 2040 horizon year. South of the business-commercial developments, 26 country residential lots are assumed.

To accommodate the background and post-development scenarios, as per the TIA, a staged program of roadway improvements will be required, which includes but may not be limited to, the items in the following tables:

**Table 7: 2030 Horizon Year TIA Recommendations**

2030 Horizon Year – TIA Recommendations		
<b>Intersection</b>	<b>Right-of-way for roundabouts <i>available</i></b>	<b>Right-of-way for roundabouts <i>unavailable</i></b>
Township Road 250 & Range Road 283	<ul style="list-style-type: none"> <li>Single lane roundabout</li> </ul>	<ul style="list-style-type: none"> <li>Signalization.</li> </ul>
All other intersections	<ul style="list-style-type: none"> <li>Modified to accommodate actual turning movements at horizon year</li> </ul>	<ul style="list-style-type: none"> <li>Modified to accommodate actual turning movements at horizon year.</li> </ul>

Figure 9: 2030 Traffic Controls and Laning

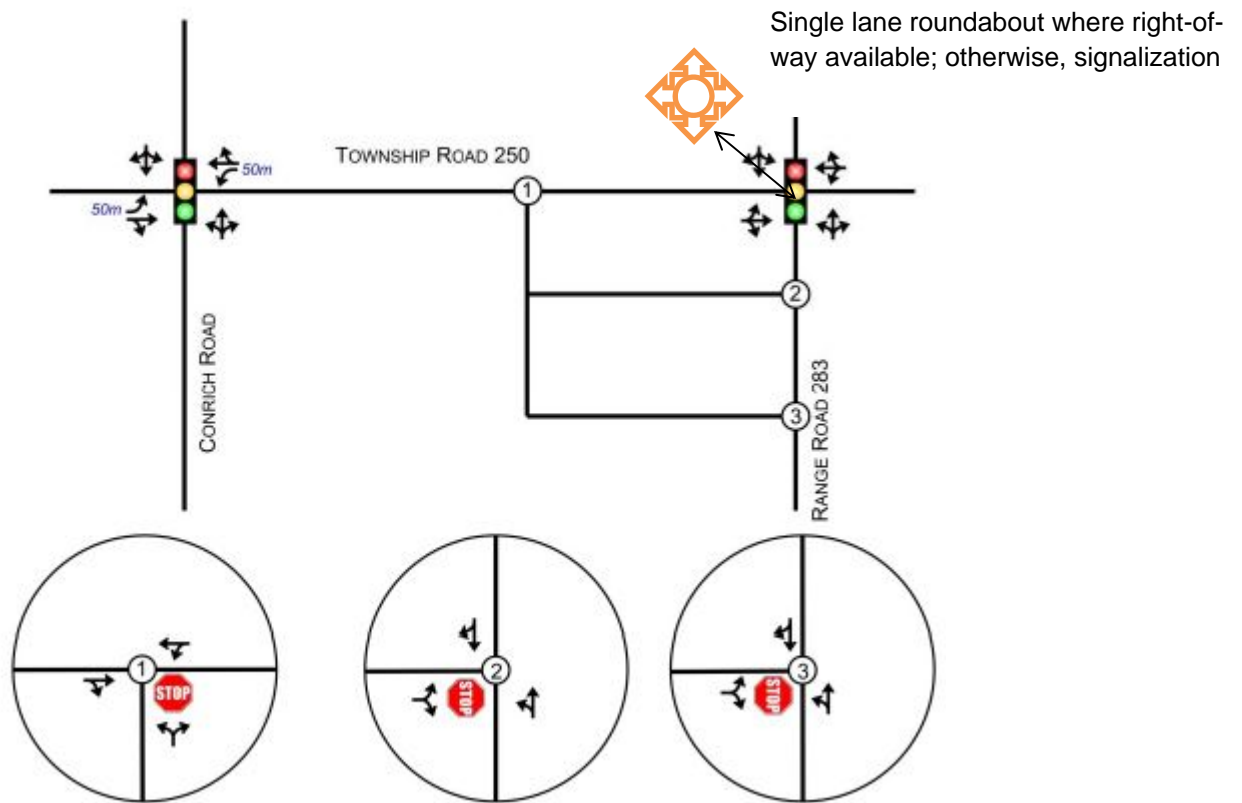
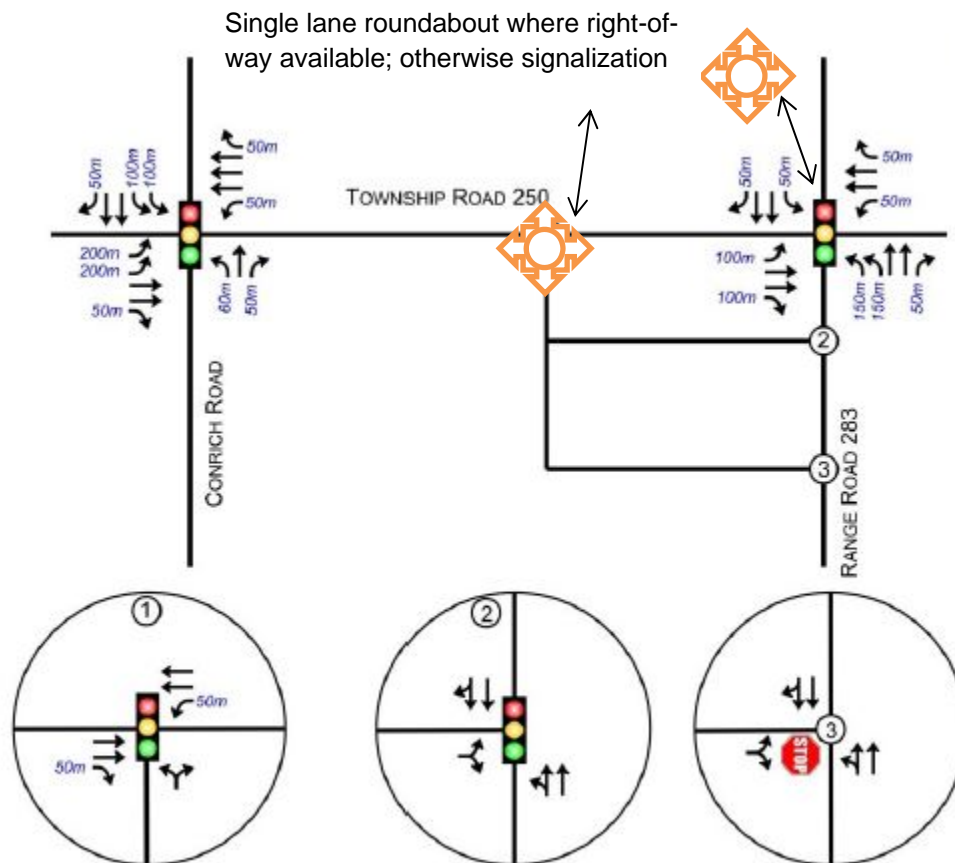




Table 8: 2040 Horizon Year TIA Recommendations

2040 Horizon Year – TIA Recommendations		
Intersection	Right-of-way for roundabouts (if available)	Right-of-way for roundabouts (if unavailable)
Township Road 250 & Conrich Road	N/A	<ul style="list-style-type: none"> <li>Add a through-lane to the eastbound movement.</li> <li>Add two through-lanes to the westbound movement.</li> <li>Add separate right-turn lanes for all movements.</li> <li>Add an eastbound, dual left-turn bay and a separate left-turn bay for north and southbound movements.</li> </ul>
Township Road 250 & Range Road 283	<ul style="list-style-type: none"> <li>Two-lane roundabout</li> </ul>	<ul style="list-style-type: none"> <li>Add a through-lane to the east and westbound movements.</li> <li>Add separate right-turn lanes for all movements.</li> <li>Add a northbound, dual left-turn bay.</li> <li>Add separate left-turn bays for all movements.</li> </ul>
Township Road 250 & Access #1	<ul style="list-style-type: none"> <li>Two-lane roundabout</li> </ul>	<ul style="list-style-type: none"> <li>Signalize.</li> <li>Add east and westbound left turn bays.</li> </ul>
Range Road 283 & Access #2	N/A	<ul style="list-style-type: none"> <li>Signalize.</li> </ul>

Figure 10: 2040 Traffic Controls and Laning



**Policies:**

- 6.1.1** All development shall comply with the Township Road 250 Traffic Impact Assessment.
- 6.1.2** Developers shall provide the lands required for the intersectional and/or road upgrades identified in the Township Road 250 Traffic Impact Assessment.
- 6.1.3** The developer shall adhere to the recommendations within the Township Road 250 Traffic Impact Assessment. If the proposed development anticipates traffic volumes that exceed those in the TIA, the developer shall provide an update to the TIA, and shall follow the stated recommendations and construct the necessary improvements within the updated TIA to the satisfaction of the County (and Alberta Transportation, if required).
- 6.1.4** Developments shall construct or contribute financially to the required road network improvements outlined in the Township Road 250 Traffic Impact Assessment Construction, and/or financial contributions shall be proportional to the size of the development and anticipated traffic generation in accordance with County policy and this Conceptual Scheme.
- 6.1.5** Developers shall be required to pay all applicable transportation off-site levies to the County at subdivision or development permit stage.
- 6.1.6** The road network within the Business Commercial area shall adhere to Figure 6: Development Concept. Additional roads in the Plan Area beyond those identified in Figure 6 are discouraged.
- 6.1.7** Where upgrades to regional transportation infrastructure or municipal servicing provided by a developer are demonstrated to benefit lands outside the Conceptual Scheme area, the County shall facilitate recovery of costs in accordance with the Infrastructure Cost Recovery Policy.

**Internal Road Network & Interfaces**

As conceptually illustrated in Figures 12 and 13, a 30 metre road right of way will separate a significant portion of the Business Commercial area from the adjacent Country Residential area. This 30 m right-of-way will contain a 13.25 metre “greenway,” including a 2 metre pathway, landscaping, and benches. The intent of this open space buffer will be to mitigate any potential negative visual impacts associated with Business Commercial developments.

As part of the internal road network, consideration of the adopted Active Transportation Plan, South County, should be incorporated into planning, with the goal of including trails and pathways for connectivity throughout.

*Active Transportation: considered any form of human powered transportation such as walking and cycling. An active transportation network is inclusive of pathways, trails,*

*sidewalks, and any dedicated active transportation facility located within a road right-of-way including, but not limited to bicycle lanes; advisory lanes; or a buffered bicycle lane.*

The remaining residential-non-residential interface area does not have the benefit of a right-of-way separation and will require additional landscaping within the Business Commercial lots. This landscape buffer is shown on [Figure 8](#), and policies are detailed as follows:

**Policies:**

- 6.2.1** The internal road network and corresponding landscape buffers and pathways shall comply with [Figure 6](#) as well as the associated cross-sections detailed in [Figure 11](#) through to [Figure 14](#).
- 6.2.2** The road standard for the 30 m right-of-way, as identified on [Figure 11](#), shall conform to [Figure 12](#) and [Figure 13](#) – 30 m Cross Section details.
- 6.2.3** The road standard for the 25 m right-of-way shall conform to [Figure 14](#) and [Figure 15](#) – 25 m Cross Section details.
- 6.2.4** Direct driveway access to the Business Commercial area from Township Road 250 shall not be permitted.
- 6.2.5** At the time of development, the developer shall provide a 15 metre right of way dedication on the south side of Township Road 250, to the satisfaction of the County, to allow for the future widening of Township Road 250 as identified in the Conrich Transportation Network Analysis.
- 6.2.6** At the time of development, the developer may be required to provide a 15 metre right-of-way on the west side of Range Road 283, to the satisfaction of the County, to allow for the future widening of Range Road 283 as identified in the Conrich Transportation Network Analysis.
- 6.2.7** At the discretion of the County, development on parcels that currently have direct access to Range Road 283 may be permitted to continue until the Conceptual Scheme road network is sufficiently completed to allow alternative access to/from the parcel(s).
- 6.2.8** Landscaping features within the residential and non-residential interface area may include landscaping, planting strips, berms, local pathways, benches, bio-swales, and linear parks.
- 6.2.9** The visual impact of non-residential buildings should be minimized by the use of berms and/or planting strips, and should incorporate natural contours and variations in height.
- 6.2.10** All roads required to provide access within the Plan Area shall be provided by the developer, to the satisfaction of the County. The developer shall enter into a development agreement with Rocky View County for the construction of internal roadways and all related infrastructure.



- 6.2.11** The proposed transportation network shall be designed to support an interconnected road and pedestrian system that promotes a range of routing options, and shall be designed in consideration of the adopted Active Transportation Plan.
- 6.2.12** The proposed transportation network of roads, pathways, and trails shall connect adjacent neighbourhoods and accommodate a range of users, including alternate modes of transportation.
- 6.2.13** The road classification and layout may be refined through further transportation analysis at the time of subdivision application. Minor changes or modifications will not require an amendment to this Conceptual Scheme.

## Municipal Reserve

Previous subdivisions within the Plan Area have fulfilled the municipal reserve requirement for much of the Plan Area. However, if municipal reserve is owing on a parcel of land at the time of subdivision, the developer will be required to provide municipal reserve pursuant to the provision of the *Municipal Government Act* and County policy.

### Policies

- 6.3.1** Reserves owing on a parcel shall be provided by the developer either in the form of land, cash-in-lieu payment, or a combination of both, pursuant to the provisions of the *Municipal Government Act* and County policy. The exact amount will be determined at the subdivision stage.

Figure 11: Interface Area Cross Section

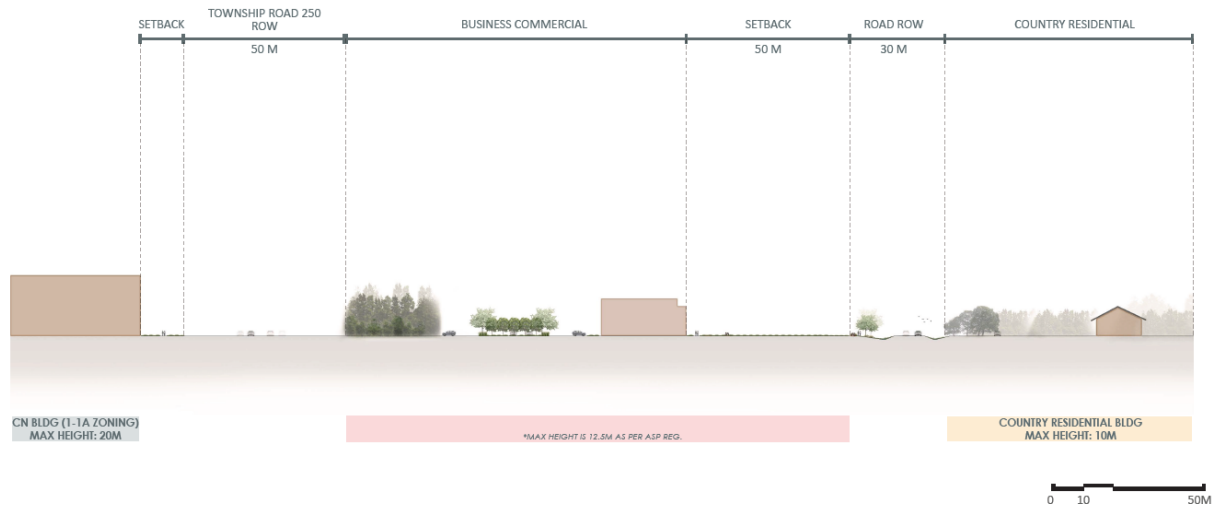


Figure 12: 30 Metre Road Right of Way

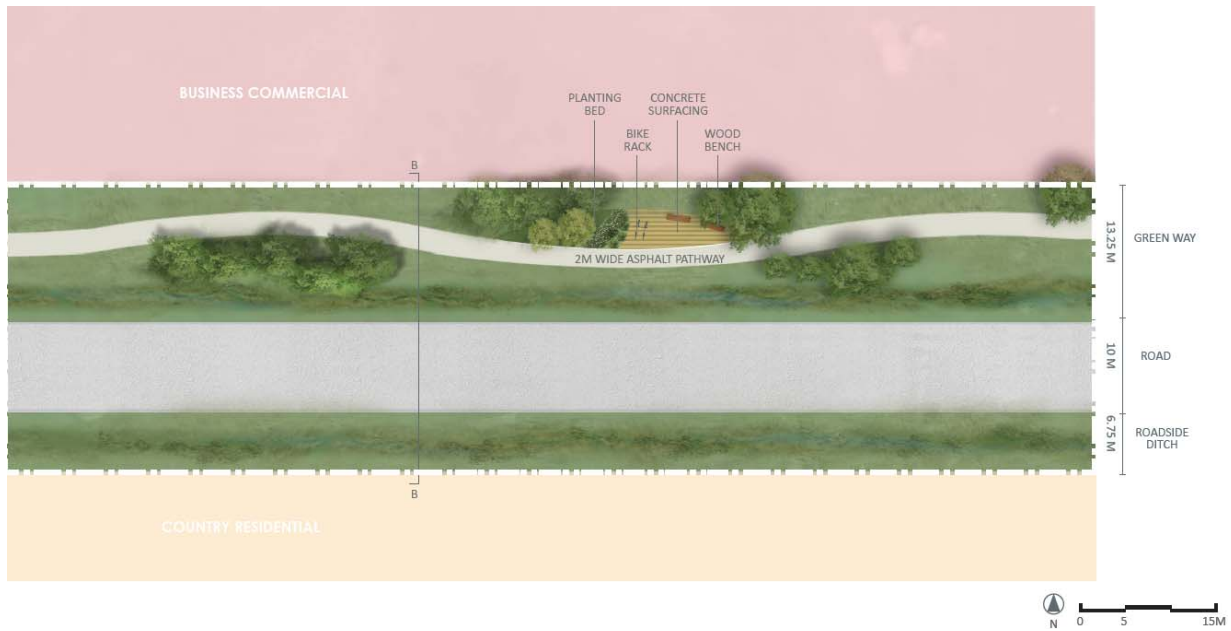


Figure 13: 30 Metre Right of Way Cross Section

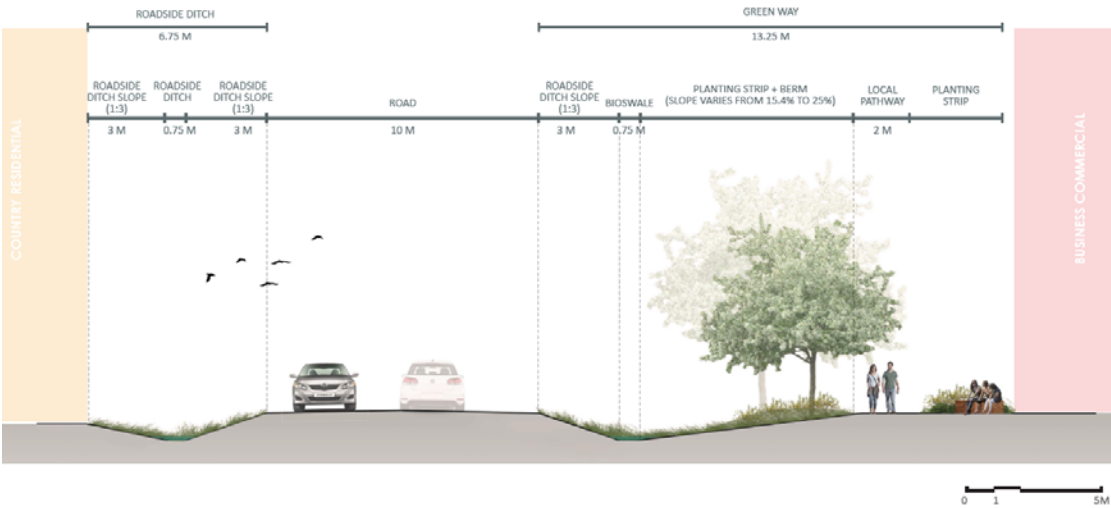
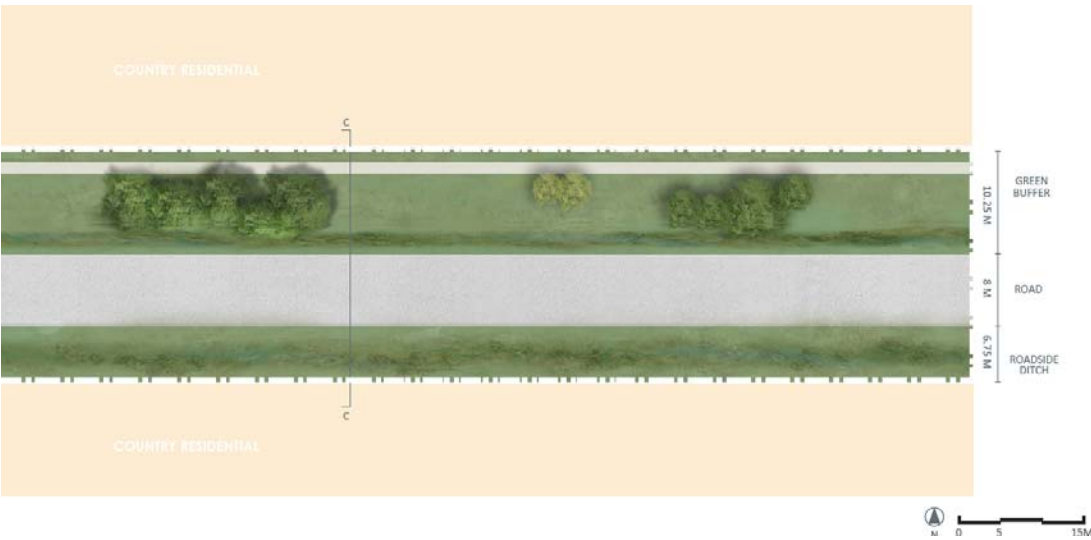
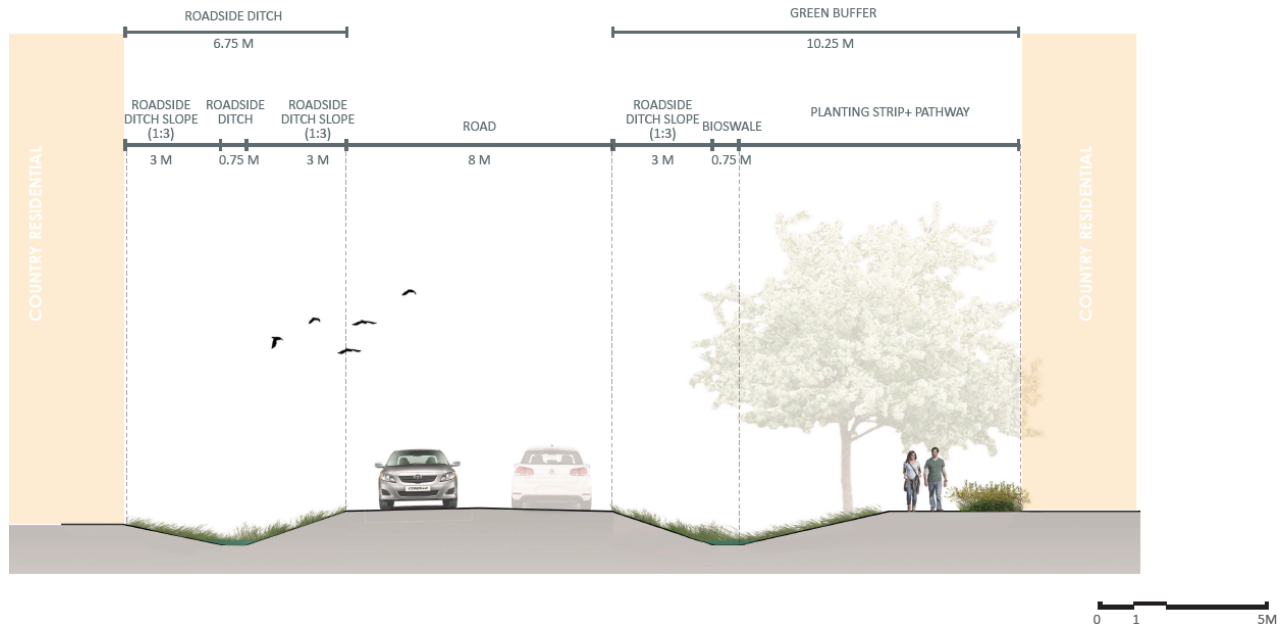


Figure 14: 25 Metre Road Right of Way





**Figure 15: 25 Metre Right of Way Cross - Section**



# 7

## Infrastructure & Services

Utilities and servicing include the critical infrastructure components that serve to facilitate the redevelopment of the Plan Area over time. Piped water is located adjacent to the north, east, and south of the Plan Area. Waste water servicing is available in the northern portion of the Plan Area, and piped stormwater has not been constructed. The proper infrastructure connections and development phasing will ensure the long-term viability of the Plan Area as it evolves over time.

### Stormwater and Drainage

In April 2018, ISL Engineering and Land Services (ISL) completed a stormwater servicing analysis entitled Conrich Stormwater Servicing Option Development, and two outcomes emerged. The first, or interim measure, is zero discharge with on-site evaporation ponds. This solution is viewed as interim until a stormwater conveyance system can be completed to allow off-site discharge. The second, or final solution, is a stormwater conveyance system that will allow for stormwater discharge.

#### **Interim Measures - Evaporative Ponds**

The divided land ownership of the Plan Area is not conducive to the creation of a single stormwater system prior to individual lots being developed. Therefore, each lot must retain 100% of its stormwater on-site (zero discharge), and lots shall maintain an overall imperviousness value of below 50 percent. The result is that each development must construct an evaporation pond of a suitable size.

### Final Solution - Discharge Pond

The recommended ultimate solution is a stormwater conveyance system that is “either a piped or ditch system within the development site, ultimately connecting to Range Road 283 and discharging south. It is recommended that the infrastructure along Range Road 283 is implemented as a piped system as opposed to the utilization of drainage ditches given the natural grading in this area” (Conrich Stormwater Servicing Option Development, p.9). Proposed stormwater piping is shown in [Figure 16: Stormwater](#). Proposed potable water servicing line locations are shown in [Figure 17: Potable Water](#). Wastewater piping is shown in [Figure 18: Wastewater](#).

**Figure 16: Stormwater**

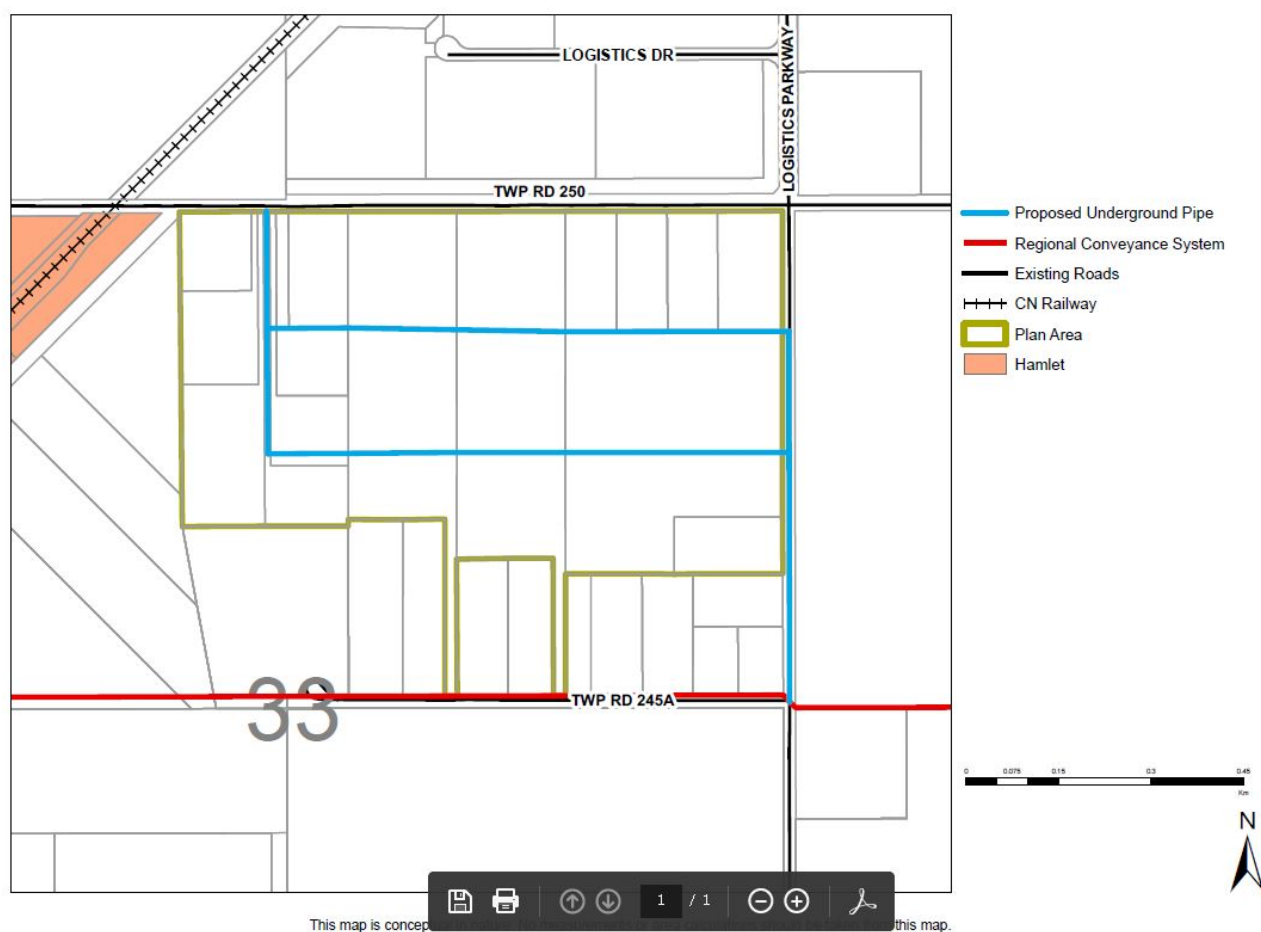
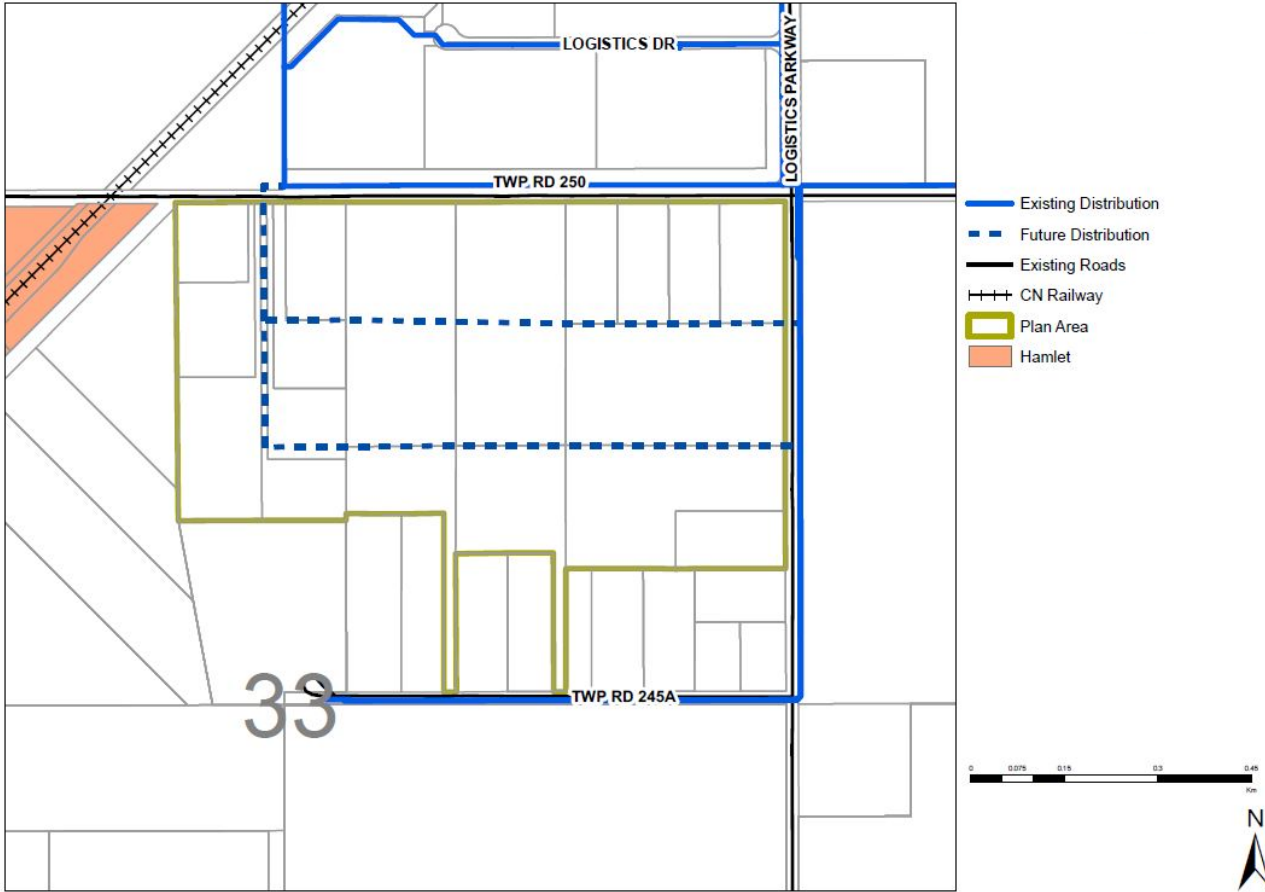
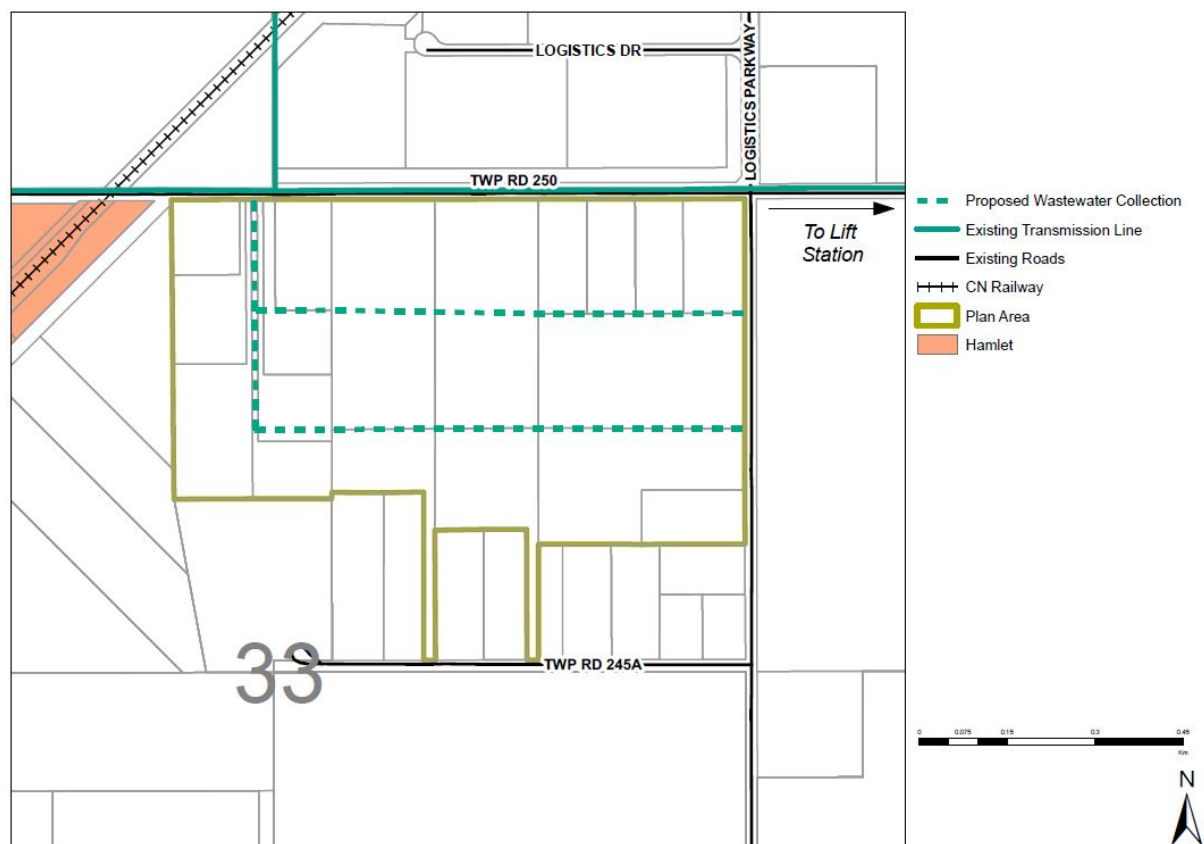


Figure 17: Potable Water



This map is conceptual in nature. No measurements or area calculations should be taken from this map.

Figure 18: Wastewater



This map is conceptual in nature. No measurements or area calculations should be taken from this map.

### Policies - Stormwater

- 7.1.1** All development applications shall provide evidence of compliance with the stormwater recommendations contained within the Conrich Stormwater Servicing Option Development, the Conrich Master Drainage Plan, and Rocky View County Standards.
- 7.1.2** Prior to the completion of a stormwater conveyance system in the Plan Area, as an interim solution, all developments shall be designed for zero stormwater discharge and include evaporation ponds sized in accordance with the Conrich Stormwater Servicing Option Development report. Where a development demonstrates the implementation of low impact development (LID) measures, the evaporation pond size may be reduced. It is recommended that land owners maintain an imperviousness value of below 50% and utilize LID measures and water re-use in the form of irrigation when possible to ensure these ponds are feasible and kept to a minimum size.

- 7.1.3** Following the completion of a stormwater conveyance system in the Plan Area, all developments shall design for a stormwater discharge rate of 0.8 L/s/ha and include retention ponds sized in accordance with the Conrich Stormwater Servicing Option Development report, provided the on-site ponds with a downstream discharge can be properly implemented.
- 7.1.4** All developments are encouraged to use low impact development solutions recommended in the Conrich Stormwater Servicing Option Development to reduce storm water run-off.

## Water and Wastewater Servicing

### Policies

#### Water Servicing

- 7.2.1** Piped water servicing shall be required for Business Commercial development.
- 7.2.2** Piped water servicing shall be required for multi-lot Country Residential subdivision.
- 7.2.3** Piped water servicing may be required for single-lot Country Residential subdivision.
- 7.2.4** The County shall require each developer to provide a financial contribution for required infrastructure upgrades at the subdivision or development permit stage in accordance with the County's Off-Site Levy Bylaw.
- 7.2.5** Land use amendments relying on County utility services shall not be supported until the County has confirmed servicing capacity exists, or will be provided, to the satisfaction of the County.
- 7.2.6** Business Commercial development requiring high water volumes may not be supported, in accordance with the County's Water Management Strategy.

#### Wastewater Servicing

- 7.2.7** Piped wastewater servicing shall be required for Business Commercial development.
- 7.2.8** Piped waste water servicing may be required for Country Residential development.
- 7.2.9** The County shall require each developer to provide a financial contribution for required infrastructure upgrades at the subdivision or development permit stage in accordance with the County's Off-Site Levy Bylaw.

- 7.2.10** Land use amendments relying on County utility services shall not be supported until the County has confirmed servicing capacity exists, or will be provided, to the satisfaction of the County.
- 7.2.11** Sanitary sewer gravity lines shall be designed in accordance with Alberta Environment and Rocky View County Servicing Standards.
- 7.2.12** Water mains and distribution pipes within the Plan Area shall be designed in accordance with Alberta Environment and Rocky View County Servicing Standards.
- 7.2.13** The water distribution system will take into account all necessary fire protection standards and best practices for water distribution and firefighting requirements.
- 7.2.14** A Wastewater Servicing Assessment may be required with subdivision application. The Wastewater Servicing Study shall be reviewed by the County to determine wastewater demand and infrastructure requirements to confirm that sufficient wastewater treatment capacity exists to service the proposed development.
- 7.2.15** A Water Use Assessment may be required with a subdivision application. The Water Use Assessment shall be reviewed by the County and water service provider to determine the water demand and infrastructure requirements to confirm that sufficient water capacity exists to service the proposed development.

## Shallow Utilities

Local utility companies will provide shallow utilities services including electricity, natural gas, telephone, cable, and high speed internet services, at a cost to the developer. This Plan anticipates that a mix of underground utilities and overhead utility lines will be accommodated within the road rights-of-way, with the use of private easements to serve Business Commercial uses.

### Policies

- 7.3.1** The Developer shall provide all shallow utilities within the Plan Area.
- 7.3.2** Each phase of the development shall be fully serviced with private shallow utility systems such as electrical, natural gas, and telecommunications.
- 7.3.3** Locations for easements and line assignments for shallow utility extensions shall be determined at the subdivision endorsement stage.



## Geotechnical Considerations

A comprehensive geotechnical assessment has not been prepared for the Plan Area, and no site-specific geotechnical assessments have been conducted on any parcels within the Plan Area boundaries. As such, detailed sub-surface conditions are unknown at this time. Future development, in alignment with land uses contemplated by this plan, will require an understanding of geotechnical conditions prior to approval.

### Policies

- 7.4.1** A geotechnical investigation may be required to support land use amendment, subdivision, and development permit applications and shall be prepared by a qualified professional.

## Solid Waste and Recycling

Solid waste and recycling must be managed through all stages of development, from construction through full build-out.

### Policies

- 7.5.1** Developers shall be responsible for the management and disposal of solid waste generated through all phases of construction, and shall adhere to all provincial and local laws regarding waste management and minimization.
- 7.5.2** Waste minimization and waste diversion practices are encouraged in the Plan Area even beyond the legal requirements.
- 7.5.3** Commercial business owners shall be responsible for providing their own solid waste services and shall adhere to all provincial and local laws regarding waste management and minimization.
- 7.5.4** Solid waste management shall be the responsibility of property owners in country residential areas unless or until local bylaws exist to supersede this responsibility.

## Emergency Services

Fire protection is currently provided under an intermunicipal agreement. However, the County is in the process of planning a fire hall in Conrich that will provide fire protection to the plan area.

Emergency services, including fire and policing, shall be provided to meet current and future needs in the area.

### Policies

- 7.6.1** In association with County Fire Services, the RCMP, and other emergency service providers, an adequate level of service shall be provided to meet current needs, as well as future needs, based on projected population growth and demographic change in the plan area.
- 7.6.2** Future subdivision shall meet the criteria and requirements for on-site firefighting measures as determined by the County.
- 7.6.3** Applications for development permits should incorporate principles of Crime Prevention Through Environmental Design (CPTED).
- 7.6.4** Subdivision and development permit applications shall include design elements that facilitate fire prevention methods and accessibility by emergency response vehicles for the suppression of fire.



# 8

## Implementation

The Conceptual Scheme outlines a vision for a future business commercial area and country residential development. The purpose of this section of the Conceptual Scheme is to outline an implementation process that will ensure the full completion of the vision for the Plan Area over time.

### New Land Use Overlay

The County's current Land Use Bylaw does not contain a land use district that would accommodate live-work opportunities at the size and scale allowed in this Conceptual Scheme. Any changes as a result of the conceptual scheme will therefore be reflected in revisions to the land use bylaw.

#### Policies

- 8.1.1** The Land Use Bylaw shall be revised to incorporate the changes with regard to the Live-Work policies in this plan.

### Planning and Development Applications

The implementation of the Conceptual Scheme will occur at all levels of development and through various types of applications, including redesignation, subdivision, and development permit applications. Conformance to this Conceptual Scheme will need to be considered at all stages of the development approvals process.

#### Policies

- 8.2.1** Applications for redesignation in the Plan Area that conform to this Conceptual Scheme, as determined by the County, are not required to update or amend this Conceptual Scheme.

**8.2.2** Applications for development of Live-Work are not required to update or amend this Conceptual Scheme.

**8.2.3** Applications for subdivision in the Plan Area that conform to this Conceptual Scheme, as determined by the County, are not required to update or amend this Conceptual Scheme. Any applications that are not in conformance with the Plan shall be required to make application to amend the conceptual scheme.

#### Infrastructure Costs and Levies

**8.2.4** Developers relying on transportation and/or utility infrastructure improvements (water, wastewater, and/or stormwater) provided by other developments shall be required to pay cost recovery as per the requirements of the applicable cost contribution agreement.

An aerial photograph of a rural landscape, showing a grid of roads and fields. The image is faded and serves as a background for the page.

# Appendices

## Appendix A: What We Heard Reports





Conceptual Scheme

# **Township Road 250 Transition Area**

## **Engagement Summary**



**ROCKY VIEW COUNTY**  
Cultivating Communities



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## PURPOSE AND INTENT OF OPEN HOUSE

In March 2017, Rocky View County (the County) hosted an open house event to discuss the Rocky View County has initiated a conceptual scheme process for the Township Road 250 area as required in the Conrich Area Structure Plan (ASP). On October 30, 2017 the County and consultants hosted an open house at the Khalsa School in Conrich for the directly affected and adjacent landowners and residents of the area. The purpose and intent of the open house was to:

- Inform residents and landowners about the project timeframes and existing ASP policies affecting the subject area;
- Answer questions and receive feedback on the proposed project and related policies and issues;
- Understand from residents/landowners the existing level of knowledge about the Conrich ASP, level of development interest and initial preferences of types of industrial development in the industrial transition area.

# FORMAT AND AGENDA

The open house was based on a typical format of information poster boards, short presentation by County staff and consultants and included opportunities for feedback by participants through answering questions by placing sticker dots on poster boards ('Dotmocracy'). See appendices for poster boards, presentation, FAQ information sheet and dotmocracy boards.

## The agenda was as follows:

5:30-6:00 pm – Drop-in / meet & greet / answer questions

6:00-6:30pm – Presentation & Questions

6:30-7:30pm – Drop-in / meet & greet / answer questions

# ATTENDANCE

There were approximately 23 people who attended the open house. Figure 1 below indicates the geographic spread of the attendees, which represents a large portion of the subject site and adjacent properties to the south. A total of 8 out of 18 properties within the subject site were represented at the open house. Five out of 11 adjacent properties to the south of the site were represented (see Figure 1).



Figure 1: Geographic Location of Open House Attendees

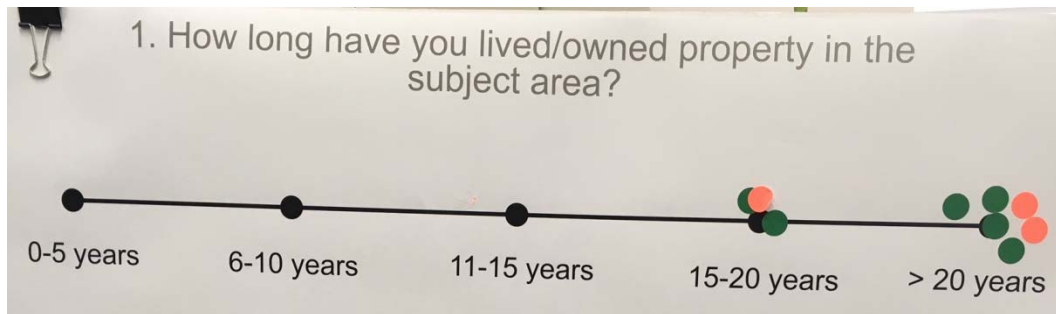
## DOTMOCRACY QUESTIONS - RESULTS

While the emphasis of the open house was to inform residents of the project and the existing ASP policies, it was determined that gathering some basic feedback from residents on key questions could help steer the future direction of the project and maximize this consultation opportunity.

Four basic questions were asked and participants had the opportunity to answer them using sticker dots directly on the poster boards. Photos of the results are provided below in Figures 2 and 3. The four questions were:

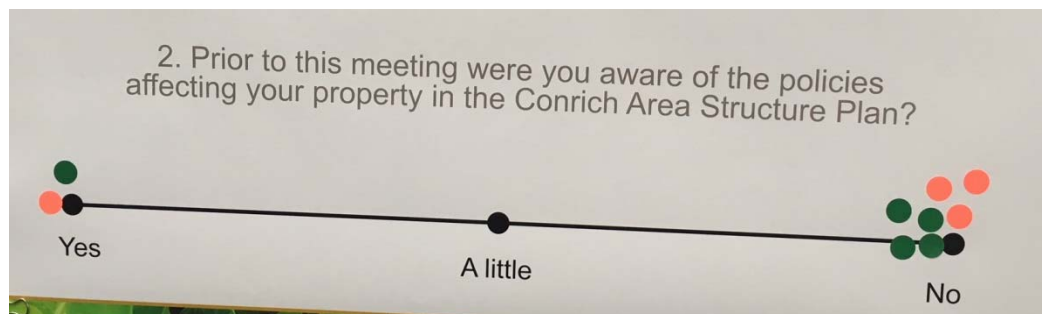
1. How long have you lived / owned property in the subject area? (answer along a spectrum between 0 to >20 years)
2. Prior to this meeting were you aware of the policies affecting your property in the Conrich Area Structure Plan? (answer along a spectrum from “Yes – A little – No”)
3. What type of development do you think is appropriate in the industrial transition area (place a dot above any or all that apply)? (answer along a spectrum from live/work to medium industrial large lots)
4. Do you intend to develop or sell your land? (answer along a spectrum from ‘never’ to ‘in the next 10-20 years’)

### Question 1 – Results & Analysis



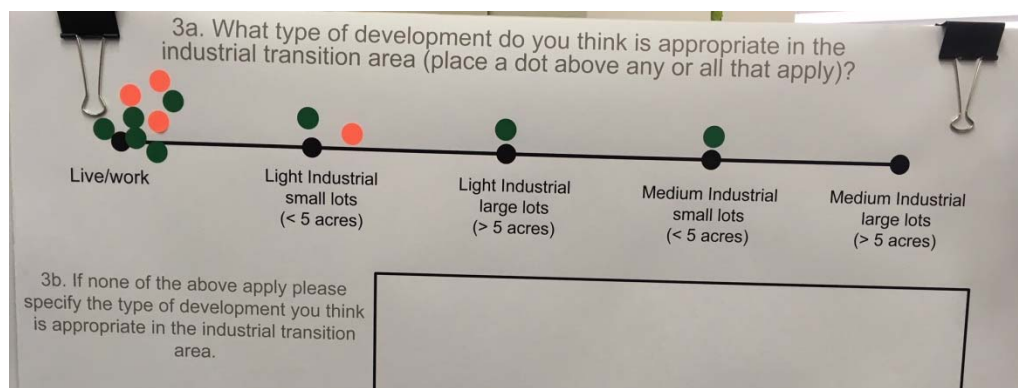
The results of Question 1 indicate that all respondents have lived or owned property in the area for more than 15 years. This is somewhat unique, as typically communities have a greater percentage of newcomers. Typical characteristics of a community with long-time residents include: rich history and understanding of the local area and strong attachment to the place, local features, and natural areas.

## Question 2 – Results & Analysis



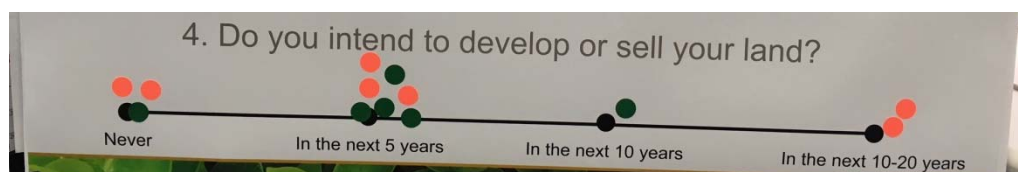
The majority of respondents stated they were not aware of the Conrich ASP policies prior to the meeting. This is important to consider as the project moves ahead, because the ASP policies are the guiding statutory policies on which the Township Road 250 Conceptual Scheme will be based. The results of this question emphasized the importance of keeping residents informed about policies affecting their land.

## Question 3 – Results & Analysis



Question 3 was an opportunity to get an early sense of community preferences for the type of industrial development that may be acceptable within the industrial transition area. The majority of respondents stated a preference for live/work type of development while others indicated acceptance of light and medium industrial development in both small and large lot formats.

## Question 4 – Results & Analysis



Question 4 asked respondents about their intentions to develop or sell their land, either in the short-term, long-term or never. The answers were distributed across the entire spectrum; however the largest portion of respondents (7 out of 13) indicated they wish to develop or sell

their land within the next five years. It should be noted that five out of 13 respondents answered either 'in the next 10-20 years' (2 respondents) or 'never' (3 respondents). Answers provided indicate the need to address transitional development policies and design within the Township Road 250 Conceptual Scheme.

## VERBAL FEEDBACK AND QUESTIONS

In addition to the formal dotmocracy questions, County staff and consultants received verbal feedback and questions from the attendees. A few of the general questions and comments have been summarized below:

- What does Industrial Transition mean?
- What does live/work mean and what does it look like?
- Stated concerns about storm water management. Concern that any future development does not create more problems for the area
- What are the requirements to connect to piped water and waste water infrastructure for the industrial transition area? How much will it cost? What type of development will trigger a requirement to connect?
- Stated concerns about existing road maintenance and the desire for paving along Twp Road 245A
- Questions and comments about the amount of right-of-way needed for expansion of Twp Rd 250.
- Concerns about traffic and rail crossing (stopping times) along Twp Rd 250
- Questions about the rationale of the Twp Rd 250 Conceptual Scheme boundary, and more specifically about the boundaries of the Industrial Transition and Country Residential designation areas identified in the Conrich ASP.
- Why are certain areas included or not included within the Conceptual Scheme boundary?
- Questions about front-ending infrastructure upgrades, off-site levies and other infrastructure costs as part of future development of the Industrial Transition area (when/how/how much)?
- What does the special policy area mean? When will that area be planned? What will be planned in that area?

## CONCLUSIONS AND NEXT STEPS

The overall intent of the open house was for landowners and residents to gain an understanding of the project, the Conrich ASP policies and to meet the core members of the project team. General feedback from attendees at the end of the evening was positive and reflected an increased understanding of the Conrich ASP policies and the project.

Feedback from the *dotmocracy* questions indicates the stakeholders are generally well-established in this area, desire live/work or light industrial land uses and have a broad spectrum

of development intentions for their land. The stated lack of prior knowledge of the Conrich ASP should be noted as the project moves ahead and additional residents participate.

### **Next Steps**

The County will use the feedback received at the October 30, 2017 open house to inform and direct further community consultation on the Township Road 250 Conceptual Scheme project.

A second public consultation is planned for mid to late-November, where residents will be asked to actively participate in shaping the conceptual scheme design and policy direction.



# Engagement Summary



Conceptual Scheme

**TOWNSHIP ROAD 250 TRANSITION AREA**



**ROCKY VIEW COUNTY**  
Cultivating Communities

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# Purpose and Intent of Open House

Rocky View County (the County) has initiated a conceptual scheme process for the Township Road 250 area as required in the Conrich Area Structure Plan (ASP). On October 30, 2017 the County and consultants hosted an open house at the Khalsa School in Conrich for the directly affected and adjacent landowners and residents of the area. The purpose and intent of the open house was to:

- Inform residents and landowners about the project timeframes and existing ASP policies affecting the subject area;
- Answer questions and receive feedback on the proposed project and related policies and issues;
- Understand from residents/landowners the existing level of knowledge about the Conrich ASP, level of development interest and initial preferences of types of industrial development in the industrial transition area.

# Format and Agenda

The open house was based on a typical format of information poster boards, short presentation by County staff and consultants, and included opportunities for participant feedback through answering questions by placing sticker dots on poster boards (dotmocracy). See appendices for poster boards, presentation, FAQ information sheet and dotmocracy boards.

## The agenda was as follows:

5:30-6:00 pm – Drop-in / meet & greet / answer questions

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## ATTENDANCE

Approximately 23 people attended the open house. Figure 1 below indicates the geographic spread of the attendees, which represents a large portion of the subject site and adjacent properties to the south. A total of 8 out of 18 properties within the subject site were represented at the open house. Five out of 11 adjacent properties to the south of the site were represented (see Figure 1).



Figure 1: Geographic Location of Open House Attendees

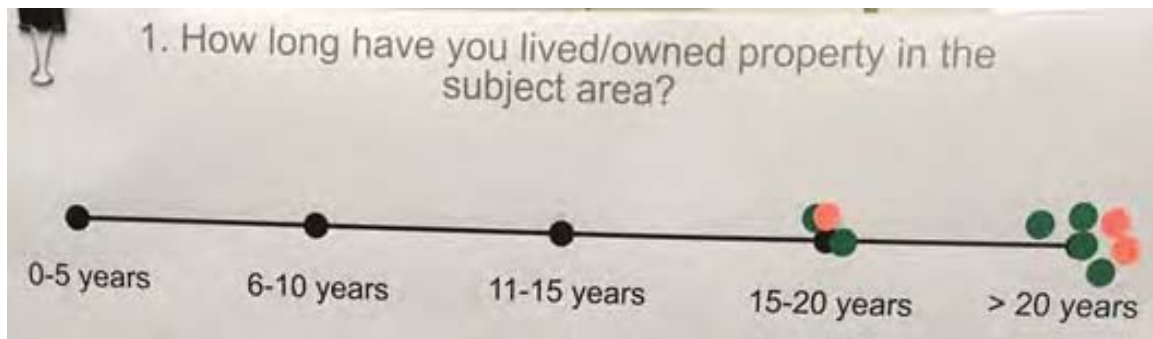
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While the emphasis of the open house was to inform residents of the project and the existing ASP policies, it was determined that gathering some basic feedback from residents on key questions could help steer the future direction of the project and maximize this consultation opportunity.

Four basic questions were asked and participants had the opportunity to answer them using sticker dots directly on the poster boards. Photos of the results for each question are provided below. The four questions were:

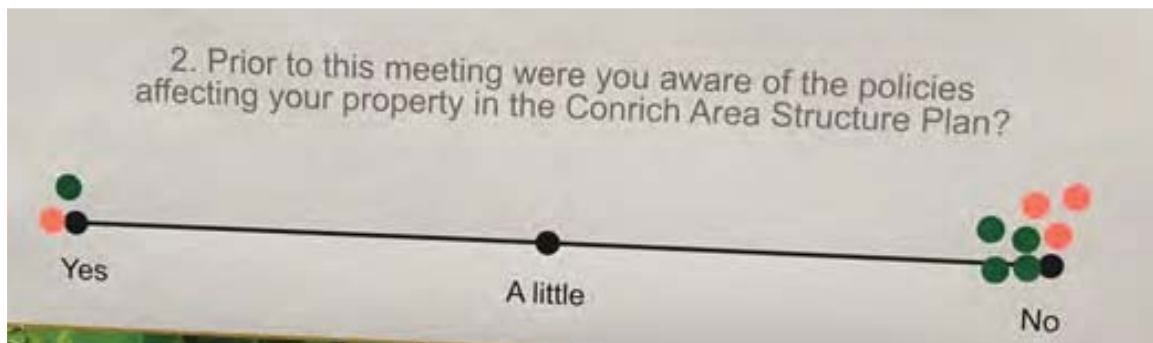
1. How long have you lived / owned property in the subject area? (answer along a spectrum between 0 to >20 years)
2. Prior to this meeting were you aware of the policies affecting your property in the Conrich Area Structure Plan? (answer along a spectrum from “Yes – A little – No”)
3. What type of development do you think is appropriate in the industrial transition area (place a dot above any or all that apply)? (answer along a spectrum from live/work to medium industrial large lots)
4. Do you intend to develop or sell your land? (answer along a spectrum from ‘never’ to ‘in the next 10-20 years’)

## Question 1 – Results & Analysis



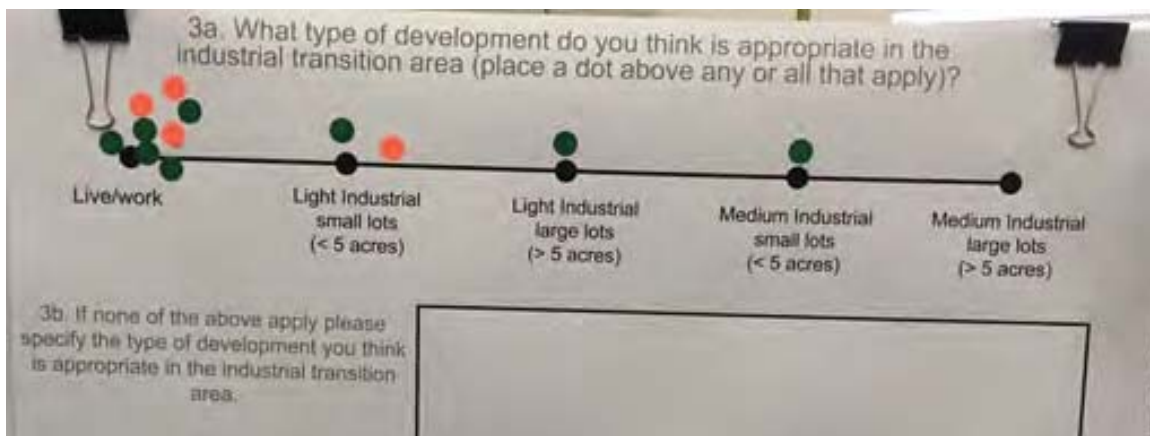
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## Question 2 – Results & Analysis



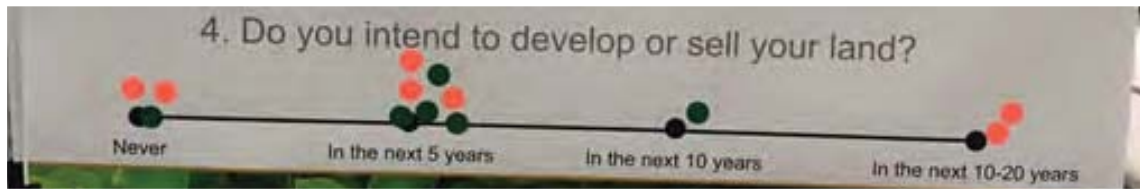
The majority of respondents stated they were not aware of the Conrich ASP policies prior to the meeting. This is important to consider as the project moves ahead, because the ASP policies are the guiding statutory policies on which the Township Road 250 Conceptual Scheme will be based. The results of this question emphasized the importance of keeping residents informed about policies affecting their land.

## Question 3 – Results & Analysis



Question 3 was an opportunity to get an early sense of community preferences for the type of industrial development that may be acceptable within the industrial transition area. The majority of respondents stated a preference for live/work type of development while others indicated acceptance of light and medium industrial development in both small and large lot formats.

## Question 4 – Results & Analysis



Question 4 asked respondents about their intentions to develop or sell their land, either in the short-term, long-term or never. The answers were distributed across the entire spectrum; however the largest portion of respondents (7 out of 13) indicated they wish to develop or sell their land within the next five years. It should be noted that five out of 13 respondents answered either 'in the next 10-20 years' (2 respondents) or 'never' (3 respondents). Answers provided indicate the need to address transitional development policies and design within the Township Road 250 Conceptual Scheme.

## Verbal Feedback and Questions

In addition to the formal dotmocracy questions, County staff and consultants received verbal feedback and questions from the attendees. A few of the general questions and comments have been summarized below:

- What does Industrial Transition mean?
- What does live/work mean and what does it look like?
- Stated concerns about storm water management. Concern that any future development does not create more problems for the area
- What are the requirements to connect to piped water and waste water infrastructure for the industrial transition area? How much will it cost? What type of development will trigger a requirement to connect?
- Stated concerns about existing road maintenance and the desire for paving along Township Road 245A
- Questions and comments about the amount of right-of-way needed for expansion of Township Road 250
- Concerns about traffic and rail crossing (stopping times) along Township Road 250
- Questions about the rationale of the Township Road 250 Conceptual Scheme boundary, and more specifically about the boundaries of the Industrial Transition and Country Residential designation areas identified in the Conrich ASP
- Why are certain areas included or not included within the Conceptual Scheme boundary?
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- What does the special policy area mean? When will that area be planned? What will be planned in that area?

# Conclusions and Next Steps

The overall intent of the open house was for landowners and residents to gain an understanding of the project, the Conrich ASP policies and to meet the core members of the project team. General feedback from attendees at the end of the evening was positive and reflected an increased understanding of the Conrich ASP policies and the project.

Feedback from the dotmocracy questions indicates the stakeholders are generally well-established in this area, desire live/work or light industrial lands uses, and have a broad spectrum of development intentions for their land. The stated lack of prior knowledge of the Conrich ASP should be noted as the project moves ahead and additional residents participate.

## Next Steps

The County will use the feedback received at the October 30, 2017 open house to inform and direct further community consultation on the Township Road 250 Conceptual Scheme project.

A second public consultation is planned for mid to late-November, where residents will be asked to actively participate in shaping the conceptual scheme design and policy direction.





Conceptual Scheme

# **Township Road 250 Transition Area**

**Stakeholder Workshop Summary – January 30, 2018**



**ROCKY VIEW COUNTY**  
Cultivating Communities

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## PURPOSE AND INTENT OF OPEN HOUSE

Rocky View County has initiated a Conceptual Scheme process for the Township Road 250 area as required in the Conrich Area Structure Plan (ASP). On January 30, 2018 the County and consultants hosted the third stakeholder workshop at the Chestermere Christian Fellowship in Conrich for the directly affected and adjacent landowners. Previous workshops occurred on November 29, 2017 and October 30, 2017.

The purpose and intent of the Stakeholder Workshop was to:

- Receive feedback from landowners and stakeholders on the proposed conceptual scheme design options, project objectives and policy directions.

## FORMAT AND AGENDA

The Stakeholder Workshop included a typical open house format of information poster boards as well as an opportunity for participants to fill in a feedback workbook and vote for their preferred option. See **Appendix A** for the Stakeholder Feedback Form.

### Voting Exercise

Participants were given the opportunity to rank four conceptual scheme design options during the workshop. A bucket was setup under each of the four concept options and participants were given four small pieces of paper with the numbers 1 through 4 written on them. Participants were told to place the piece of paper with a number 1 in the bucket located under their preferred option. Participants could then place their other numbers within the buckets under the other concept design options. In this way each participant ranked the concept options.

## ATTENDANCE

There were approximately 17 people who attended the event. Figure 1 below indicates the geographic spread of the attendees, which represents a large portion of the plan area and adjacent properties to the south. A total of 6 out of 18 properties within the subject site were represented at the open house. 6 out of 11 adjacent properties to the south of the site were represented (see Figure 1).

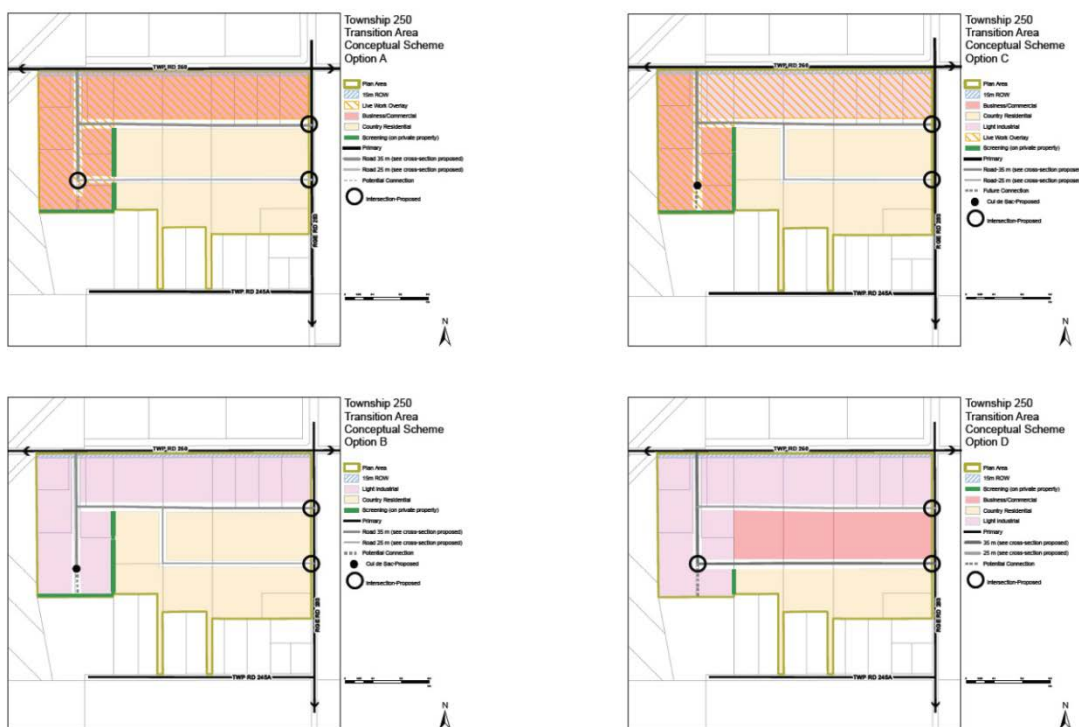


Figure 1: Geographic Location of Open House Attendees

## FOUR CONCEPT DESIGN OPTIONS

Participants were given the opportunity to rank four concept designs (see figure below). Option A focused on Business/Commercial land uses with a clear grid road network and an opportunity for a live-work overlay policy. Option B identified light industrial uses with a slightly modified grid road network. Option C had the same road network as Option B, but instead provided the live-work overlay as well as a mixture of business/commercial and light industrial land uses. Option D was the only option that proposed an expansion of the non-residential area with a business/commercial area and a grid road network.

### 4 Concept Design Options

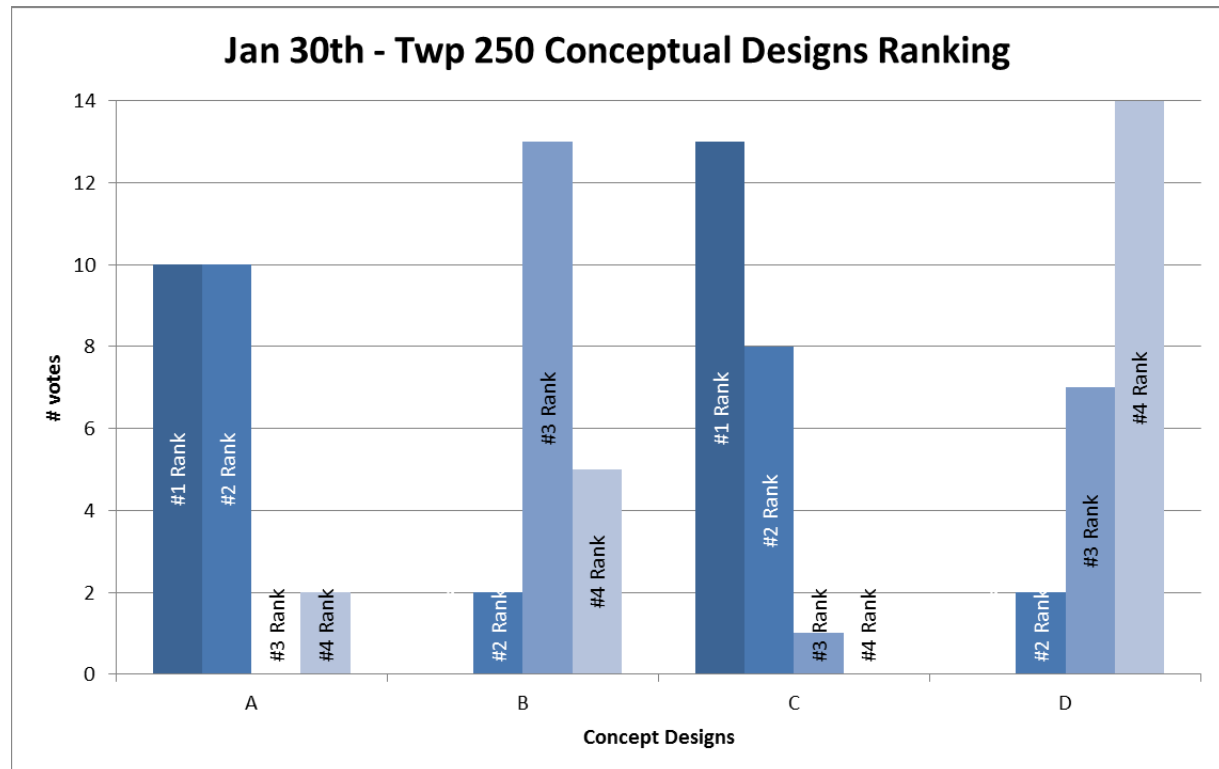


## RESULTS

### Bucket Voting/Ranking

The bucket voting or ranking of the concepts resulted in a clear preference for Concept Designs C and A. Concept C outranked all others with 13 first place votes followed by 10 first place votes for Concept A. Concept Design B was ranked by the majority of respondents as the third best option and Concept D was clearly the least preferred concept option. See the below table and chart for full ranking results:

Ranking Results				
Concept	#1 Rank	#2 Rank	#3 Rank	#4 Rank
A	10	10	0	0
B	0	2	13	5
C	13	8	1	0
D	0	2	7	14



## ANALYSIS

Based on the results, respondents appeared to prefer maintaining the established L-shaped non-residential area, matching the policies in the Conrich ASP. Option D, which proposed an expansion of the non-residential area was the least preferred concept. Further, respondents appear to have a preference for business-commercial land uses compared to light industrial land uses, as shown by the low ranking of Option B and the high ranking of Options A and C. Additionally, respondents did not appear to show a preference for road structure, as the highest ranked options (A and C) had different road networks. Another common element of both Option A and C was the provision of a Live-Work Overlay policy area.

In conclusion, it appears based on the respondents who voted at the workshop there is a preference for:

- maintaining the L-shape of the non-residential area,
- adding a live-work overlay policy, and
- focusing policy more on business-commercial land uses and less on light industrial land uses.

## VERBAL AND WRITTEN FEEDBACK

In addition to the *voting* process, County staff and consultants received written and verbal comments from the attendees. A few of the general comments have been summarized below:

### Comments

- Live-work overlay is strongly desired from current residents
- Business-commercial land uses with smaller lots are preferred to light industrial uses
- Business hours, traffic, noise, and other operation aspects must be controlled to minimize the impact on existing residents
- Existing landowners should not have to apply for live-work zoning
- 15m dedication of land on the south side of Twp Rd 250 should not be required

## CONCLUSIONS & NEXT STEPS

The overall intent of this third Stakeholder Workshop was for landowners and residents to rank and vote on proposed conceptual scheme design options and provide any additional feedback on the general direction of the conceptual scheme policies.

Feedback from this third Stakeholder Workshop will closely inform a draft Conceptual Scheme.

### Next Steps

The County and consultants will use the feedback received at the January 30, 2018 workshop to inform and direct a draft Conceptual Scheme on the Township Road 250 Conceptual Scheme project. The next community open house will present a draft Conceptual Scheme for comment.



## Appendix B: Potential Build-Out Scenarios

Development will be directed by market conditions, developer interest, and the requirement to construct the necessary infrastructure to support the development. The requirement to construct County standard roads and infrastructure to serve any Business Commercial or country residential development will likely result in development proceeding first on the edges of the Plan Area. Development will then likely proceed inward as infrastructure and roads are constructed to service the Plan Area. The following illustration shows how the plan area may build-out over time. The illustration also highlights that lots in the middle of the plan area will likely only develop after lots on the edges have fully constructed the necessary infrastructure.

Below is one series of many potential configurations for future development, and is provided for illustration only. This is by no means limiting, and alternate configurations may be proposed.

**Figure 19: Potential Build out Scenario\***

\*Conceptual drawing only. Buildings are not to scale.





1

The first full service Business Commercial lots are developed, including the construction of a municipal standard road with a temporary cul-de-sac.

2

The second full service Business Commercial lots are built along the west side of the Plan Area, including consolidation of numerous panhandle driveways.

3

Development on the west side is extended to the south, the temporary cul-de-sac is removed in favour of another one. The first resubdivision in the Country Residential area begins to occur.

4

The Business Commercial area is fully developed, including municipal standard roads and water and sewer servicing. The temporary cul-de-sac has been removed in favour of a full road connection. The Country Residential area resubdivision has pushed further to the west.

5

The Country Residential area is now fully resubdivided into smaller lots and the road network is fully intact.

