



Race Track and Entertainment Centre

Master Site Development Plan for Cell A-1 and A-7

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1.0 INTRODUCTION

1.1 Vision

The lands east of the Queen Elizabeth II (“QE2”) Highway and south of Route 566 are currently being developed as a commercial destination comprised of the CrossIron Mills Mall, the Century Downs horse racing complex and light industrial/commercial developments. The CrossIron Mills Mall is experiencing growth and the Century Downs horse race track is operating successfully, contributing to the creation of a complete entertainment destination. Development south of Century Downs Drive includes a variety of commercial, services and retail businesses that complement the surrounding uses.



Figure 1 - Context Map



Figure 2 - Horse racing example

1.2 Purpose

The purpose of this document is to provide guidance for the development of Race Track and Entertainment Centre (Cell A-1) and development south of Century Down Drive (Cell A-7). The MSDP establishes the general development principles for the project area, including entranceways, parking and landscaping, pedestrian circulation, regional pathways and architectural character.

1.3 Master Site Development Guiding Principles

The MSDP provides specific site design controls for Cell A-1 and A-7, to be used by Rocky View County (RVC) to guide subsequent development applications with the overarching goal of developing a high quality, integrated project.



Figure 3 - Horse racing example



Figure 4 - Horse racing example

1.4 Balzac East Area Structure Plan (BEASP)

The project area falls within the North Business Area of the Balzac East Area Structure Plan. The MSDP and proposed amendment adhere to the Business Use Performance Standards and Development Guidelines in the BEASP. The guidelines in the BEASP address lighting mitigation, landscaping, stormwater management, and water conservation strategies, among other items.

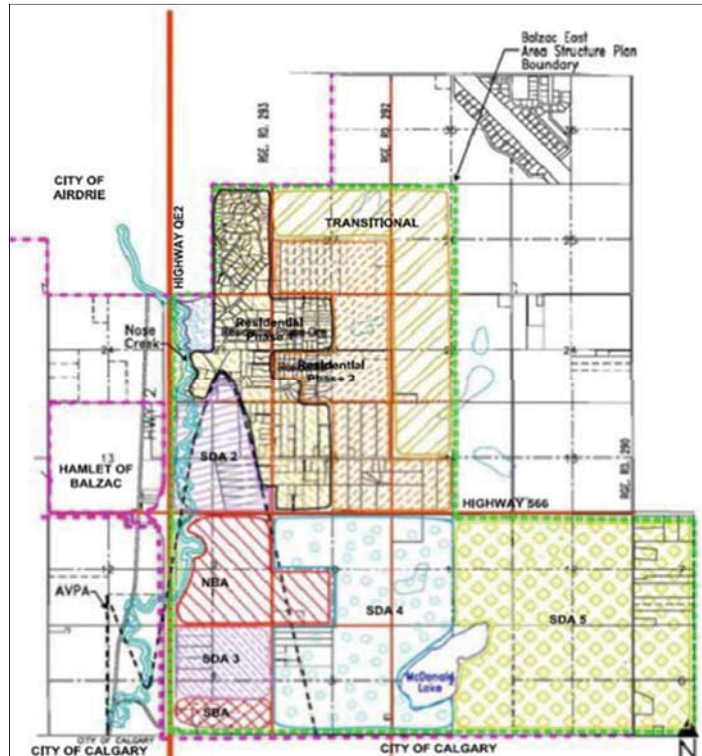


Figure 5 - Balzac East ASP Land Use Strategy (Bylaw C-5177-2000)



Figure 6 - DC-109 Development Cells (Bylaw C-6234-2006)

1.5 Direct Control District -109 (DC-109)

Regulations in DC-109 for Cell A-1 and A-7 have been integrated into the MSDP, as follows:

Maximum Limits

Maximum Height of Buildings

- ✓ Grandstand - 26 m (85 ft.)
- ✓ Hotel - 46 m (150 ft.) – To be measured from lobby floor elevation of Hotel facility.
- ✓ All other buildings – 15 m (49 ft.)
- ✓ Accessory Buildings: 5 m (16.5 ft.)

Maximum Building Footprints do not apply to cell A-1.

Maximum Building Footprint for Cell A-7: 7,500 m² (80,729 ft²)

Number of freestanding signs per lot: 8

Number of accessory buildings per lot: 8

The minimum building setbacks are:

- ✓ 6 meters from public roads.
- ✓ 3 meters from all other roads.
- ✓ 3 meters from side yards of adjacent sites.

Subdivision Regulations

Special Regulations

- ✓ The number of stalls required for each Cell is based on Parking and Loading Needs Assessment. A summary of the parking assessment is described in the Racing and Entertainment Centre – Balzac Transportation Impact Assessment Update, Bunt and Associates 2013, under separate cover. The Plan provides an integrated approach to the design of the parking facilities, the landscape and pedestrian systems, and identifies the traffic accommodation and flow for the internal development area.
- ✓ For the purpose of interpreting regulations contained in Section 4.4.3 d. Landscaping, of the Balzac East Area Structure Plan, the area contained within the oval of the horse racing track will not be considered as part of the total Cell area, nor as contributing to calculations of the areas required for landscaping treatment.
- ✓ The MSDP provides a comprehensive and Detailed Physical Design, Landscaping and Architectural Concepts to ensure that:
 - i. The complex integrates with the landscape, and the design seeks to mitigate the large scale of the development on the landscape;
 - ii. The appearance of parking areas from adjacent Highways is enhanced, and that the expansive appearance of parking areas is limited by breaking up their surface coverage with significant areas of landscaping, pedestrian pathways, sidewalks, and boulevards and/or other features.

- iii. Pedestrian movement throughout the development area is facilitated and encouraged through a well-developed network of sidewalks, pathways and public gathering areas. Pedestrian movement systems are an integral part of the overall development concept.
- iv. Development in Cell A-1 and Cell A-7 is sensitive to and integrates with adjacent development.

2.0 DEFINITION OF CELL A-1 AND CELL A-7

The MSDP comprises the Race Track and Entertainment Centre (Cell A-1, ± 57.25 ac in size) and the area south of Century Downs Drive (Cell A-7, ± 11.04 ac in size). The MSDP area is bounded by the following:

North – Balzac Commercial Campus (Zoned DC-99)

East – Dwight McLellan Trail & Ham West Master Site Development (Zoned DC-99) South – Crossiron Drive

West – Ivanhoe Cambridge Lands (Zoned DC-109)

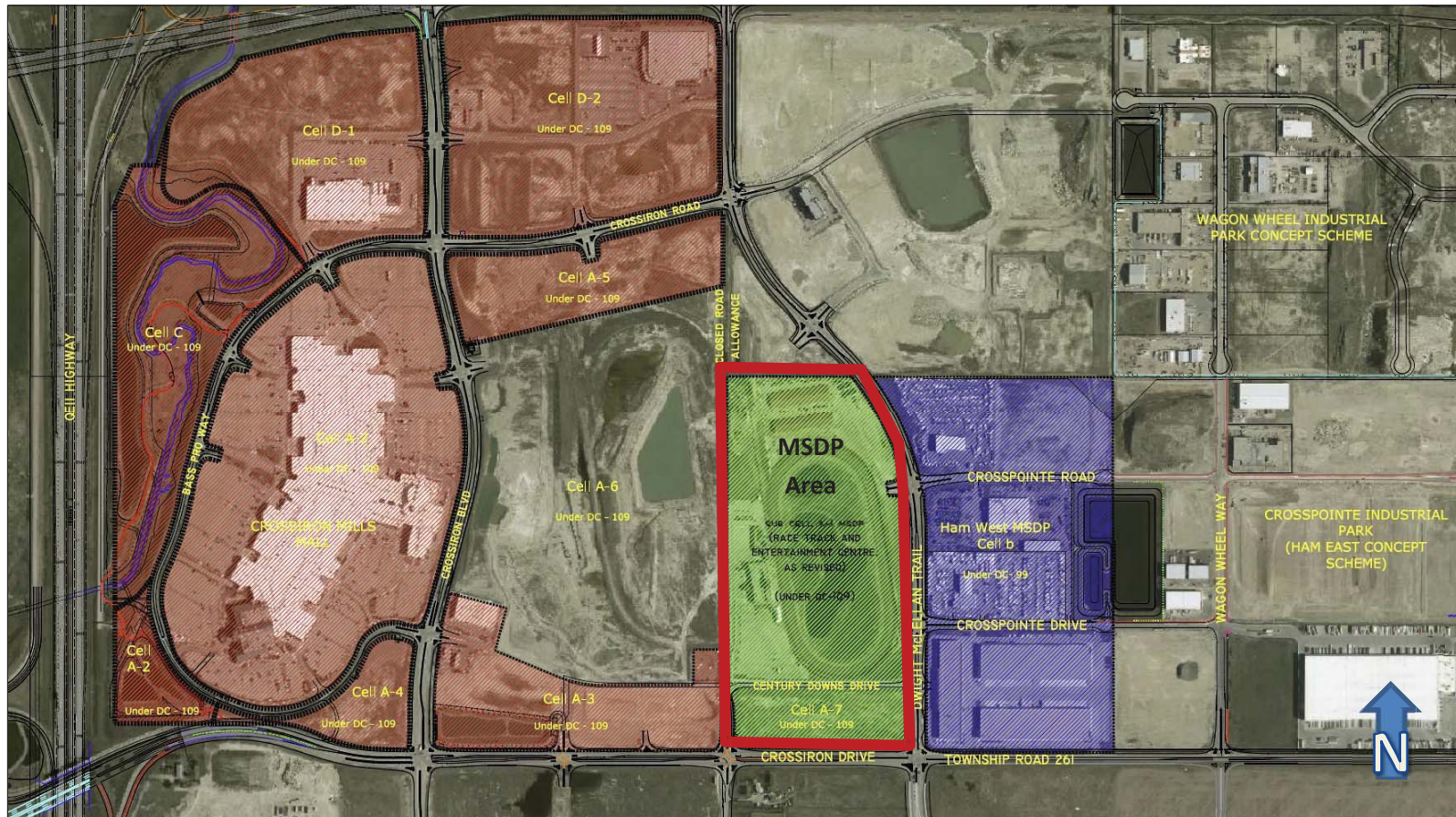


Figure 7 - Regional Context Map

2.1 Site Opportunities

- ✓ Located between two urban municipalities on a major Provincial highway.
- ✓ Ideal access from the Queen Elizabeth 2 Highway along CrossIron Drive.
- ✓ The site is located five miles north of the Calgary International Airport.
- ✓ The proximity to the CrossIron Mills commercial/retail development is seen as a draw for visitors to the racetrack facility.
- ✓ Existing residential developments to the north and south are considered in the design for future development.
- ✓ The existing east entrance along Dwight McClellan Trail is proposed for use as a gated private entrance for the horse racing operation.

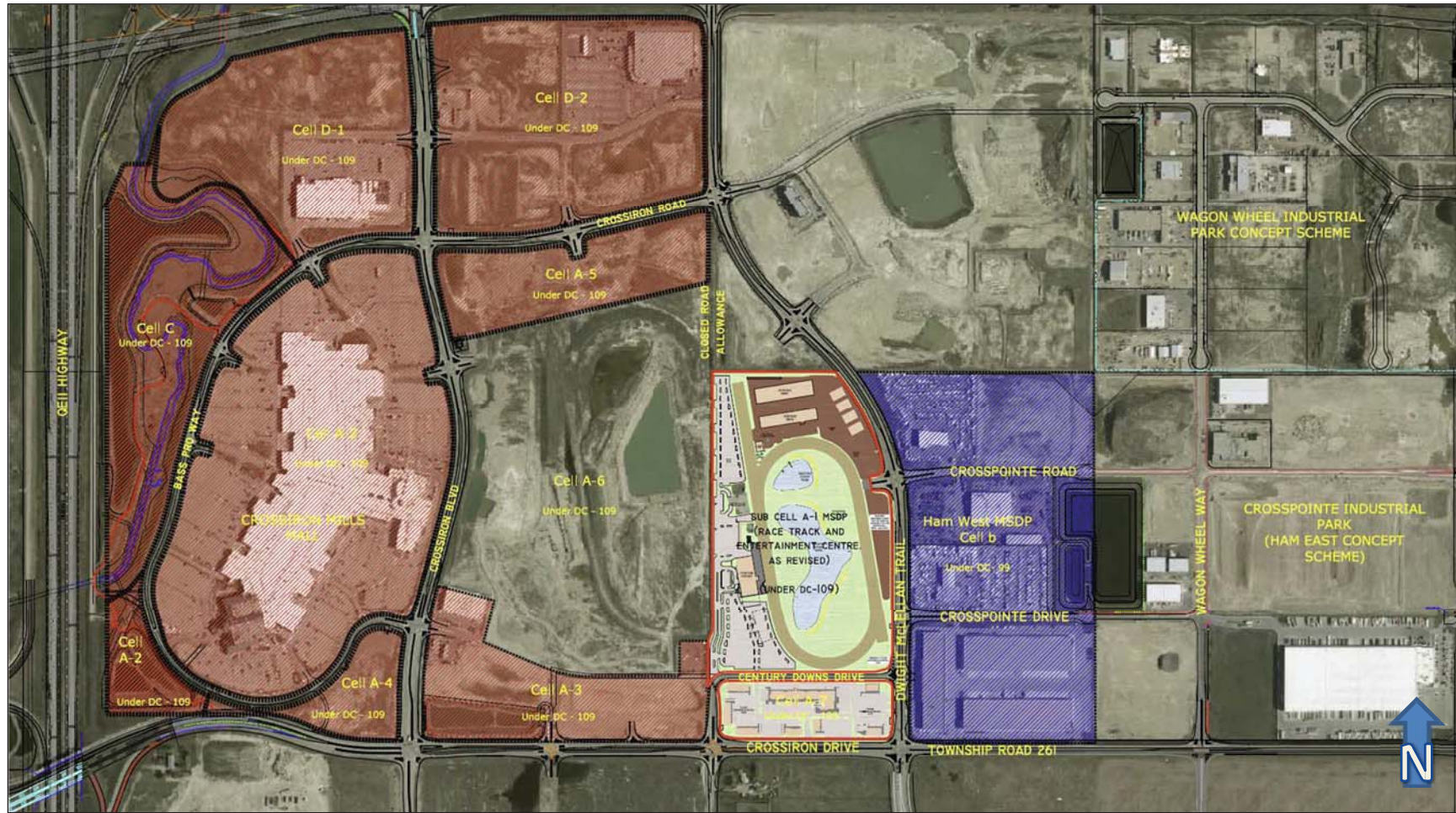


Figure 8 - Site Plan in Regional Context

3.0 MASTER SITE CONCEPT

3.1 The Master Site Concept

The goal of this development is to build a Racing & Entertainment Centre (REC) that will revitalize and bring a new facility to the racing and entertainment industry in Alberta. As defined in DC-109 Bylaw C-6234-2006:

***“Racing Entertainment Centre” (REC)** - means an indoor and outdoor facility, where the principle use is a horse racing track, including the necessary sub-cells of stables, security and grooms quarters, viewing areas & grandstand, animal health care services, vehicle parking (structured & at-grade), and accessory buildings for grounds-keeping, storage and security. Other uses include: other horse and non-horse events, restaurants, drinking establishments, cafeterias, entertainment services, gaming establishment (to include casino, slot machines, pari-mutuel betting, and simulcast/internet wagering), offices, retail stores and services, tourist information services and facilities, radio/TV/multimedia studios, and hotel. This cell is to also include Child Care Facilities for the use of Racing Entertainment Centre employees and contractors only.”*

Designed as a multi-purpose entertainment facility, the REC seamlessly integrates a modern racing facility with gaming, dining, and a possible future hotel. The REC is the focal point of the development consisting of a grandstand component on various levels offering different racing experiences including dining, lounges, and a gaming floor. Located east of the REC is the race track, barns, maintenance building and paddock area.

As shown on the Site Plan, development consists of a race track bounded with the Racing Entertainment Centre, a possible Barn Lounge and Jockey Building, a horse trailer parking area and/or a future potential location for a fourth barn or commercial activities, a possible future hotel to the north or south of the Racino, a possible commercial site at the north west corner of Dwight McLellan Trail and Century Down Drive, and future commercial, retail and service development to the south of Century Downs Drive.

There are two entrances to Cell A-1 from Century Downs Drive on the south side and one entrance from Dwight McClellan Trail on the east side. The entrance from Dwight McLellan Trail is currently a private entry for jockeys and horse trailers. Access for a possible future commercial site on the east side of the track will also be through this existing, signalized intersection.

Parking areas are located on the west side and south end of the race track. Smaller fields of parking are provided with landscaping and site lighting features. The parking area has been designed to allow expansion to the north, should a hotel be constructed in the future.

A portion of the internal access road and parking on the west side of the development area is located on a closed road allowance, previously owned by the County. In 2014 the eastern portion of the road allowance was consolidated with the lands to the east to form the REC development area.

Development south of Century Downs Drive (within Cell A-7) will consist of commercial, service and retail establishments that are consistent with uses in the surrounding area. A proposed site layout is shown conceptually in the proposed site plan. The final site layout (buildings, parking and landscaping) and final uses will be determined at Development Permit stage.



Figure 10 - illustration of the Racing Entertainment Centre

3.2 Public Realms

Public realms are delineated by sidewalks and a Regional Pathway which define the southern and eastern perimeters of the site. The design incorporates landscaping to make the connection between public, semi-public and private spaces aesthetically pleasing. The Regional Pathway system and public sidewalks will connect to the entry gate and on-site pedestrian circulation system. The public sidewalk on the north side of Century Downs Drive provides access through the site. Maintenance of sidewalks on private property will be the responsibility of the landowner.

Conceptual Pedestrian Circulation Overview

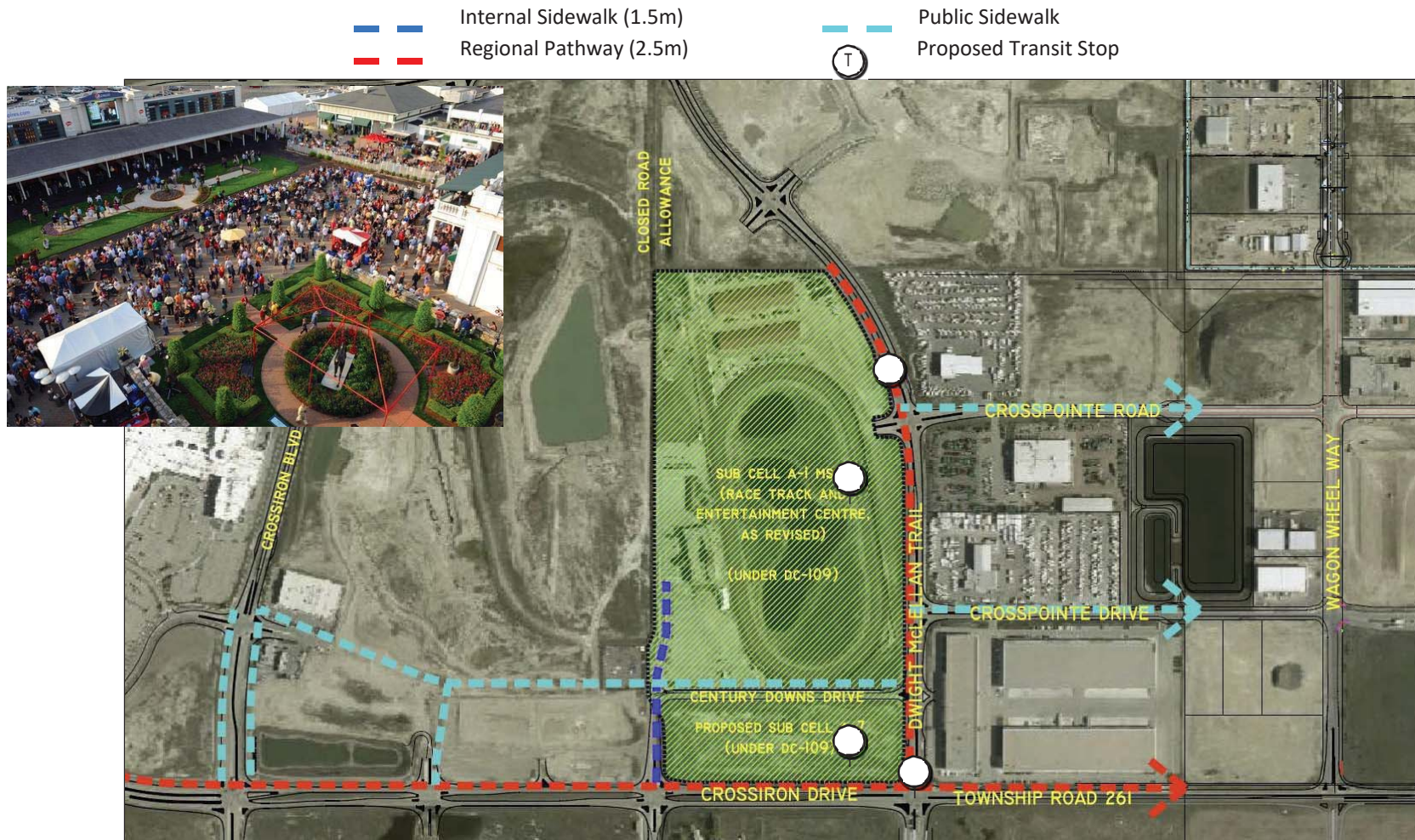


Figure 11 - Proposed Pedestrian Routes with External Connections

3.3 Landscaping

Landscaping of Cell A-1 and A-7 follows the requirements of the Balzac East ASP, DC-109 and Council policy which does not permit potable water to be used for irrigation purposes. In addition, the strategies outlined in the Comprehensive Landscape Strategy (CLS)-are applied where appropriate. These strategies include approaches to perimeter landscaping, parking lot treatments, main vehicular entry road and building edge landscaping, and irrigation.

Since the County does not permit potable water to be used for irrigation, stormwater will be utilized to water landscaping as well as the racetrack surface in Cell A-1, as outlined in the Pond Report for the Racetrack and Entertainment Centre (Kellam Berg, 2014). Landscape zones from the East Balzac – Retail and Racing Entertainment Centre Comprehensive Landscape Strategy, 2006 have been utilized to produce landscape concepts for the MSDP. Landscape Zones are identified for the area north of Century Downs Drive (Cell A-1). Corresponding Landscape concepts are illustrated by section in Appendix A.

Detailed landscaping plans will be submitted with subsequent Development Permits.



Figure 12 - Conceptual Example of Landscaping Cross Section

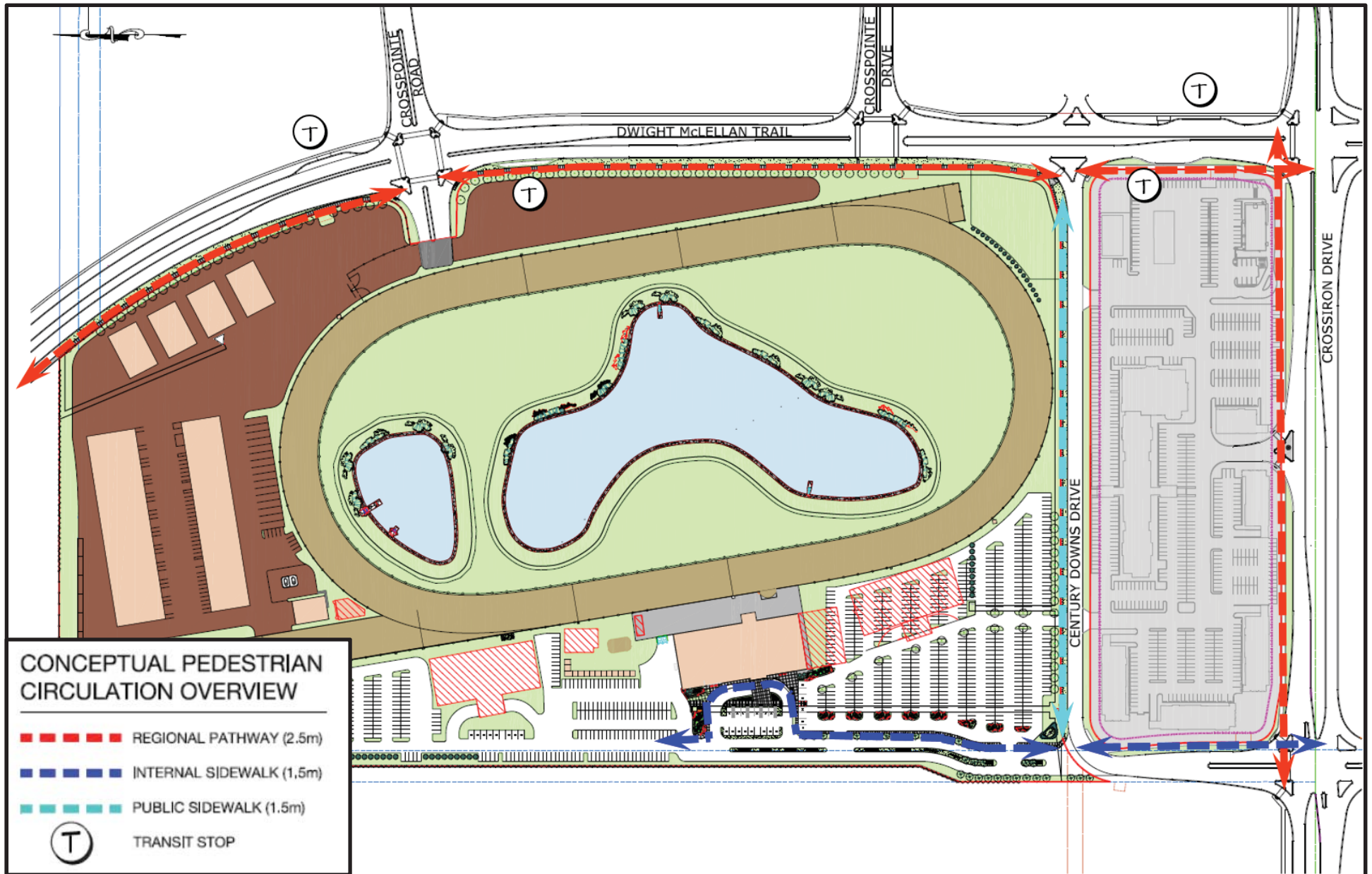


Figure 13 - Conceptual Pedestrian Circulation Overview

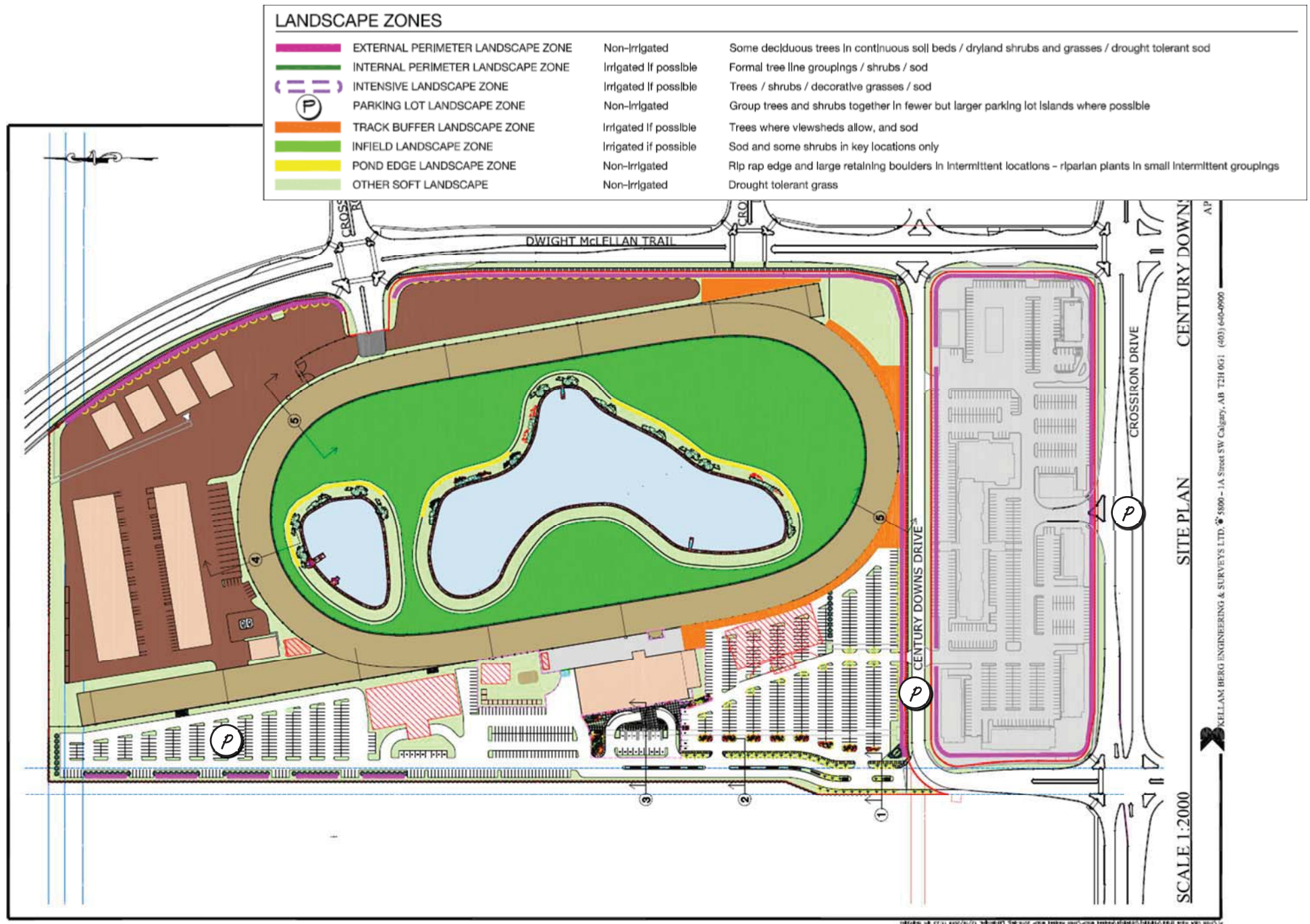


Figure 14 - Proposed Landscape Zones

3.4 Transportation and Parking Requirements

A Transportation Impact Assessment (TIA) was completed by Bunt and Associates in 2015. The report outlined intersection improvements as well as parking requirements for the development. A trip generation review has also been undertaken for the proposed development in Cell A-7 (*Bunt and Associates, 2019*).

A parking review undertaken by Bunt and Associates (2015) has been utilized in parking lot landscaping and design. The MSDP provides an integrated approach to the design of the parking facilities, the landscape and pedestrian systems, and identifies the traffic accommodation and flow for the internal development area. The parking lot island layout shown in the Site Parking Plan is schematic only. The final layout will be determined at the Development Permit Stage.

Two public entrances to the Racino and racetrack are provided from Century Downs Drive, as shown on the Site Parking Plan. There are also two entrances proposed from Century Downs Drive to the commercial, service, retail area south of Century Downs Drive.

It is expected that the existing entrance for jockeys and horse trailers from Dwight McLellan Trail will remain private unless commercial development is proposed to the south of this entrance. Should commercial development be proposed, this signalized entrance from Dwight McLellan Trail will be reconfigured to provide a public entrance from Dwight McLellan Trail to the commercial Site. The following entrances are shown on the Site Parking Plan:

- 1) Main entrance - Century Downs Drive;
- 2) Secondary entrance - Century Downs Drive;
- 3) Existing jockey entrance - Dwight McLellan Trail;
- 4 and 5) Cell A-7 access - Century Downs Drive; and
- 6) Cell A-7 Right In/Right Out - Crossiron Drive.

Policies:

3.4.1 The specific design and alignment of the internal access system shall be determined by detailed engineering at the Subdivision and/or Development Permit stage as per the terms of a Development Agreement and/or Development Permit process, to the satisfaction of the County.

3.4.2 Offsite infrastructure requirements will be identified in subsequent TIA updates at each stage of Development Permit and/or Subdivision in the plan area as per the County's Servicing Standards. Any improvements required for the development, shall be constructed/implemented by the developer through the terms of the County's Development Agreement process, to the satisfaction of the County

3.4.3 Maintenance of all internal roadways and/or walkways will be the responsibility of the Owner.

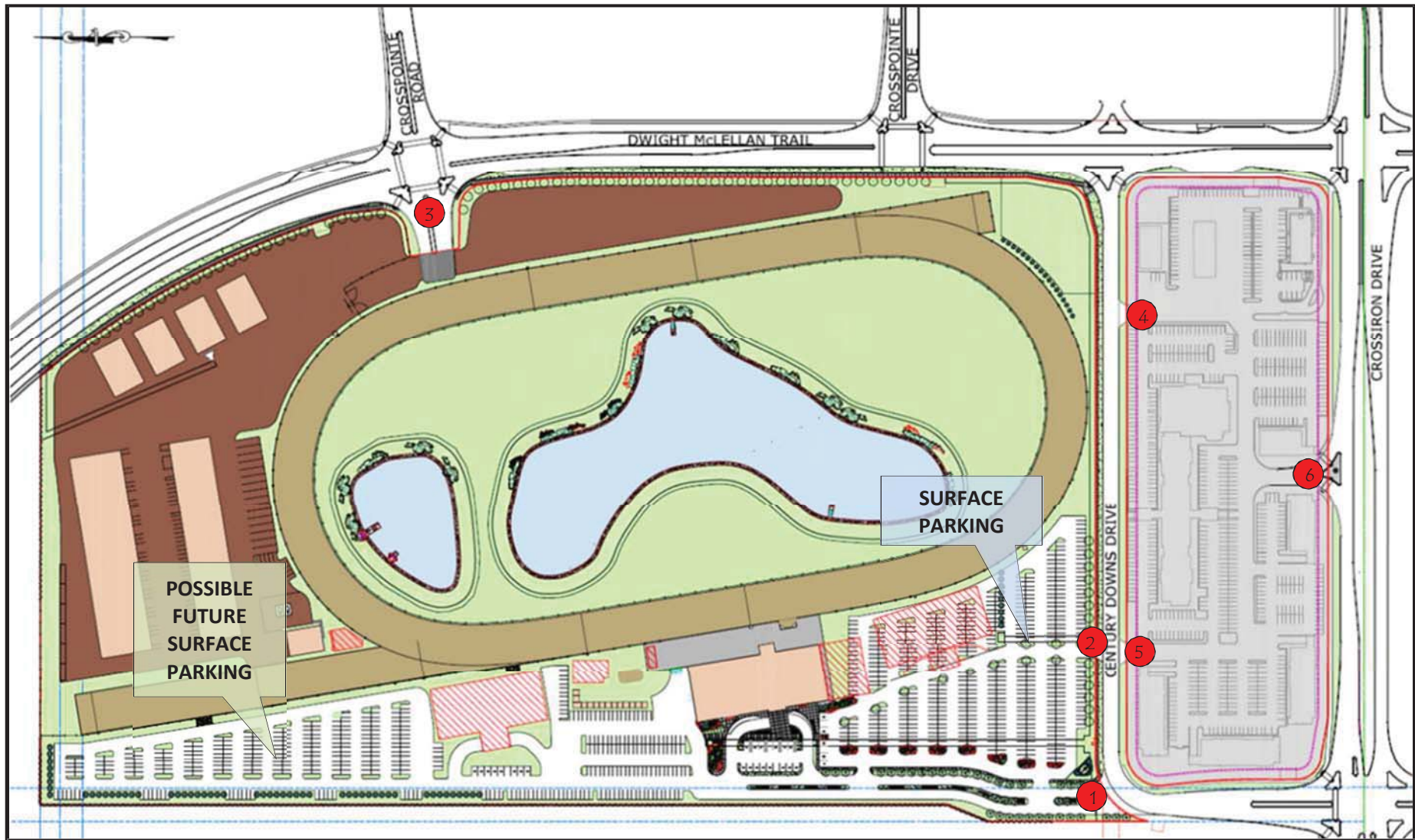


Figure 15 - Site Parking Plan

4.0 SIGNAGE AND LANDMARKS

4.1 Signage Overview

A Signage Plan reinforcing the image and theme of the REC was submitted to the Municipality upon application for a Development Permit. The Plan was in accordance with the Land Use Bylaw and special district provisions prepared for the site. Temporary signage will not be permitted with the exception of temporary signs required during development or building construction.

4.2 Signage at Major Entries to Cell A-1 and A-7

The main public entry points are from CrossIron Drive, Dwight McLellan Trail and Century Downs Drive. Entry signs at all of these access points will have a similar theme. A private gated entrance for jockeys and track staff located on Dwight McClellan Trail will also have appropriate signage.

Signage at the north and south ends of the development will involve collaboration between Century Casinos and Ivanhoe Cambridge, employing theme and congruency as unifying devices.

Century Casinos and Ivanhoe Cambridge will carry the idea of the joint venture approach to signage to the way finding signage as a further commitment to integrate the two developments.

4.3 Preliminary Master Site Signage Plan

Site signage elements addressing the various hierarchies of orientation will be located throughout the site. Signage location diagrams and examples included in this MSDP are intended as a template for detailed signage approvals. It is possible that some of



Figure 16 - Major Entry Signage example

the signage locations shown will not receive signage elements in the future. When the Project name and logo are developed, it will be an integral part of the signage imagery. The proposed locations of signs are shown in the Signage Plan and examples of possible sign types are shown on the following pages. The final locations of signs will be determined at Development Permit stage.

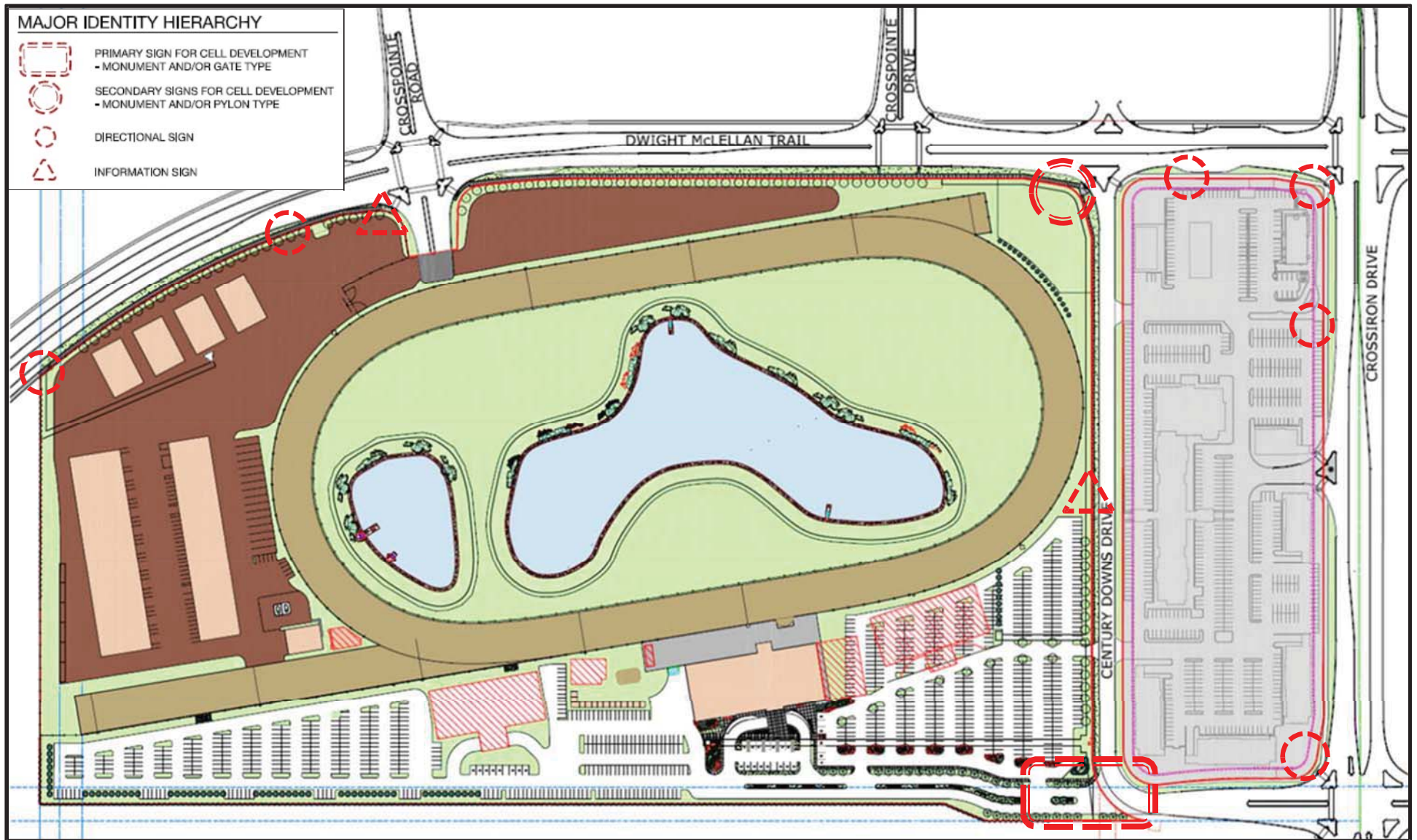


Figure 17 - Signage Plan - Major identity Hierarchy for signage

4.4 Signage Imagery and Concepts

Site signage, on-site signage and building signage will all require separate Development Permit submissions. The following images illustrate similar types and sizes of signage used in comparable developments.



Figure 18 - Example of Signage Character

5.0 LIGHTING

According to BEASP Development Guidelines (Section 4.4.3.c), all on-site lighting will be located, oriented, and shielded to prevent adverse effects on adjacent properties and to protect the safe and efficient function of the Calgary International Airport, Highway 2 and Highway 566. A detailed Lighting Plan has been submitted and approved as part of the main Development Permit application for the racetrack and Racino.

5.1 Reducing Light Pollution through Dark Sky Principals

As much as possible, lighting throughout the site will comply with the principals International Dark-Sky Association's (IDA) Mission Statement. Utilizing technical policies of the Dark Sky Principals such as:

- ✓ Energy savings resulting in economic benefits
- ✓ Superb nighttime ambience and quality of life
- ✓ Conservation of nocturnal wildlife and ecosystems
- ✓ Increased visibility, safety, and security at night by reducing glare
- ✓ Seek specific solutions that mitigate light pollution, including:
- ✓ Reduce total light in the nocturnal environment (total lumens in use) through;
 - i. Densities (i.e. lumens per square foot or square meter)
 - ii. Warranting
 - iii. Controls (e.g. on/off capabilities, time-of-night sensors)
 - iv. Energy Codes
- ✓ Shielding and directionality
- ✓ Consideration of spectral distribution

The following figure shows examples of fixtures which have shrouds to meet Dark Sky Initiative requirements.

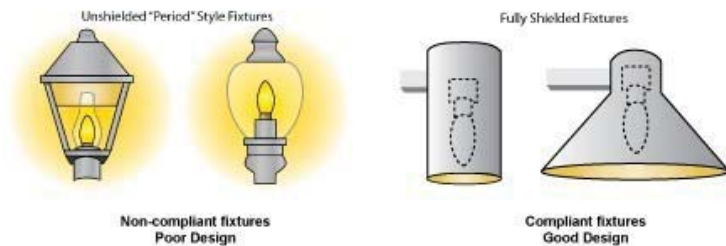


Figure 19 - Examples of Fully Shielded Fixtures

5.2 Lighting Types

Different types of lighting will be established and scaled to the particular needs of the different zones within the MSDP, including public lighting and project site lighting. The race track lighting, for instance, is highly specific to this function; parking lot lighting is a part of the architectural landscape and has been designed to achieve continuity across the entire Cell 'A' site. Where feasible this development will adhere to the principals of the Dark Skies Initiative.

5.3 Project Site Lighting

Site lighting will provide safety, security, and night ambience. The lighting zones include parking areas, entries, pedestrian walkways, and amenities, graphics, signage, architectural and landscape features, and service areas. Lighting features in these zones will be from the same family of fixtures, for example, material, color and lighting effect.

The general parking area features pole mounted fixtures providing consistent illumination and a natural color while reducing glare. Light pollution is mitigated through the use of shrouds on the parking lot light standards. Low mounted lighting along pedestrian sidewalks will reinforce pedestrian scale.

Service areas are lit through surface mounted wall fixtures. Concealed metal lighting sources will suit the theme when determined.

5.4 Race Track Lighting

The race track lighting is angled 30 degrees downward and focused, using optic lenses and shrouds, to optimally light the race while emitting as little light pollution as possible.

6.0 ARCHITECTURAL OVERVIEW

6.1 Massing and Materials

As shown in the Proposed REC Site Plan, the design presents massing in a way where the tallest and largest buildings are clustered toward the centre of the project site, adjacent to the racetrack. The track grandstand seating must face east to avoid sunlight in the eyes of patrons in the late afternoon and evening. All buildings on the periphery of the site are low one and two- storey structures that will relate well to the architecture on the adjoining Cells and adjacent developments.

The setbacks are set at an average of 6 m from the property line. On the development site due south of the track, a setback of 6 m is specified from the property line to parking lot.

Conceptual Architectural Renderings for the REC building are shown in Appendix B. Building materials include concrete masonry units, masonry, stone (natural or artificial), EIFS, acrylic stucco, prefinished metal, precast concrete aluminum framed glazing, and wood. Murals in slight relief, in a different shade of beige, may be used to decorate exterior walls of the Racino building, to achieve a subtle effect.

The pedestrian circulation system provides links to adjacent roads and the regional pathway and directs pedestrians to the building entrances.

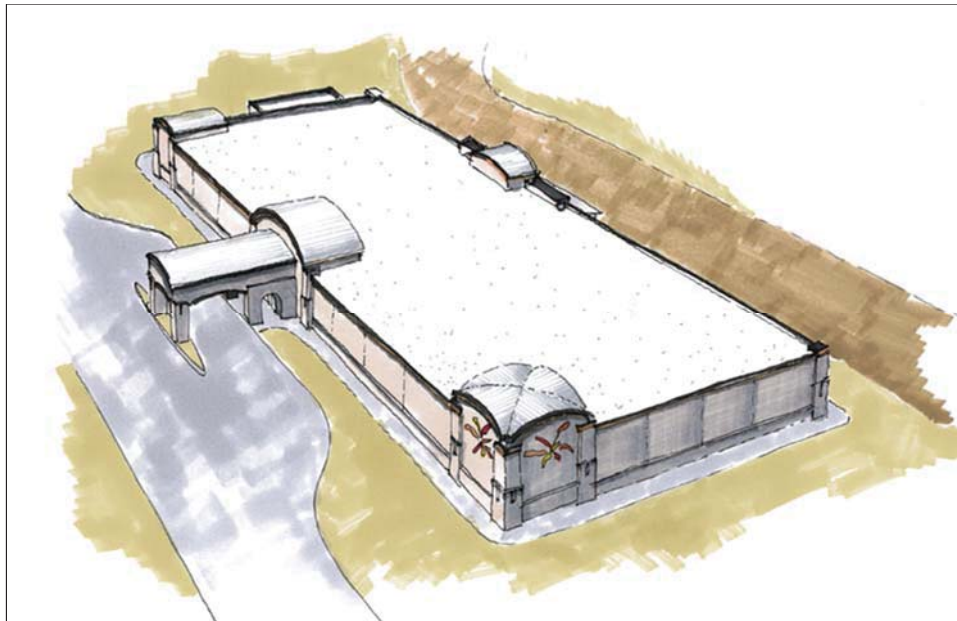


Figure 20 - Conceptual Rendering Racetrack and Entertainment Centre

6.2 Building Entries

All entries to the REC will be prominently located in plan and will be expressed as such on the façade(s). Architectural renderings depicting building entrances are shown in Appendix B.

6.3 Loading and Garbage

According to BEASP Development Guidelines

- ✓ Garbage and waste material within a Business Land Use and/or Business Park will be stored in weather-proof and animal-proof containers located within buildings or adjacent to the side or rear of buildings that will be screened from view by all adjacent properties and public thoroughfares.
- ✓ All loading docks and garbage bins will be situated in the least prominent location and placed behind landscape or architectural screens. The design and location will meet Rocky View County requirements and will be determined at the Development Permit stage.
- ✓ Loading areas facing public areas will be screened in a manner consistent with the architectural treatment of adjacent buildings or landscaping.

The race track will continue to operate as a ship in track. The horses will only be on the grounds for four to eight hours on racing days. Racing days will occur two to four times per week. The waste generated under these circumstances is expected to be minimal. Equine waste will be collected separately from other waste during and after the race day and deposited in covered Roll-Off containers. Empty Roll-Off containers will be brought in to replace filled ones. Equine waste will not remain on the site for extended periods of time.

7.0 SITE SERVICES

7.1 On Site Servicing

On site water and sanitary services for Cell A-1 are connected to County infrastructure at Dwight McLellan Trail and Century Downs Drive. There are existing water and sanitary services available for both possible hotel sites and there is servicing on the east side of the track to accommodate a possible fourth barn or commercial uses. Payment of the Water Cost Contribution was made as part of the Development Agreement process under 2013-DP-15287 to secure 25 m³/day for the subject lands. Potential servicing connections for Cell A-7 are shown conceptually on the Servicing Plan. There may be a requirement for multiple servicing connections when building locations are finalized at Development Permit stage.

Policies

Sanitary

7.1.1 The specific design, alignment, pipe sizing and configuration of the wastewater distribution system shall be determined by detailed engineering at the Subdivision and/or Development Permit stage as per the terms of a Development Agreement and/or Development Permit process, to the satisfaction of the County.

7.1.2 Any offsite infrastructure requirements (if required) will be identified at each stage of Development Permit and/or Subdivision in the plan area as per the County's Servicing Standards. Any improvements required for the development shall be constructed by the developer through the terms of the County's Development Agreement process, to the satisfaction of the County.

7.1.3 A detailed estimation of wastewater usage will be required at time of future Development Permit and/or Subdivision stage. If additional capacity is required to service the proposed development beyond the amount currently allocated to the Site, then additional capacity will be required to be purchased in accordance with the Master Rates Bylaw, as amended.

Water

7.1.4 The specific design, alignment, pipe sizing and configuration of the water distribution system shall be determined by detailed engineering at the Subdivision and/or Development Permit stage as per the terms of a Development Agreement and/or Development Permit process, to the satisfaction of the County.

7.1.5 Any offsite infrastructure requirements (if required) will be identified at each stage of Development Permit and/or Subdivision in the plan area as per the County's Servicing Standards. Any improvements required for the development shall be constructed by the developer through the terms of the County's Development Agreement process, to the satisfaction of the County.

7.1.6 A detailed estimation of water usage will be required at time of future Development Permit and/or Subdivision stage. If additional capacity is required to service the proposed development beyond the amount currently allocated to the Site, then additional capacity will be required to be purchased in accordance with the Master Rates Bylaw, as amended.



Figure 21 - Servicing Plan

7.2 Stormwater Management

A fully functioning storm pond is located within the infield to service the entire site. The privately owned system will collect and reuse stormwater for on-site irrigation of landscaping, including the track. All site stormwater is directed back into the site to achieve zero discharge of stormwater off the site, as outlined in the Stormwater Management Plan submitted with the original MSDP for Cell A-1 and the Master Drainage Plan for East Balzac 2006.

The “Pond Report for Race Track and Entertainment Centre” (Kellam Berg, 2014) provides design details for the storm drainage system. The storm drainage system was registered under the Environmental Protection and Enhancement Act (EPEA) in 2014. A Stormwater Management Plan will be undertaken for Cell A-7. Necessary Alberta Environment registration documentation for the stormwater infrastructure will be provided prior to issuance of subsequent development permits.

Policies:

7.2.1 At time of future Development Permit and/or Subdivision stage, updates to the Stormwater Management Report/Plan is required for the subject lands.

7.2.2 Future development will be subject to Erosion and Sediment Control Reporting and Plans at the Development Permit and/or Subdivision stage.

7.2.3 The specific design, alignment, sizing and configuration of the stormwater management system shall be determined by detailed engineering at the Subdivision and/or Development Permit stage as per the terms of a Development Agreement and/or Development Permit process, to the satisfaction of the County.

7.3 Utilities

Gas meters, pad-mounted transformers and other service elements will be screened with a landscape or architectural device. Shallow utilities shall be provided within the plan area at no expense to the county and appropriate easements shall be provided to any utility company providing services. All site electrical will be buried beneath grade. Some overhead electrical service off-site will parallel public roads. Water and sanitary services will connect to existing infrastructure adjacent to the site.

8.0 IMPLEMENTATION

8.1 Development Permit Applications

Development permits are to be general accordance with the Master Site Development Plan.

Policies:

8.1.1 Proposed size, location and layout of development are conceptual only and may change at future Development Permit stage. No amendments will be required to the Master Site Development Plan for changes in proposed number of freestanding signs, accessory buildings, building size, layout and location.

8.1.2 Council shall be the Development Authority for the proposed hotel in Cell A-1.

8.1.3 The Development Authority shall be responsible for approving the Development Permit for the proposed commercial/retail development located to the south of Century Downs Drive.

8.2 Process for Individual Site Plan(s) & Permit Application(s)

This Process will include two stages:

1. Development Permit applications for the various phases or buildings.
2. Building Permit applications for all locations.

8.3 Building Permit applications for all locations

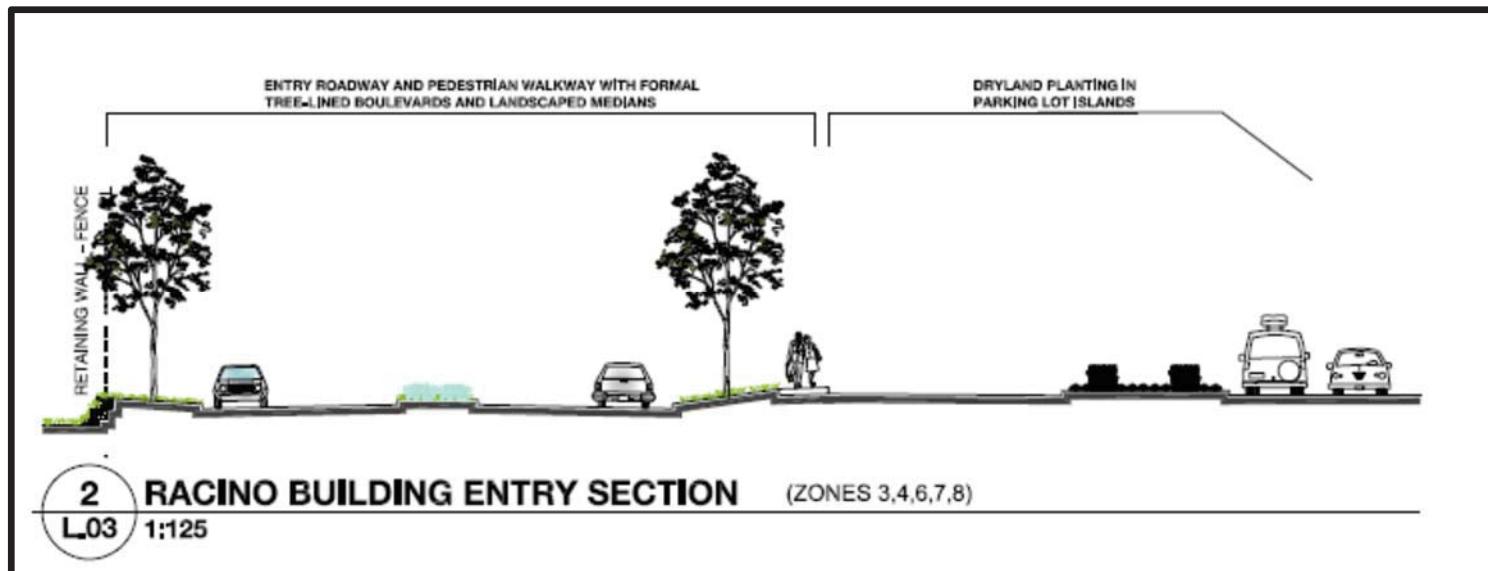
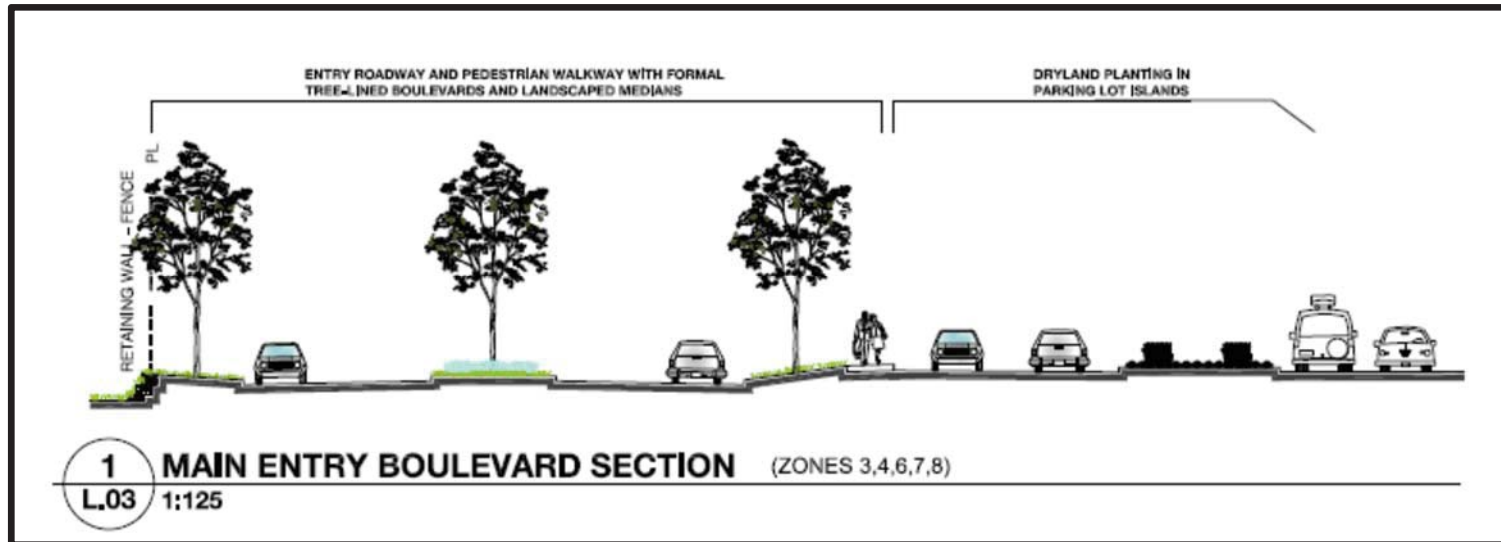
The process for individual Development and Building Permit applications shall be in accordance with the requirements of Rocky View County. Subsequent applications will include, but not be limited to the, the REC, the barns, a possible future hotel and commercial sites.

9.0 Supporting Technical Studies (under separate cover)

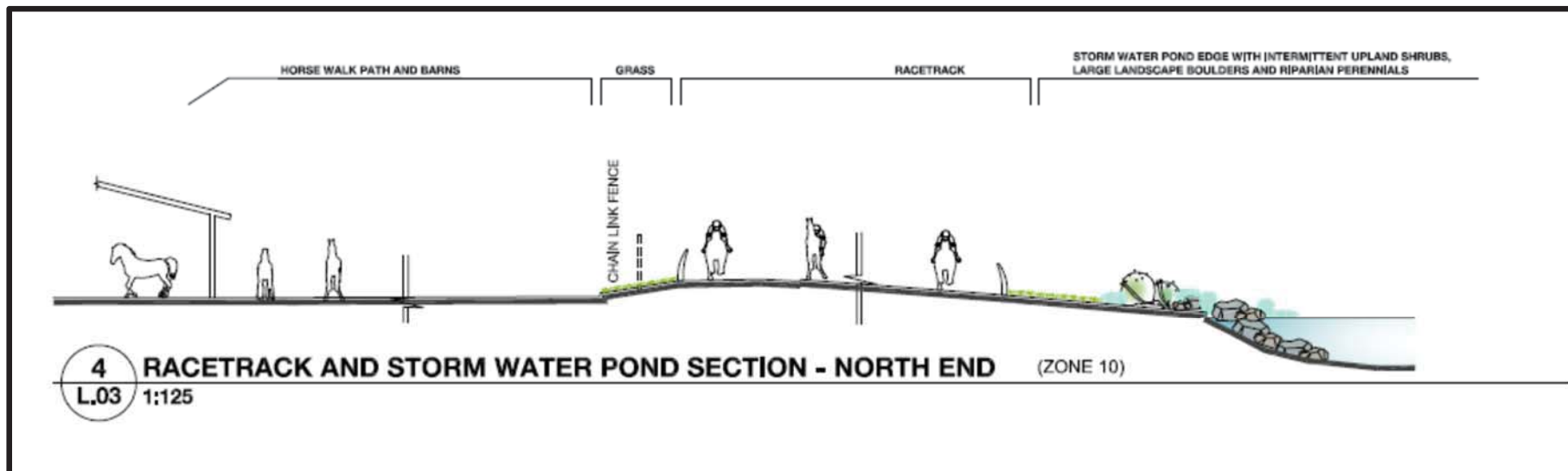
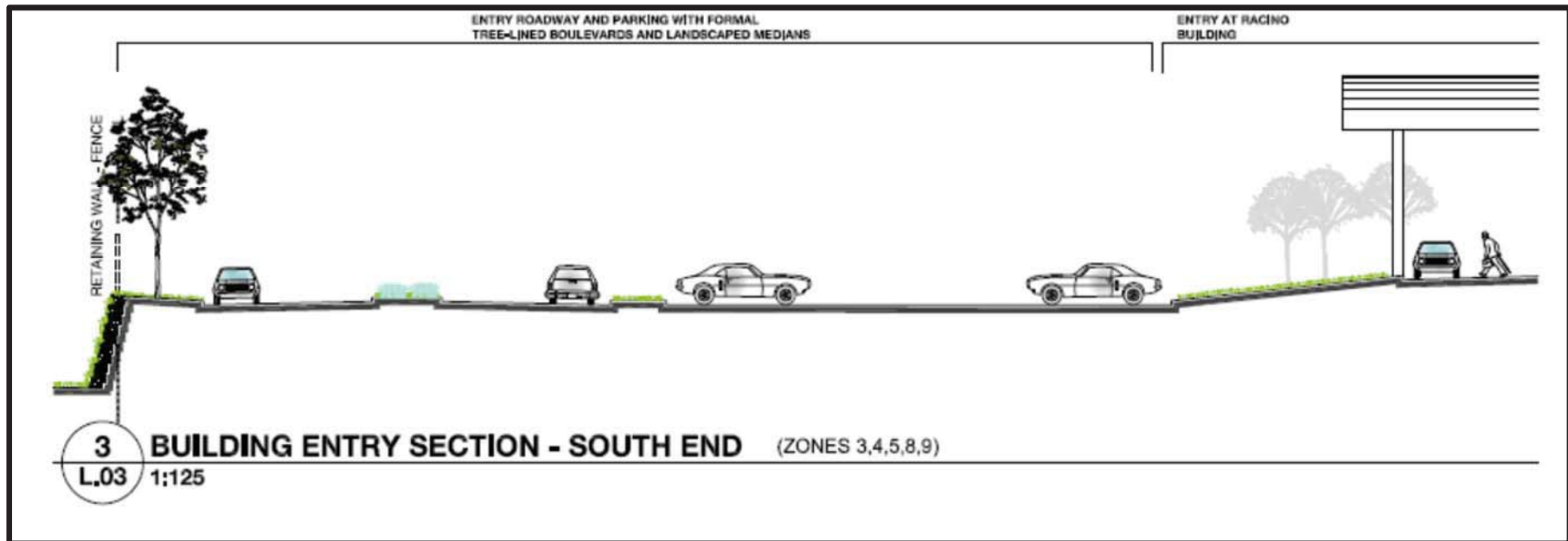
1. Racing and Entertainment Centre, Near Town of Balzac, Transportation Impact Assessment Update – Final Report, Bunt and Associates, November 2013.
2. REC MSDP 2015, Race Track and Entertainment Centre, TIA Update Letter December 10, 2015, Bunt & Associates.
3. Century Downs Trip Generation Review, April 17, 2019, Bunt & Associates.
4. Stormwater Management Report for Race Track and Entertainment Centre, December 2013.
5. CrossIron Landing Trip Generation Review, Bunt & Associates, October, 2019.

10.0 APPENDICES

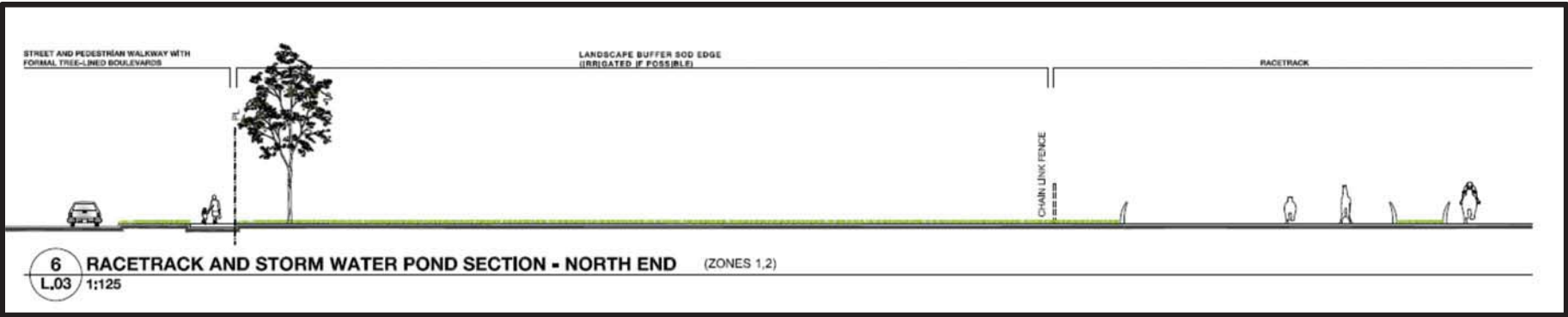
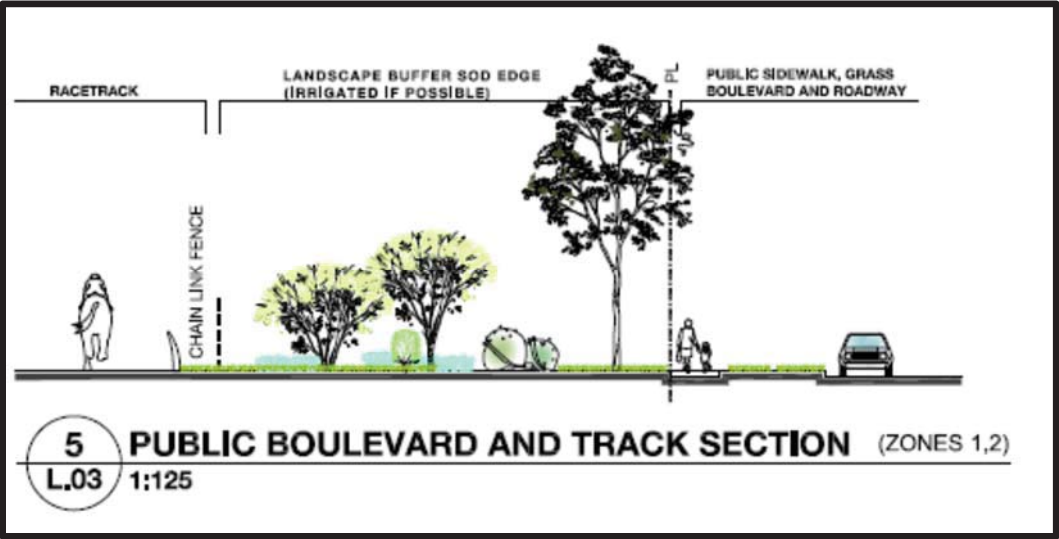
APPENDIX A – LANDSCAPE CONCEPTS BY SECTION



APPENDIX A – LANDSCAPE CONCEPTS BY SECTION (continue)

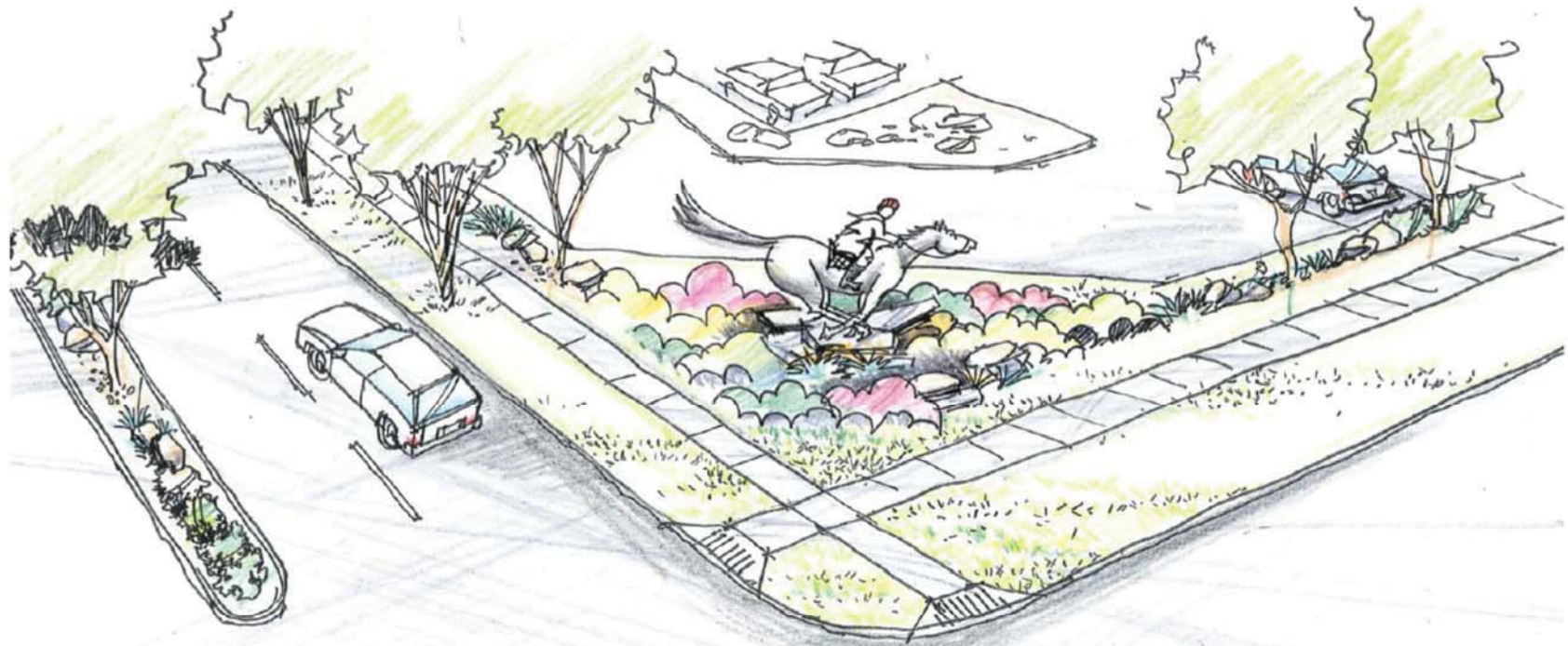


APPENDIX A – LANDSCAPE CONCEPTS BY SECTION (continue)



APPENDIX B – CONCEPTUAL ARCHITECTURAL RENDERINGS

CONCEPT OF MONUMENT FEATURE AT MAIN ENTRANCE

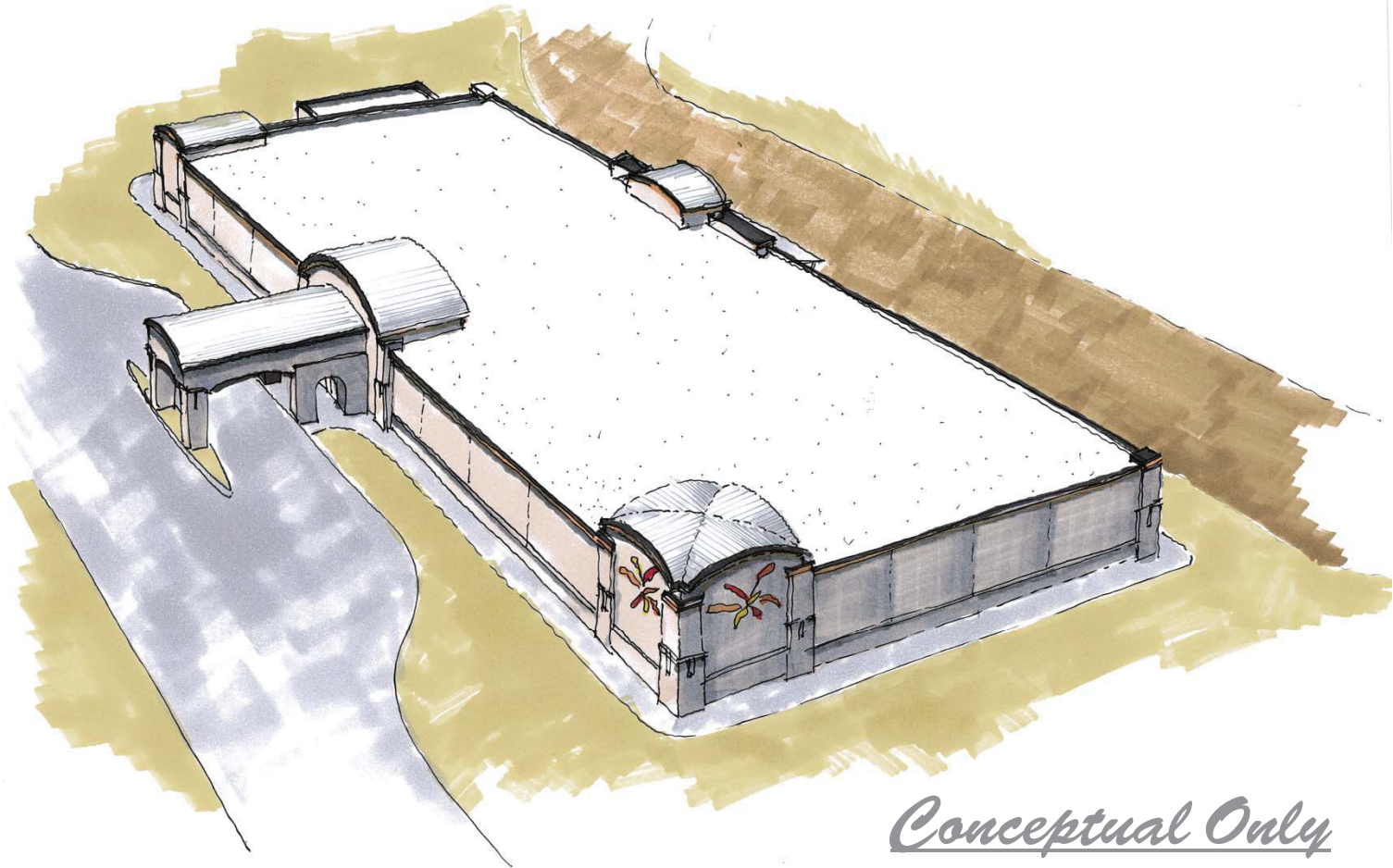


MONUMENT FEATURE AT MAIN ENTRANCE

Conceptual Only

APPENDIX B – CONCEPTUAL ARCHITECTURAL RENDERINGS

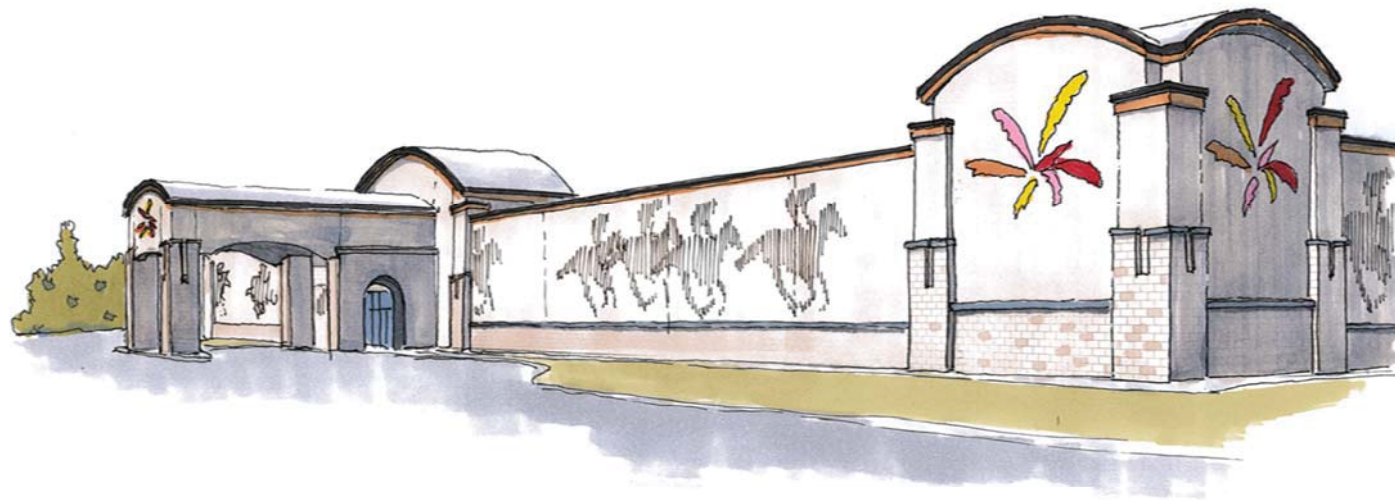
AERIAL VIEW OF THE REC BUILDING



Conceptual Only

APPENDIX B – CONCEPTUAL ARCHITECTURAL RENDERINGS

FRONT FAÇADE OF THE REC BUILDING



Conceptual Only

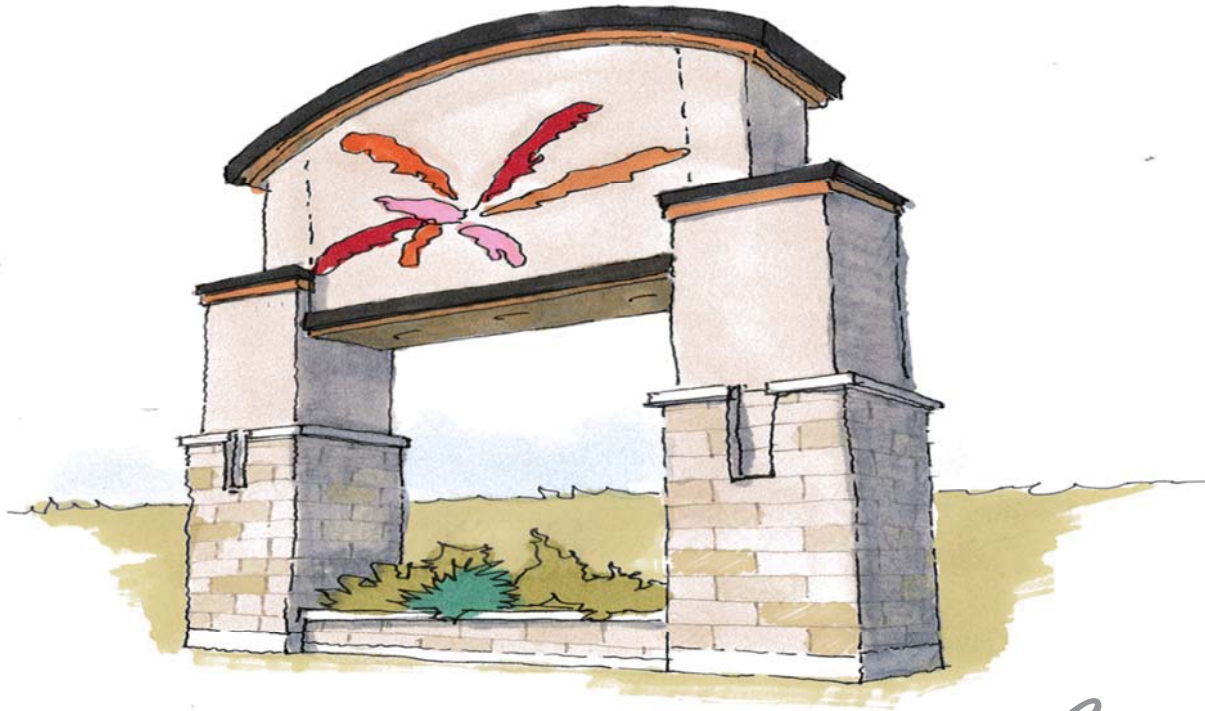
APPENDIX B – CONCEPTUAL ARCHITECTURAL RENDERINGS

VIEW OF THE REC APRON DEPICTING THE GRANDSTAND AND REAR ENTRANCE



APPENDIX B – CONCEPTUAL ARCHITECTURAL RENDERINGS

VIEW OF CONCEPTUAL SITE ENTRY MONUMENT



Conceptual Only