

CONCEPTUAL SCHEME
**HIGH PLAINS
INDUSTRIAL PARK**



ROCKY VIEW COUNTY
Cultivating Communities

Bylaw C-6930-2010
Adopted July 27, 2010

**ROCKY VIEW COUNTY
BYLAW C-6930-2010**

A Bylaw of Rocky View County to adopt a Conceptual Scheme known as the “High Plains Industrial Park Conceptual Scheme”.

WHEREAS the Council deems it desirable to adopt the Conceptual Scheme, and

WHEREAS the *Municipal Government Act*, 2000, c. M-26 as amended, allows the County to adopt plans relating to the future subdivision and development of lands within the County; and

WHEREAS the Council of Rocky View County has received an application to adopt a Conceptual Scheme to provide a framework for subsequent redesignation, subdivision and development within Sections 1 & 12-26-29-W4M; consisting of an area of approximately \pm 518 hectares (1280 acres), as defined in Schedule 'A', the “High Plains Conceptual Scheme,” attached to and forming part of this Bylaw; and

WHEREAS a notice was published on Tuesday, June 29, 2010 and Tuesday, July 6, 2010 in the Rocky View Weekly, a newspaper circulating in Rocky View County, advising of the Public Hearing for Tuesday, July 27, 2010; and

WHEREAS Council held a Public Hearing and have given consideration to the representations made to it in accordance with Section 692 of the *Municipal Government Act*, being Chapter M-26 of the Revised Statutes of Alberta, 2000, and all amendments thereto.

NOW THEREFORE the Council enacts the following:

1. That the “High Plains Conceptual Scheme,” attached hereto as Schedule ‘A’, be adopted to provide a framework for subsequent redesignation, subdivision and development within Sections 1 & 12-26-29-W4M, consisting of an area of approximately \pm 518 hectares (1280 acres), as defined in Schedule 'A', attached to and forming part of this Bylaw;
2. The Bylaw comes into effect upon the date of its third reading.

Division: 7

File: 36401004/06401001/003/004/006/008/06412001/002/004/006 2009-RV-273

First reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on Tuesday, June 22, 2010, on a motion by Deputy Reeve Loudon.

Second reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on Tuesday, July 27, 2010, on a motion by Reeve Habberfield.

Third reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on Tuesday, July 27, 2010, on a motion by Councillor Rheubottom.

REEVE OR DEPUTY REEVE

MUNICIPAL SECRETARY

SCHEDULE 'A'
FORMING PART OF BYLAW C-6930-2010

A Conceptual Scheme affecting the area within Sections 1 & 12-26-29-W4M, herein referred to as the
'High Plains Industrial Park Conceptual Scheme'.

ROCKY VIEW COUNTY CONCEPTUAL
SCHEME BYLAW C-6930-2010

OFFICE CONSOLIDATION

This document has been consolidated for convenience only. A copy of the original Bylaw and all amending Bylaws can be obtained from Rocky View County. This office consolidation is composed of the following Bylaws.

Bylaw	Amendment Type	Date of Approval
C-6930-2010	Original Bylaw	July 27, 2010
C-7002-2010	Stage 1 Outline Plan Is appended	January 11, 2011
C-7189-2012	Stage 2 Outline Plan Appendix 'C'	September 11, 2012
C-7286-2013	Stage 3 Outline Plan Appendix 'D'	July 21, 2013
C-7515-2015	Stage 6 Outline Plan Appendix 'E'	September 29, 2015
C-7820-2018	Stages 4 & 5 Outline Plan Appendix 'F'	February 12, 2019

ACKNOWLEDGEMENT PAGE

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1.0 INTRODUCTION

1.1 Purpose

This Conceptual Scheme (CS) provides a comprehensive planning framework to guide the development of the High Plains Industrial Park in a form which is consistent with Rocky View County's strategic objective of accommodating larger format business development opportunities with reduced servicing requirements within the East Balzac major business/employment node.

1.2 Background

Given its proximity to the Calgary International Airport and regional transportation & utility servicing corridors, the East Balzac area is ideally situated to accommodate business development. Adopted by the County in 2001, the Balzac East Area Structure Plan (BEASP) establishes land use and development expectations which encourage a variety of business development types within the influence of the Calgary International Airport. Since adoption of the BEASP in 2001, the County has partnered with major industry stakeholders to resolve a number of transportation and utility servicing infrastructure considerations required to facilitate development within the area. With the recent opening of the CrossIron Mills Regional Shopping Centre, the County is continuing to work alongside a variety of industry partners to implement strategic regional economic development opportunities within the East Balzac area.

1.3 Development Rationale

The demand for commercial/business land development within the Calgary Region is expected to continue. In response to this demand, Rocky View County recently expanded the Balzac East ASP to include lands situated east of McDonald Lake (including the CS area). The expanded BEASP area will accommodate extension of existing and/or planned industrial development within the area.

The High Plains Industrial Park Conceptual Scheme's development framework has been prepared in accordance with the provisions of the BEASP.

1.4 Conceptual Scheme Objectives

The High Plains Industrial Park Conceptual Scheme:

- a) Describes the summary findings of a comprehensive baseline review of current conditions within the CS area to identify any significant development constraints which require appropriate mitigation strategies in support of future development;
- b) Describes a development rationale to support implementation of a comprehensive land use, subdivision, and development framework within the CS area;
- c) Determines appropriate transportation and utility service infrastructure improvements required to facilitate development within the Plan area – evaluated within the context of infrastructure capacities and/or requirements for the entire Balzac East area;
- d) Determines the configuration of public open space infrastructure within the CS area;

- e) Establishes expectations for the delivery of community support infrastructure within the CS area;
- f) Establishes a logical development phasing strategy to facilitate the orderly progression of development throughout the CS area, over time;
- g) Summarizes the conclusions of a community consultation program to inform and educate affected stakeholders and to ensure the Conceptual Scheme's development framework responds to identified concerns; and
- h) Rationalizes the proposed development in accordance with the County's adopted statutory and non-statutory policy framework.

2.0 THE CONCEPTUAL SCHEME AREA

2.1 Location

As shown on **Figure 1 – Balzac Regional Context**, the Conceptual Scheme area is situated within the eastern portion of the County's East Balzac business/industrial growth node.

The Conceptual Scheme (CS) area is bound by Range Road 291 to the west, Highway 566 to the north, Range Road 290 to the east and Township Road 260 / 144th Avenue NE (The City of Calgary) to the south.

2.2 Local Context

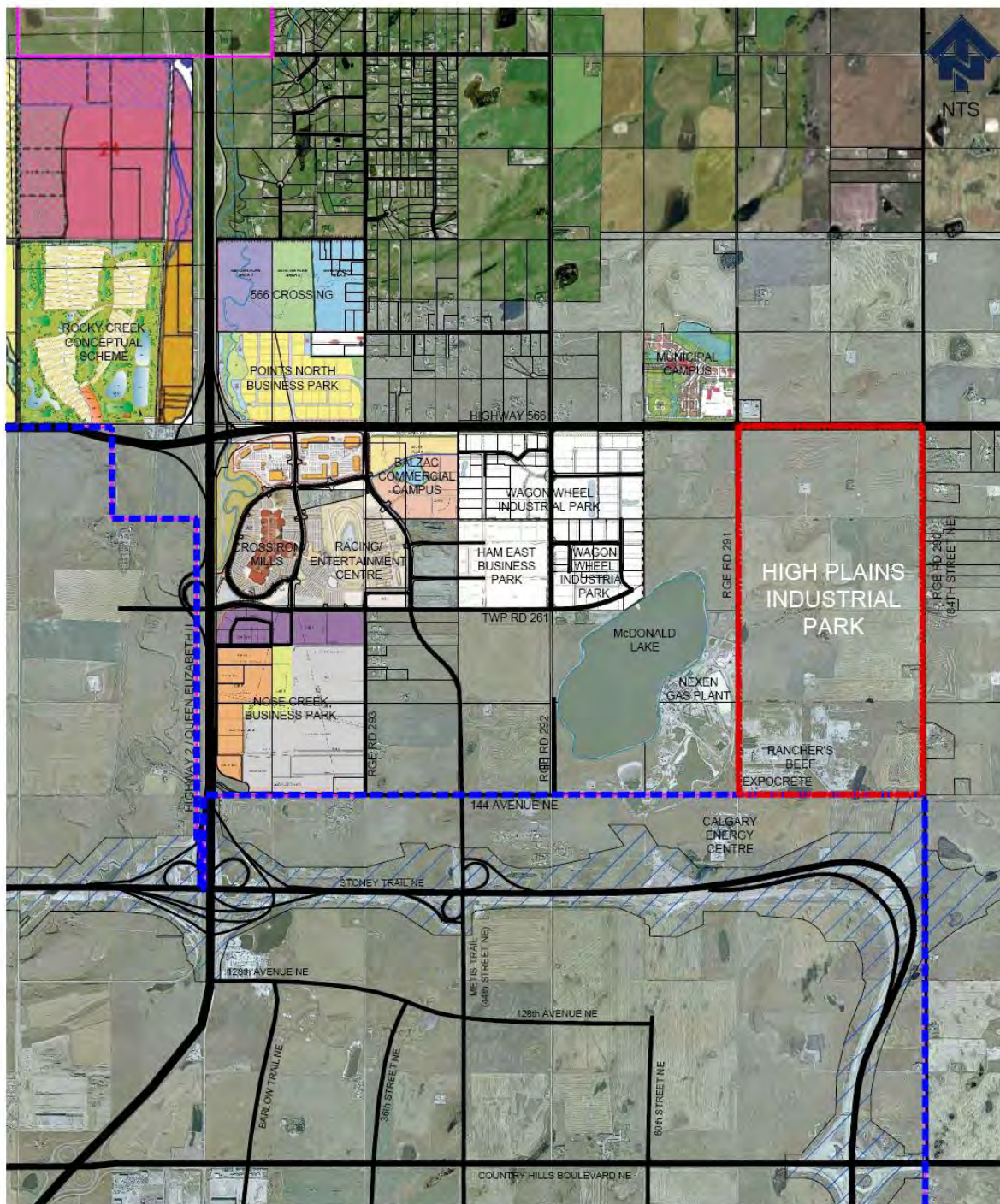
The character of the surrounding lands is significantly impacted by the influence of the Nexen Sour Gas Plant which includes 'heavy' industrial processing activities that visually dominate the eastern portion of the entire BEASP area.

Existing developments within the southeast portion of the CS area include a Meat Packaging Facility (Sunterra Beef Ltd.), a concrete materials supplier (Expocrete Concrete Products Ltd.) and the Calgary Energy Centre (Enmax). The nature of these existing industrial developments is characterized by a 'heavy' industrial format that is markedly different from the comprehensively planned commercial developments situated within the balance of the Balzac East ASP area.

2.3 Legal Descriptions & Ownership

As shown on **Figure 2 – Legal Descriptions**, the Conceptual Scheme study area includes approximately 515 ha (1,274 ac) with current ownership and legal descriptions described as per the following:

Legal Description	Owner	Area (ac)
NW 12-26-29-W4M	Highfield Stock Farm Inc., CLT Contracting Ltd., Irene Jones, Clint & Beverly Havens	157.95
NE 12-26-29-W4M	Highfield Stock Farm Inc., CLT Contracting Ltd., Irene Jones, Clint & Beverly Havens	158.07
SW 12-26-29-W4M	Yellowstone Property Consultants Corp.	160
SE 12-26-29-W4M	Highfield Stock Farm Inc. & CLT Contracting Ltd.	142.65
Lot 1, Blk 1, Plan 071 5608	Grant & Katherine Shuttleworth	16.31
NW 1-26-29-W4M	1405275 AB Ltd. (in trust for Highfield Stock Farm Inc. & CLT Contracting Ltd.)	160
NE 1-26-29-W4M	1405275 AB Ltd. (in trust for Highfield Stock Farm Inc. & CLT Contracting Ltd.)	160
SW 1-26-29-W4M	Sunterra Beef Ltd.	80.33
Lot 1, Blk 1, Plan 061 2816	Sunterra Beef Ltd.	58.71
Lot 2, Blk 1, Plan 071 1938	Expocrete Concrete Products Ltd.	20.1
SE 1-26-29-W4M	Shell Canada Limited	160
TOTAL		1,274.17



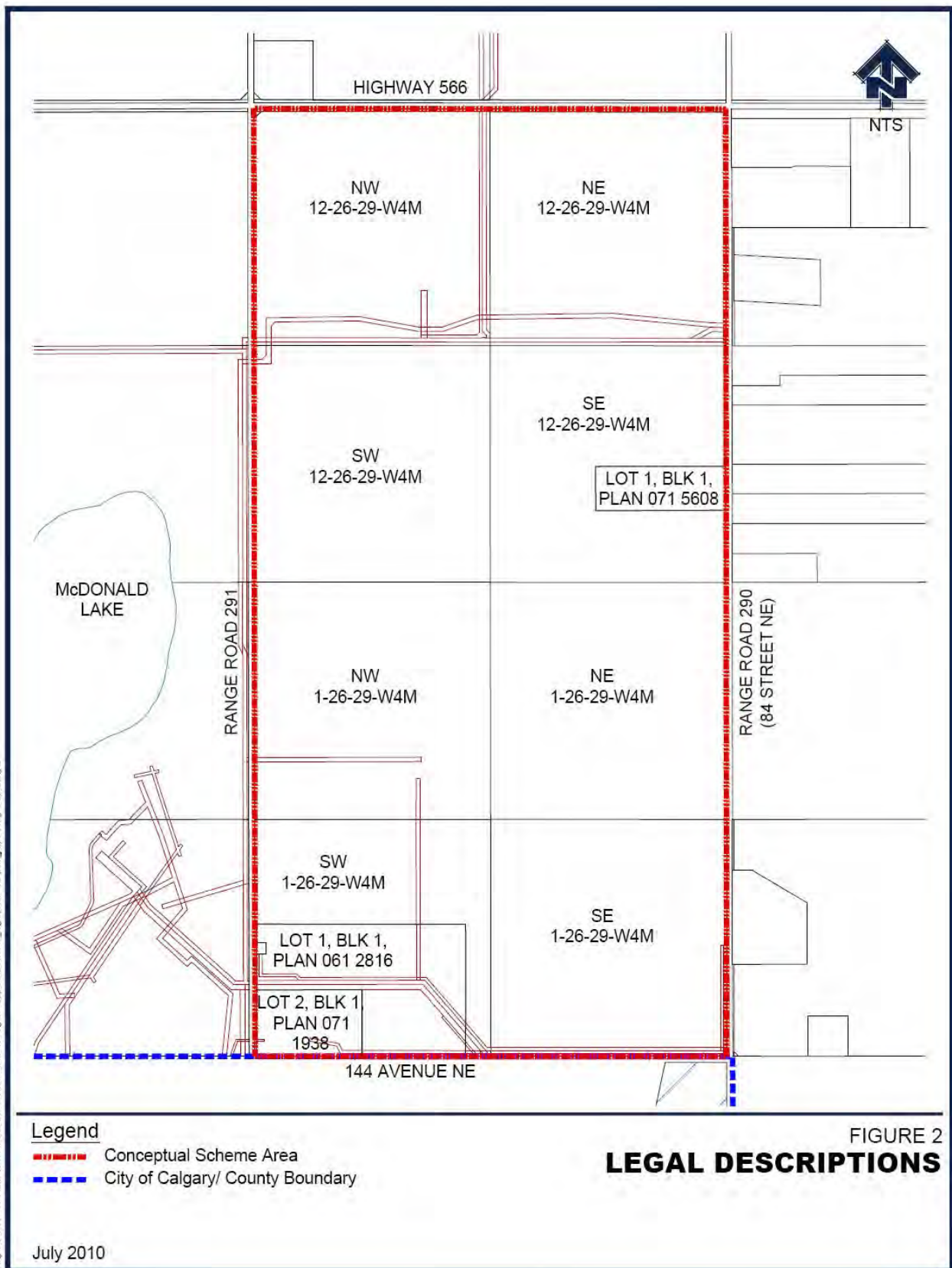
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- Conceptual Scheme Area
- City of Calgary/ County Boundary

FIGURE 1
BALZAC REGIONAL CONTEXT MAP

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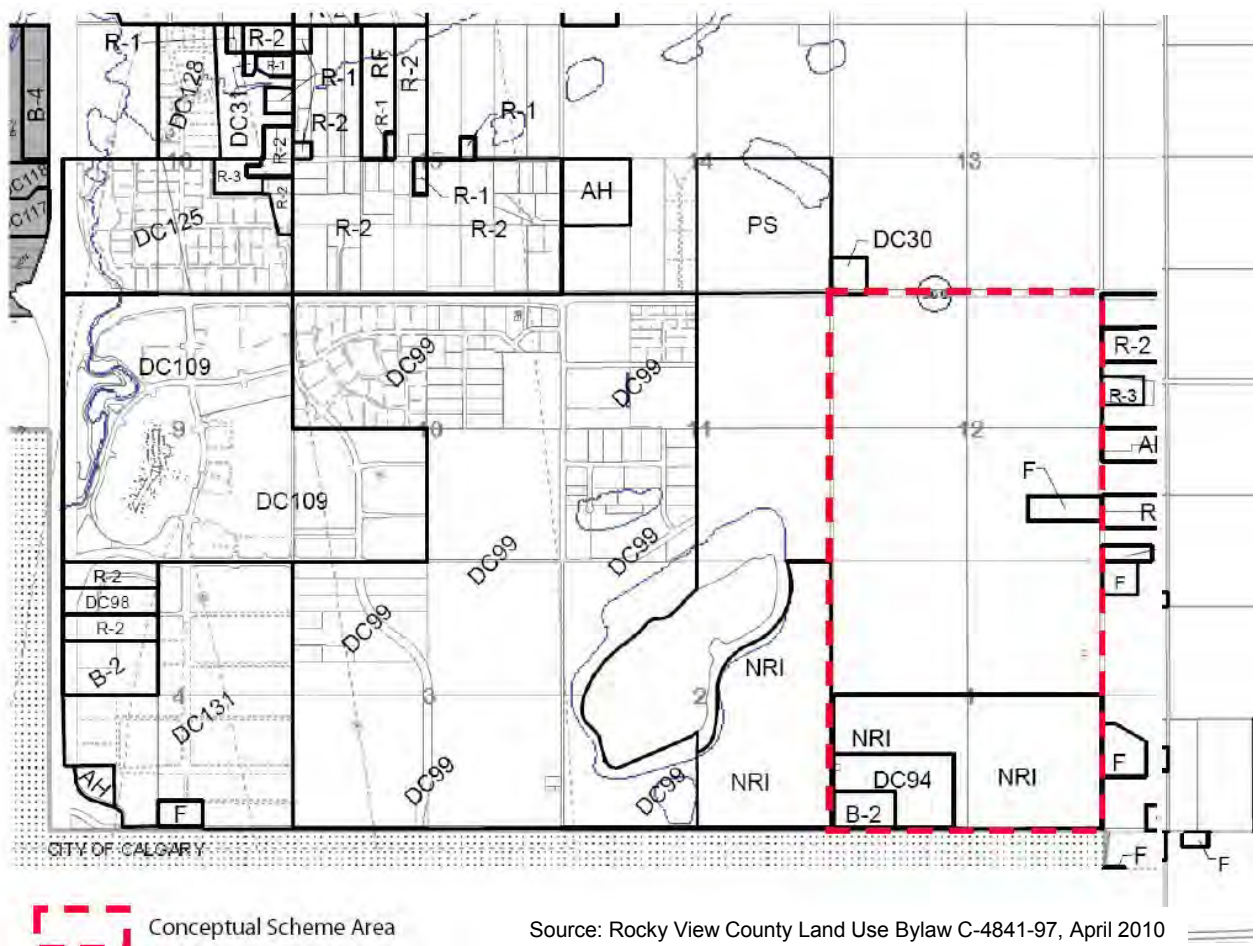


2.4 Existing Land Use

The subject lands are predominantly designated Ranch & Farm District (RF) in accordance with the County's Land Use Bylaw C-4841-97. There is one Farmstead parcel (F) within the SE Sec. 12 and the S ½ Sec. 1 includes Natural Resource Industrial (NRI) designation - former Shell Refinery; a General Business District (B-2) - Expocrete Concrete product retailer and a Direct Control District (DC-94) – former Rancher's Beef meat packaging facility.

Lands located directly west of Sec 1 are designated Natural Resource Industrial (NRI) - including the Nexen Sour Gas Facility. Lands directly west of Sec 12 are proposed for business development. Lands further to the west include a variety of Direct Control designations (DC 99, DC 109 & DC 131) which accommodate comprehensively planned business park development. DC 99 includes the Wagon Wheel Industrial Park, the Balzac Commercial Campus and the Ham East Industrial Park (and other proposed business developments); DC 109 includes the CrossIron Mills and Racing & Entertainment Centre uses and DC 131 includes the Nose Creek Business Park.

Lands located north of Highway 566 include an evolving mixture of Direct Control Districts (DC) in support of business park uses, Ranch & Farm District (RF), Agricultural



Holdings District (AH) and a mix of country residential development with Residential One

and Residential Two District (R1 & R2) with corresponding parcel sizes ranging from ± 2 acres. The County recently redesignated the $\frac{1}{4}$ section directly northwest of the CS area to Public Service District (PS) to accommodate the anticipated development of a new Municipal Campus and supportive civic uses.

Lands directly north and east of the Plan area include a mix of Ranch & Farm District (RF) and Agricultural Holdings District (AH) interspersed with a range of country residential development with Residential Two and Three Districts (R2 & R3).

2.5 Existing Developments

As illustrated on **Figure 3 – Existing Site Conditions**, the Conceptual Scheme area includes predominantly agricultural lands under cultivation. Access is available from Range Roads 290 and 291. Highway 566 bounds the Plan area to the north; however, direct access to the highway is not currently available from the CS area.

Three (3) buildings sites have been developed within the CS area each including single family residences with accessory buildings serviced by individual groundwater wells and private sewage disposal systems. Two 'lease sites' supply natural gas (sour) to the regional distribution network. A telecommunications facility (cell tower) is located within a portion on NE 1.

The southern portion of the site includes a concrete product manufacturing facility (Expocrete), a meat packaging facility (Sunterra), the Nexen Gas Plant, and the Calgary Energy Centre (Enmax) electrical power generation facility. Existing industrial uses within this area are predominantly 'heavier' in nature with processing activities with potential to create more off-site impacts for surrounding lands.

2.6 Surrounding Land Use Context

2.6.1 Stoney Trail NE Freeway and Transportation & Utility Corridor (TUC)

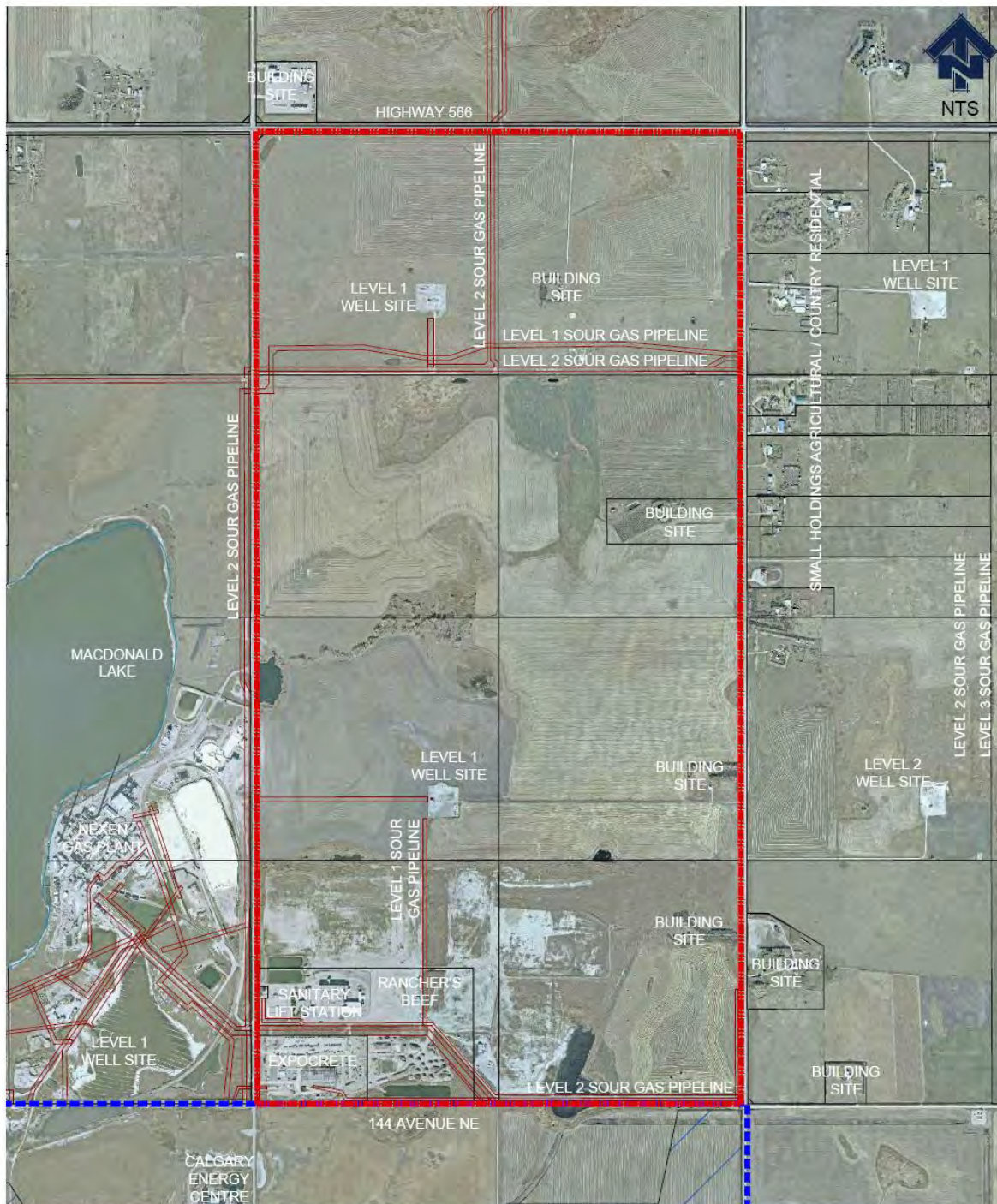
The objective of the Province of Alberta's Transportation & Utility Corridor (TUC) program is to accommodate implementation of a new roadway system with corresponding power lines, pipelines, regional water and sewer lines and telecommunication lines to 'ring' the City of Calgary as illustrated.

The TUC accommodates long-term planning of a number of transportation & utility facilities and also provides open space in an area that will be eventually surrounded by urban development.

The Province established the TUC to protect rights-of-way necessary to implement the Calgary 'Ring Road' and related utility infrastructure.



It is intended to provide a long-term solution to many land use and servicing challenges associated with developing major linear facilities in the regional context.



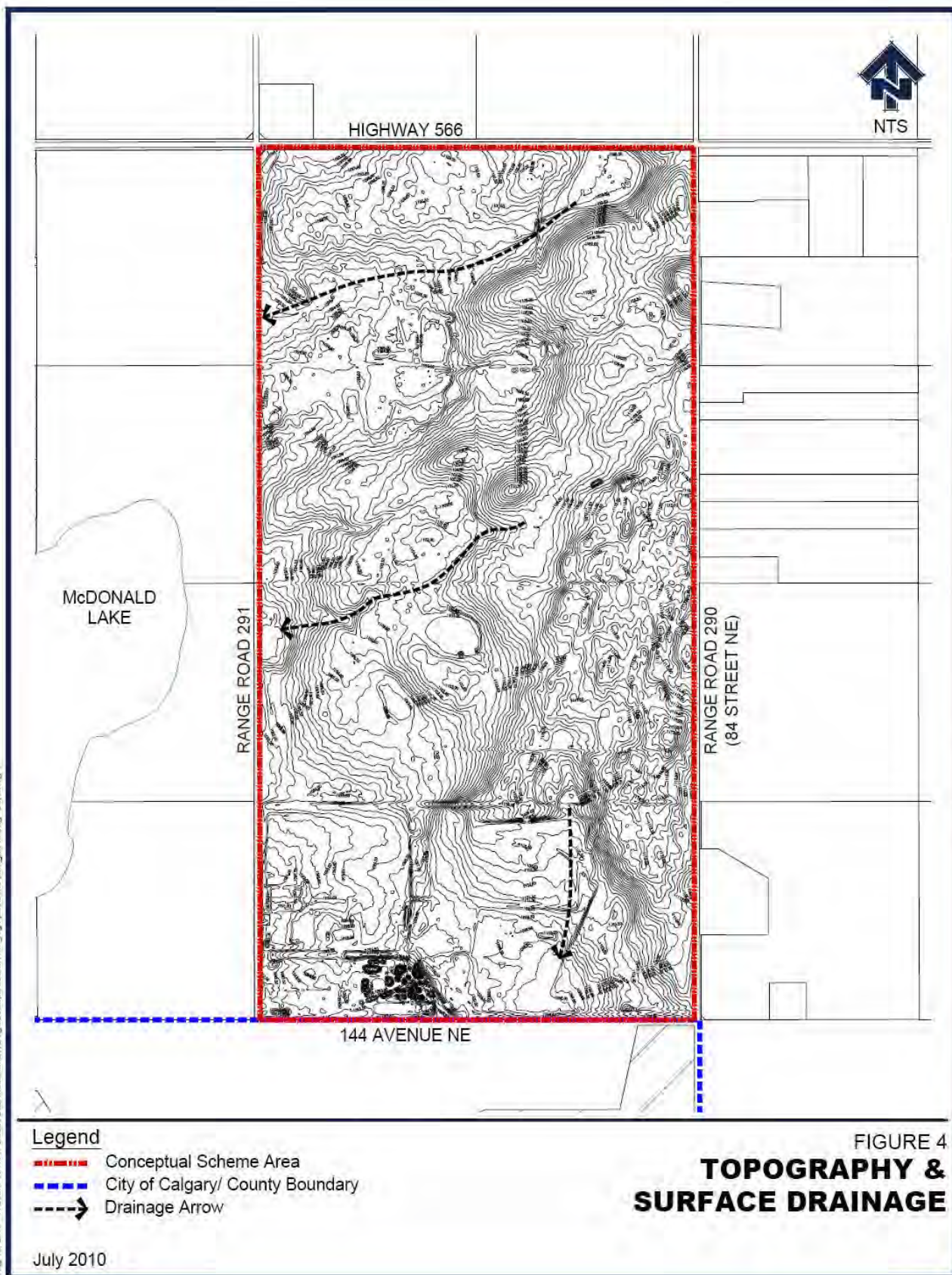
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- - - - - Conceptual Scheme Area
- - - - - City of Calgary/ County Boundary

FIGURE 3
EXISTING SITE CONDITIONS

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2.6.2 The Calgary International Airport

The Conceptual Scheme area is not located within the Noise Exposure Forecast (NEF) contours of the Calgary International Airport Vicinity Protection Area (AVPA) regulation nor is it located within the Calgary International Airport Zoning Regulation. As such, development proposed within the CS area is not expected to negatively impact the ongoing operation of the airport.



However, the CS area is located within the Terminal Surveillance Radar (TSR) Site as defined by the Electronic Facilities Protection Area. As such, building heights within this area are restricted to a maximum of 38 – 56 m. Accordingly, all development / building permit applications should be circulated to NAV Canada at the development permit application stage to ensure compliance with this height restriction.

2.7 Site Conditions

2.7.1 Topography & Surface Drainage Considerations

As illustrated on **Figure 4 – Topography & Surface Drainage**, the subject lands are flat to gently rolling with topography generally sloping across the site from the northeast towards the southwest. Natural drainage occurs via overland sheet flow generally within three (3) intermittent drainage courses that traverse portions of the site.

The CS area includes numerous dugouts and trap-lows with several engineered storm ponds previously developed to service existing industrial uses.

2.7.2 Sour Gas Well & Pipeline Considerations

As illustrated by **Figure 3 – Existing Conditions**, the CS Area includes a variety of utility rights-of-way including two (2) Level 1 Sour Gas production wells and a variety of Level 1 & 2 sour gas distribution lines. The alignment of roads, parcel configuration and building placement affecting all development within the CS area will respect all applicable building setback requirements as may be required by the Energy Resource Conservation Board (ERCB).



Conceptual Scheme Area
Area studied by 2009 Environmental Impact Assessment (Westhoff Engineering Resources Ltd.)

2.7.3 Archaeological & Historical Resource Considerations

In June 2008, Bison Historical Services Ltd. completed a Historical Resource Overview (HRO) in accordance with the requirements of the Province of Alberta's Historical Resources Management Branch.

The conclusions of this study indicate the site is not likely to contain archaeological or historical resources that require development avoidance/mitigation measures. The Province concurred with these conclusions and has provided a development clearance for the CS area in accordance with the Historical Resource Act.

Policies:

Policy 2.7.3.1 Preparation of Historical Resource Overviews are not required at each Outline Plan and/or Subdivision stage.

2.7.4 Biophysical Considerations

Westhoff Engineering Resources Ltd. completed a Wetland Impact Assessment (WIA) and an Environmental Impact Assessment (EIA) in support of proposed development within the CS area.

The EIA was prepared in accordance with policies referencing same included within the County's Municipal Development Plan and the preliminary Guidelines for Review of Biophysical Assessments.

The WIA concluded the CS area contains ten (10) existing wetlands with classifications ranging from Class I to Class V according to the Stewart and Kantrud (1971) Wetland Classification System with an additional two (2) wetlands which reflect recent mapping completed by Ducks Unlimited (as per #11 & #12). As part of the WIA preparation process, the habitat within each of these existing wetland complexes was specifically studied. The conclusions of the report recommend that seven wetlands may be removed from the site - with three remaining (to be integrated within the development's proposed stormwater management system). Approximately 10.38 ha of existing wetlands will be lost as a result of the proposed development (47 % of the total wetlands within the CS area). This loss is considered to be residual in nature, is relatively high in magnitude, is not reversible and will persist over the long term. However, as per the Provincial Wetland Restoration / Compensation Guide (February, 2007), a program of wetland compensation will be implemented at the detailed subdivision phase to mitigate this residual effect. Provided the proposed compensation meets Alberta Environment (AENV) requirements under the *Water Act*, this loss of wetlands is not considered a significant residual impact of the proposed High Plains Industrial Park.

The EIA determined the CS area contains a 112 vegetation species – one (1) of which is Yellow Cress, *Rorippa curvipes*, which is considered by the Province to be a rare plant. The study recommends the topsoil within the area including the Yellow Cress be stripped and placed into a wet meadow zone of any constructed wetlands to be built within the site.

The EIA concluded that the existing deciduous vegetation that connects wetland areas #4 & #5 should be retained so as to preserve existing habitat areas for wildlife species (raptors and small mammals) encountered within the site.

Overall, the EIA concluded the CS area is not an environmentally significant site. Significant residual impacts on vegetation and wildlife are not expected as a result of anticipated site development.

The EIA recommends all major development preparation activities occur outside the mid March to end July timeframe so as to avoid the nesting season of existing wildlife species found within the site. If avoidance of this timeframe is not possible, a professional biologist should be retained during construction activities to monitor and mitigate any potential for wildlife disturbances.

It is noted that the EIA/WIA prepared in support of this Conceptual Scheme has not examined existing conditions within SW 12-26-29-W4M and SW 1-26-29-W4M (given that these lands are not owned by the Conceptual Scheme proponents). However, similar environmental conditions are expected within these specific titled areas.

Policies:

Policy 2.7.4.1 A Biophysical Assessment and/or Wetland Impact Assessment may be prepared in support of specific Outline Plans affecting these titled areas if deemed necessary by the County and/or Alberta Environment.

2.7.5 Geotechnical Considerations

In November, 2008, Sabatini Earth Technologies Inc. prepared a preliminary Geotechnical Investigation within the Plan area. A total of 31 boreholes were advanced within the site to determine the subsurface soil and groundwater conditions.

The soil profiles were variable with layers of clay, silt, sand and clay till overlying bedrock. Water levels within standpipes were recorded 1 week after drilling and ranged from 1.4 m – 7.4 m below ground surface.

The investigation concluded that, based on field and office assessments, existing site conditions within the CS area are not expected to present significant development restrictions.

2.7.6 Environmental Site Assessments

In February 2008, Biophyllia Inc. prepared a Phase 1 Environmental Site Assessment (ESA) affecting all of the N ½ Section 01-26-29-4. The study recommended that any required development setbacks should be incorporated in the future subdivision design to respect the existing sour gas well located in the southeast corner of the NW 1-26-29-4. Previous assessment of this well site in 2000 noted a leaking pipeline may have created environmental issues that will require further mitigation at the development stage. The study also suggested that contaminations may have migrated onto the site from the nearby Shell/Turbo refinery site which could require mitigation. A groundwater

well associated with the abandoned farm house will require reclamation at the subdivision stage.

In April 2006, Environmental Diagnostics prepared a Phase 1 Environmental Site Assessment (ESA) affecting all of the N ½ Section 12-26-29-4. The study concluded that the site does not contain any significant environmental impairment but did recommend various debris piles be removed from the site and properly disposed of.

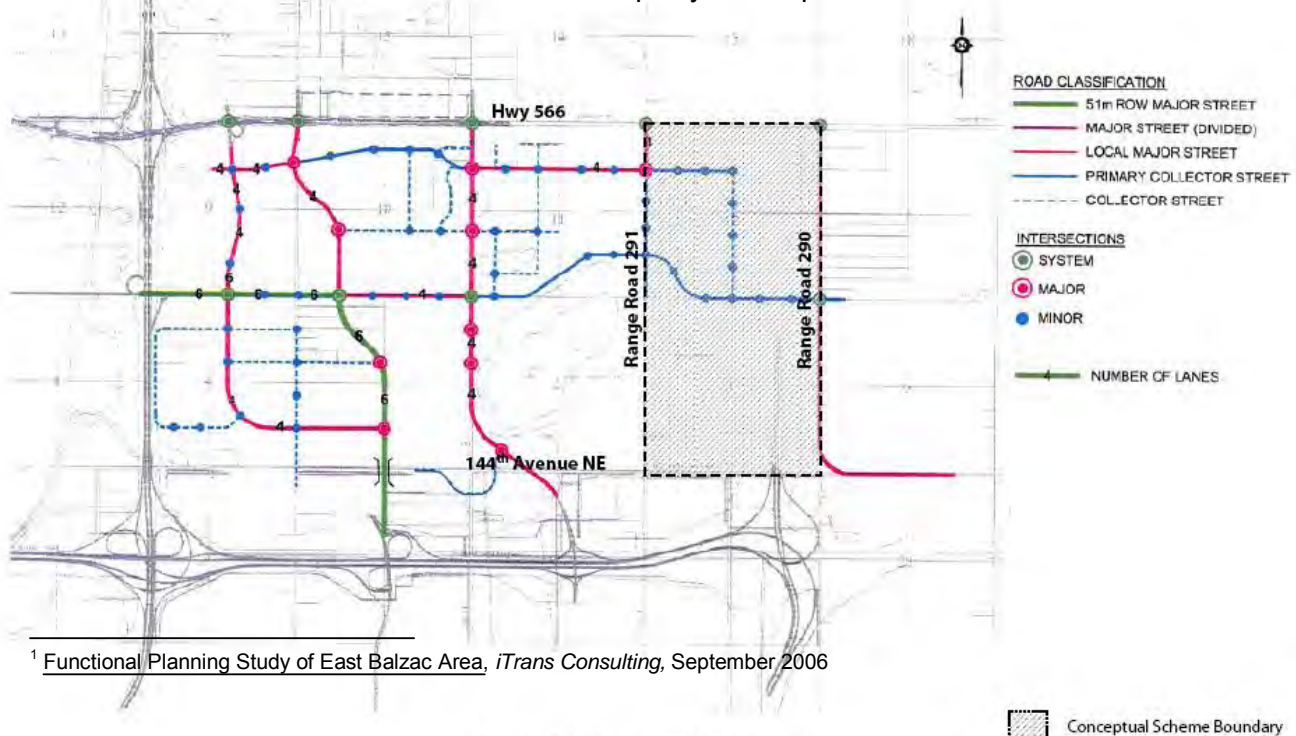
In February 2008, Environmental Diagnostics prepared a Phase 1 Environmental Site Assessment (ESA) affecting the SE Section 12-26-29-4. The study did not conclude any significant environmental impairment within the site.

As of the date of this Plan's adoption, Environmental Site Assessments have not been completed affecting SW 12-26-29-4 and S ½ Sec. 01-26-29-4, as these lands are not owned by the Conceptual Scheme's proponents. However, Environmental Site Assessments will be completed within these lands prior to their redevelopment as part of the Outline Plan preparation process.

2.7.7 Existing Transportation Considerations

The subject lands can be accessed from Range Roads 290 & 291. Range Road 291 is paved from Highway 566 south to 144th Avenue NE and Range Road 290 (84th Street NE) is a municipal grid road with a gravel surface between Hwy 566 and 144th Avenue NE.

In September 2006, Rocky View County prepared a Functional Planning Study to comprehensively account for ongoing developments occurring within the East Balzac area.¹ The conclusions of this study have established a functional design for major public roadways within the BEASP area. The configuration of future subdivision within the Plan area will conform to the Municipality's anticipated future road network.



¹ Functional Planning Study of East Balzac Area, iTrans Consulting, September 2006

Figure ES1– Future Road Network

3.0 THE HIGH PLAINS INDUSTRIAL PARK

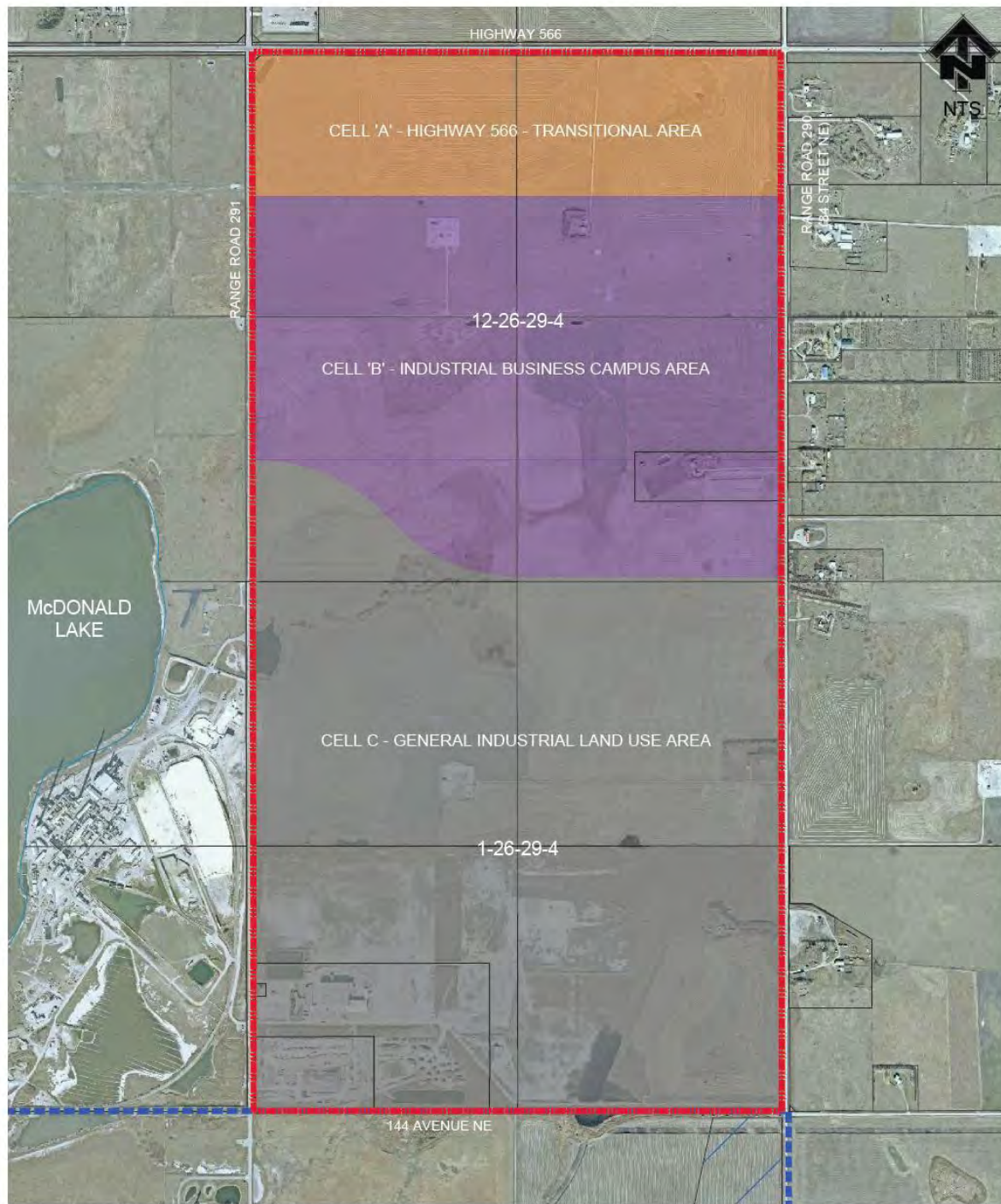
3.1 Development Vision

Looking ahead 15 to 20 years, the High Plains Industrial Park has been built-out through implementation of a logical sequence of subdivision and development phases which have resulted in a master-planned business commercial/industrial development that accommodates a variety of strategic economic development opportunities in keeping with the Rocky View County Growth Management Strategy.

The High Plains Industrial Park includes:

- A comprehensive mix of business commercial & industrial land uses of varying development intensity situated across the site.
- High quality, 'campus -style' business park uses situated within the northern portion of the CS area.
- More intensive and 'heavier' industrial uses within the central and southern portions of the CS area. Industrial developments are anticipated to increase in land use intensity with proximity to the Nexen Sour Gas Plant.
- A comprehensively-planned transportation distribution system with a defined hierarchy of arterial, collector and internal subdivision roads integrated with the surrounding regional road network.
- Implementation of municipal potable water and sanitary sewer services with an overall design that has considered potential enhancements to the County's regional utility systems including such elements as a treated water reservoir and/or peak clipping wastewater surge tanks.
- Implementation of an innovative stormwater management strategy including a complex of integrated engineered facilities specifically designed to capture, treat and retain 100 % of the surface drainage generated within the CS area. Treated stormwater will be used to supplement industrial processing activities and provide a supply for local irrigation.
- A regional pathway extending across the northern boundary of the site (within an enhanced landscaped area buffer paralleling Highway 566) to provide a regional pedestrian linkage with connection to CrossIron Mills and developments in West Balzac.
- Several publicly-accessible open space amenities (to be integrated with stormwater management facilities) situated within the northern portion of the CS area – connected by an internal pathway system to promote opportunity for pedestrian mobility.
- Implementation of design guidelines that coordinate the Industrial Park's visual aesthetic with surrounding land uses through implementation of site specific architectural controls.

The phased development of business/industrial development within the Plan area is anticipated over a 15 - 20 year horizon and is expected to be implemented in response to the availability of infrastructure and market demand.



Legend
---- Conceptual Scheme Area
---- City of Calgary/ County Boundary

FIGURE 5
GENERAL LAND USE CONCEPT

July 2010

3.2 Land Use Concept

In accordance with the provisions of the Balzac East ASP Special Development Area #5, the location of anticipated land use within the subject lands is illustrated by **Figure 5 – General Land Use Concept**. The site is divided notionally into three (3) development cells generally described as follows:

- **Cell ‘A’ – Highway 566 Transitional Area** will accommodate business uses that are compatible with adjacent country residential developments and the Rocky View ‘Municipal Campus’ (to be situated on the ¼ section directly to the northwest). Implementation of development with enhanced architectural design standards will establish and maintain an attractive and cohesive built form within this area. Detailed architectural controls will ensure all developments within this area consider design elements such as building heights & massing, façade treatments, landscaping, lighting, signage and placement of parking, loading & outside storage areas. Industrial uses with the potential to negatively impact surrounding parcels with considerations such as noise, odours and emissions will not be permitted in this area. A linear public open space will be provided within Cell ‘A’ directly south of the Highway 566 right-of-way by dedication of a municipal reserve (MR) parcel enhanced with appropriate landscaping treatments and a regional pathway. An internal pedestrian system within Cell ‘A’ may be developed to facilitate looped connection between the regional pathway along Highway 566 and stormwater management facilities anticipated in this area. An attractive landscaped business park entry feature will be provided at the northeast corner of Cell ‘A’.
- **Cell ‘B’ – Industrial Business Campus Area** will accommodate a variety of business uses including a mix of regional commercial, office & light industrial developments –comprehensively planned in a campus-style setting. The form of development within Cell ‘B’ will blend with land use situated in Cell ‘A’, however, the overall character of building in this area will display a gradual transition away from the enhanced ‘visual’ aesthetic in Cell ‘A’ towards business industrial developments of a more ‘general’ nature anticipated in Cell ‘C’. Business uses that create potential for limited off site intrusions (such as noise and visual impacts) may be permitted in Cell ‘B’; however, appropriate mitigation measures must be implemented to address these potential impacts. A linear municipal reserve (MR) parcel will be extended along the Range Road 290 & 291 frontages and enhanced with landscaping elements to provide a visual buffer between industrial developments and the surrounding lands.
- **Cell ‘C’ – General Industrial Land Use Area** will accommodate a wide range of ‘general’ industrial developments that typically involve larger formats with reduced servicing requirements. The type of uses anticipated in this area may include industrial developments with extensive outside storage requirements which require a variety of lot sizes and parcel configurations. Additionally, Cell ‘C’ will accommodate a range of ‘heavier’ industrial processing activities with potential for impacts to adjacent properties through considerations such as visual & noise obtrusions and particle emissions. Any such development will be required to appropriately mitigate off-site impacts in accordance with all municipal, provincial and/or federal regulation governing same. Business types

which can be benefited by access to rail may be accommodated through a spur extension from the existing CP rail line situated southwest of the Nexen Gas Plant.

Policies:

- Policy 3.2.1** *Land uses within the Conceptual Scheme Area shall include a full range of business uses involving commercial and industrial developments which are consistent with the policies of the Balzac East Area Structure Plan.*
- Policy 3.2.2** *Land uses within Cell 'A' will include business developments with an 'enhanced' visual aesthetic. Development standards and architectural guidelines shall be implemented within Cell 'A' to ensure the Highway 566 and Range Roads 290/291 business frontages present an attractive visual interface in accordance with the provisions of the Balzac East Area Structure Plan's Special Development Area #5.*
- Policy 3.2.3** *Land uses within Cell 'B' will include a combination of regional commercial, office & light industrial developments – comprehensively planned in a campus-style setting. The form of development within Cell 'B' will blend with the character of land uses situated in Cell 'A', however, business frontages in this Cell will include a hybrid of the enhanced 'visual' aesthetic prominent in Cell 'A' with business developments of a more 'general' nature anticipated in Cell 'C'.*
- Policy 3.2.4** *Land uses within Cell 'C' will include 'general' industrial developments with a variety of lot sizes and parcel configurations. Businesses are expected to include large-format operations with extensive outside storage requirements. Heavier industrial processing activities may create potential for impacts to adjacent properties which will be required to demonstrate methods to successfully mitigate and minimize such impacts in accordance with relevant Federal, Provincial and municipal standards, to the satisfaction of the County.*
- Policy 3.2.5** *The area available for development in the east 1/2 of Section 1 and 12 will be determined pending the location of the Stoney Freeway Connection.*

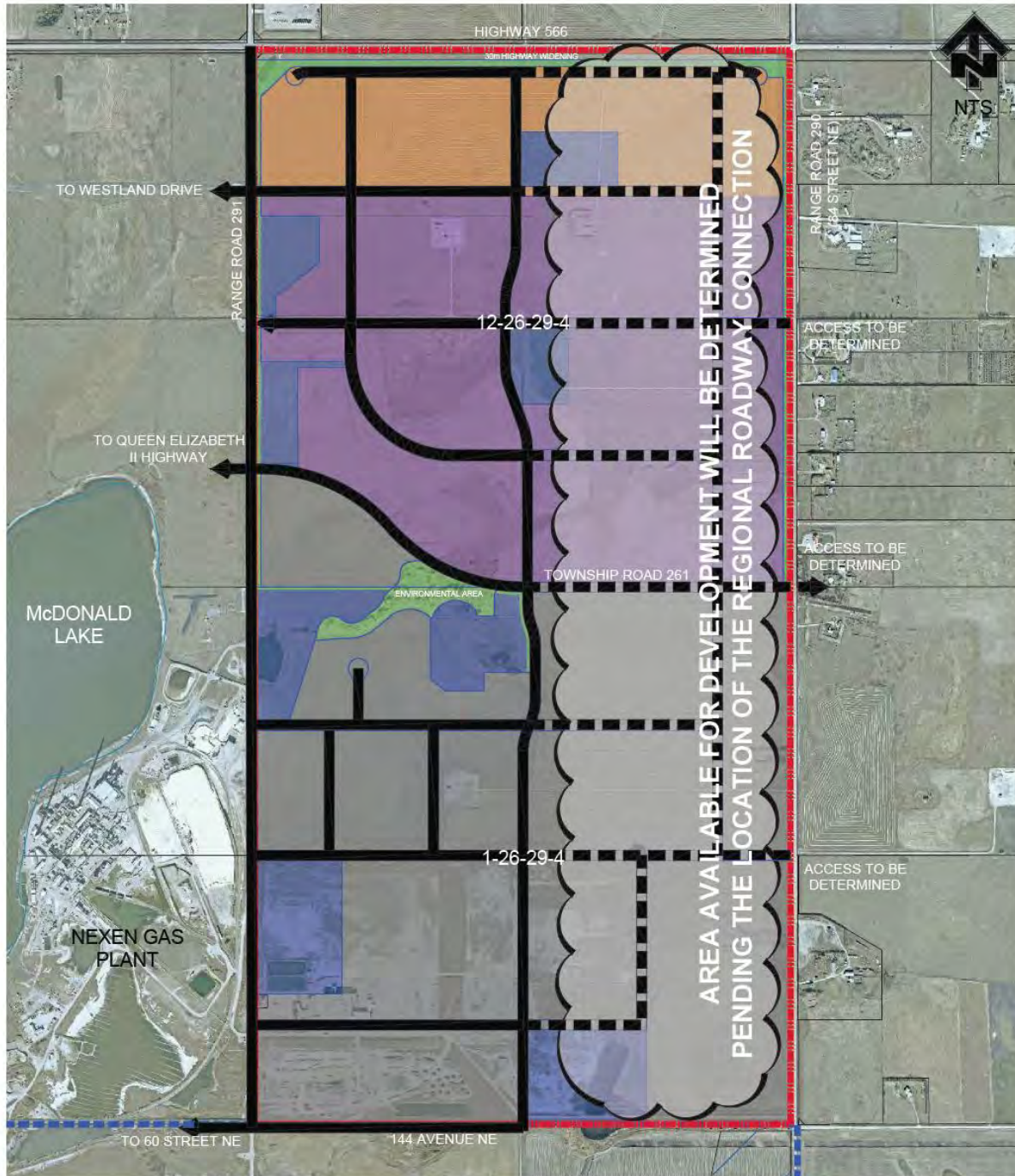
3.3 Subdivision & Development Concept

The High Plains Business Park's anticipated subdivision configuration will implement the general land uses anticipated by **Figure 5 – General Land Use Concept** with more comprehensively-planned 'campus style' commercial/industrial development within the northern portion of the CS area with a transition to more 'general' industrial development in the central and southern portions of the site (anticipated to include a wide range of parcel sizes and lot configurations).

The configuration of subdivision anticipated within the High Plains Industrial Park is generally illustrated by **Figure 6 – Development Concept**. Given the relatively large

area included within the Conceptual Scheme boundary, the specific illustration of anticipated lot layouts has not been defined at this scale. Instead, the detailed parcel configurations will be established through subsequent phasing of development through the preparation of Outline Plans.

The orientation of development within the CS area will be determined, in large part, by the alignment of roadways and the placement of stormwater management facilities. The Development Concept illustrates a series of 'blocks' which will be divided into smaller parcels tailored to suit specific business development users (as dictated by the County's approval of individual Outline Plans and accompanying land use and subdivision applications).



Legend

- Conceptual Scheme Area
- City of Calgary/ County Boundary
- Proposed Road Layout
- Potential Road Layout (subject to alignment of Hwy 566 / Stoney Freeway Regional Road Connection)

- Highway 566 Transition Area
- Industrial Business Campus Area
- General Industrial Area

- Stormwater Retention Areas
- Linear Open Space Buffer (including Regional Pathway)

FIGURE 6
DEVELOPMENT CONCEPT

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Phasing of development is generally anticipated to occur on a quarter by quarter section basis subject to the availability of appropriate transportation and utility infrastructure, as further described in Section 10.0 of this Conceptual Scheme.

Alberta Transportation has indicated a regional road connection is required between Highway 566 and the Stoney Trail NE. The proposed alignment of this regional road will impact the overall configuration of development within the eastern portion of the CS area. Until such time the alignment of this road is determined, the development configuration within the eastern half of the Plan area is to be considered as a 'shadow plan' for 'contextual purposes' only.

The actual development configuration within the east portion of the Conceptual Scheme (including specific alignments of internal roads and potential linkages with adjacent lands east of Range Road 290) shall be confirmed through the preparation of Outline Plans as provided for in accordance with Section 10.0 of this Plan.

Consideration of Outline Plans affecting the eastern portions of the Conceptual Scheme may only be considered at such time Alberta Transportation confirms the alignment of the Hwy 566 / Stoney Trail NE connection is confirmed as illustrated on Figures 6, 7 and 13 of this Plan.

Policies:

Policy 3.3.1 Until such time Alberta Transportation confirms the alignment of the Hwy 566 / Stoney Trail regional road connection, the anticipated transportation configuration within the eastern portion of the Plan area is considered a 'shadow plan' for contextual planning purposes only.

Policy 3.3.2 The actual development configuration within the east portion of the Conceptual Scheme (including specific alignments of internal roads and potential linkages with adjacent lands east of Range Road 290) shall be confirmed through the preparation of Outline Plans as provided for in accordance with Section 10.0 of this Plan, to the satisfaction of the County and Alberta Transportation.

4.0 TRANSPORTATION INFRASTRUCTURE

4.1 The High Plains Industrial Park Regional Transportation Network

The anticipated roadway system for the High Plains Industrial Park is illustrated in **Figure 7 –Transportation Plan**. The CS area will be accessed by the surrounding ‘East Balzac Regional Transportation System’ including Highway 566, Range Road 291, Township Road 261, Township Road 260 (144th Avenue NE) and a regional road connection between Highway 566 and the Stoney Trail NE (anticipated to include the Range Road 290 ROW). Regional traffic will be conveyed to and from the CS area from various strategic access points from these surrounding roadways.

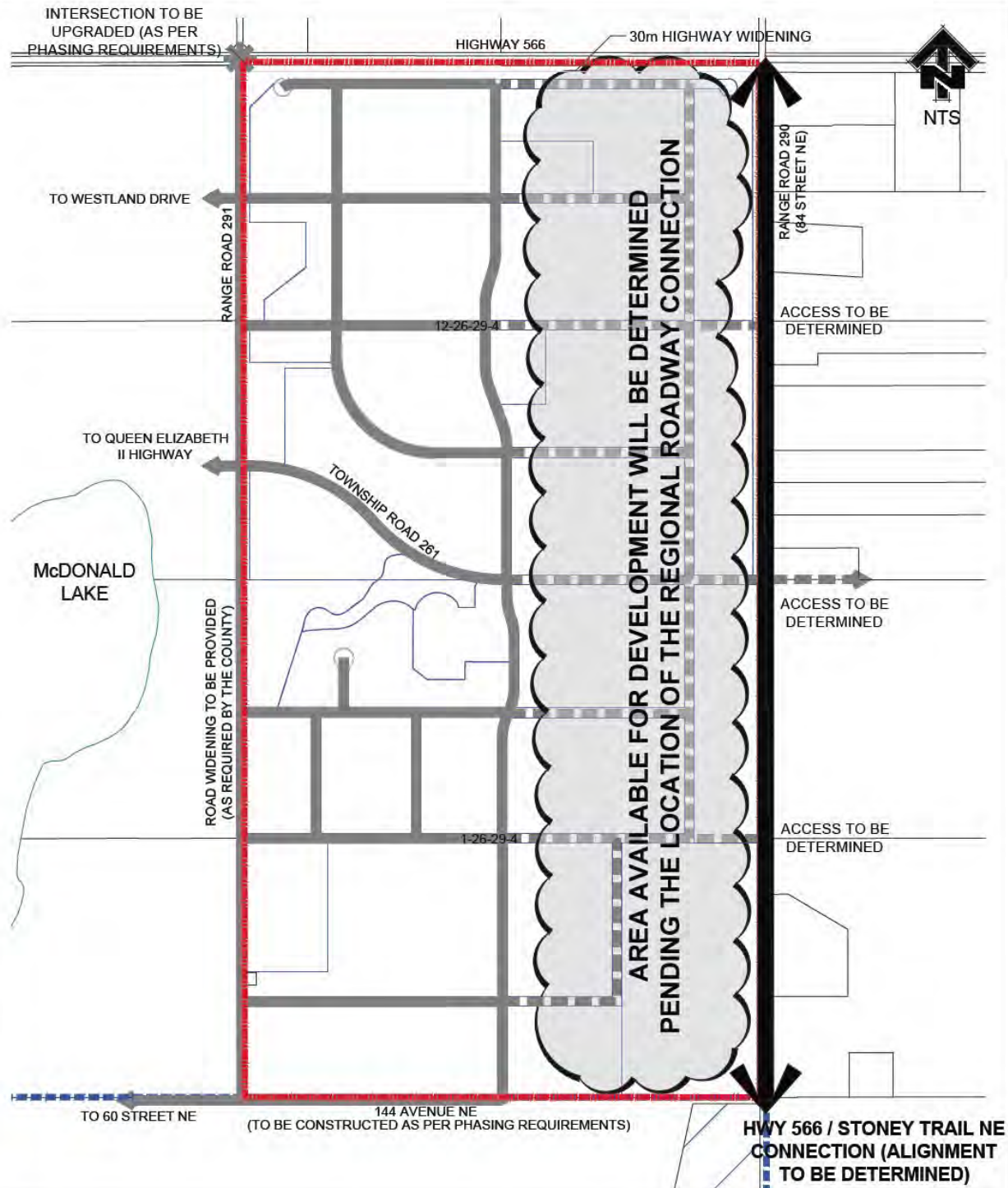
Highway 566 and Township Road 261 will provide the primary east-west connections in support of development within the CS area. In accordance with the expectations of the Balzac East ASP, up to 30 m of road dedication will be provided along the northern boundary of the CS area to facilitate Alberta Transportation’s anticipated improvements to Highway 566 (including ROW widening and additional traffic lanes). An appropriate treatment will be established at Highway 566 / Range Road 291 intersection as required by the timing of development within the CS area and adjacent lands. Similarly, Township Road 261 will be extended through the CS area to complete a continuous roadway connection from Range Road 290 (84th Street NE) with the Queen Elizabeth II highway.

Township Road 261 will be established via dedication of road right-of-way as illustrated on **Figure 7**. Township Road 261 is anticipated to extend to the eastern portion of the CS area and may be extended onto lands situated east of Range Road 290 (subject to determination of the Highway 566 / Stoney Trail NE connection).

Later development phasing within the south portion of the CS area will warrant construction of Township Road 260 (144th Avenue NE) as an additional east – west connection. As this road ROW is situated within the City of Calgary, intermunicipal collaboration will be required to implement this portion of the transportation infrastructure system – which will also provide access to lands within the City planned for future industrial land uses (as per City of Calgary Northeast Residual Policy Area).

Range Road 291 will provide the primary north-south road connection for the CS area. This municipal road currently includes a 20 m right-of-way (ROW) with a paved finished surface. Dedication of additional road right-of-way will be required to increase the Range Road 291 ROW to 36 m - with up to 8 m of widening to be provided along the entire western boundary of the Plan area. Widening of this road’s existing finished surface will be completed by the developer as phasing of development within the Plan area warrants.

As already discussed, Alberta Transportation has indicated a new regional road connection between Stoney Trail NE and Highway 566 will be provided along the eastern boundary of the CS area. Although specific details in support of this roadway have not been determined, its’ alignment is expected to include the Range Road 290 ROW. Range Road 290 (84th Street NE) is a municipal grid road with a 20 m ROW and a gravel finished surface. Pending the determination of this regional road connection, no direct road connections from the CS area onto Range Road 290 (as it presently exists) are anticipated.



Legend

- Conceptual Scheme Area
- City of Calgary/ County Boundary
- Proposed Road Layout
- Potential Road Layout (subject to alignment of Hwy 566 / Stoney Freeway Regional Road Connection)

FIGURE 7

TRANSPORTATION PLAN

July 2010

A variety of internal collector roads and internal streets will be established to convey local traffic from the CS area onto the adjacent collector roads. Direct access from individual parcels within the CS area onto the surrounding roadways will be carefully controlled to ensure appropriate access management considerations are respected.

4.2 Traffic Impact Assessment (TIA)

In 2010, a Traffic Impact Assessment (TIA) was prepared by Bunt & Associates Engineering in support of this Conceptual Scheme. The TIA assessed the future transportation network, the cumulative impacts on the road network, and any possible mitigation measures associated with the expected traffic generated by the proposed development within the CS area (as well as other ambient background traffic growth). The TIA acknowledged the 2006 Functional Planning Study (iTrans) prepared for the East Balzac Area.

For traffic analysis purposes, the TIA assumes the High Plains Industrial Park, at full build out, will include approximately 12,000,000 ft² (Gross Floor Area) of developed building area. Correspondingly, the site is expected to generate approximately 12,000 total vehicle trips (external & internal) during the weekday PM peak hour period.

To accommodate this anticipated traffic generation, a staged program of supporting roadway improvements will be required to upgrade existing and construct new regional transportation infrastructure summarized as follows:

- Highway 566 will require widening to six (6) lanes.
- Township Road 261 will be constructed (with four (4) lanes).
- 144th Avenue will require four (4) lanes.
- Appropriate improvements at the Highway 566 / Range Road 291 intersection and the 144th Avenue NE / Range Road 291 intersection.
- Full build out of the CS area will require a connection between Highway 566 and the Stoney Trail NE (as described in 4.2.1). **Pending construction of this regional connection, it is possible to build out a portion of the CS area and surrounding lands with access provided via Range Road 291 at Highway 566.** The TIA concludes approximately half of the CS area could be supported without the inclusion of the Highway 566 / Stoney Trail NE regional transportation connection.

The TIA assumptions note that traffic conditions identified for the long term scenario are not due solely to the proposed High Plains Industrial Park development. Rather, the future traffic forecast volumes include a combination of both background and site-generated traffic volumes. The absence of the Highway 566 / Stoney Trail NE regional connection will impact traffic routing through the area, and as such, ongoing development within the CS area and the balance of East Balzac ASP area will require carefully coordination by the County, local developers and multiple levels of municipal and provincial jurisdictions.

Given the scale of the proposed development, and the relatively lengthy build out horizon contemplated by this Plan, the implementation of specific transportation

infrastructure improvements shall be determined through a staged program to be guided by multiple updates to the Traffic Impact Assessment prepared by the developer at each subsequent development phase.

4.2.1 Stoney Trail NE / Highway 566 Regional Roadway Connection

The 2006 Functional Planning Study (iTrans) indicated a connection between the Stoney Trail NE and Highway 566 should be provided to alleviate mounting traffic congestion affecting regional roadways within the East Balzac area. In the absence of this regional road connection, the intersection at Highway 566 and Range Road 291 is expected to exceed its' long term capacity. As such, determination of this regional roadway connection is a primary transportation consideration which must be resolved within the development horizon of this Plan.

Although the County has expressed a preference for Alberta Transportation to situate the Hwy 566 / Stoney Trail NE connection within the Range Road 290 (84th Street NE) statutory road allowance, the Province has not yet confirmed how this regional transportation linkage will be accommodated. Alberta Transportation has suggested that potential alignments may be situated east or west of the Range Road 290 road allowance to accommodate this regional road connection.

The Province has indicated that a 'technical assessment' must be completed to establish expectations concerning how this regional roadway connection will be provided (e.g. specific roadway alignment and corresponding rights-of-way, access management, etc.). Until this technical assessment is completed, Alberta Transportation has indicated that the phasing of development within the eastern portion of the CS area may not proceed, and concurrently, all anticipated traffic movements in support of development within the western portion of the CS area shall be oriented towards Highway 566 via Range Road 291.

As illustrated on Figures 6 & 7, the CS recognizes Alberta Transportation's expectations in this regard and specifies that the configuration of roadways within the eastern portion of the CS area is considered 'conceptual' only and illustrated for contextual planning purposes. **Accordingly, the phasing of development within the eastern portion of the CS area shall not proceed until Alberta Transportation confirms the alignment of the Hwy 566 / Stoney Trail NE regional road connection.**

As further described in Section 10, the phasing of development within the western portion of the CS area may proceed subject to the operational capacities of the Highway 566 / Range Road 291 intersection.

Policies:

Policy 4.2.1 *The anticipated configuration of the transportation network within the Plan area is generally illustrated on ~~be in accordance with~~ Figure 7 – Transportation Plan, to the satisfaction of the County and Alberta Transportation.*

Policy 4.2.2 *Direct access from individual lots onto Highway 566 shall not be permitted.*

- Policy 4.2.3** *Dedication of up to 30 m of right-of-way shall be provided for highway widening purposes adjacent to Highway 566, to the satisfaction of Alberta Transportation.*
- Policy 4.2.4** *Dedication of additional ROW may be required from lands adjoining Range Roads 291 and Highway 566 to accommodate intersection improvements, to the satisfaction of Alberta Transportation and the County.*
- Policy 4.2.5** *Dedication of up to 8 m of right-of-way for road widening purposes may be required adjacent to Range Roads 291, to the satisfaction of the County.*
- Policy 4.2.6** *Direct access from individual lots in the CS area onto Range Roads 291 may be restricted based on the recommendations of a Traffic Impact Assessment, to the satisfaction of the County and Alberta Transportation.*
- Policy 4.2.7** *Roadway improvements along Twp. Rd. 260 (144th Avenue NE) shall be implemented at the appropriate phase of development to the satisfaction of the County and the City of Calgary.*
- Policy 4.2.8** *At each phase of subdivision, the Developer shall provide and maintain appropriate emergency vehicle access in accordance with County standards.*
- Policy 4.2.9** *An update to the Traffic Impact Assessment (TIA) shall be prepared to establish expectations for required local and regional transportation infrastructure improvements in support of Outline Plan submission and prepared at each subdivision phase, to the satisfaction of the County and Alberta Transportation.*
- Policy 4.2.10** *Until such time Alberta Transportation confirms the alignment of the Hwy 566 / Stoney Trail regional road connection, the anticipated transportation configuration within the eastern portion of the Plan area is considered a 'shadow plan' for contextual planning purposes only.*
- Policy 4.2.11** *The actual development configuration within the east portion of the Conceptual Scheme (including specific alignments of internal roads and potential linkages with adjacent lands east of Range Road 290) shall be confirmed through the preparation of Outline Plans as provided for in accordance with Section 10.0 of this Plan, to the satisfaction of the County and Alberta Transportation.*
- Policy 4.2.12** *Subject to Policy 4.2.10, the phasing of development within the western portion of the CS area will be subject to the operational capacities of the Highway 566 / Range Road 291 intersection.*

4.3 Local Roadway Considerations

The roadway system proposed within the High Plains Industrial Park is illustrated on **Figure 7 – Transportation Plan**. Local roadways within the CS area will be designed to convey traffic from the Industrial Park onto the surrounding regional roadway network.

ROW's within the CS area will predominantly include 'rural' cross sections with low impact development mechanisms (i.e. bio-swales, roadside ditching, grassed swales, etc.). Township Road 261 will include an 'urban' cross section with concrete curbs & gutters. Roadways within Cell A – Highway Interface Area may also be required to include urban cross sections to maintain consistency with development standards within the balance of the East Balzac area. The County may consider implementing rural cross sections within Cell A subject to the conclusions of the 'Outline Plan specific' stormwater management plan and overall aesthetic design considerations.

As illustrated on **Figure 6 – Development Concept**, the Highway Interface Area Industrial (Cell A) is proposed to include business lots that face onto a parallel frontage road (abutting a 20 m linear MR adjacent to Highway 566). The Conceptual Scheme acknowledges the typical development configuration within the East Balzac area presents the rear of lots onto the highway frontage. The Conceptual Scheme proponents believe that added value can be established for the parcels abutting the highway by facing them towards Highway 566 to maximize their business exposure without negatively affecting the visual appearance of the development along this corridor. The proposed cross section will increase the typical building setback from Highway 566 from 50 m to 63.



The specific configuration of road ROW's and cross sections for all roadways will be established at the Outline Plan stage to the satisfaction of the County.

All roads within the CS area will be constructed by the developer.

Policies:

- Policy 4.3.1** *All internal roads and associated intersections with adjacent regional roadways shall be constructed by the developer to the satisfaction of the County, Alberta Transportation and/or the City of Calgary (where relevant jurisdiction applies).*
- Policy 4.3.2** *Except for Township Road 261, all roadways within the CS area will include rural cross sections that include low impact development (LID) implementation mechanisms.*
- Policy 4.3.3** *Township Road 261 will include an urban cross section with concrete curbs and gutters.*
- Policy 4.3.4** *Roadways within Cell A – Highway Interface Area may also be required to include urban cross sections to maintain consistency with adjacent developments.*
- Policy 4.3.5** *Specific road standards will be established at each phase of development through the Outline Plan preparation process, to the satisfaction of the County.*

- Policy 4.3.6** *Emergency (or secondary) access shall be provided until such time as a direct link occurs with adjacent road networks to the satisfaction of the County.*
- Policy 4.3.7** *Determination of exact locations for road approaches from individual lot may be deferred to the Development Permit stage.*
- Policy 4.3.8** *Payment of fees as per the County's applicable Transportation Off-Site Levy Bylaw shall occur at the subdivision stage.*
- Policy 4.3.9** *The County's Infrastructure Recovery Policy shall apply to applicable road improvements completed within the CS area.*
- Policy 4.3.10** *A Traffic Impact Assessment (TIA) shall be prepared in support of each Outline Plan submission and at each subdivision stage to establish expectations for required local transportation infrastructure improvements.*

4.4 Rail Servicing (Canadian Pacific Railway)

Canadian Pacific Railway (CPR) operates a branch rail line which connects with the main line situated just south of the Hamlet of Balzac. This branch line is located within a right-of-way alignment just north of Township Road 260 (144th Avenue NE) and was constructed to service a privately-owned rail spur line extending onto the Nexen Gas Plant site.

Given the anticipated allocation of 'heavier' business land uses within the southern portion of the Plan, implementation of rail-serviced industrial developments may be accommodated within the southern portion of the CS area.

As part of the Outline Plan process affecting lands within the south portion of the CS area, the developer shall consult with Canadian Pacific (CP) to determine specific requirements for potential rail access.

5.0 UTILITY SERVICING INFRASTRUCTURE

5.1 Potable Water Supply

The High Plains Industrial Park will be serviced with potable water supplied from Rocky View County's regional water system as per the MPE Engineering Ltd water model analysis (which has determined the basic network criteria required in support of proposed development within the CS area).

In 2010, Focus Engineering completed a Servicing Design Brief which includes a schematic illustrating how the High Plains Industrial Park can be serviced with potable water – as illustrated by **Figure 8 – Potable Water Servicing Plan**. The completed analysis considers criteria established by Rocky View County's Servicing Standards for Subdivision & Road Construction and Alberta Environment Guidelines.

At build out, the High Plains industrial park will require a system of distribution mains as generally illustrated on **Figure 8**. Generally, the minimum size of internal distribution mains will include 250 mm diameter with looped connections provided by 400 mm and 600 mm mains as required. The sizing of these mains shall accommodate minimum pressures required to provide mandatory fire suppression levels. Peak hourly demands are estimated at minimum 275.8 kPa with minimum pressure at maximum day (& fire flow demand) of 151.7 kPa.

A connection with the existing 400 mm PVC distribution main along RR 291 (previously constructed to service the Meat Packaging Facility in the southwest portion of the CS area) is expected to service the initial development phase(s) within the CS area. The internal distribution system will consist of either 250 mm or 300 mm distribution mains to be fed by the larger 400 mm PVC transmission main.

The County's water model completed by MPE Engineering Ltd. indicates a requirement for a looped 600 mm transmission main within the CS area to connect a regional transmission main directly south of Highway 566 with a proposed distribution main situated west of the Plan area (via Township Road 261).

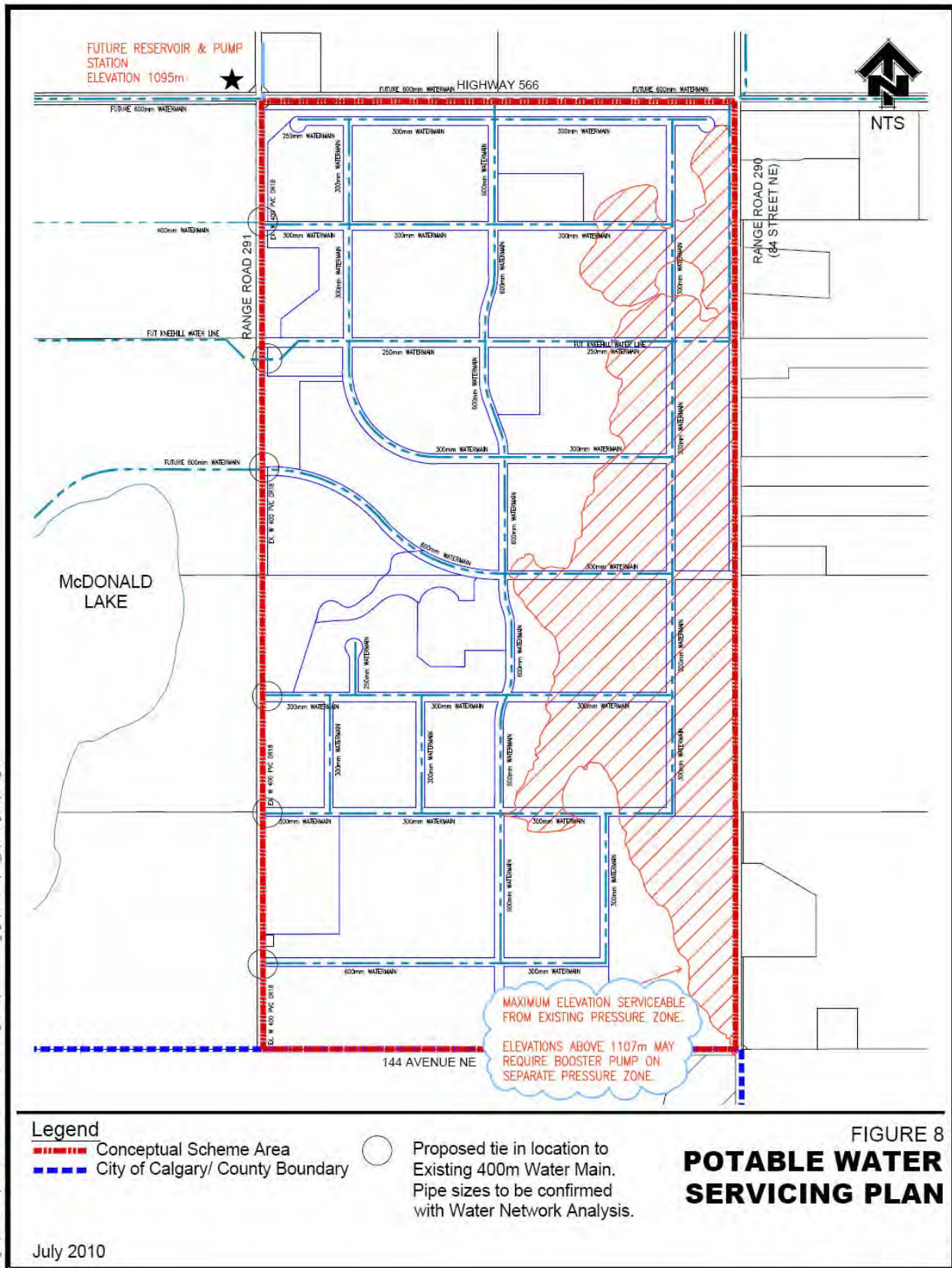
Resulting from the increase in elevation across the site from the west to east, a separate pressure boundary is anticipated over the 1,107 m contour interval. As such, a booster (or booster with reservoir) may be required to service the eastern portion of the CS area. Further analysis of the water system will be required to identify the timing and location of this infrastructure in support of each associated development phase.

The actual pipe sizes and alignments will be subject to detailed analysis at such time each phase of development is considered. All water distribution infrastructure shall be constructed by the developer in accordance with the County's and/or the Province's development requirements. Where such infrastructure is demonstrated to have benefit for potable water service affecting lands outside the CS area, the developer shall be entitled to Cost Recoveries in accordance with County procedures governing same.

Notwithstanding the expectation for larger format / limited service commercial/industrial development within the CS area, water conservation measures will be implemented

within the business park primarily through the mandatory restriction of potable water usage for irrigation purposes.

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Policies:

- Policy 5.1.1** *The High Plains Industrial Park shall be serviced with potable water by the East Rocky View Water System.*
- Policy 5.1.2** *The configuration of potable water infrastructure required to service the CS area is illustrated on Figure 8 – Potable Water Servicing Plan.*
- Policy 5.1.3** *All potable water infrastructure within the CS area shall be constructed by the developer to the satisfaction of the County and Alberta Environment.*
- Policy 5.1.4** *The application of the County's Infrastructure Recovery Policy shall apply to applicable potable water infrastructure improvements.*
- Policy 5.1.5** *The potable water distribution system shall accommodate minimum design requirements necessary to provide appropriate fire suppression within the CS area.*
- Policy 5.1.6** *Payment of fees as per the County's applicable infrastructure contribution requirements shall occur at the subdivision stage.*
- Policy 5.1.7** *Water Conservation will be implemented within the CS area through mandatory prohibition of potable water usage for irrigation purposes.*
- Policy 5.1.8** *Water Conservation will be augmented through implementation of developer-imposed architectural controls that will specify utilization of low-flow plumbing fixtures (where applicable).*

5.2 Waste Water Management

The CS area will be serviced with a 'gravity-fed' sanitary sewer system designed to convey all wastewater generated within the site to two (2) local sanitary sewer lift stations – which will subsequently convey effluent into the County's East Rocky View Waste Water Transmission Line (ERVWWTL).

All development within the CS area will connect with the County's existing 600 mm sanitary force main (ERVWWTL) that extends through the subject lands within the N ½ Section 12. The 600 mm force main is pressurized by a sanitary lift station situated along Range Road 294 immediately east of the CrossIron Mills Regional Mall and was designed with capacity to service the entire East Balzac area.

The 2010 Focus Engineering Servicing Design Brief proposes a schematic for a waste water collection system to service the CS area at full build out – as illustrated on **Figure 9 – Waste Water Servicing Plan**. The analysis considers criteria established by Rocky View County's Servicing Standards for Subdivision & Road Construction and Alberta Environment Guidelines.

The minimum size of the internal sanitary conveyance system will include 200 mm PVC mains as required to accommodate an expected flow of 0.35 L/s/ha. The alignments of the mains shall be designed to direct effluent to two (2) lift stations situated within naturally-occurring low points within the site – which will effectively divide the CS area

into two sanitary basins (north and south). Each basin shall be serviced by a local lift station and separate force mains installed to convey effluent to connection points with the County's existing 600 mm force main (ERVWWTL).

A sanitary servicing study will be completed at subsequent design stages to confirm the number of sanitary basins and main sizes for their catchment areas. The placement of lift stations and their locations will also be confirmed in coordination with adjacent developments to maximize their servicing efficiencies and limit the frequency of connection points into the existing 600 mm sanitary force main (ERVWWTL).

5.2.1 South Sanitary Basin

The servicing area within the south sanitary basin is estimated at approximately 243.1 ha. A feasible location for the local lift station servicing this basin appears to be where the County's existing lift station (Rancher's Beef) is situated, east of Range Road 291 northeast of the Nexen Gas Plant, as illustrated on **Figure 9 - Waste Water Servicing Plan**.

The sanitary servicing study to be completed will investigate opportunity to utilize existing facilities associated with the Rancher's Beef local lift station and the 250 mm sanitary force main for servicing of the south sanitary basin and more specifically, the initial development phases of the High Plains Industrial Park. Potential facility upgrades that may be required over the course of development will also be determined. Specifics for pump station and force main requirements, valves and connections are to be confirmed at detailed design stages.

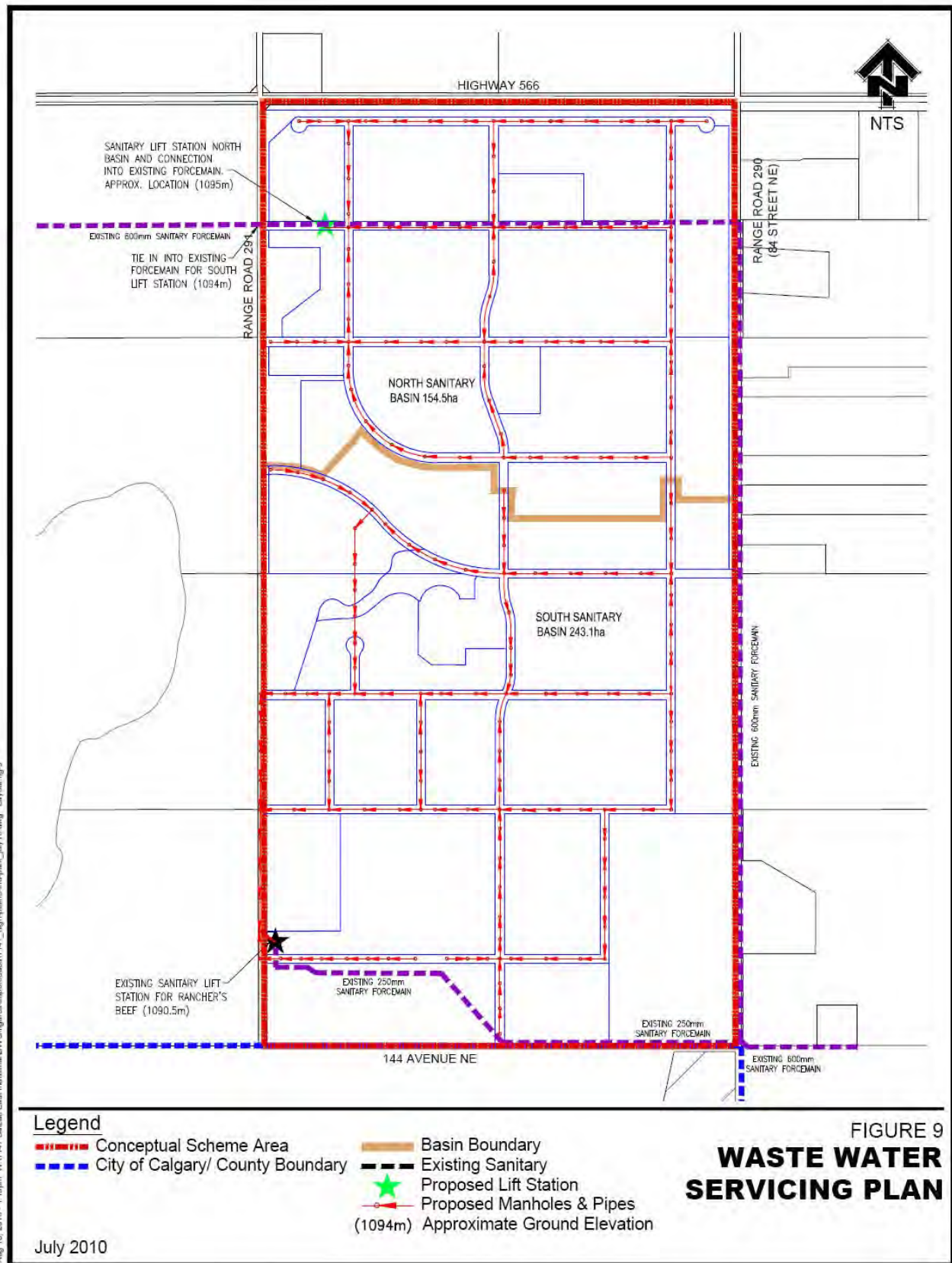
5.2.2 North Sanitary Basin

The servicing area within the north sanitary basin is estimated at approximately 154.5 ha. The north basin local lift station is proposed to be situated in the NW 12-26-29-W4M, just south of the existing 600 mm force main as illustrated on **Figure 9 - Waste Water Servicing Plan**. The force main from the north basin lift station would tie directly into the existing 600 mm force main. Back flow valves will be installed as necessary to prevent flow reversals. Specifics for pump station and force main requirements, valves and connections are to be confirmed at detailed design stages.

Policies:

- Policy 5.2.1** *The High Plains Industrial Park will be serviced by the County's East Rocky View Waste Water Transmission Line (ERVWWTL).*
- Policy 5.2.2** *The general configuration of the waste water infrastructure system required to service the CS area is illustrated on Figure 9 – Waste Water Servicing Plan.*
- Policy 5.2.3** *All waste water infrastructure within the CS area shall be constructed by the developer and shall be designed in coordination with adjoining projects to the satisfaction of the County.*
- Policy 5.2.4** *The County's Infrastructure Recovery Policy shall apply to applicable waste water infrastructure improvements.*

Policy 5.2.5 Payment of fees as per the County's applicable Waste Water Infrastructure Off-Site Levy Bylaw shall occur at the subdivision stage.



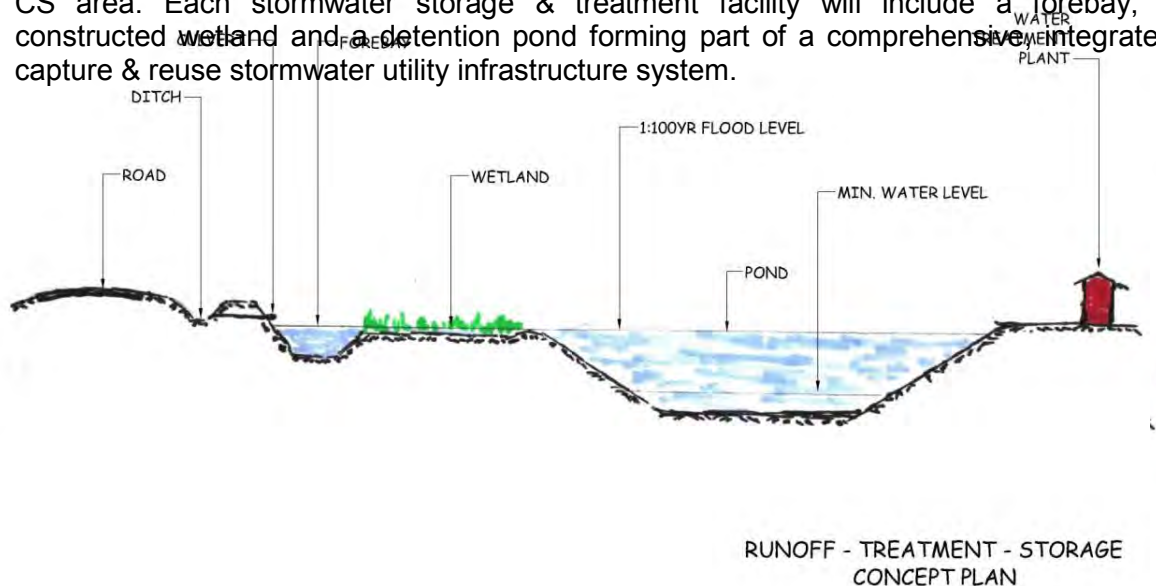
5.3 Stormwater Management

In 2010, Westhoff Engineering Resources Inc. prepared a Staged Master Drainage Plan (SMDP) to describe the drainage concept proposed to manage the stormwater within the entire CS area.

The SMDP describes stormwater management strategies and identifies infrastructure requirements to implement a surface runoff 'capture and reuse strategy' within the CS area. This includes establishing the sizing of the stormwater storage facilities and the design criteria for the major and minor conveyance system components for ultimate development conditions. The SMDP was developed based on the West Nose Creek Watershed Water Management Plan and proposes a 'zero discharge' model of addressing stormwater within the CS area.

As illustrated on **Figure 10 - Stormwater Management Plan**, the High Plains Industrial Park will include a series of engineered retention pond facilities - each constructed with sufficient capacity to retain surface drainage during extended periods of precipitation. Each of the pond areas will store and treat surface drainage from the tributary catchment area through a 'treatment train' process combination of natural filtration and mechanical treatment. Wherever possible, existing wetlands shall be retained and enhanced as part of development within the site.

A total of eight (8) stormwater storage & treatment facilities will be developed within the CS area. Each stormwater storage & treatment facility will include a forebay, a constructed wetland and a detention pond forming part of a comprehensive, integrated capture & reuse stormwater utility infrastructure system.



Each facility will also include an enhanced mechanical treatment process to enable the re-use of treated storm drainage to offset and/or augment 'potable' water required by certain consumptive industrial processing activities (in accordance with all required municipal and provincial regulatory approval requirements). A local distribution system will be established to convey the treated stormwater to the surrounding lots to support industrial processing activities, irrigation of green spaces and enhanced evaporative systems when required. The operation and maintenance of the treated stormwater distribution system will be determined at the Outline Plan stage to the satisfaction of the County.

Captured runoff will be stored, treated and distributed for re-use within the CS area to augment potable water use in support of industrial processing activities. The total amount of all wastewater effluent generated from such stormwater re-use will subsequently be placed into the County's sanitary system at a level that is equal to or less than the maximum allowable sanitary discharge rate as permitted by the County. Some of the industrial 'treated' stormwater re-use demand may be consumptive, and therefore, the specific volume of subsequent sanitary effluent will vary. However, the total effluent quantity discharged into the County's sanitary system shall not exceed a pre-determined maximum discharge rate. If the amount of treated stormwater re-used by industrial uses within the CS area is less than anticipated, the equivalent amount of treated stormwater that is not discharged through the sanitary system (as effluent) will be retained within the drainage facility and subsequently utilized for irrigation purposes and/or evaporated using a variety of enhanced evaporation techniques.

The utilization of rural road cross sections with LID ditch conveyance systems is required to implement the proposed stormwater management system within the CS area. The roadside ditches will include vegetation to filter sediment and accommodate some uptake of suspended / dissolved pollutants. Run off from the ditches will enter a forebay within each drainage facility before entering the 'treatment train' process (i.e. naturalized wetland, detention pond and mechanical treatment facility). In no cases shall the roadside ditches be used to 'store' surface drainage.

The High Plains Industrial Park stormwater concept's stormwater management model proposes to capture and retain 100 % of the surface drainage generated within the CS area. Implementation of a combination of stormwater treatment/re-use and detention/evaporation techniques will 'dispose' of all surface drainage generated by precipitation events up to a 1:100 year storm event without requiring a downstream spill. As is the case for any development, in the 'catastrophic' precipitation event exceeds a 1:100 storm, the drainage facilities within the CS area may exceed their capacities resulting in a downstream spill. In this unlikely event, surface drainage would be conveyed by roadside ditch to a culvert crossing under the Range Road 291 ROW to be directed to McDonald Lake.

This novel storm drainage design contemplates a unique 'synergistic' infrastructure management process that will achieve a 'value-added' benefit to the overall stormwater utility system within the CS area as well as providing a benefit to local industrial processors who can utilize the treated surface drainage. The Conceptual Scheme acknowledges that Alberta Environment must be carefully consulted at each phase of development as required by the evolving Provincial regulatory environment respecting licensing and re-use of surface drainage. Likewise, the collective experience and knowledge gathered by all stakeholder participants will inform and enhance the subsequent design and decision-making process of the stormwater management process as contemplated at each Outline Plan stage.

Policies:

Policy 5.3.1 The proposed stormwater management system within the CS area is generally illustrated on Figure 10 - Stormwater Management Plan.

- Policy 5.3.2** *Each stormwater management facility shall be constructed within a Public Utility Lot (PUL) to be dedicated to the Municipality at the subdivision stage.*
- Policy 5.3.3** *The stormwater management infrastructure system within the CS area shall implement a Capture & Reuse strategy in the spirit of the Provincial Water for Life Strategy and Rocky View County's Servicing Standards for Subdivision and Road Construction.*
- Policy 5.3.4** *The stormwater management system shall include a treatment train process include lot source control, conveyance via bio-engineered systems, forebay, wetland (natural and/or constructed) and storage ponds. All these elements shall be designed to supply treated stormwater as an alternate water supply and augment potable water requirements for industrial processing activities, irrigation of green spaces and enhanced evaporation systems within the CS area.*
- Policy 5.3.5** *The total amount of treated stormwater to be discharged into the ERVWWTL (as effluent) shall not exceed a maximum waste water discharge rate (as determined by the County).*
- Policy 5.3.6** *The operation and maintenance of the treated stormwater distribution system shall be determined at the Outline Plan stage, to the satisfaction of the County.*
- Policy 5.3.7** *Where implementation of source control techniques are proposed within individual lots, an easement and restrictive covenant shall be registered against each affected title authorizing the County to remedy any situation which negatively impacts the positive operation and maintenance of any on-site 'source control' stormwater management consideration.*
- Policy 5.3.8** *Specific updates to the Staged Master Drainage Plan prepared in support of the Conceptual Scheme will be required at each Outline Plan phase of development, to the satisfaction of the County and Alberta Environment.*
- Policy 5.3.9** *The collective experience and knowledge gathered by all stakeholder participants will inform and enhance the subsequent design and decision-making process of the stormwater management process as contemplated at each Outline Plan stage.*

5.4 Stormwater Research Partnership Opportunity

The stormwater drainage system proposed within the High Plains Industrial Park will include a substantial inventory of constructed industrial wetlands which will form part of an integrated local 'Integrated Water Management Plan'. The function of these wetlands will provide polishing treatment to surface drainage in advance of potential re-use for industrial processing and/or irrigation purposes within the CS area.

As component to previous development projects within the Calgary region, Highfield Stock Farms Inc. and partners have successfully advocated for innovative opportunities to develop partnerships amongst multiple public and private stakeholders to advance

local wetland research opportunities – where presently these opportunities are either under-funded or non-existent.

As part of the stormwater management system within the CS area, Highfield Stock Farms Inc. and partners propose to partner with Olds College (and other private industry partners) to undertake a program of research and assessment respecting the design, function and relative success of ‘cold climate’ constructed urban wetland facilities. Highfield Stock Farms Inc. and partners believe that an innovative program to finance this proposed research program could be implemented involving a variety of private and public stakeholder interests. Such financing strategies will be explored in detail at each Outline Plan stage of development within the CS area.

Policies:

Policy 5.4.1 The potential to implement an ongoing constructed wetland research program within the High Plains Industrial Park shall be investigated at each stage of development within the CS area, to the satisfaction of the County, Alberta Environment and Olds College.

Policy 5.4.2 An innovative funding strategy to finance the operation of the wetland research program may be established amongst multiple public and private stakeholders to be described at the Outline Plan stage.

5.5 Shallow Utilities

Shallow utility services including electric power, natural gas, telephone, cable and high speed internet services will be provided by local utility companies. It is anticipated that a mix of underground utilities and overhead utility lines will be located within the road rights-of-way and private easements as required to serve all new industrial lots. Where overhead utilities are contemplated, the height of this infrastructure will respect safe clearances for vehicle and equipment access for each titled lot.

Utility infrastructure is expected be installed overhead within the ‘General Industrial Area’ while utilities are expected to be placed underground within the ‘Highway 566 Transitional Area’ and the ‘Industrial Business Campus Area’ where aesthetic considerations require an enhanced architectural standard.

In order to accommodate anticipated development within the CS area, existing franchised utility infrastructure surrounding the site may require staged upgrading matched to the anticipated phasing of development. A summary of the various franchised utility services is as follows:

5.5.1 Electrical Power

As illustrated on **Figure 11 – Existing Shallow Utilities**, Fortis owns a ‘3-phase’ overhead power line situated within the Range Road 291 road allowance. This existing infrastructure provides approximately 4 MVA of electrical load – which is expected to accommodate the power requirement for the initial phasing of development within the CS area.

Fortis has also indicated that required upgrades to their existing infrastructure system affecting the entire East Balzac Area which may affect the servicing of subsequent development phasing in the CS area. System upgrades are typically implemented by Fortis based on customer demand. However, since the East Balzac area is rapidly building out, Fortis has indicated they are reviewing their existing cost and consumption models to determine how best to meet the future demand.

Specific details regarding the provision of electrical power infrastructure will be confirmed at the Outline Plan stage.

5.5.2 Natural Gas

There is a high pressure natural gas pipeline owned by ATCO Pipelines located immediately to the south of the CS area within the Transportation Utility Corridor (TUC).

ATCO Pipelines has indicated that natural gas service may be extended into the CS area from this existing distribution main; however, in order to do so, a site for a new pressure station will be required within the High Plains Industrial Park. The location for this required pressure station is anticipated within the southern portion of the CS area.

Specific details regarding the provision of natural gas infrastructure will be confirmed at the Outline Plan stage.

5.5.3 Telephone

As illustrated on **Figure 11 – Existing Shallow Utilities**, Telus owns an existing fiber optic line installed within the Range Road 291 road allowance. This existing line has sufficient capacity to provide phone service for the initial phases of development within the CS area; however, the service will need to be upgraded to support latter phasing.

Specific details regarding the provision of telephone infrastructure will be confirmed at the Outline Plan stage.

5.5.4 Cable

Shaw has not typically been active in the East Balzac area to date, and as such, has no existing infrastructure in the area. Shaw has been using Telus fiber optic lines to provide service to customers within the East Balzac area.

Specific details regarding the provision of cable infrastructure, if required, will be confirmed at the Outline Plan stage.

Policies:

Policy 5.5.1 Shallow utilities shall be provided within the CS area at the sole expense of the developer within appropriate rights-of-way established at the detailed subdivision design stage.

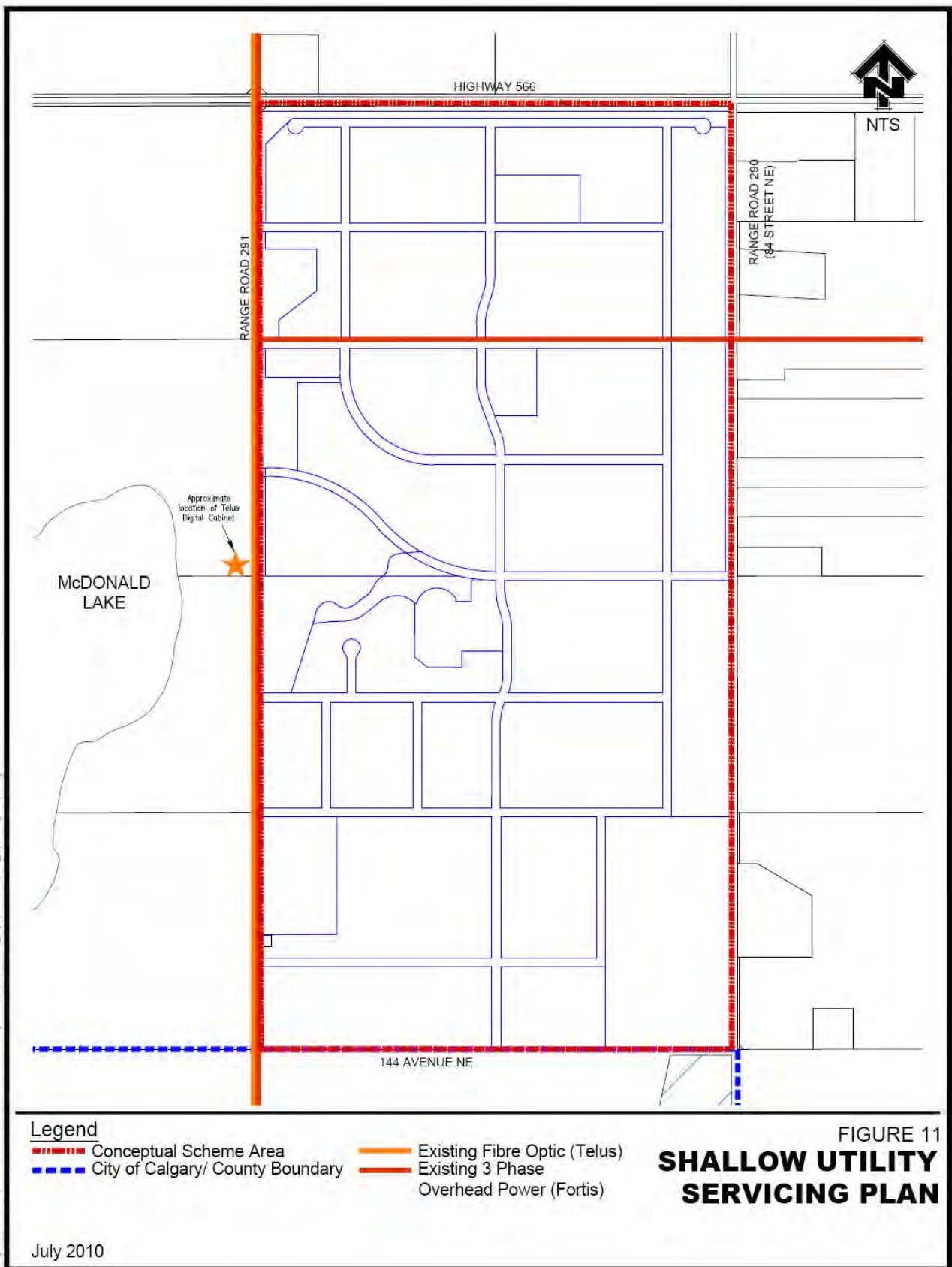
Policy 5.5.2 Shallow utilities shall be placed within easements located on private property. Consideration may be provided to install overhead power lines within the road allowances.

Policy 5.5.3 Shallow utilities shall be installed underground within the Highway 566 Transitional Area and Industrial Business Campus Area, and may be installed overhead within the General Industrial Activity Area.

5.6 Solid Waste Management

Solid Waste Management will be the responsibility of each property owners. A local commercial solid waste disposal service will be contracted by a High Plains Business Park Lot Owner's Association to be created at the subdivision stage.

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6.0 OPEN SPACE

The anticipated dedication of public open spaces within the High Plains Industrial Park is generally illustrated by **Figure 12 – Open Space Plan**.

As per the provisions of the Balzac East ASP, wherever possible, dedication of municipal reserve (MR) in SDA#5 should be provided in the form of linear trails and/or buffer strips along the northerly and north easterly boundaries of the Plan area adjacent to Highway 566 and Range Roads 290 & 291. Accordingly, linear municipal reserve will be dedicated within the CS area along the Highway 566 frontage and along portions of the Range Road 290 and 291 frontages. Additional municipal reserve may be dedicated surrounding the stormwater management facilities within the CS area to provide landscaped amenity spaces for patrons and employees of local businesses.

An environmental reserve (ER) parcel will be dedicated to protect the deciduous forest area that connects the existing wetlands within portions of NW 1 and SW 12.

All other required public land dedication as required by the Municipal Government Act will be provided by the developer at each phase of development in the form of cash-in-lieu of land.

6.1 Highway 566 Linear Open Space Buffer

Existing developments occurring within the western portion of the Balzac East Area Structure Plan have been carefully designed to accommodate a sensitive interface alongside the Hwy 566 frontage area. This development expectation will be similarly implemented within the High Plains Business Park.

A linear park space will be established across the entire northern boundary of the Conceptual Scheme along the Hwy 566 frontage. As conceptually illustrated, a 20 m linear Municipal Reserve will be dedicated directly south of the highway ROW to be developed with a variety of drought-resistant landscaped features and a regional pathway connection (to connect with adjacent commercial developments occurring to the west). An area of 'private' landscaping will also be provided within each business lot fronting onto this linear park space to further enhance and compliment the visual amenity of this feature and sufficiently buffer business development along Hwy 566.

HIGHWAY 566 OPEN SPACE TREATMENT



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- Legend**
- Regional Trail
 - Internal Pathway/ Sidewalk
 - Municipal Reserve
 - Public Utility Lot

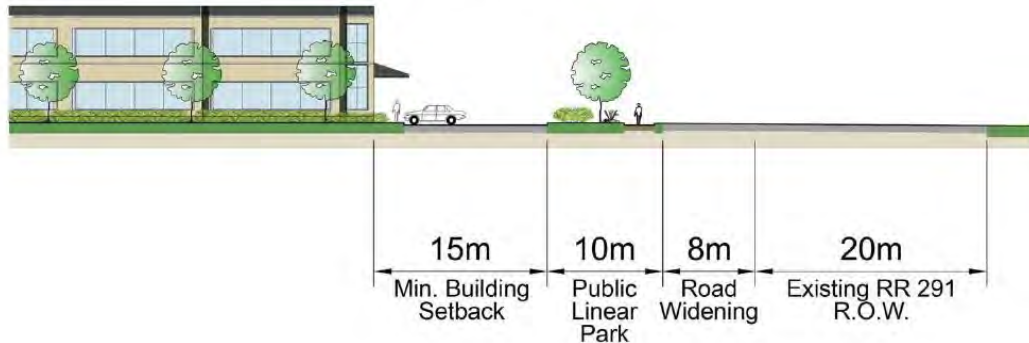
FIGURE 12
OPEN SPACE PLAN

July 2010

6.2 Range Road 291 Linear Open Space Buffer

As illustrated on **Figure 12**, the primary linear open space traversing the northern boundary of the CS area will be extended along the boundary roads situated directly east and west of the Plan. A 10 m linear MR shall be provided along the Range Roads 291 frontages to extend from Highway 566 southerly to the stormwater management facility to be constructed in NW 1-26-29-W4M. This linear MR will include landscaping features and a regional pathway connection.

RANGE ROAD 291 OPEN SPACE TREATMENT



As illustrated on **Figures 6 & 12**, a linear MR is also proposed to extend along the eastern boundary of the CS area (Range Road 290) from Highway 566 to Township Road 261. This linear MR is anticipated to include a similar configuration to that which is proposed along Range Road 291. However, a proposed cross section has not been included in this Conceptual Scheme pending Alberta Transportation's confirmation of the functional alignment for the Highway 566 / Stoney Trail NE regional road connection.

6.3 High Plains Industrial Park - Landscaped Entry Feature

A landscaped entrance feature will be constructed at the intersection of Hwy 566 and Range Road 291. This feature will establish an attractive 'way finder' to provide a visual cue to reference the arrival at the High Plains Industrial Park. The specific architectural design of the entrance feature will ensure it compliments the surrounding landscapes and does not negatively impact the safety and function of the adjacent regional transportation network.



6.4 Pedestrian Network

Implementation of a pedestrian network is anticipated within the CS area as generally illustrated on **Figure 12 – Open Space Plan**.

The primary focus of the pedestrian network within the High Plains Industrial Park will orient towards the East Balzac Regional Trail which will be constructed within a linear MR adjacent to Highway 566.

Pathway connections within the CS area will facilitate pedestrian mobility onto the East Balzac Regional Trail System by trails constructed within MR's adjacent to Range Roads 290 & 291 and by a local pathway system (to be provided within private easement areas) and/or by sidewalks provided within road rights-of way.

The pedestrian network will also include a pathway connection along the road ROW extending from Township Road 261 to Township Road 260 (144th Avenue NE) (to be provided within private easement areas) and/or by sidewalks provided within road rights-of way.

The specific mechanisms required to implement the pedestrian system within the CS area shall be determined at the Outline Plan stage.

In some cases, dedication of linear MR may be provided surrounding the High Plains Industrial Park's stormwater facilities to include enhanced landscaping features and local pathways. The specific design and configuration of these proposed MR dedications will be determined at the Outline Plan stage to the satisfaction of the County.

6.5 Environmentally Sensitive Area

As described in Section 2.6.4 of this Plan, Westhoff Engineering Resources Ltd. completed an Environmental Impact Assessment (EIA) and a Wetland Impact Assessment (WIA) to determine if the CS area contains environmental sensitivity.

The conclusions of the EIA recommend that the area of deciduous forest which connects two existing wetlands situated within the NW 1 and SW 12 should be preserved. As illustrated on **Figures 6 & 12**, this existing vegetated area is proposed to be established as an 'Environmental Area' to ensure it remains intact and is not negatively disturbed by proposed development. The specific configuration of the 'Environmental Area' and the exact mechanism for its preservation shall be established at the appropriate development phase through the Outline Plan preparation process.

Policies:

- Policy 6.5.1** *Municipal Reserve shall be provided through dedication of land and payment of cash-in-lieu of land pursuant to the provisions of the Municipal Government Act. The configuration of open space dedication within the CS area is generally illustrated on Figure 12 – Open Space Plan.*
- Policy 6.5.2** *A 20 m MR shall be provided adjacent to Highway 566. A regional pathway connection shall be constructed within this MR by the developer to the satisfaction of the County.*
- Policy 6.5.3** *A 10 m MR shall be provided adjacent to Range Roads 290 & 291 to include a regional pathway connection to be constructed by the developer to the satisfaction of the County. The specific configuration of the open space treatment adjacent to Range Road 290 will be determined at such time the alignment of the Highway 566 / Stoney Trail NE regional road connection is finalized.*
- Policy 6.5.4** *A landscaped entry feature may be constructed by the developer at the intersection of Highway 566 and Range Road 291. An*

additional landscaped entry feature may also be established at the intersection of Hwy 566 and Range Road 290.

- Policy 6.5.5*** *The pedestrian system within the Conceptual Scheme shall include a combination of pathways (constructed in MR or within private access easement areas) and/or sidewalks (constructed within road rights-of-way).*
- Policy 6.5.6*** *In some cases, dedication of linear MR may be provided surrounding the High Plains Industrial Park's stormwater facilities to include enhanced landscaping features and local pathways. The specific design and configuration of these proposed MR dedications will be determined at the Outline Plan stage to the satisfaction of the County.*
- Policy 6.5.7*** *The existing deciduous forest area within portions of NW 1 and SW 12 shall be established as an Environmental Reserve (ER) to preserve it from development disturbance. The specific size, configuration and area of the ER shall be determined at the Outline Plan stage.*
- Policy 6.5.8*** *The Developer shall prepare a Landscaping Plan to detail all proposed plantings and pedestrian facilities within the MR areas in support of each phase of development.*
- Policy 6.5.9*** *The Developer shall be responsible to implement all improvements within the MR areas as determined by the Landscaping Plan.*
- Policy 6.5.10*** *The Municipality shall assume responsibility for maintenance of all MR areas following their Final Acceptance in accordance with the terms of a Development Agreement.*

7.0 COMMUNITY SUPPORT INFRASTRUCTURE

7.1 Fire Response

Fire protection will be provided from the existing Fire Station located in East Balzac. A secondary response will be provided from the City of Calgary and/or the City of Airdrie subject to the provisions of a Mutual Aid Agreement.

Fire suppression will be provided by the municipal potable water supply system with appropriate design that considers appropriate hydrant spacing.

7.2 Police Response

Police service within the CS area will be provided by the Royal Canadian Mounted Police Detachments situated in the City of Airdrie and/or the Town of Chestermere (to be supported by service provided by Rocky View Community Peace Officers).

7.3 Emergency Service Response

The High Plains Industrial Park will be serviced by 911 Emergency Services with an emergency locator system set up for each individual property. EMS response is anticipated from EMS facilities situated in the City of Airdrie and/or the City of Calgary.

7.4 Emergency Response Plan

Given the 'heavy industrial' nature of anticipated development within the High Plains Industrial Park, the preparation and implementation of an Emergency Response Plan is an important consideration to ensure an appropriate strategy is followed in the event of an emergency. As such, the Outline Plan prepared in support of each phase of anticipated development will include a strategy to respond to emergency situations that may arise within the CS area.

Policies:

Policy 7.4.1 As component to an Outline Plan prepared in support of each phase of development in the CS area, the developer shall prepare an Emergency Response Plan to establish protocols in the event of circumstances that require emergency response.

7.5 Voluntary Recreation Contribution Fund

The proponent of the High Plains Industrial Park envisions the establishment of local business development opportunity as an important consideration to the County's strategic objective of creating 'complete' community developments. Although no residential development is contemplated within this development, certain recreation improvements will be provided by the developer which will contribute to the overall amenity of the surrounding community.

To further enhance this recreation amenity, the developer will consider contributing to the County's 'Voluntary Contribution Fund' at the subdivision and/or development permit application stage.

STANDARDS

The High Plains Industrial Park Conceptual Scheme contemplates the creation of a master-planned business commercial/industrial subdivision that accommodates a variety of strategic economic development opportunities in keeping with the Rocky View County Growth Management Strategy.

As a stated objective, Rocky View County is developing Commercial, Office and Industrial Guidelines intended to guide the establishment of attractive, cohesive and recognizable business development landscapes throughout the entire municipality.

The specific type and scale of business development anticipated within the CS area will vary greatly from 'high-quality' highway commercial developments in the north to more general 'heavy' industrial land uses involving processing and manufacturing activities within the central and southern portions of the site. In all cases, business land use within the High Plains Industrial Park is expected to establish and maintain an appropriately attractive development form. However, the uniform application of one specific set of architectural guidelines across the entire site would be problematic due to the wide variability of anticipated built form expected within the Highway 566 Interface Area, Business Industrial Campus Area and the General Industrial Area (as illustrated on **Figure 5 - General Land Use Concept**).

As part of an Outline Plan prepared in support of each phase of development, the developer will be required to prepare specific architectural guidelines to ensure the form of development within each land use area maintains a cohesive built-form within the context of development expected in each of the land use areas proposed by this Conceptual Scheme – in keeping with the stated Development Guidelines listed in the Balzac East Area Structure Plan. The architectural controls to be prepared as part of each Outline Plan will establish specific design criteria expectations to be implemented at each development phase such as:

- Consistency of Public Realm design elements;
- Requirements for landscaping (public and private);
- Implementation of gateway principles;
- Generalized building form and character (architectural theme);
- Anticipated massing of buildings (particularly in relation to Hwy 566 and perimeter roads);
- Variety in surface materials (colour & texture) and roof line articulation;
- Location & design of off-street parking & loading areas;
- Location & design of shared vehicular circulation areas (between individual lots);
- Location & design of lighting elements;
- Location & design of signage elements;
- Location & design of outside storage elements; and
- Location & design of fencing & screening elements.

Policies:

Policy 8.0.1 Site design and architectural controls will be established for each phase of development within the CS area to be detailed within Outline Plans prepared in accordance with the Development Guidelines listed in Sec. 4.4.3 of the Balzac East Area Structure Plan.

Policy 8.0.2 Architectural controls shall be implemented by the developer at the subdivision and/or development permit stage.

8.1 High Plains Industrial Park Landscaping Concept

The overall aesthetic of development within the CS area will be predominantly influenced by the form and character of landscaping elements provided within the site within public land dedications and private lots.

Integrated landscaped areas will be provided within the High Plains Industrial Park not only for site enhancement, but also to aid in the management of surface drainage. By carefully designing and implementing grading, drainage and plantings, landscaped elements will aid in the conveyance, management, treatment and storage of stormwater (in addition to visually enhancing the appearance of the development). Improvement of runoff quality and quantity will ultimately result in reduced erosion and less sedimentation and contamination in the receiving water bodies.

Key locations of the development will require landscape treatment. These will include:

- entrance features (lighting, planting, signage)
- collector roadways
- building facades
- parking lots
- site perimeter/buffer areas
- stormwater ponds
- pathways/site connectivity

Appendix 'A' attached to this Conceptual Scheme provides a more elaborate description of these above-referenced landscaping elements and is intended to provide a reference upon which more specific implementation criteria will be based.

The implementation of this landscaping concept will occur through specific design expectations established at each phase of development through preparation of a Outline Plan.

POLICIES

Developments within the Conceptual Scheme area is subject to the land use policies contained within the Rocky View County's Growth Management Strategy, the Rocky View County/City of Calgary Inter-municipal Development Plan (IDP), the County's Municipal Development Plan (MDP); and the Balzac East Area Structure Plan (BEASP). These statutory and non-statutory planning policies are briefly described as follows:

9.1 Rocky View County Growth Management Strategy (GMS)

Rocky View 2060 proposes a land-use vision for Rocky View that is based on principles of sustainability, Smart Growth, and triple-bottom-line decision-making. It is designed to respond to existing and new development pressures and proposals and provide a framework to direct where and how development is most desirable for the good of the community. The Growth Management Strategy anticipates significant expansion of Rocky View's business community with the development of substantial industrial and commercial parks, especially in the Balzac area. Business nodes will be thriving industrial and commercial centers in which Rocky View will collaborate with the private sector to attract and support business development. These major business nodes will function as regional employment centers, and provide an assessment balance to other forms of development in the municipality.

The GMS identifies the East Balzac area as a Business Node - strategically located between the City of Calgary and the City of Airdrie, and adjacent to the Queen Elizabeth II Highway. The current and planned availability of water and sewer services, combined with superb transport accessibility and visibility, will foster relatively high valued and intensive development in the Balzac area. The area is strategically located to serve as the initial and core magnet for further commercial, industrial, and mixed-use development in the municipality. The Balzac node is expected to provide the strongest focus of new high-valued business development in Rocky View.



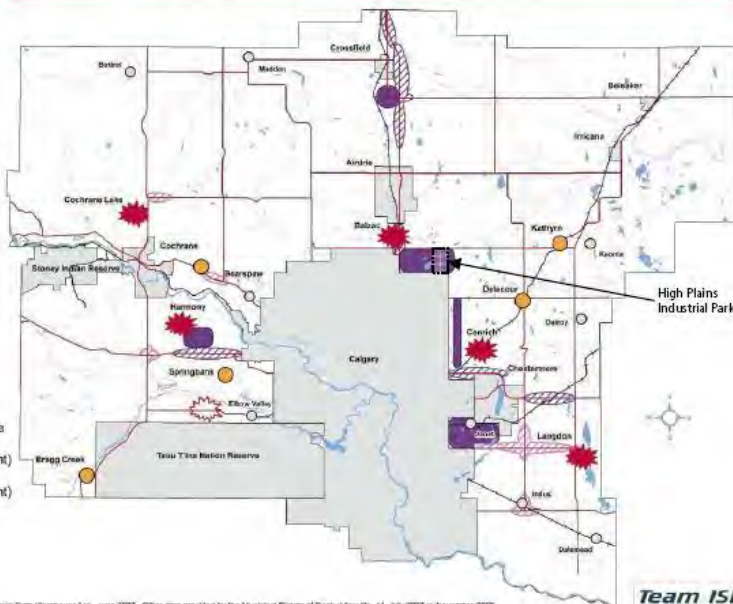
This map is conceptual, not to scale and for illustrative purposes only.

Legend

- Growth Node
- Potential Growth Node
- Community Core
- Existing Community
- Business Node/Regional Employment Centre
- Major Business Corridor (Nodal Development)
- Minor Business Corridor (Nodal Development)

Prepared April 16, 2009.

FIGURE 1 - Growth Management Strategy Map



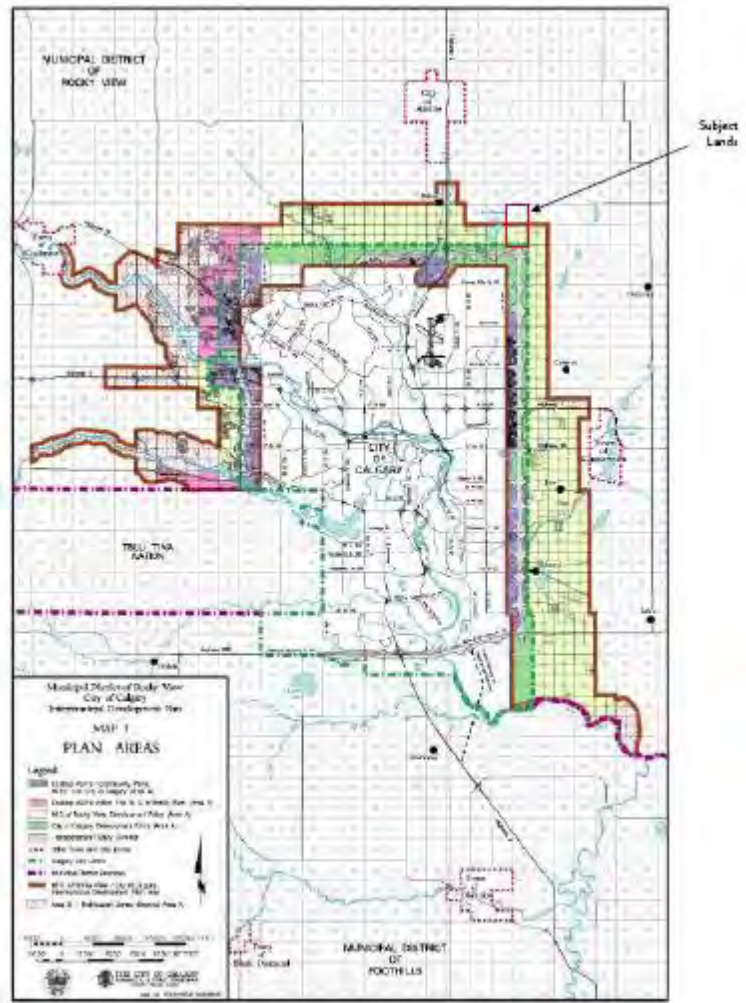
Base mapping compiled from the Provincial Geo-Administrative Digital Base, Spatial Data Usersource Ltd., June 2007. Other data provided by the Municipal District of Rocky View No. 44, July 2007 to November 2008.

Team ISL

9.2 Rocky View / Calgary Intermunicipal Development Plan (IDP)

The subject lands are partially located within the boundary of the Rocky View County / City of Calgary Intermunicipal Development Plan (IDP) November, 1998. According to the IDP, all of Section 1-26-29-W4 is located within Policy Area 'A' which indicates that the County's relevant development policies prevail when evaluating development proposals (i.e. the Balzac East Area Structure Plan).

Policies in the IDP address the provision of commercial/industrial developments within the Municipality to ensure the installation of their supporting transportation & utility servicing does not compromise regional infrastructure such as the Queen Elizabeth II, TUC and Nose Creek. The High Plains Industrial Conceptual Scheme is consistent with the policy intent of the IDP.



9.3 Municipal Development Plan (MDP)

The Municipal Development Plan (1997) encourages the provision of commercial/industrial land uses that strengthen the economy of the Municipality. More specifically, the MDP promotes the development of comprehensively planned business parks to be located in close proximity to regional transportation & utility servicing infrastructure. The High Plains Industrial Park is consistent with this municipal objective as described in the MDP's Section 6.0.

The MDP also encourages new development provide open space to promote opportunities for recreation and social gathering. Accordingly, a linear open space shall be dedicated along the northern boundary of the site to contain a regional pathway connection to facilitate pedestrian activities throughout the East Balzac area.

The MDP also encourages inter-municipal cooperation when proposed development has potential to impact mutual planning interests. Recognizing that development within the

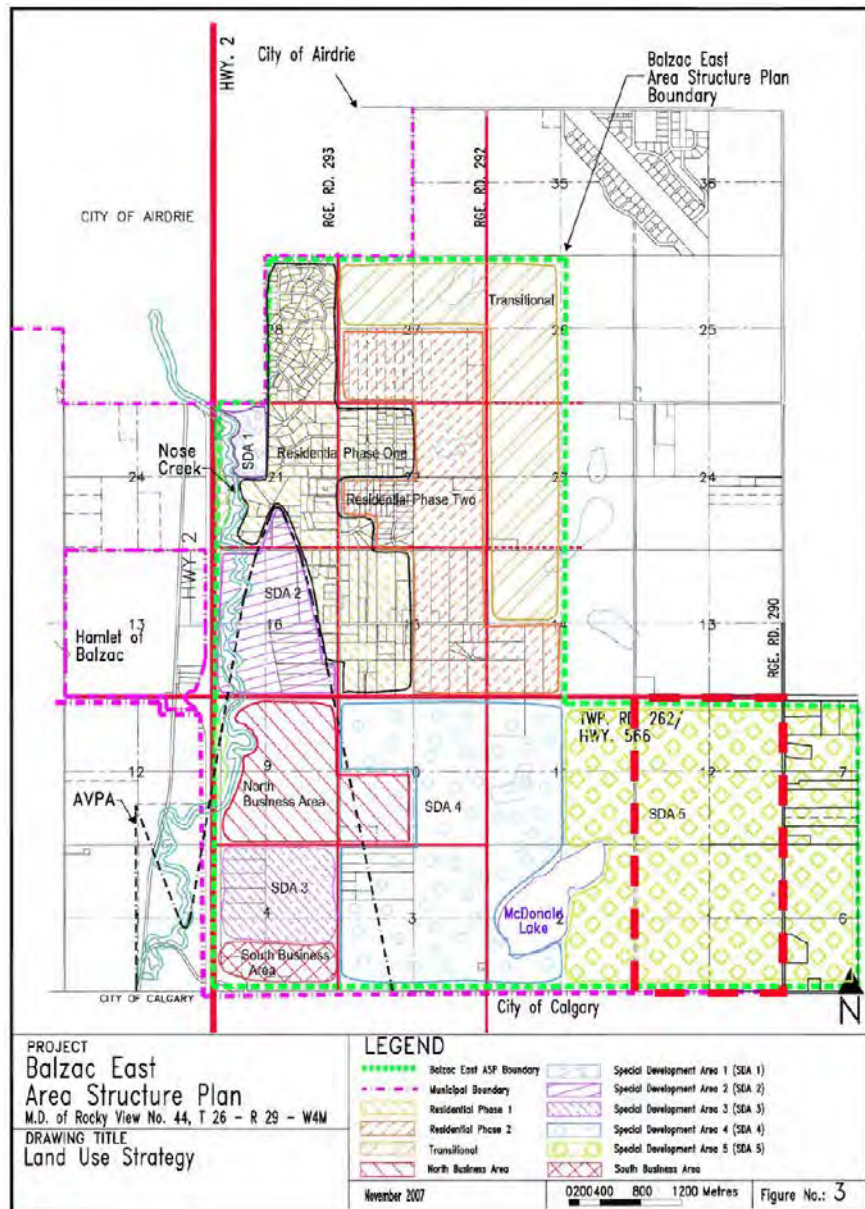
subject lands has potential to influence land uses within the City, the developer met with representatives from the City to share the preliminary vision in support of the High Plains Industrial Park. Likewise, the County referred the Conceptual Scheme application to the City of Calgary for official comment.

9.4 Balzac East Area Structure Plan (BEASP)

The Balzac East Area Structure Plan (BEASP) was adopted by the County in 2001 to guide the development of a planned employment area within the influence of the Noise Exposure Forecast (NEF) of the Calgary International Airport in addition to areas of country residential subdivisions. Lands located south of Highway 566 and east of the Queen Elizabeth II are expected to develop with a variety of business developments with an increase in scale and intensity from north to south.

In September 2008, the Municipality amended the BEASP to include additional lands situated east of Range Road 292 to be included within a 'Special Development Area' (SDA#5).

With the proximity to existing and planned industrial and business development, the extension of similar uses within SDA #5 is justified by the recent ASP amendment. There are market interests to pursue developments in the Balzac area with larger lots with a lower level of some municipal servicing. At the present time, there is no industrial land in this part of the County that caters to this type of business's land development



Conceptual Scheme Area

requirements.

The recent BEASP amendment establishes specific criteria for development within SDA#5 as further described in Section 3.3 of this Conceptual Scheme.

The High Plains Industrial Park Conceptual Scheme has been prepared to respond to these BEASP policy provisions and includes a land use strategy that addresses these aforementioned criteria.

9.5 Rocky View County Economic Development Strategy

The County's adopted Economic Development Strategy notes the following strengths and opportunities related to the East Balzac area.

- The Nexen Gas Plant is a primary draw for this specific area.
- Additional business industrial land uses that complement the existing developments are appropriate for this area.
- Proximity to the City of Calgary, the City of Airdrie, Highway 2, as well as the Calgary International Airport all support business the development of industrial land uses within this area.

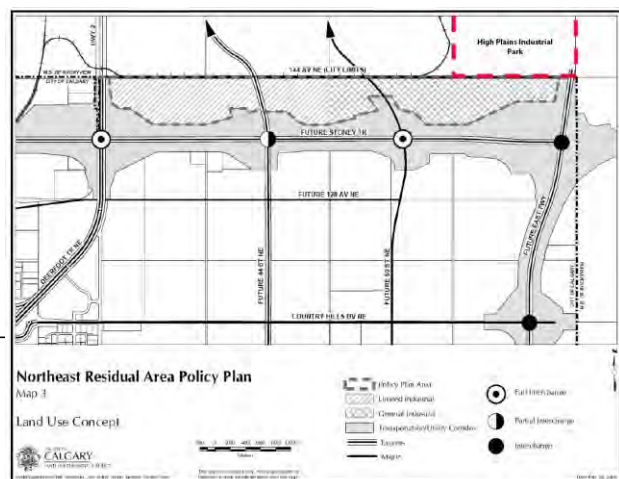
The Strategy document identifies a number of general opportunities. Many of these opportunities could be accommodated in the Balzac East area.

- General industrial developments
- 'Multi-modal' warehouse developments
- Manufacturing, warehousing and distribution
- Value-added agriculture processing activities
- Comprehensive 'campus-style' business park developments
- Eco-Energy Resources
- Niche markets for Specialty Items
- Strong agricultural business base

The High Plains Industrial Park development concept is consistent with the County's strategic objective to accommodate a full range of industrial developments within this portion of the Municipality.

9.6 The City of Calgary North East Residual Area Policy Plan

Lands directly to the south of the Plan area include \pm 700 acres of undeveloped lands within the City of Calgary situated north of the Stoney Trail. The City recently completed the North East Residual Area Policy Plan to establish a broad-based policy objective for this area. The lands are expected to function predominantly as



an industrial area, initially as a limited serviced area until full urban services are provided. Land in the western portion of the area will consist of limited industrial uses while land in the eastern portion of the area (adjacent to the subject lands) will include general industrial uses. The area will attract development which can co-exist with both sour gas protection setbacks and airport noise setbacks, while providing the required services for the development. The eventual provision of full urban services to the area will facilitate higher density development.

The provisions of this City of Calgary Policy Plan do not apply within Rocky View County, and as such, its' reference in this Conceptual Scheme is for contextual purposes only. Notwithstanding, the form of development anticipated within the High Plains Industrial Park is consistent with the type of development which is expected in City's Residual Area Policy Plan.

10.0 IMPLEMENTATION

10.1 Development Phasing

As is generally illustrated by **Figure 13 – Phasing Plan**, development phasing within the CS area will begin within the southwest portion of the Plan area (Pt NW & NE 1-26-29-W4M) and extend across the site from this location matched to the availability of supporting infrastructure.

An Outline Plan must be first be approved by Rocky View County Council prior to business development proceeding within the CS area. The anticipated boundaries of each Outline Plan are notionally illustrated on **Figure 13**.

Adjustments to the proposed Outline Plan boundaries may occur without amendment to the Conceptual Scheme provided the developer can demonstrate a logical infrastructure implementation rationale for doing so.

As described in Section 3.3 and 4.2.1 and illustrated on Figures 5, 6, 7 & 13 of this Plan phasing of development within the eastern portion of the Conceptual Scheme area (which may require access to Range Road 290) shall be restricted until such time the Highway 566 / Stoney Trail NE regional roadway connection has been confirmed by Alberta Transportation.

10.2 The High Plains Industrial Park – The Outline Plan Preparation Process

This High Plains Industrial Park Conceptual Scheme establishes a strategic development framework to guide the implementation of phased development within the site, over time, that is supportive of the policies established by the Balzac East Area Structure Plan.

Typically, the policy framework established by a Conceptual Scheme is implemented through subsequent land use amendment, subdivision and development approval processes. Within the BEASP area, a Conceptual Scheme is expected to establish a bridge between the Area Structure Plan policy and the implement processes of land use, subdivision and development/building permits.

The High Plains Industrial Park Conceptual Scheme includes a relatively large area (\pm 1,270 ac), and as such, the scope and magnitude of the anticipated development horizon (15 – 20 years) challenges the ability to detail specific expectations for development within the site – as is typically defined by a Conceptual Scheme process.

As such, the development implementation process anticipated by this Conceptual Scheme proposes an ‘intervening’ planning approval process referred to as an ‘Outline Plan’. This plan will bridge the gap between the municipal statutory land use policy for the area (BEASP), the somewhat general ‘over-arching’ development policy framework established within this Conceptual Scheme, and the more detailed subsequent land use amendment, subdivision and development permit approval processes.

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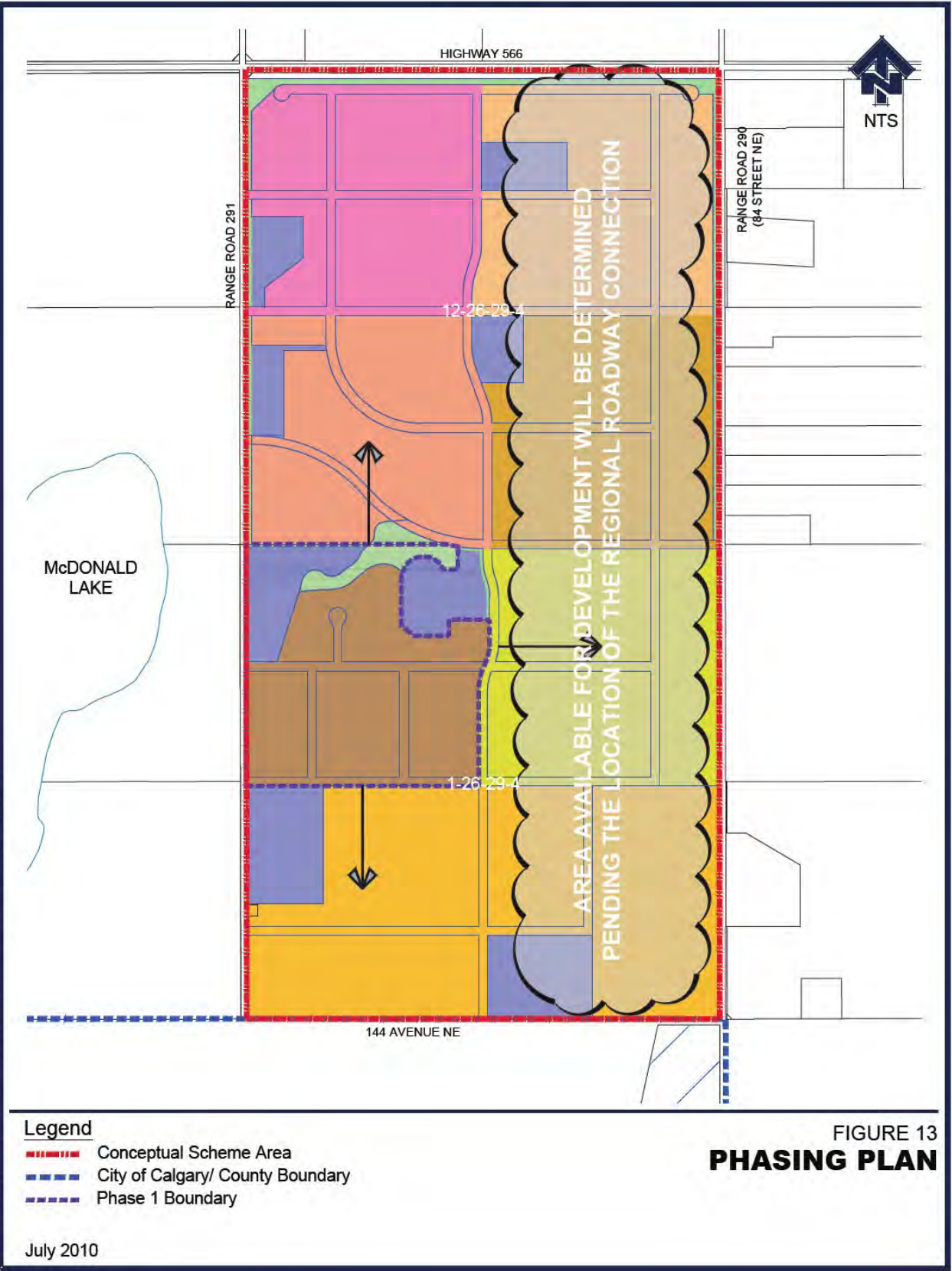


FIGURE 13
PHASING PLAN

Prior to the County considering a land use amendment and subdivision application for phased commercial/industrial development opportunities within the Conceptual Scheme area, an Outline Plan must be prepared by the developer to detail specific development criteria as may be required to implement a particular phase of the project.

Each Outline Plan will require adoption by Rocky View County Council and will be appended into this Conceptual Scheme via a statutory bylaw amendment process.

10.3 Outline Plan Evaluation Criteria

Outline Plans prepared in support of each anticipated phase of development shall include a rationale for the implementation of land use and subdivision by establishing the expectations for development through a detailed description of the following considerations:

- Expected allocation of specific land use within the development phase;
- An illustration of anticipated lot configurations within the subdivision area;
- An update to the Master Utility Servicing Plan to detail the specific mechanisms and expectations regarding implementation of utility service infrastructure in relation to the requirements of the overall CS area and surrounding lands;
- An update to the Traffic Impact Assessment to detail the specific mechanisms and expectations required to implement local and regional roadway improvements in relation to the requirements of the overall CS area and surrounding lands;
- An update to the Stormwater Staged Master Drainage Plan to detail the specific mechanisms and expectations regarding implementation of stormwater infrastructure in relation to the requirements of the overall CS area and surrounding lands;
- Where dedication of Municipal Reserve is contemplated, a Landscaping & Public Amenities Plan to detail the anticipated public improvements therein including expectations for use and maintenance responsibilities;
- Specific criteria to inform the preparation of an Emergency Response Plan as may be required to ensure a strategy is put in place to address emergency responses and/or evacuations in the event of a major industrial accident;
- Specific criteria to inform the implementation of a land use amendment as may be required to implement the specific subdivision/development phase; and
- A summary of specific performance standards and architectural controls as may be required to implement development considerations such as architectural theme, parking & loading expectations, fencing and screening considerations, signage & lighting elements, etc.

10.4 Anticipated Land Use Designations

All lands within the CS area shall require a redesignation to an appropriate land use designation as provided by the County's Land Use Bylaw prior to being considered for subdivision and development for business purposes. As part of the County's

redesignation process, Council will first consider an Outline Plan adoption to establish and detail specific expectations required to support implementation of each development phase.

In some cases, where the County's Land Use Bylaw is not able to accommodate a specific land consideration within the CS area, as part of an Outline Plan process, the developer may propose either a Direct Control Bylaw or a new land use district (to be added to the County's Land Use Bylaw as may be required).

10.5 Construction Management Plan

The Balzac East Area Structure Plan policy 4.7.4(e) indicates that prior to initiation of construction, a Construction Management Plan shall be prepared to address dust control, noise, truck routes and access to the site, and potential for interference with any nearby residential areas.

Policies:

Policy 10.5.1 A Construction Management Plan shall be provided by the developer at each phase of development, to the satisfaction of the County.

Policy 10.5.2 Stripping and grading should be limited to the areas required for development and servicing of each subdivision phase.

10.6 Subdivision & Road Naming

An application for Subdivision & Road Naming will be submitted with each Outline Plan. Naming of roads within each phase of development is expected to follow standard naming conventions as required by the County.

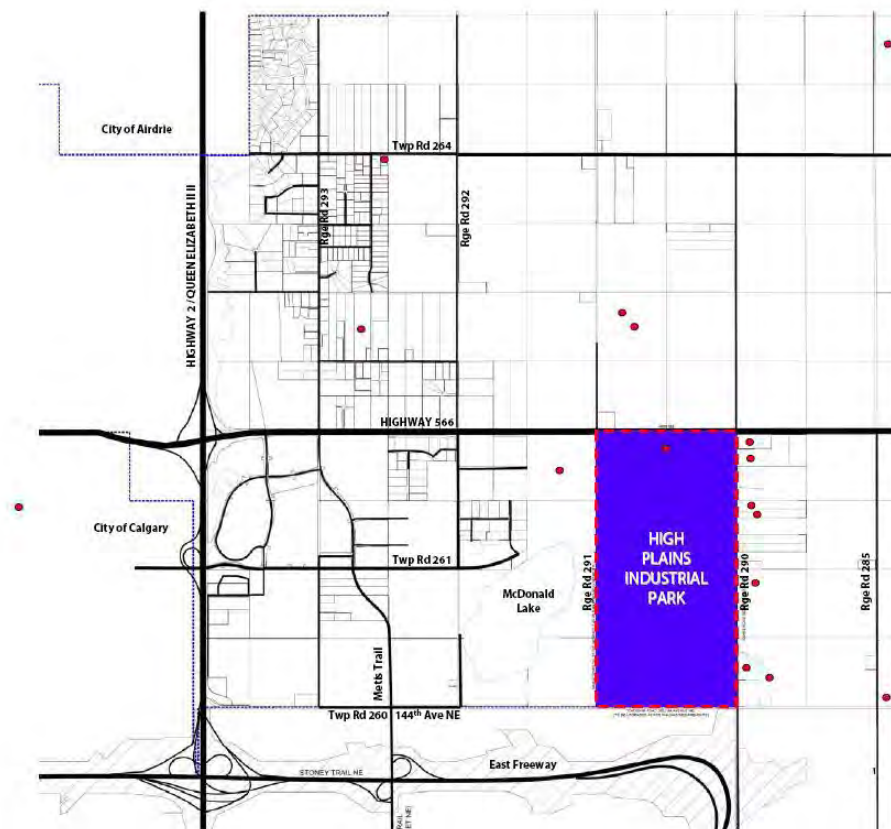
11.0 PUBLIC CONSULTATION

Throughout the Conceptual Scheme's preparation process, the proponents of the Plan have endeavored to maintain open and transparent communication with local stakeholders including:

- Adjacent landowners
- Alberta Environment
- Alberta Transportation
- City of Calgary
- Nexen
- Rocky View County Administration

The content of the Conceptual Scheme was prepared with the interests of these stakeholders in mind.

On March 23, 2010, an Open House was convened at the Balzac Community Hall in order to present the draft Conceptual Scheme and supporting details to the community. The information session was well-attended with twenty seven (27) guests registered on a sign-in sheet. Each attendee was asked to indicate where their land interests were in relation to the CS area (as illustrated by the red dots).



Each attendee was asked to complete a comment sheet in order to solicit general community feedback regarding the project including ways to improve it. Two (2) comment sheets were provided, both of which expressed support for the project as proposed.

SUPPORTING TECHNICAL STUDIES (UNDER SEPARATE COVER)

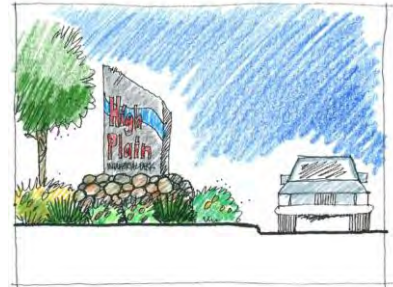
1. Historical Resource Overview Clearance Letter – Alberta Culture & Community Spirit
2. Preliminary Geotechnical Investigation – Sabatini Earth Technologies Inc., November 2008
3. Phase 1 Environmental Site Assessment(s) – Biophyllia Inc., February 2008, Environmental Diagnostics, April 2006 & Soil & Environ Diagnostics, June 2008
4. Environmental Impact Assessment & Wetland Impact Assessment – Westhoff Engineering Resources Ltd., November 2009
5. Staged Master Drainage Plan – Westhoff Engineering Resources Ltd., November 2009
6. Traffic Impact Assessment – Bunt & Associates Engineering, November 2009
7. Utility Servicing Design Brief – Focus Engineering, December 2009 (updated April 2010)

APPENDIX 'A' – PROPOSED LANDSCAPE CONCEPT

High Plains Industrial Park

landscape concept

Located northeast of the City of Calgary and in the foothills of the Rocky Mountains, High Plains Industrial Park in East Balzac will cover eight quarter-sections of land. The site is flanked by Highway 566 to the North, Range Road 291 to the West, and Range Road 290 to the East. Typical land use in the surrounding area includes agriculture, country residential, and the adjacent Nexen gas plant. The site also plays host to numerous sour gas wells and pipeline right-of-ways.



The project goal is to not only create an innovative and aesthetically appealing development, but to create a top-quality, environmentally sensitive and sustainable industrial park.

In addition to the development standards and specifications set out by Rocky View County, the High Plains Industrial Park will include integrated landscaped areas with the built environment not only for site enhancement, but also to aid in the management of surface drainage. By correctly implementing grading, drainage and planting, landscaping will aid in the conveyance, management, treatment and storage of stormwater (in addition to visually enhancing the appearance of the development). Improvement of runoff quality and quantity will ultimately result in reduced erosion and less sedimentation and contamination in the receiving water bodies.

Key locations of the development will include landscape treatments such as:

- entrance features (lighting, planting, signage),
- collector roadways
- building facades
- parking lots
- site perimeter/buffer areas
- stormwater ponds
- pathways/site connectivity

Portions of High Plains Industrial Park, specifically those quarters in the north, will be designed to have an 'enhanced' architectural quality, while areas further south will have a more 'general' industrial appearance.

The aesthetic of the landscaping will be achieved through a manicured and structured scheme, including plantings of trees, shrubs, ornamental grasses and perennials in mulch or stone beds. Emphasis will be placed on aesthetic plantings in highly visible, public locations such as key entrances to the industrial park, along primary collector roadways, at building entrances and facades, and in parking areas in the light industrial/office areas. Use of manicured turf grasses with



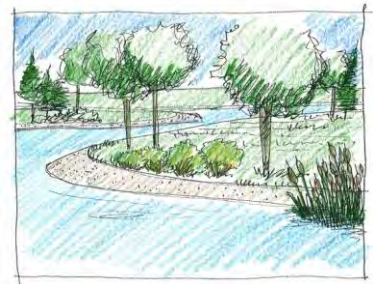
higher levels of maintenance requirements will be limited to higher-profile areas of the industrial park within proximity to regional collector roads.

Collector Roadways



The perimeter buffer planting areas within the development will have an informal appearance characterized by flowing natural plantings in random configurations, open ditches and vegetated swales. Manicured turf areas, if used, will be limited to public, building entrance areas, with low-maintenance naturalization seed mix used in lower-profile areas. In no cases shall potable water be used for irrigation purposes, so manicured areas shall require careful design and implementation so as to minimize the maintenance requirements.

The site contains several naturally-occurring wetlands with attendant vegetated areas. As part of the overall development of High Plains Industrial Park, eight stormwater ponds are proposed for the collection, storage, and potential re-use of stormwater. Vegetated swales will direct stormwater overland to their collection points, with opportunities for functional and aesthetic landscape enhancement of these areas. By incorporating aquatic or wetland species along channels and pond perimeters, filtration

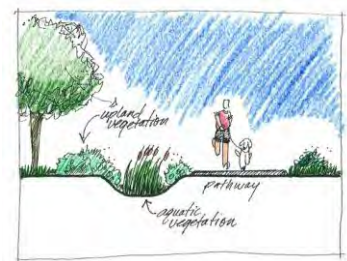


of sediment and contaminants will occur, reducing erosion and contributing to the management of stormwater. Landscape enhancement of the stormwater management ponds will not only improve the appearance of the site, but also provide habitat for flora and fauna. Provision of seating area, picnic facilities and interpretive nodes could further enhance the site aesthetic and experience.

Stormwater Ponds



Because the drainage swales are connected to the stormwater ponds as well as to each other, opportunities may exist for pedestrian circulation, mainly within the northern portion of the development. Linear connection areas will be visually enhanced with plantings to encourage pedestrian movements, provide buffers between site uses, allow for habitat linkages, and create opportunity to connect local pedestrian facilities with the regional pathway system. Once again, seating and interpretive nodes may be



provided for site users.

Pathways



In all situations, the careful selection and strategic placement of suitable plant materials is paramount. Plant materials will be specifically selected because they are either native to the region, aquatic or wetland species, or well-adapted, drought-tolerant ornamentals. A goal of the High Plains Industrial Park development is to use appropriate plant materials which require minimal irrigation (if at all). Collected surface drainage water may be used for irrigation purposes, however, use of potable water will be strictly restricted.

In addition to aesthetics, main factors contributing to suitable plant material selection include zone hardiness and adaptation to local climatic, geographic and hydrologic conditions; availability; year-round landscape value; maintenance requirements; and drought tolerance. Where possible, plant materials will be grouped into beds. Plantings above normal water level will be mulched with shredded wood mulch or river rock, suppressing weed growth and retaining moisture, thereby reducing maintenance and watering requirements. Appropriate native plant materials will be incorporated into the plantings in both ornamental and functional roles, strengthening the relationship between the built and natural environment and resulting in an innovative and effective approach to landscaping and stormwater management. Grasses, whether for manicured turf or naturalization, will be blends suitable for arid conditions.



References for plant material selection and for confirmation of

suitability include:

Alberta Yards & Gardens, Alberta Agriculture, Food & Rural Development, 1999

NatureScape Alberta, Myrna Pearman & Ted Pike, 2000

Creating the Prairie Xeriscape, Sara Williams, 1997

Trees & Shrubs for the Prairies, Landscape Alberta Nursery Trades Association

Trees & Shrubs of Alberta, Kathleen Wilkinson, 1990

Parking Lots



In moving forward with preliminary and detailed landscape design, further research into specific Rocky View County requirements, site assessment and ongoing discussion as it relates to stormwater quality, landscape design and best management practices will take place with the County, stakeholders, developer and members of the consulting team.

APPENDIX 'B'

STAGE 1 OUTLINE PLAN

STAGE 1 OUTLINE PLAN



**ROCKY VIEW COUNTY
BYLAW C-7002-2010**

A Bylaw of Rocky View County to amend the “High Plains Conceptual Scheme” by adding to the appendix the Stage 1 Outline Plan.

- WHEREAS** the Council deems it desirable to amend the Conceptual Scheme, and
- WHEREAS** the *Municipal Government Act*, 2000, c. M-26 as amended, allows the County to adopt plans relating to the future subdivision and development of lands within the County; and
- WHEREAS** the Council of Rocky View County, has received an application to amend a Conceptual Scheme. The amendment provides a framework for subsequent redesignation, subdivision and development within a portion of the NW-1-26-29-W4M and NE-1-26-29-W4M, consisting of an area of approximately \pm 58 hectares (143 acres), as defined in Schedule 'A', the “Stage 1 Outline Plan,” attached to and forming part of this Bylaw; and
- WHEREAS** a notice was published on Tuesday, December 21, 2010 and Tuesday, December 28, 2010 in the Rocky View Weekly, a newspaper circulating in Rocky View County, advising of the Public Hearing for Tuesday, January 11, 2011; and
- WHEREAS** Council held a Public Hearing and has given consideration to the representations made to it in accordance with Section 692 of the *Municipal Government Act*, being Chapter M-26 of the Revised Statutes of Alberta, 2000, and all amendments thereto.

NOW THEREFORE the Council enacts the following:

1. That the “Stage 1 Outline Plan,” attached hereto as Schedule „A”, be appended to the High Plains Conceptual Scheme in order to provide a framework for subsequent redesignation, subdivision and development within the NW-1-26-29-W4M and NE-1-26-29-W4M, consisting of an area of approximately \pm 58 hectares (143 acres), as defined in Schedule 'A', attached to and forming part of this Bylaw; and
2. The Bylaw comes into effect upon the date of its third reading.

Division: 7

File: 06401003/004 2010-RV-133

First reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on Tuesday, December 7, 2010, on a motion by Councillor Habberfield.

Second reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on Tuesday, January 11, 2011, on a motion by Councillor Habberfield.

Third reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on Tuesday, January 11, 2011, on a motion by Councillor Boehlke.

REEVE OR DEPUTY REEVE

MUNICIPAL SECRETARY

SCHEDULE 'A'
FORMING PART OF BYLAW C-7002-2010

A Outline Plan affecting the area within a portion of the NW-1-26-29-W4M and NE-1-26-29-W4M, herein referred to as the „Stage 1 Outline Plan“.

ACKNOWLEDGEMENT PAGE

Prepared For:



Consultant Team:

- Brown & Associates Planning Group
- Bunt & Associates Transportation Planners & Engineers
- ERW Consulting Ltd.
- Focus Engineering, Geomatics & Planning
- Lawson Projects
- Westhoff Engineering Resources, Inc.

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1.0 INTRODUCTION

1.1 The High Plains Industrial Park Conceptual Scheme

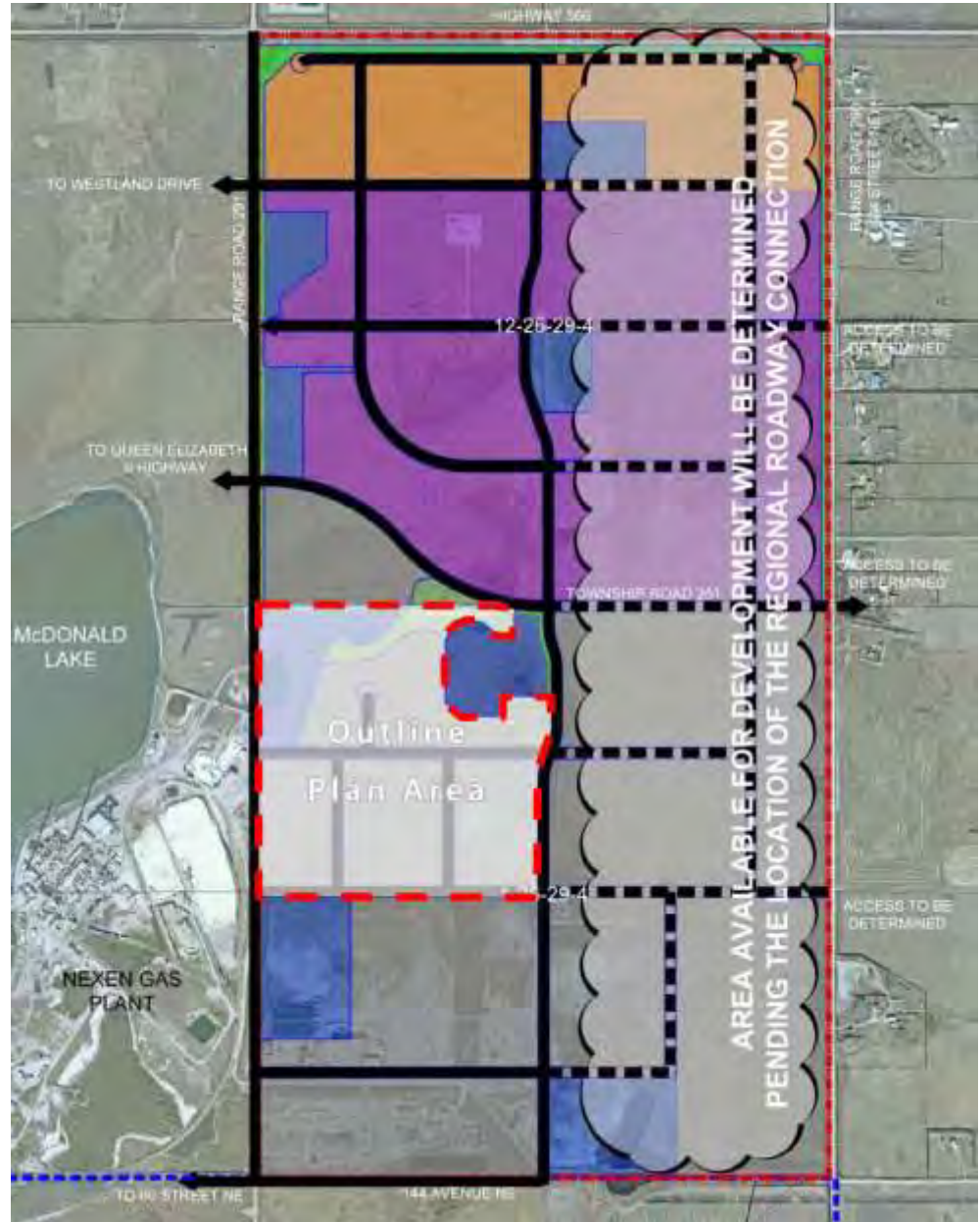
The High Plains Industrial Park Conceptual Scheme is a non-statutory planning document adopted by Rocky View County, by Bylaw, in accordance with the requirements of the Balzac East Area Structure Plan.

The Conceptual Scheme is a 'broad-based' policy intended to set general expectations regarding the implementation of a comprehensively-planned business developments within Sections 1 & 12, Twp 26, Rge 29, W4M.

1.2 Purpose of this Outline Plan

As per the requirements of Section 10 of the High Plains Industrial Park Conceptual Scheme, Outline Plans must be prepared at each phase of development prior to implementing land use redesignation and subsequent subdivision. The Outline Plan process will describe the following phase-specific considerations:

- Land use amendment required to implement development within the Outline Plan boundary;
- Anticipated phasing of subdivision within the Outline Plan including proposed parcel sizing & configurations;
- An update the High Plains Industrial Park Master Utility Servicing Plan (Focus, 2010) to detail the required implementation of utility service infrastructure;
- An update the High Plains Industrial Park Traffic Impact Assessment (Bunt, 2009) to detail the required implementation of local and regional roadway improvements;



- An update the High Plains Industrial Park Staged Master Drainage Plan (Westhoff, 2010) to detail the required implementation of stormwater infrastructure;
- Where dedication of Municipal Reserve is contemplated, a Landscaping Plan to detail the anticipated improvements;
- Clarification of specific criteria regarding a strategy to address emergency responses and/or evacuations in the event of a major industrial accident; and
- A description of performance standards and controls to implement a specific architectural theme, parking & loading expectations, fencing and screening considerations, signage & lighting elements, etc.



The Stage 1 Outline Plan has been prepared to address these development considerations pursuant to the High Plains Industrial Park Conceptual Scheme (Bylaw C-6930-2010) as adopted on July 27, 2010.

This Outline Plan will be adopted by Council, by Bylaw, and appended to the High Plains Industrial Park Conceptual Scheme.

2.0 STAGE 1 OUTLINE PLAN AREA

2.1 Stage 1 Outline Plan Area

The Stage 1 Outline Plan area includes portions of the NW & NE 1-26-29-W4M containing ± 58 ha (± 143 ac).



2.2 Ownership

The site is owned by 1405275 AB Ltd. (in trust for Highfield Stock Farm Inc. and CLT Contracting Ltd.).

2.3 Site Description

The subject lands include predominantly undeveloped agricultural lands presently under cultivation. Nexen operates a Level 1 Sour Gas Well within the southeast portion of the site. The Plan area includes existing

wetlands and an area of deciduous vegetation that follows a natural drainage course within the northern portion of the parcel.

3.0 DEVELOPMENT CONSIDERATIONS

3.1 Development Concept

At build-out, the Stage 1 Outline Plan area is anticipated to include approximately twenty one (21) lots with a minimum parcel size of ± 1 ha (± 2.5 ac), two (2) Public Utility Lots (PUL), one Municipal Reserve parcel (MR) and an Environmental Reserve (ER).



3.2 Potable Water & Waste Water Servicing

Potable water will be supplied by the County's East Rocky View Water Transmission system. The developer will construct all required water distribution infrastructure within the Stage 1 Outline Plan area as generally described by the High Plains Industrial Park Servicing Design Brief (Focus, 2010).

Water conservation will be implemented through metered usage, mandatory potable water restrictions and architectural controls registered against each titled lot by restrictive covenant.

Waste water service will be provided by the County's **East Rocky View Waste** Water Transmission Line (ERVWWTL). The developer will construct a collection and distribution infrastructure within the Stage 1 Outline Plan area. Effluent will be directed to the **County's** existing sanitary lift station situated in SW 1-26-29-W4M within Area A, Plan 071 0376 and redirected to **the County's** 600 mm ERVWWTL situated in proximity to the Outline Plan area. **If the County's existing lift station** does not have capacity to receive and redirect effluent as contemplated, the developer will facilitate upgrades to the existing lift station or construct a new lift station, as per County requirements to be determined at the subdivision stage.

The developer will contribute applicable water contribution fees and waste water offsite levies to connect the Stage 1 Outline Plan area with the regional water & wastewater utility infrastructure at the subdivision stage as per County requirements. Where the developer provides potable water and wastewater infrastructure that is demonstrated to benefit lands outside of the Stage 1 Outline Plan area, **the County's** Infrastructure Cost Recovery policy will apply.

3.3 Stormwater Management

The stormwater management system within the Stage 1 Outline Plan area will implement a comprehensive infrastructure network designed to treat stormwater at three levels described as follows:

- 1st level filtration to occur within a perimeter landscaping area situated at the edge of each lot to capture sediments and provide initial stormwater quality enhancement. Stormwater will be conveyed towards the PUL facility via roadside ditches including bio-engineered systems designed to reduce run-off volumes and continue stormwater quality enhancement;
- 2nd level filtration to occur within the PUL through a conventional forebay to provide sediment trapping and continue stormwater quality enhancement. Stormwater will be passed through existing and constructed wetland complexes within the PUL in order to provide cleansing through natural biological processes. **Stormwater will then be deposited into the PUL's retention pond;**
- 3rd level filtration with '**polishing**' to occur within the detention pond via natural processes to achieve the final stormwater quality enhancement. Stormwater will be disposed from the PUL through implementation of natural and enhanced evaporative techniques and irrigation activities.

The **design of the PUL's retention pond** will include sufficient capacity to capture and retain a 1:100 yr precipitation event based on continuous simulation modelling.

As a secondary benefit to the filtration process, the PUL facility is expected to include an additional treatment component to facilitate stormwater re-use within the Stage 1 Outline Plan area. A mechanical treatment plant will be constructed by the developer to treat the stormwater from the retention pond to an appropriate standard to accommodate its re-use within local industrial processing activities – thereby decreasing potable water requirements. The developer will also construct a local distribution system to convey treated stormwater to business lots within the Outline Plan area. A Utility Corporation will be created to operate the plant and distribution infrastructure and manage the sale of the treated stormwater to local

business operators. A portion of the profits from the Utility Corporation is expected to be used to fund ongoing wetland research within the PUL site by Olds College. The ownership and operation of this Utility Corporation will be determined at the subdivision stage in accordance with Alberta Environment regulatory



requirements. The County reserves the right to participate as an active or passive partner within the Utility's operational model – to be determined by a Memorandum of Agreement at the subdivision stage.

The Stage 1 Outline Plan's management system proposes an innovative stormwater treatment and reuse process. Lessons learned from its' implementation will be used when considering future development phasing. The relative success of this stormwater management process will be evaluated at each Outline Plan stage as per Section 5.3 of the High Plains Industrial Park Conceptual Scheme.

Existing grades within the portion of the Outline Plan area situated south of the sour gas pipeline ROW generally direct surface drainage towards the southwest. Re-grading this area towards the PUL would require approximately 3 m of fill which is not considered feasible. As such, an 'interim' trap-low will be constructed within the southwest portion of the site sized to retain surface drainage up to a 1:100 year event with a minimum 0.3 m freeboard. Surface drainage from this trap-low will be pumped to the PUL via a pipe/ditch combination as illustrated. When the PUL stormwater management facility associated with the area directly south of the subject lands is constructed, the trap-low will be removed and surface drainage redirected accordingly. The lot containing the interim trap-low could then be redeveloped.

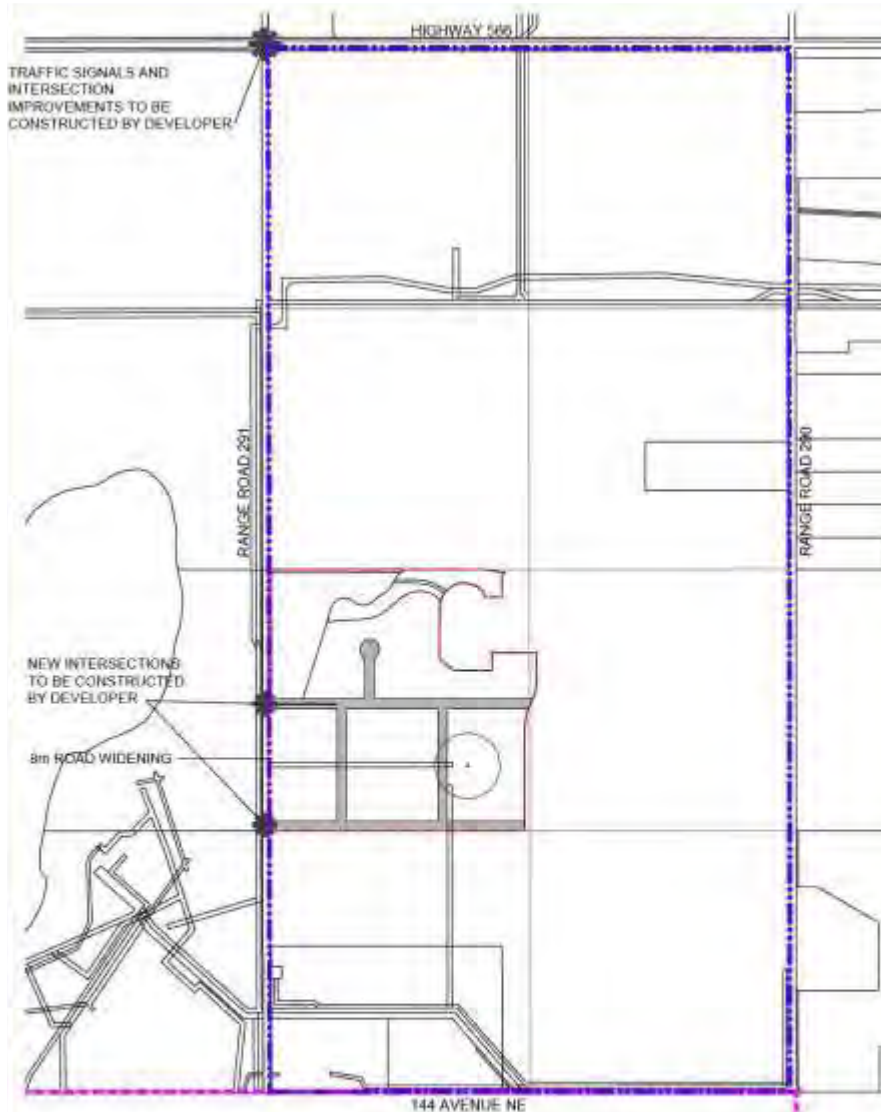
As described in Section 3.8 of this Outline Plan, an applied research wetland laboratory will be established within the PUL to provide an opportunity for Olds College to conduct aquatic vegetation research, species production, treatment performance analyses and operational monitoring. The working laboratory is expected to provide benefit to the operation of the stormwater facility within the PUL and produce documentation of lessons learned therein which may inform and educate the development industry at large. It is expected that a portion of funds generated by the operation of the treated stormwater utility will be provided to Olds College to be used for ongoing aquatic vegetation research, species production and operational modeling.

3.4 Transportation

The Stage 1 Outline Plan will be accessed from Highway 566 via Range Road 291 – a paved municipal statutory road allowance.

Rge Rd 291 includes a paved road with lanes and surfacing with capacity to accommodate the traffic generated within the Outline Plan area. However, the Rge Rd 291 ROW must be widened from 20 m to 36 m. Up to 8 m of ROW widening will be dedicated within the Stage 1 Outline Plan area. An equivalent 8 m of additional ROW will be dedicated by the landowners/developers abutting the west side of the Range Road 291.

The High Plains Industrial Park Traffic Impact Assessment (Bunt, 2009) evaluated the capacity of Rge Rd 291 and its intersection with Highway 566 to consider the additional traffic expected to be generated by development in the Stage 1 Outline Plan area.



The TIA evaluated the capacity of the Highway 566 / Rge Rd 291 intersection. Analysis confirmed that traffic turning movements will operate within acceptable capacity parameters, with exception of the northbound direction. The northbound movements will function at a LOS F and with a v/c ratio greater than 0.90, during the PM peak hour, which indicates insufficient capacity and an increased delay in the northbound direction. As such, this intersection requires upgrading to a TYPE IV design (as per Alberta Transportation design guidelines) with signalization in order to support development within the Stage 1 Outline Plan. It is noted that the existing intersection may have capacity to accommodate initial subdivision & development phasing

without requiring completion of the intersection upgrades; however, they are required at full build out. Requirements for specific roadway upgrades will be determined at the subdivision stage.

As these intersection improvements will benefit lands situated outside of the Stage 1 Outline Plan area, the County will implement the Infrastructure Cost Recovery policy accordingly.

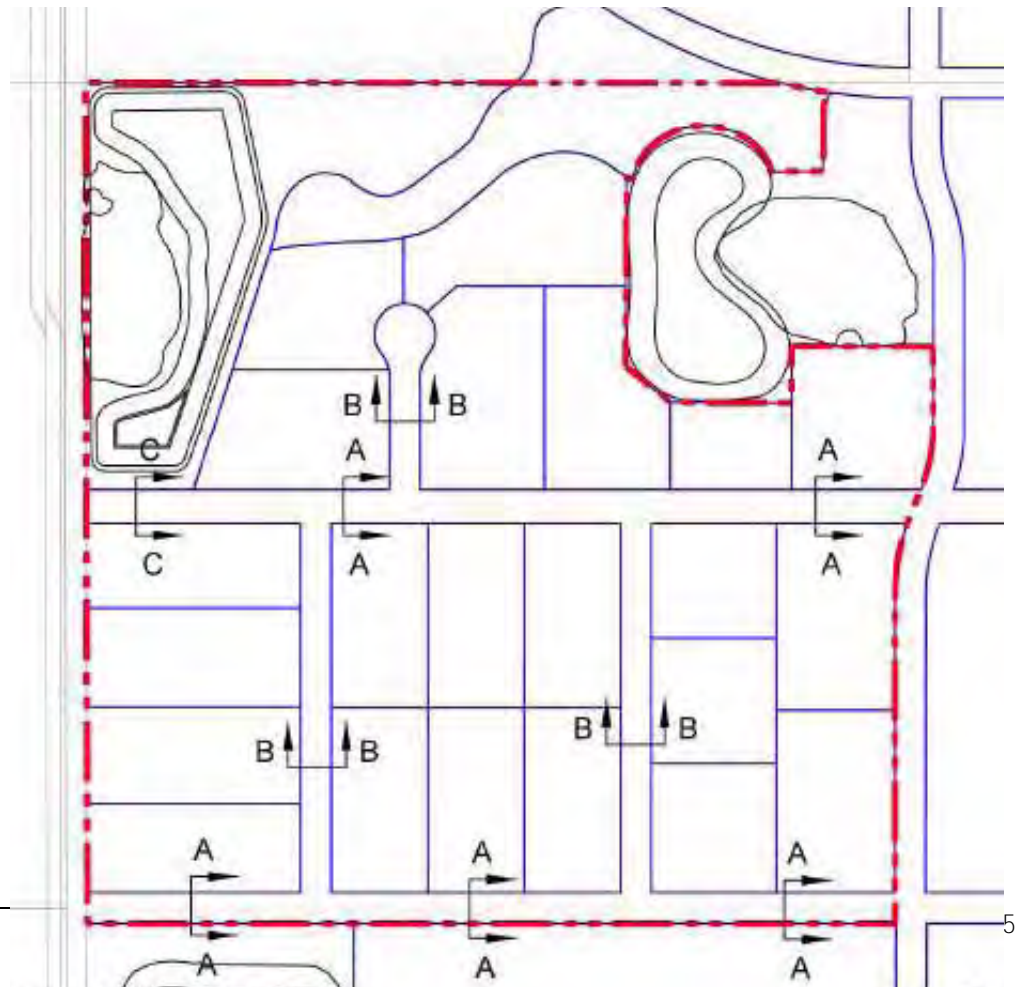
The transportation network within the Stage 1 Outline Plan will include an internal public road system with two connections from Rge Rd 291.

All local roads within the Stage 1 Outline Plan area will be constructed by the developer as follows:

- The finished roadway surface will include a paved asphalt surface **constructed within public ROW's**;
- The main access road from Range Road 291 will include a divided median structure with separate east & westbound travel lanes;
- Vegetative plantings will be implemented within the ditch area to facilitate and enhance the **stormwater 'treatment train' process described in Section 3.3**;
- Water and Wastewater distribution mains and over-head power infrastructure will be installed within the public road rights-of-way;
- Shallow utilities (natural gas & telecommunication) will be installed within a private utility easements situated on private lands immediately adjacent to the public road ROW's; and
- All internal roads will be enhanced with landscaping located within private easement areas along each parcel frontage – to include a berm and appropriately-scaled plantings. The landscaping easement will be privately owned and maintained by the High Plains Development Association (HPDA).

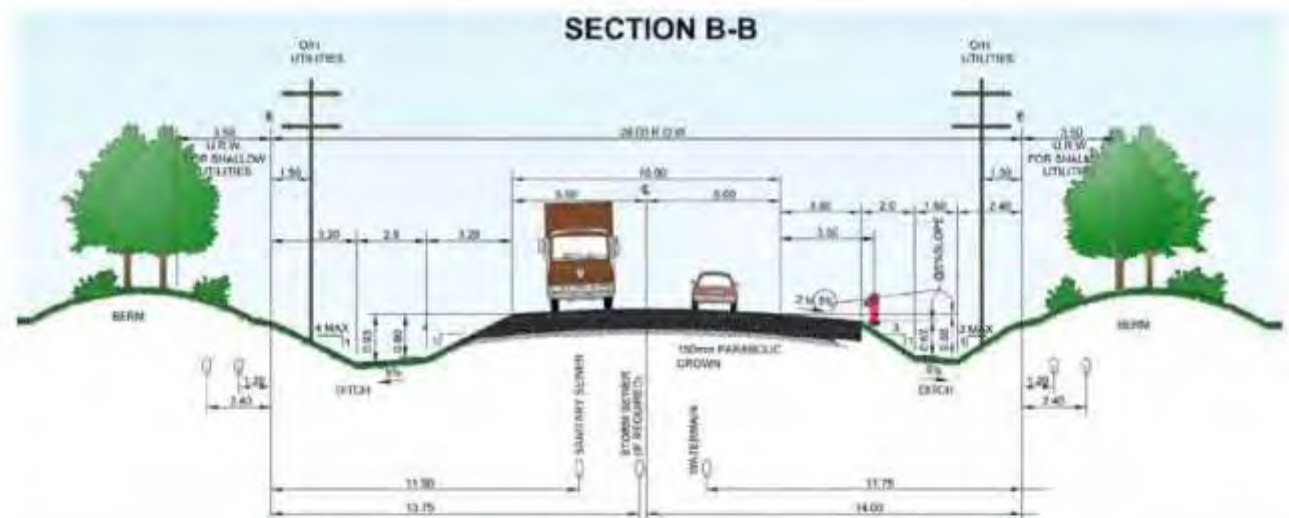
Three (3) road right-of-way cross-sections are proposed within the Stage 1 Outline Plan area as illustrated by the referenced Key Plan and the details on the following page. The actual roadway standards will be determined at the subdivision stage as per County requirements.

Connectivity of roads between the Stage 1 Outline Plan area and the adjacent lands is expected to proceed as generally illustrated on Figures 6 & 7 of the High Plains Industrial Park Conceptual Scheme. Specific alignments of all public road extensions onto adjacent



lands will be confirmed by subsequent Outline Plans.

PROPOSED ROADWAY CROSS SECTIONS



3.5 Shallow Utilities

As described in the High Plains industrial Park Servicing Design Brief (Focus, 2010), Fortis Inc. and Telus Communications operate electrical and telecommunications distribution infrastructure that can be conveniently extended to the Stage 1 Outline Plan area.

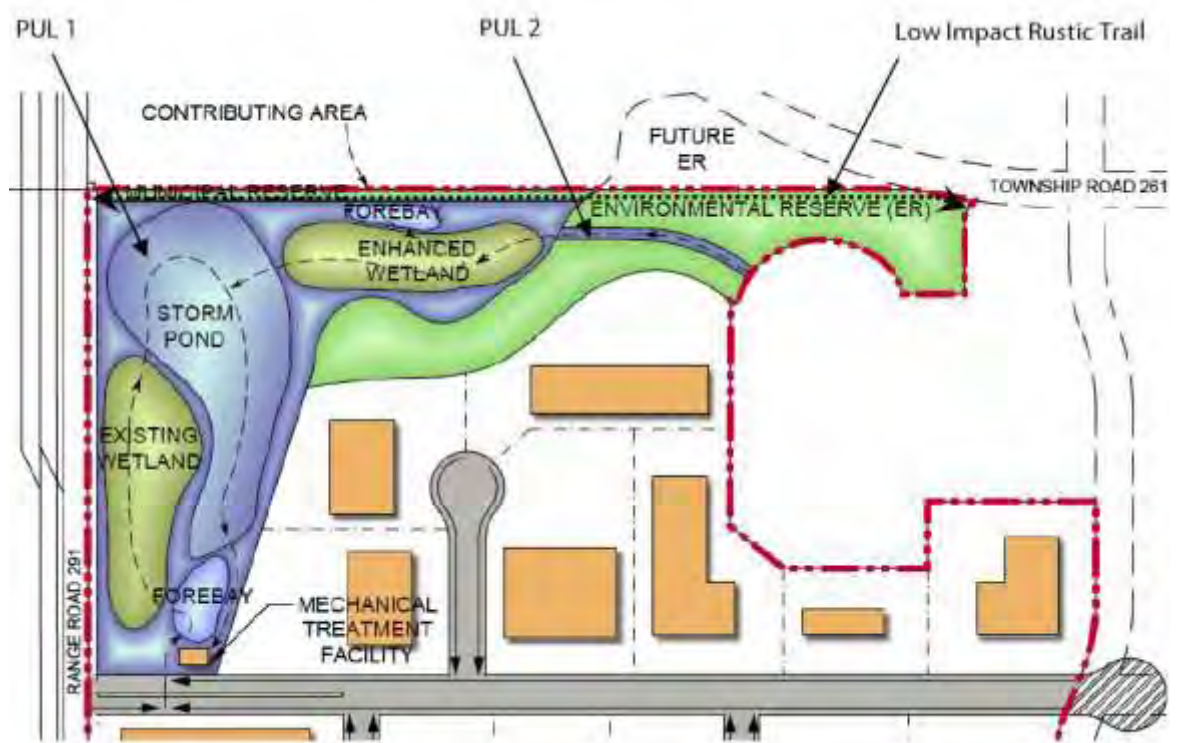
There is no existing natural gas distribution infrastructure in the vicinity of the subject lands. Atco Gas has recently installed a new gate station along Range Road 292, approximately 1.6 km south of Highway 566. Atco Gas is expected to extend natural gas service to the Stage 1 Outline Plan area from this gate station.

All shallow utilities required within the Stage 1 Outline Plan area are expected be located within a common utility easement abutting the public road ROW – except for electrical services which will be located overhead within an alignment inside the road ROW. Specific utility line assignments will be finalized at the subdivision stage as per County requirements.

3.6 Open Space

Open space system within the Stage 1 Outline Plan area will include two (2) Public Utility Lots and an Environmental Reserve (ER).

PUL 1 will contain the integrated stormwater management 'treatment train' process facilities including a sediment forebay, an existing wetland, an enhanced wetland (to be constructed), a stormwater detention pond and a stormwater mechanical treatment plant.



PUL 2 will include a drainage course that will ultimately connect the Stage 1 Outline Plan's storm retention/treatment facilities with the PUL area to be developed in support of the adjacent lands directly to the east. PUL 2 will preserve and enhance the overland drainage connection that presently exists between the two existing wetland complexes situated in the N ½ Sec. 1-26-29-W4M.

The High Plains Industrial Park Environmental Impact Assessment (Westhoff Engineering Resources, Inc., 2009) indicates the natural drainage course that connects two existing wetland areas in the N ½ 1-26-29-W4M contains a mature deciduous tree stand that is likely to contain nesting habitat for birds. As such, this area will be dedicated as Environmental Reserve (ER).

A ± 10 m wide linear Municipal Reserve (MR) will be dedicated along a portion of the northern edge of Stage Outline Plan area. The developer will construct a low impact rustic trail to connect with the regional pathway extending north along Rge Rd 291 (affecting lands north of the Outline Plan area).

Additional MR may be dedicated adjacent to the stormwater management facility to provide opportunity for landscaped enhancements and passive recreational amenities (e.g. picnic tables, rustic trail, etc.). The actual configuration of this MR will be determined at the subdivision stage as per County requirements. A Landscaping Plan will be prepared by the developer to establish the preferred type of landscaping enhancements and passive recreation amenities to the satisfaction of the County.

The balance of outstanding Municipal Reserve affecting lands within the Stage 1 Outline Plan area will be provided by payment of cash-in-lieu of land or deferred to the N ½ Section 12-26-29-W4M in accordance with Sections 667 and 669 of the Municipal Government Act.

The specific configuration of the integrated open space area within the Stage 1 Outline Plan including the PUL's, MR and ER will be determined at the subdivision stage.

3.7 Development Statistics

HIGH PLAINS INDUSTRIAL PARK STAGE 1 OUTLINE PLAN AREA			
	ha(±)	ac(±)	%(±)
Business Lot Area	38.18	94.32	66%
Roads	7.76	19.18	13%
Road Widening	0.58	1.43	1%
Municipal Reserve (MR)	0.4	0.99	1%
Public Utility Lot (PUL)	7.71	19.05	13%
Environmental Reserve (ER)	3.22	7.95	6%
TOTALS	57.85	142.92	100%

3.8 Olds College Industrial Wetland Research Partnership

Olds College is an educational facility committed to researching development challenges through implementation of leading-edge wetlands technologies. Olds College wishes to develop solutions for stormwater quantity and quality issues associated with all forms of development - in accordance with the Provincial Water for Life Strategy.

The proponents of the Stage 1 Outline Plan are also committed to implementing leading-edge treatment wetlands technology in this regard.

As described in the High Plains Industrial Park Conceptual Scheme, and Section 3.3 of this Outline Plan, a unique research & funding partnership will be established as a secondary benefit to the stormwater facilities within the Public Utility Lot (PUL) to enhance Olds College's **ongoing wetland** treatment research.

A partnership between the developer, Olds College and Rocky View County is expected to be implemented within the Stage 1 Outline Plan area summarized as follows:

- The Public Utility Lot (PUL) will be owned by the County. The developer will construct the stormwater management facilities and associated infrastructure necessary to implement the **'treatment train process'** described in the Conceptual Scheme;



- Olds College will conduct ongoing research and undertake educational activities within the PUL facility with respect to the **operational efficiency of the stormwater 'treatment train'**;
- A Utility Corporation will be established to manage the operation of stormwater facility at the subdivision state; and
- A portion of the Utility Corporation's **annual income generated through** the sale of treated stormwater to adjacent industrial users will be donated to Olds College to offset their costs associated with ongoing wetlands research within the PUL facility.

Specific operational details required to implement the **Stage 1 Outline Plan's** stormwater utility and the research partnership will be established at the subdivision stage. The County reserves the right to participate as an active or passive member of this partnership to be established by a Memorandum of Understanding.

A low impact rustic trail may be constructed by the developer within portions of the MR, PUL & ER to provide pedestrian access to the wetland treatment facilities for educational purposes.

3.9 Sour Gas Well Site & Pipeline Rights-of-Way

As discussed in Sec. 2.3, the Stage 1 Outline Plan area includes a Level 1 sour gas production well and related pipeline distribution facilities which are expected to remain in situ for the foreseeable future. As such, these facilities must be incorporated into the proposed subdivision configuration.

The well site is expected to be isolated within a titled lot. At such time **its'** operation is discontinued, the facilities will be removed and the lands appropriately reclaimed by the operator in accordance with all applicable regulatory requirements. In the interim, all buildings within adjacent lots will respect a 100 m setback in accordance with Energy & Resource Conservation Board (ERCB) regulations.

All **pipeline URW's** within the Stage 1 Outline Plan area will be incorporated within pipeline **utility ROW's** included within title to each lot. Wherever possible, parcel boundaries will be designed to straddle pipeline utility ROW's so as to be locate them within required rear and/or side yard building setbacks. Caveats will be registered on each title to advise each owner regarding building restrictions associated with the pipeline ROW's.

3.10 Emergency Response Considerations

The Balzac Sour Gas Processing Plant is situated adjacent to the Stage 1 Outline Plan area. This industrial facility includes an integrated sour gas processing operation supplied by over 120 production wells and associated conveyance pipelines within a 30 km radius of the plant site. The primary product conveyed to the Gas Plant via pipeline is sour gas (hydrogen sulphide or 'H₂S') which is a toxic substance. As mandated by the Energy Resource & Conservation Board (ERCB), Nexen works diligently to safeguard adjacent lands and minimize negative environmental impacts.

Nexen has established an Emergency Planning Zone (EPZ) surrounding the Balzac Gas Plant. They are required to compile and maintain a contact list of all landowners within the EPZ. If an incident at the plant site has potential to impact surrounding lands, Nexen would declare an emergency and initiate contact will all EPZ members to convey evacuation orders.

All lands within the Stage 1 Outline Plan are **situated within the Balzac Gas Plant's** EPZ. As such, all future business lot owners will be added **to Nexen's** call-out list and will be contacted in the event of an incident at the Gas Plant that may require an evacuation or shelter in place.

Anticipated uses within the Stage 1 Outline Plan area may include industrial activities with potential to create offsite impacts in accordance with the Industrial – Industrial Activity **District of the County's Land Use** Bylaw (C-4841-97). Development permit applications for any such use will be referred to the Rocky View Emergency Services Department (and all relevant local, provincial and federal regulatory agencies) to ensure these types of uses do not present safety concerns for adjacent businesses and landowners. Appropriate mitigation measures will be implemented as a condition of development permit approval as required.

4.0 ARCHITECTURAL GUIDELINES

4.1 Overview

The included Architectural Guidelines are intended to establish general expectations for site development within the Stage 1 Outline Plan area.

A Restrictive Covenant referencing the architectural guidelines will be registered as an encumbrance on each title with specific development criteria to be enforced by **business lot owner's associates to be created** by the developer – to be referred to as the High Plains Development Association (HPDA). Lot owners will be required to submit site and building design plans to the HPDA for review and approval prior to seeking approvals from Rocky View County. Lot owners will be expected to provide an endorsement from the HPDA to Rocky View County with an application for development permit and/or a building permit.

4.2 Building Orientations

All site and building design layouts will conform to the regulations listed in Section 69 Industrial – Industrial Activity District (I-IA) contained within Rocky View County Land Use Bylaw C-4841-97.

Buildings will be oriented so that the main building entrance will face the public road that provides direct access to same.

The design of building facades facing the public roads will incorporate variations in finish and colour, offsets, and pedestrian scaling techniques designed to visually break the building mass and provide a high quality appearance.

The use of “green” building techniques which result in enhanced energy efficiencies will be encouraged.



4.3 Parking & Loading

Public parking will be located near the front of each lot and will be situated in general proximity to the main entry of each building.

Large-scale parking, service and loading areas will be located at the sides and/or rear of the building and will be screened from adjacent properties and public roads.



Open outdoor storage areas will be screened from adjacent properties and public roads. Any materials to be stored outside, that are not located within an enclosed building, will be placed and maintained in an orderly fashion at all times.

Wherever possible, parking and outdoor storage areas will incorporate LID principles including the use of permeable paving, on-site stormwater detention and treatment areas, and vegetated swales to **implement 'source control'** stormwater best management **practices aimed at 'pre-treating'** surface drainage prior to its release into the roadside ditch system.

4.4 Fencing

Fencing at the front of buildings will be discouraged. If so required for visual screening and/or security purposes, it will include a low-scale, decorative design and be located behind street side landscaping in accordance with the requirements of the County's Land Use Bylaw.



Perimeter fencing situated at the side & rear property boundaries will be utilized to provide screening that reduces negative visual impacts of outdoor storage and large scale parking when viewed from public roads and adjacent parcels.

All fencing will include high quality materials and be designed and constructed to maintain its' original quality over time without the requirement of frequent maintenance.

4.5 Lighting

All exterior lighting within each private lot will consider “dark sky” principles to mitigate light pollution.

All exterior light fixtures will include full cut-offs to direct light downward and prevent light spillage from the Stage 1 Outline Plan area onto adjacent lands (and to the sky).

4.6 Signage

Lot and building signage will include a high quality design & construction compatible with the overall building design (and will be sized in accordance with the requirements of the County’s Land Use Bylaw).

No back-lit signage will be permitted within the Stage 1 Outline Plan area.

4.7 Landscaping

As discussed in Sec. 3.4, a landscaped easement area will be established abutting all public roads within the Stage 1 Outline Plan area.

A Landscaping Plan will be prepared by a qualified professional to detail the type and extent of plantings within the landscaped easement area, as a condition of subdivision. Plant materials must be appropriate for the Calgary region and include an overall uniform theme and design which is complimentary to the aesthetic of the industrial development.



All landscaped improvements within this easement area will be provided by the developer and maintained by the High Plains Development Association (HPDA). The HPDA will ensure the landscaped easement area is adequately irrigated during precipitation-free periods (by using treated stormwater supplied within the development as described by Section 3.3). Use of potable water to irrigate the landscaped area within the Stage 1 Outline Plan shall be prohibited.

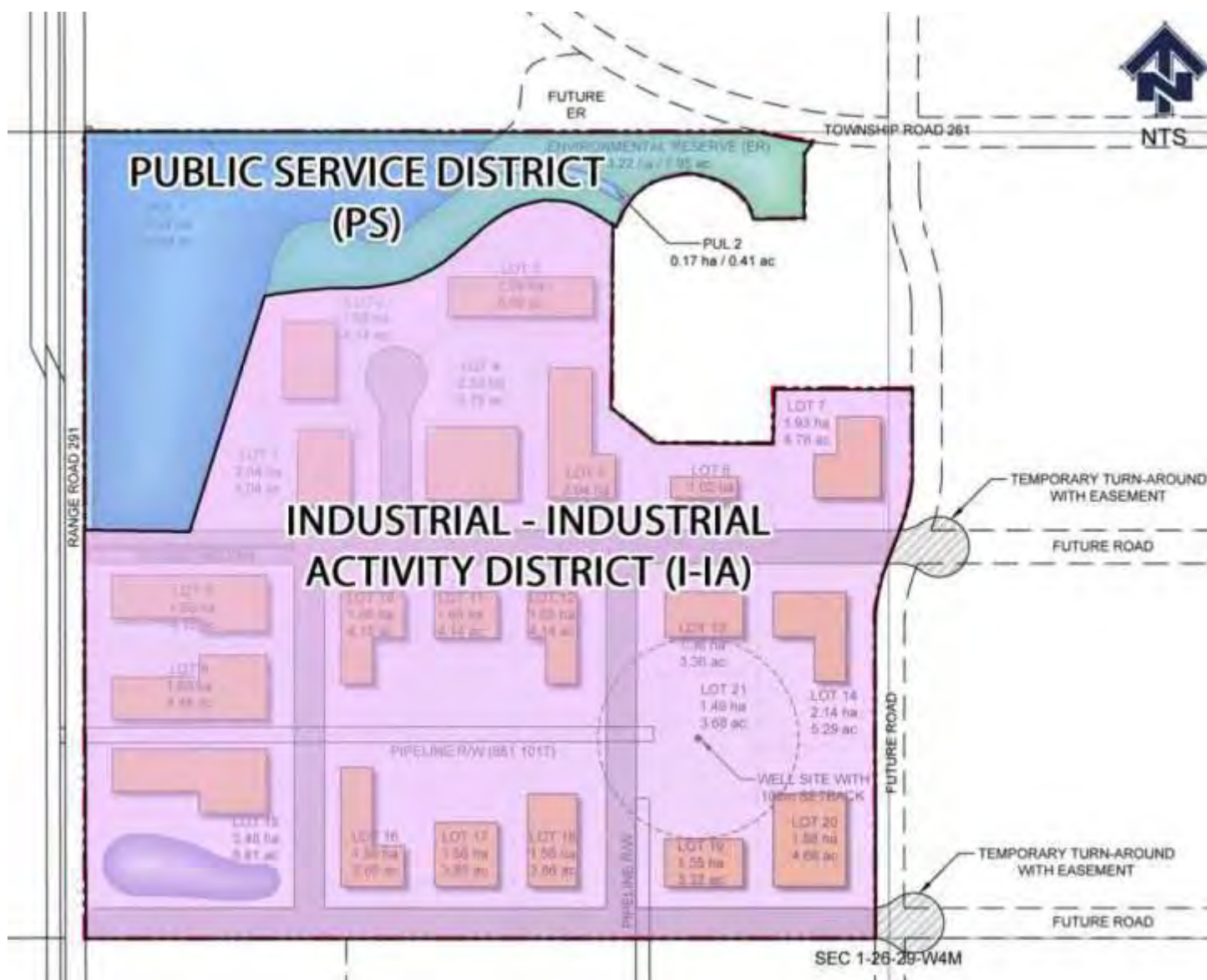
As per Section 4.4.3.d(iii) of the Balzac East Area Structure Plan, a minimum of 10 % of each business lot will be landscaped. A Landscaping Plan will be prepared by a qualified professional at the development permit stage to establish how the 10 % landscaped requirement will be implemented. The landscaped easement

area abutting the public roadways will be included in the calculation to determine the 10% area requirement.

5.0 IMPLEMENTATION

5.1 Land Use

The Stage 1 Outline Plan area is expected to be redesignated Industrial – Industrial Activity District (I-IA) and Public Service District (PS) in accordance with the County's Land Use Bylaw C-4841-97.



5.2 Subdivision Phasing

Implementation of subdivision within the Stage 1 Outline Plan area is anticipated to proceed in multiple phases.

The initial subdivision phase will include the PUL facility which will be constructed by the developer to secure positive surface drainage within the Plan area and generally establish grades across the site. The proposed Municipal Reserve, Environmental Reserve and Road Widening will also be dedicated. All road right-of-ways within the Outline Plan area may be dedicated at the first subdivision phase, however, only a portion of the northern internal roadway is expected to be constructed. Temporary turn-arounds will be provided at the end of all roadways. The area situated north of the initial road construction (adjacent to the PUL) is expected to include a variety of smaller parcels while one or several larger parcels are anticipated south of the road. All required transportation and utility servicing infrastructure will be implemented by a Development Agreement Process as per County requirements.

Subsequent phasing of subdivision is expected to infill the larger subdivision blocks within the southern portion of the Outline Plan area with a variety of parcel sizes and configurations as provided by the I-IA land use district. Transportation and utility service infrastructure will be constructed by the developer as per Development Agreements associated with each subdivision phase.

5.3 Supplemental Technical Studies (required at each subdivision phase)

As condition of each phase of subdivision, the developer will provide the following technical documentation to be prepared by a qualified professional:

- An update to the Traffic Impact Assessment (Bunt & Associates, 2009);
- An update to the Servicing Design Brief (Focus, 2010);
- An update to the 2009 Staged Master Drainage Plan (Westhoff Engineering Resources, Inc., 2009);
- A Landscaping Plan (affecting the **private landscaping easement area abutting public road ROW's** and lands to be dedicated as MR);
- Confirmation of developer-imposed Architectural Controls in accordance with Section 4.0 of this Outline Plan and Section 4.4 of the Balzac East Area Structure Plan;
- A Road Naming Application; and
- Other requirements as may be required by the County.

5.4 High Plains Development Association (HPDA)

The developer will establish the High Plains Development Association (HPDA) as condition of subdivision. The HPDA will be responsible for:

- Maintaining the private landscaping easement area abutting all public roads within the Stage 1 Outline Plan area;
- Managing a contract with a waste management provider (for refuse);
- Reviewing all applications for development permit and/or building permit to ensure compliance with the High Plains Industrial Park architectural control guidelines;
- Maintaining an updated contact list of all business owners within the Stage 1 Outline Plan area as may be required by Nexen in accordance with ERCB regulations governing Emergency Planning Zone criteria; and

- Collecting all necessary fees from each member Lot Owner to provide for implementation of aforementioned responsibilities.

APPENDIX C

STAGE 2 OUTLINE PLAN

HIGH PLAINS INDUSTRIAL PARK STAGE 2 OUTLINE PLAN

ADOPTED SEPTEMBER 11th, 2012



**ROCKY VIEW COUNTY
BYLAW C-7189-2012**

A Bylaw of Rocky View County to amend Bylaw C-6930-2010, being the High Plains Industrial Park Conceptual Scheme

WHEREAS the Council deems it desirable to amend the said Bylaw, and

WHEREAS the Council of Rocky View County has received an application to amend Bylaw C-6930-2010 by appending the "Stage 2 Outline Plan." The amendment provides a framework for subsequent redesignation, subdivision and development within the SW-12-26-29-W4M and a portion of the SE-12-26-29-W4M as shown on the attached Schedule 'A', the "Stage 2 Outline Plan"; and

WHEREAS Council held a Public Hearing and has given consideration to the representations made to it in accordance with Section 692 of the Municipal Government Act, being Chapter M-26 of the Revised Statutes of Alberta, 2000, and all amendments thereto.

NOW THEREFORE the Council enacts the following:

1. That Bylaw C-6930-2010 be amended by appending the "Stage 2 Outline Plan," attached hereto as Schedule 'A', in order to provide a framework for subsequent redesignation, subdivision and development within the SW-12-26-29-W4M and a portion of the SE-12-26-29-W4M as shown on the attached Schedule 'A', forming part of this Bylaw; and
2. The Bylaw comes into effect upon the date of its third reading.

Division: 7
File: 06412002/006 – 2012-RV-049

First reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on July 24, 2012, on a motion by Councillor Habberfield.

Second reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on September 11, 2012, on a motion by Councillor Habberfield.

Third reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on September 11, 2012, on a motion by Councillor Solberg.

REEVE OR DEPUTY REEVE

MUNICIPAL CLERK

SCHEDULE 'A'

The Stage 2 Outline Plan for the High Plains Industrial Park is distributed under separate cover.

ACKNOWLEDGEMENTS

OWNERS/DEVELOPERS:



NORMAN DAMKAR



THE PROJECT TEAM:

B&A Planning Group
Bunt & Associates

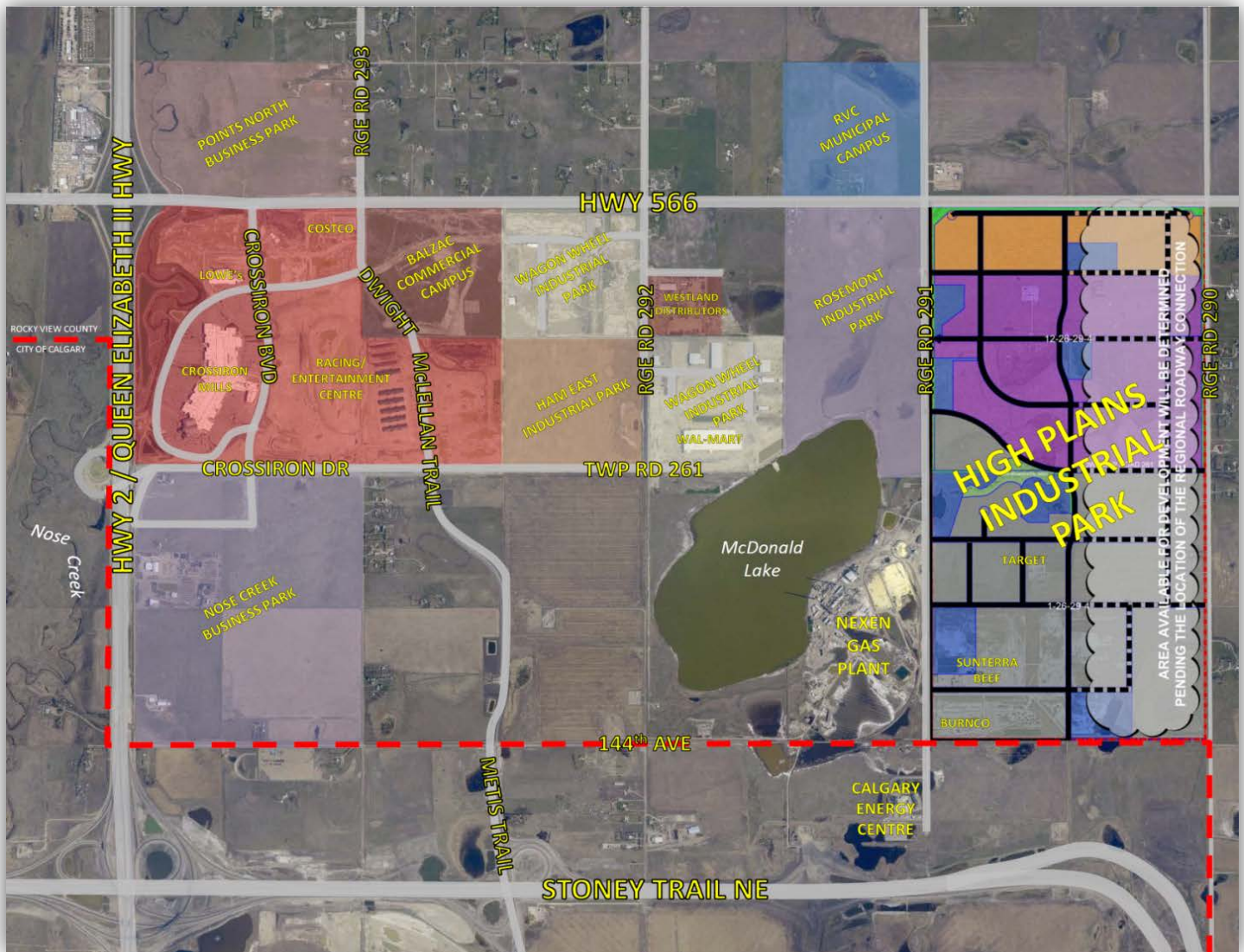
IBI Group
Lawson Projects

Sweetgrass Consultants Ltd.
Westhoff Engineering Resources Inc.

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EAST BALZAC CONTEXT



The High Plains Industrial Park Conceptual Scheme (HPIP CS) is a 'broad-based' policy that sets general expectations to guide implementation of comprehensively-planned industrial and commercial business developments within the eastern portion of the East Balzac area.

The HPIP CS establishes a general land use concept proposing industrial land uses within the southern portion of the site, a mix of industrial/business park uses within the central portion, and regional commercial developments within the northern portion (adjacent to Hwy 566).

The HPIP CS generally restricts development within the eastern portions of the site pending confirmation of the alignment and access management criteria for the anticipated northern extension of the Stoney Trail NE by Alberta Transportation.



PURPOSE OF THE OUTLINE PLAN

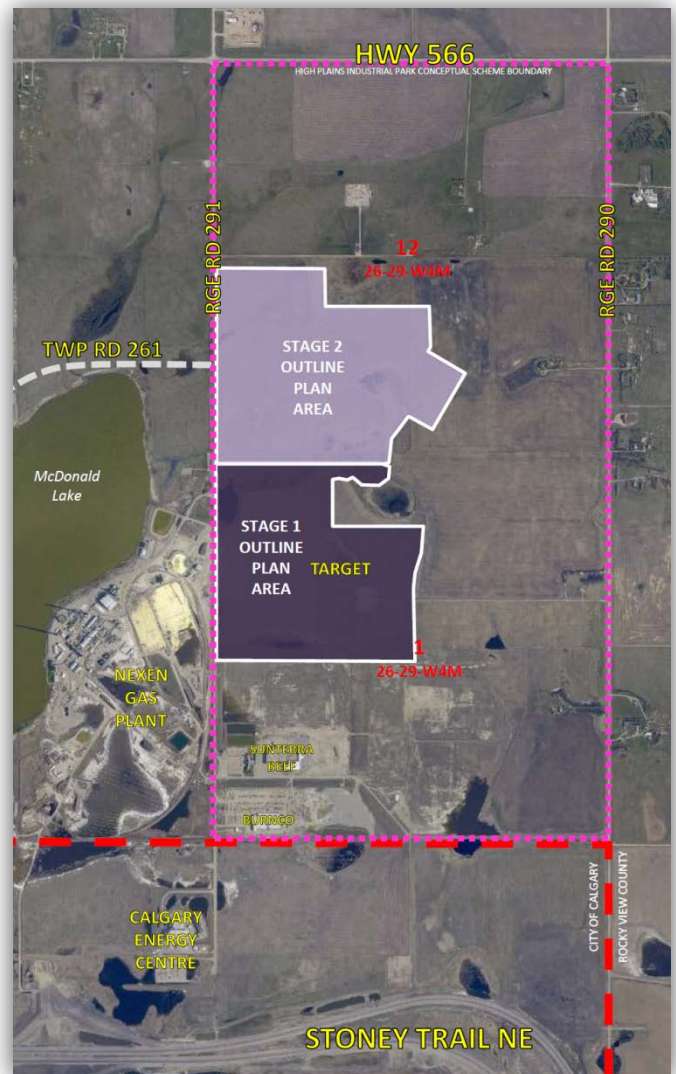
As per the requirements of Section 10 of the High Plains Industrial Park Conceptual Scheme, an Outline Plan must be prepared at each phase of development prior to Council approving land use redesignation and subsequent subdivision within the proposed development area.

The Outline Plan must address the following:

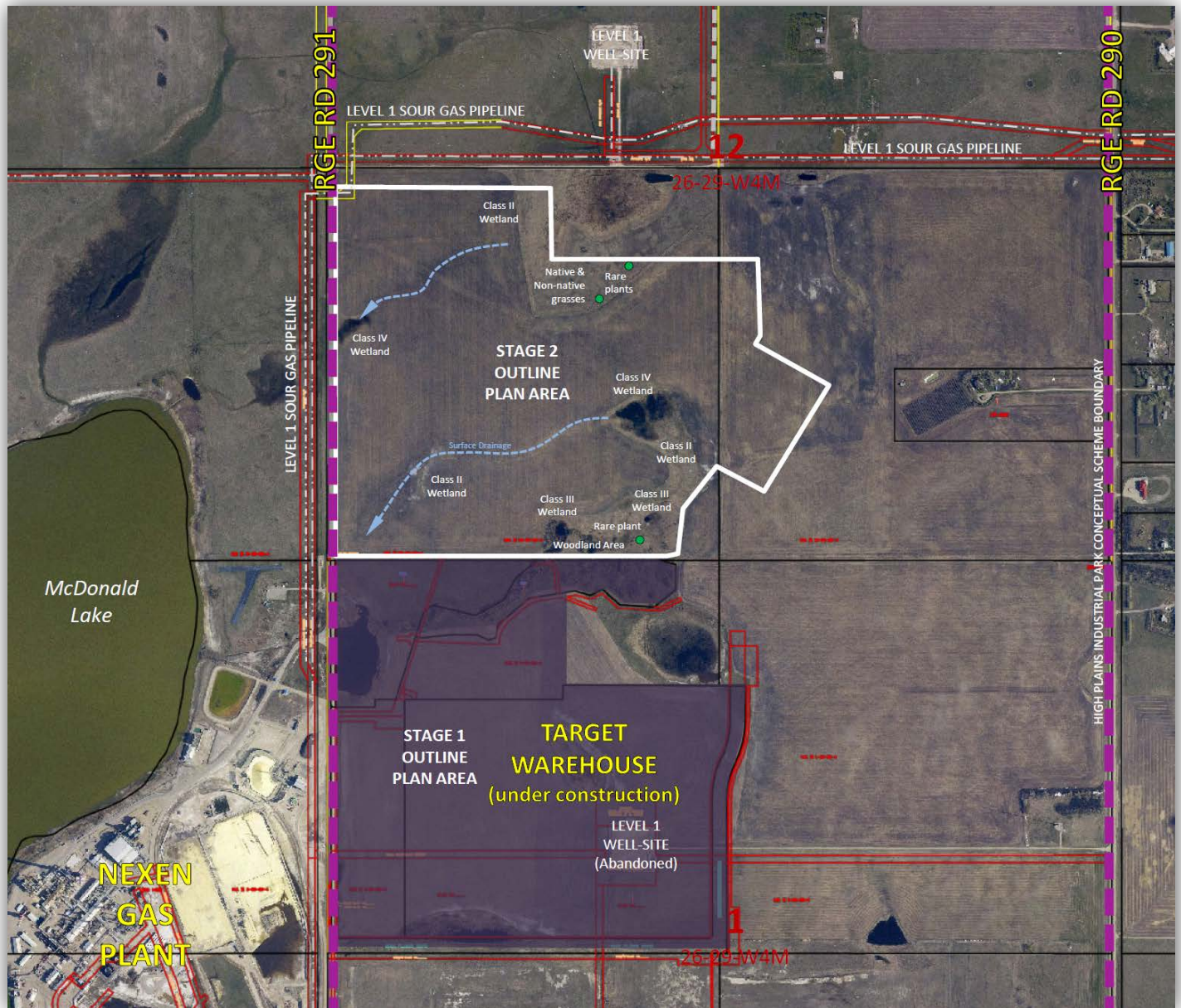
- A Land use amendment required to implement development within the Outline Plan boundary;
- Anticipated phasing of subdivision within the Outline Plan including proposed parcel sizing & configurations;
- An update to the High Plains Industrial Park Master Utility Servicing Plan as required to describe proposed implementation of utility service infrastructure within the Outline Plan area;
- An update to the High Plains Industrial Park Traffic Impact Assessment to describe proposed implementation of local and regional roadway improvements as required within the Outline Plan area;
- An update to the High Plains Industrial Park Staged Master Drainage Plan to describe proposed implementation of stormwater infrastructure as required within the Outline Plan area;
- Where public recreation areas are proposed, a Landscaping Plan to detail how improvements will be implemented within Municipal Reserve parcels;
- An update to the High Plains Industrial Park Staged Master Drainage Plan to describe proposed implementation of stormwater infrastructure as required within the Outline Plan area;
- Where public recreation areas are proposed, a Landscaping Plan to detail how improvements will be implemented within Municipal Reserve parcels;
- Clarification of how emergency response considerations can be implemented within the Outline Plan area; and
- A description of performance standards and controls to implement a specific architectural theme, parking & loading considerations, fencing, screening & buffering considerations, signage & lighting elements, etc. within the Outline Plan area.

The Stage 2 Outline Plan addresses these development considerations pursuant to the requirements of the High Plains Industrial Park Conceptual Scheme.

The Stage 2 Outline Plan will require adoption by Council, by Bylaw, and will be subsequently appended into to the High Plains Industrial Park Conceptual Scheme.



EXISTING SITE CONDITIONS



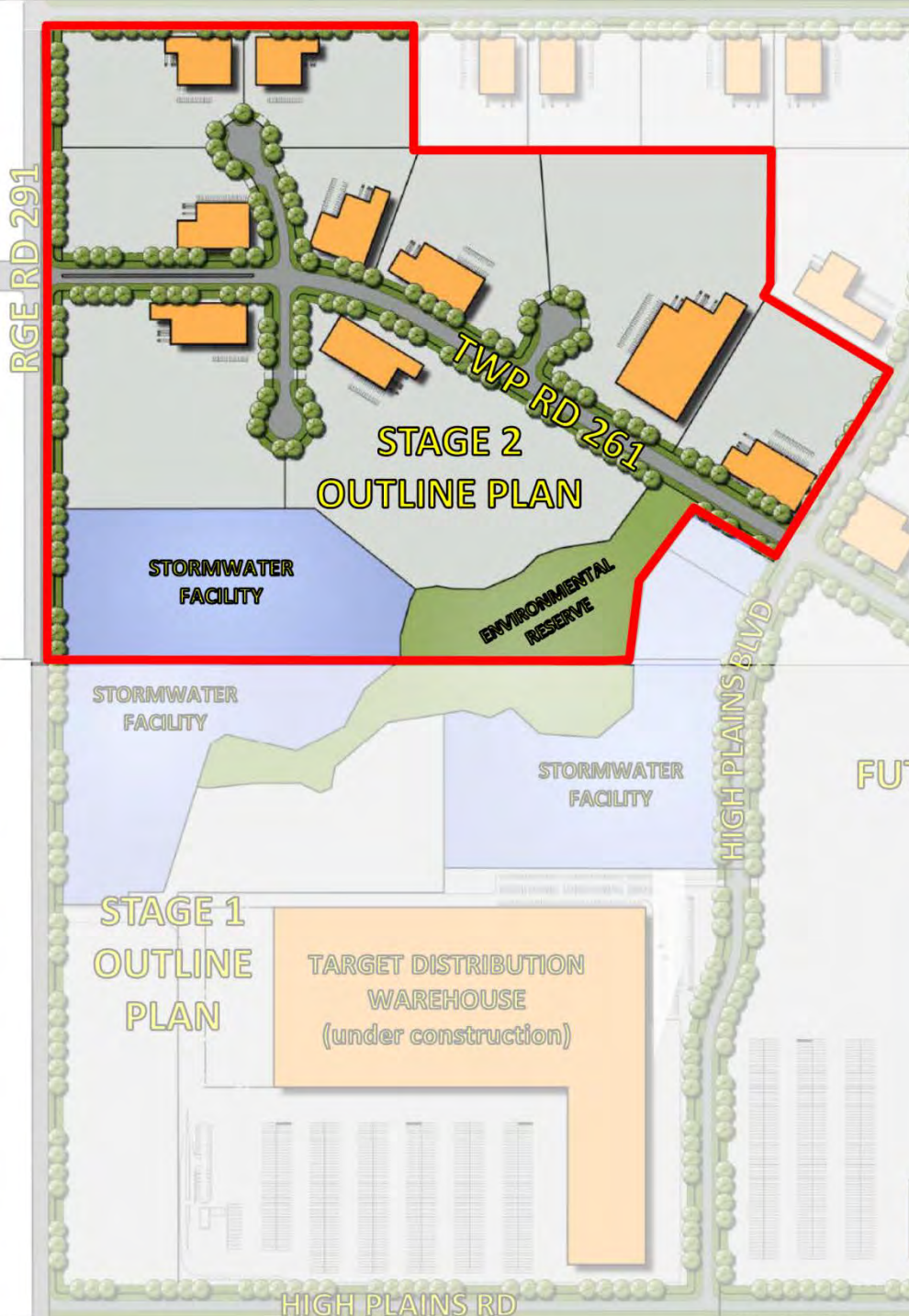
The Stage 2 Outline Plan area includes ± 154 ac (± 62 ha) within portions of SW & SE 12-26-29-W4M owned by Highfield Stock Farm Inc., CLT Contracting Ltd., Maxima Developments Inc. and Norman Damkar.

The site is undeveloped and has flat to gently-rolling topography sloping to the southwest. As determined by the **Biological Impact Assessment (Sweetgrass Consultants Ltd. September, 2011)**, the site contains a number of existing wetlands, native & non-native grasslands, a woodland area and rare plant species. The developer acknowledges disturbance to wetland areas must be endorsed by Alberta Sustainable Resource Development & Alberta Environment. Disturbances to native grasses and rare plant species will be mitigated by relocating top soil and vegetative materials into designated environmental areas to be established within the site.

Existing sour gas facilities (Nexen) situated directly west and north of the site include Level 1 pipelines that do not require building setbacks. The Level 1 pipeline crossing the extreme northwest portion of the site will be integrated with the configuration of subdivision and building orientations in that area.

STAGE 2 DEVELOPMENT CONCEPT

FUTURE OUTLINE PLAN AREA



The Stage 2 Outline Plan is expected to contain nine (9) industrial lots sized between ± 5 and ± 24 ac.

Access will be from Rge Rd 291 via Twp Rd 261 – a primary collector road to be constructed by the developer to service an internal subdivision road network with approaches to each lot.

The Plan area will be fully-served by the County's regional potable water & wastewater infrastructure. The developer will construct a utility distribution network within the site as required to extend municipal services. The developer will also provide all applicable infrastructure levies, assessments and contributions to the County.

Stormwater management will be accommodated within a Public Utility Lot (PUL) in the southwest corner of the site. The drainage facilities within the Stage 2 Outline Plan will be integrated with those being constructed by the developer within the Stage 1 Outline Plan immediately south of the site.

An Environmental Reserve (ER) will be dedicated within the southeast portion of the site to preserve existing wetlands & wooded areas. The alignment of the ER will abut an existing Environmental Reserve parcel within the Stage 1 Outline Plan.

A linear Municipal Reserve (MR) will be dedicated along the Rge Rd 291 frontage. The developer will construct a pathway within the MR at the subdivision stage. The alignment of the proposed MR will connect with an existing linear MR within the Stage 1 Outline Plan area.

FUTURE OUTLINE PLAN AREA

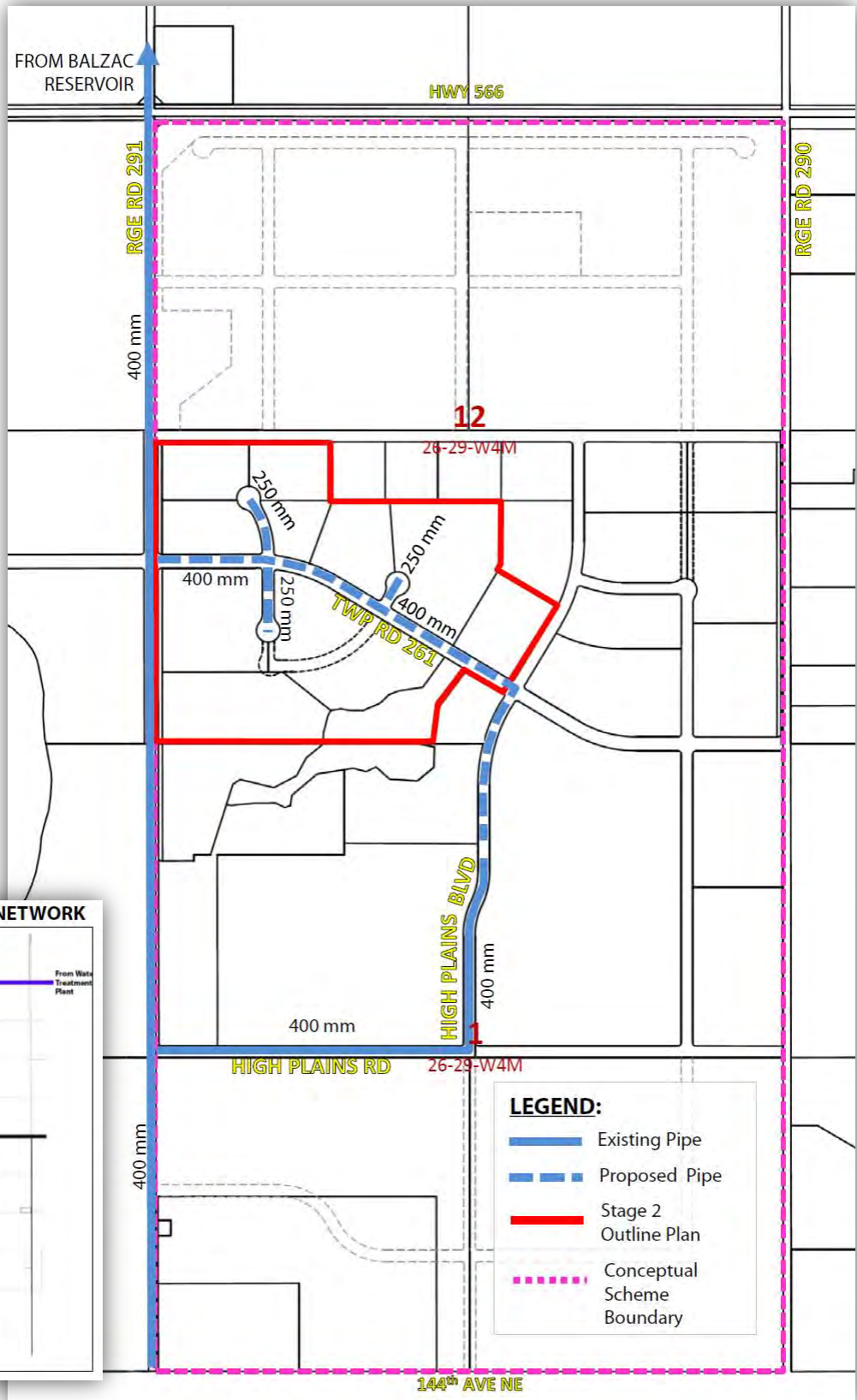
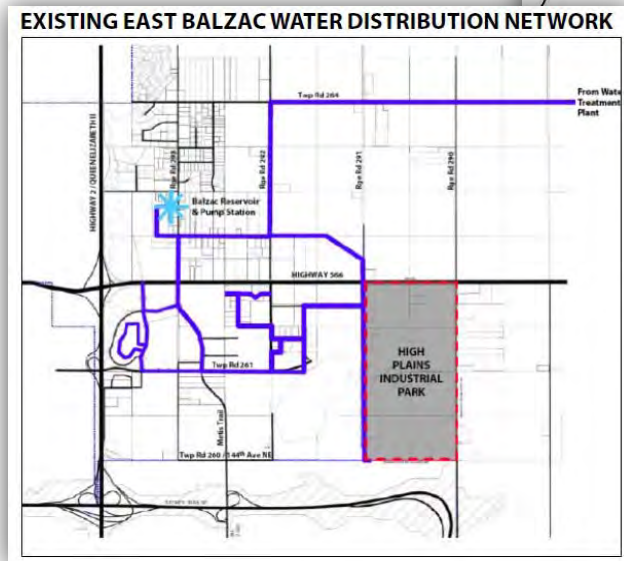
POTABLE WATER SERVICE

Potable water will be supplied by the East Rocky View Water system.

The developer will construct a potable water distribution network within the site as conceptually illustrated and further described by the **HPIP Stage 2 Outline Plan Utility Servicing Design Brief (IBI Group, March 2012)**.

Water conservation will be implemented through metered consumption and mandatory potable water restrictions. The design of the water distribution network will accommodate fire suppression in accordance with applicable regulatory requirements.

The developer will contribute the applicable payment for the Water Contribution Agreement at the subdivision stage. Where the developer implements potable water infrastructure in support of the Stage 2 Outline Plan that benefits surrounding properties, the County's Infrastructure Cost Recovery policy will apply.



WASTEWATER SERVICE

Wastewater service will be provided by the East Rocky View Wastewater Transmission Line (ERVWWTL).

The HPIP Conceptual Scheme area includes two Sanitary Basins: the North Basin and the South Basin. The North Basin is further delineated into two (2) sub-basins and the South Basin into seven (7) sub-basins as illustrated.

Wastewater generated within the South Basin will be directed to the existing Rancher's Beef Lift Station.

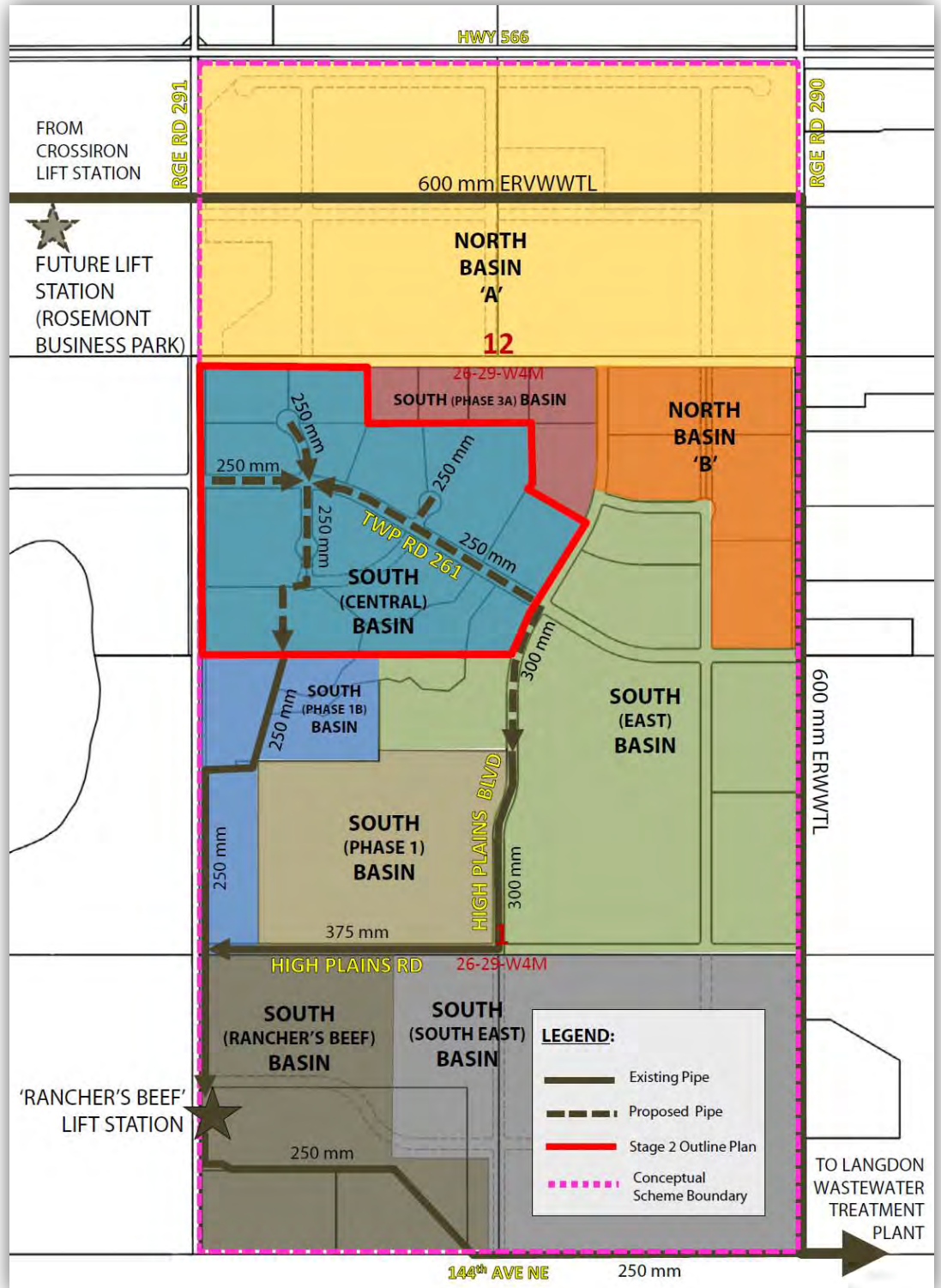
Wastewater in the North Basin will be directed to a Lift Station to be constructed in the Rosemont Business Park – which will be designed with capacity to service the North Basin and adjacent lands to the west of the HPIP CS area.

The developer will construct a wastewater collection network within the site as conceptually illustrated and further described by the **HPIP Stage 2 Outline Plan Utility Servicing Design Brief (IBI Group, March 2012)**.

A detailed sanitary analysis will be completed by the developer at the subdivision state to confirm capacity of the Rancher's Beef Lift Station. As required, any necessary upgrades will be completed by the developer.

The developer will contribute the applicable payment for the County's Wastewater Offsite Levy at the subdivision stage.

Where the developer implements wastewater infrastructure in support of the Stage 2 Outline Plan that benefits surrounding properties, the County's Infrastructure Cost Recovery policy will apply.



STORMWATER MANAGEMENT

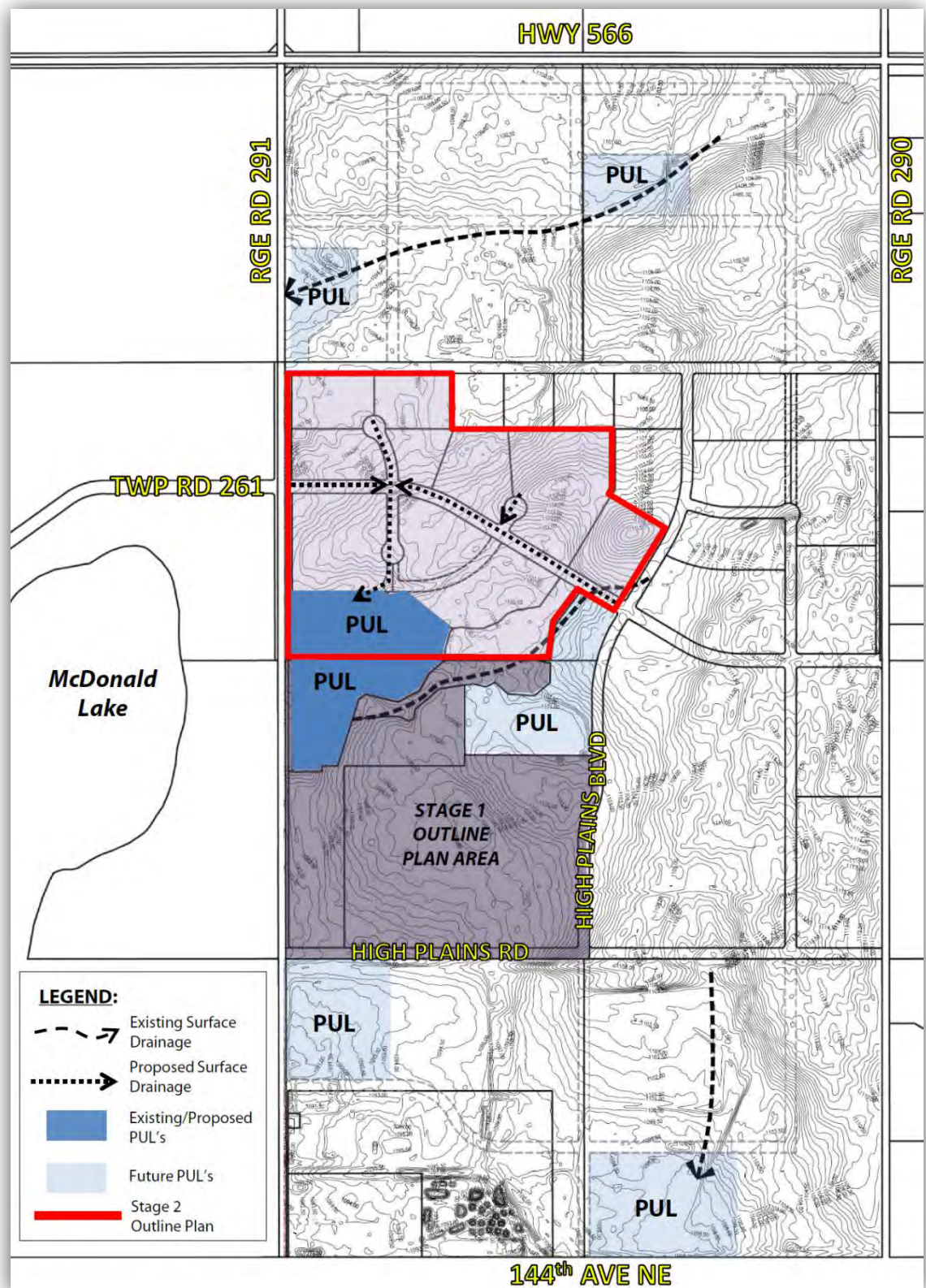
Stormwater management in the Stage 2 Outline Plan Area will be accommodated by surface drainage facilities to be constructed by the developer as conceptually illustrated and further described by the **Stormwater Management Plan for the Stage 2 Outline Plan (Westhoff Engineering Resources Inc., 2012)**.

Surface drainage generated within the site will be conveyed to a Public Utility Lot (PUL) to be constructed in the southwest portion of the site.

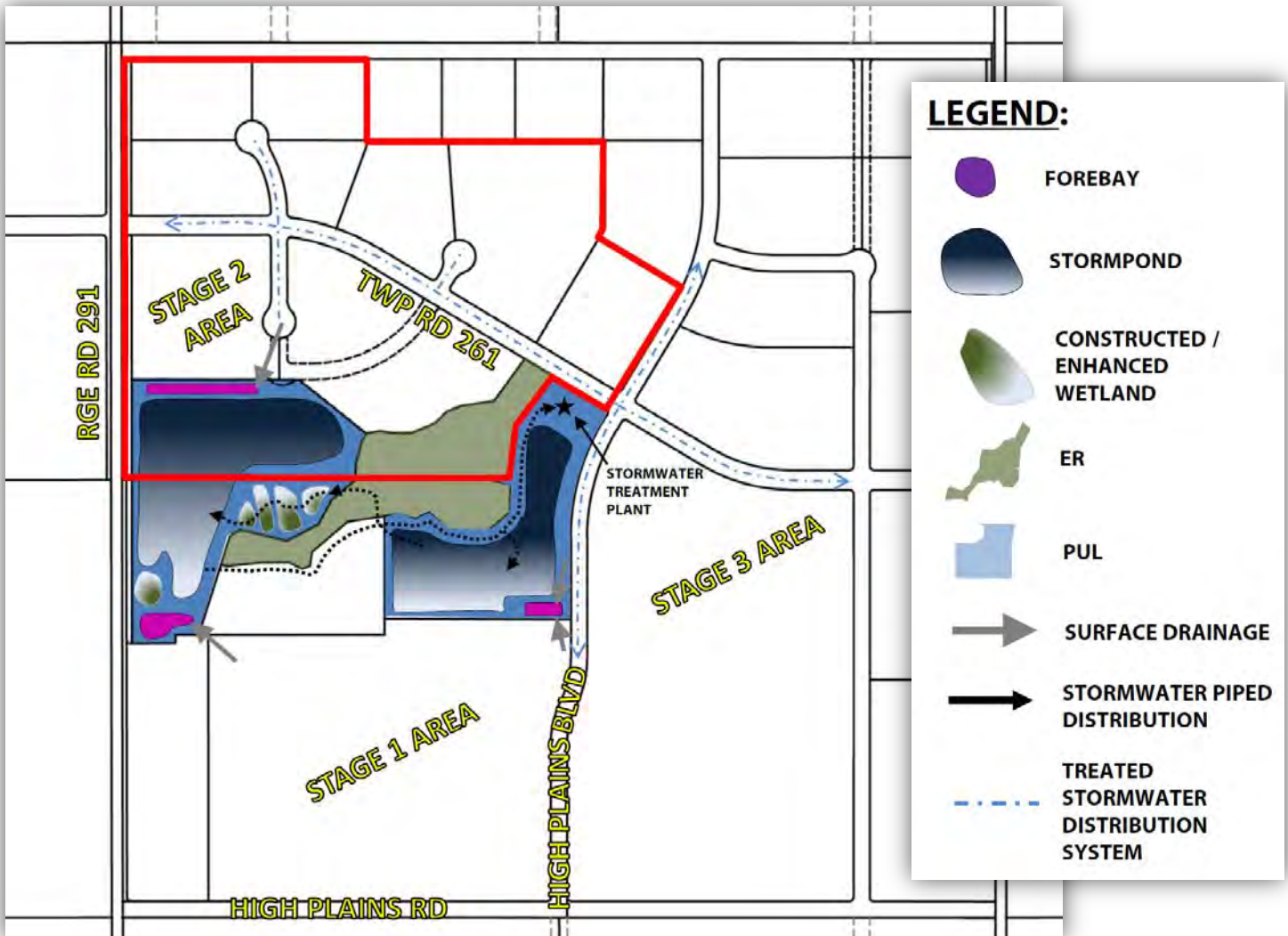
The design of the PUL will include capacity to retain 100 % of surface drainage generated within the site without the requirement of a downstream discharge.

The stormwater management analysis assumes the occurrence of extended periods of precipitation leading to a greater than a 1:100 year storm event and was measured using a continuous modeling simulation.

In the event existing wetlands are disturbed within the site, approvals for same will be required from Alberta Environment and Alberta Sustainable Resource Development



STORMWATER MANAGEMENT



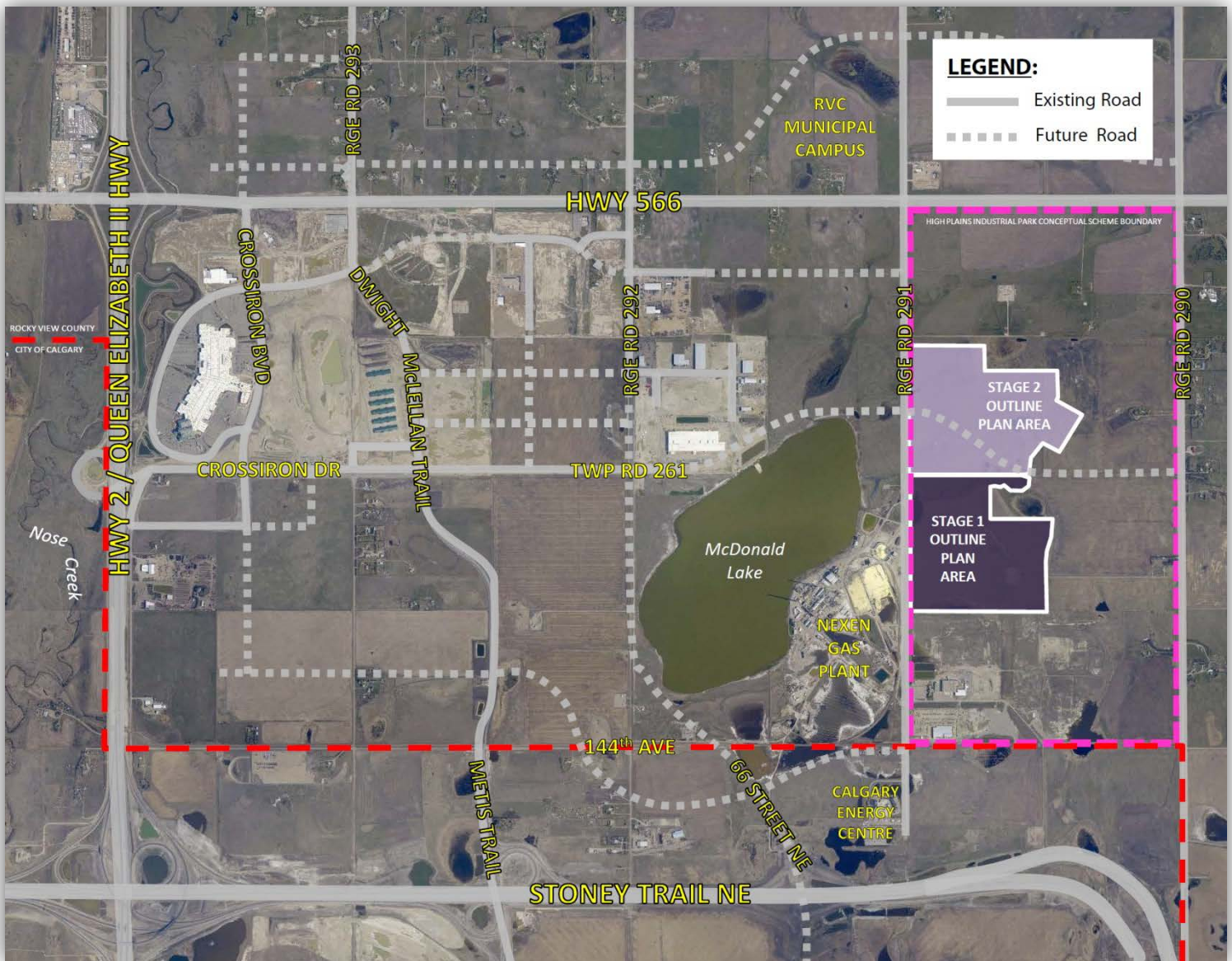
Stormwater management facilities within the PUL will include a forebay, an enhanced wetland and a detention pond designed to implement a 'treatment train' intended to improve surface water quality via natural filtration processes. The Stage 2 Outline Plan's PUL will be combined with similar facilities being constructed in the Stage 1 Outline Plan area (and with facilities to be constructed within future Outline Plan areas). Stormwater will be disposed of using operational considerations such as enhanced evaporation techniques and for irrigation of landscaped areas and adjacent agricultural lands. An internal distribution system will be installed by the developer to convey treated stormwater for these purposes.

As per the framework established by the HPIP CS, a mechanical treatment plant will be constructed by the developer so that treated stormwater may be used to provide industrial lot owners with a non-potable water supply for consumptive processing activities, thereby augmenting the County's potable water system. A Utility Corporation will likely be established to own and manage the treated stormwater distribution system, to the satisfaction of the County and applicable regulatory authorities.

As described in the HPIP Conceptual Scheme, the developer is working with Olds College to implement a cold climate wetland research facility within the HPIP area. Specific operational details required to implement this research partnership will be established at the subdivision stage. The County reserves the right to participate as an active or passive member of this partnership.

TRANSPORTATION

EAST BALZAC TRANSPORTATION NETWORK



The East Balzac Transportation Network has been designed to facilitate access between the County's primary business and employment area and the surrounding regional road network.

The County has determined specific requirements for the East Balzac transportation network in accordance with a Global Traffic Impact Assessment (HDR | iTrans, December, 2010). All development in the High Plains Industrial Park (HPIP) must rationalize specific transportation requirements within the context of the County's Global TIA.

The Stage 2 Outline Plan area will be accessed from Hwy 566 via Rge Rd 291. Twp Rd 261 will also provide a direct access from the Plan area to Hwy 2 and indirect access to Stoney Trail NE via Metis Trail.

The segment of Twp Rd 261 between Rge Rd 291 & 292 is expected to be available by 2013.

TRANSPORTATION

HIGH PLAINS INDUSTRIAL PARK CONCEPTUAL ROAD NETWORK

The High Plains Industrial Park Conceptual Scheme contemplates a conceptual road network as illustrated.

This road network is to be implemented by the developer in accordance with a staged program of transportation improvements as contemplated by each Outline Plan.

The developer is presently constructing segments of High Plains Rd & High Plains Blvd, upgrading the Hwy 566 / Rge Rd 291 intersection and widening portions Rge Rd 291 – as a condition of subdivision for the Stage 1 Outline Plan area.

The **High Plains Industrial Park Stage 2 Traffic Impact Assessment (Bunt & Associates Engineering, 2012)** was prepared to support this Outline Plan.

The TIA concludes that, with specific improvements to Rge Rd 291 and the intersection at Hwy 566, the Stage 2 Outline Plan road system will function within acceptable capacity parameters.

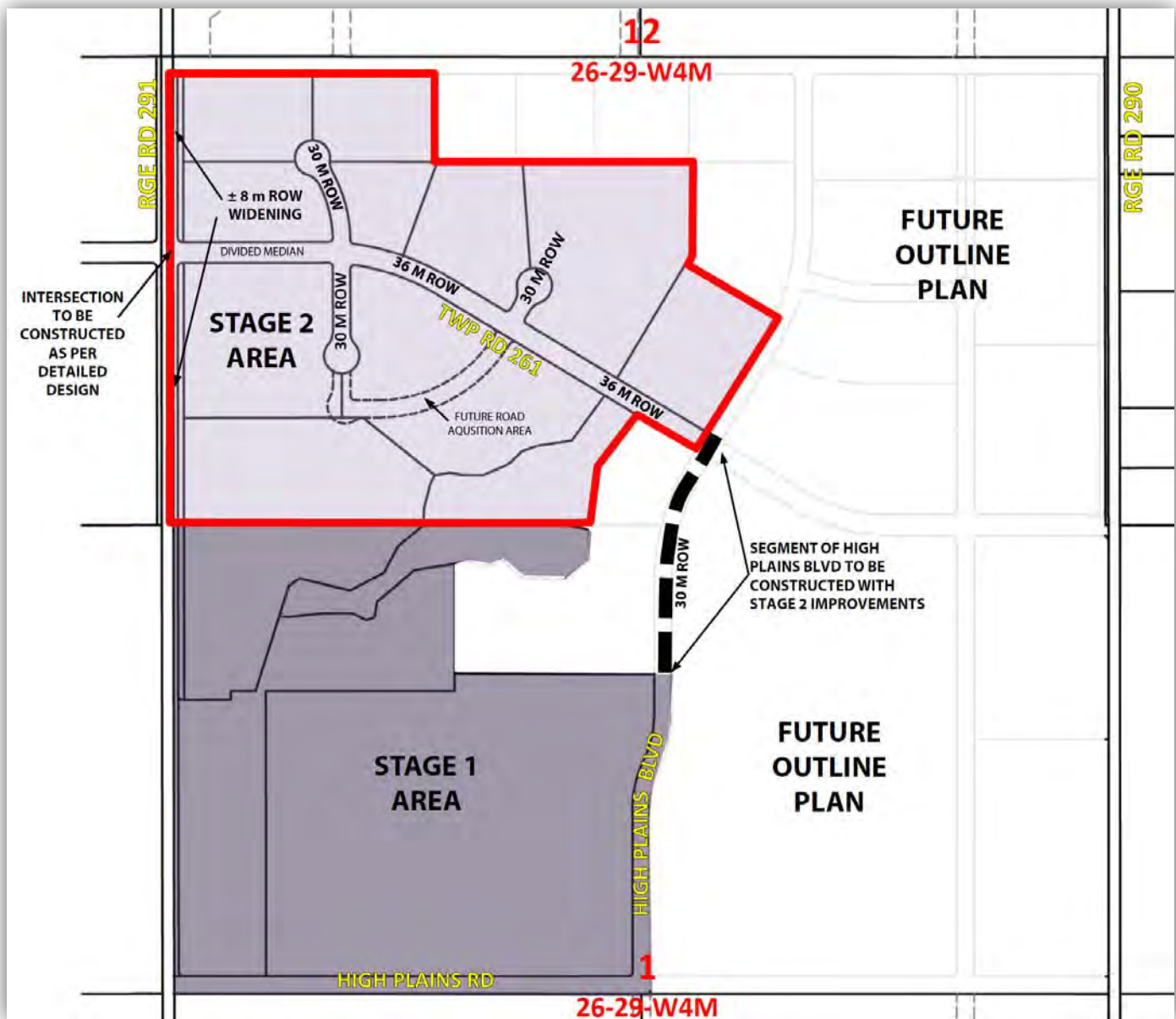
Twp Rd 261 will be a 'primary collector' within the Stage 2 Outline Plan area and will service an internal subdivision road system. Direct access from lots abutting Twp Rd 261 will be restricted and spacing of intersections will be expected to maintain a minimum 300 m offset.



All roadways will be constructed by the developer in accordance with the County's engineering requirements. ROW's will include rural cross-sections with ditching to convey surface drainage.

TRANSPORTATION

STAGE 2 OUTLINE PLAN ROAD NETWORK



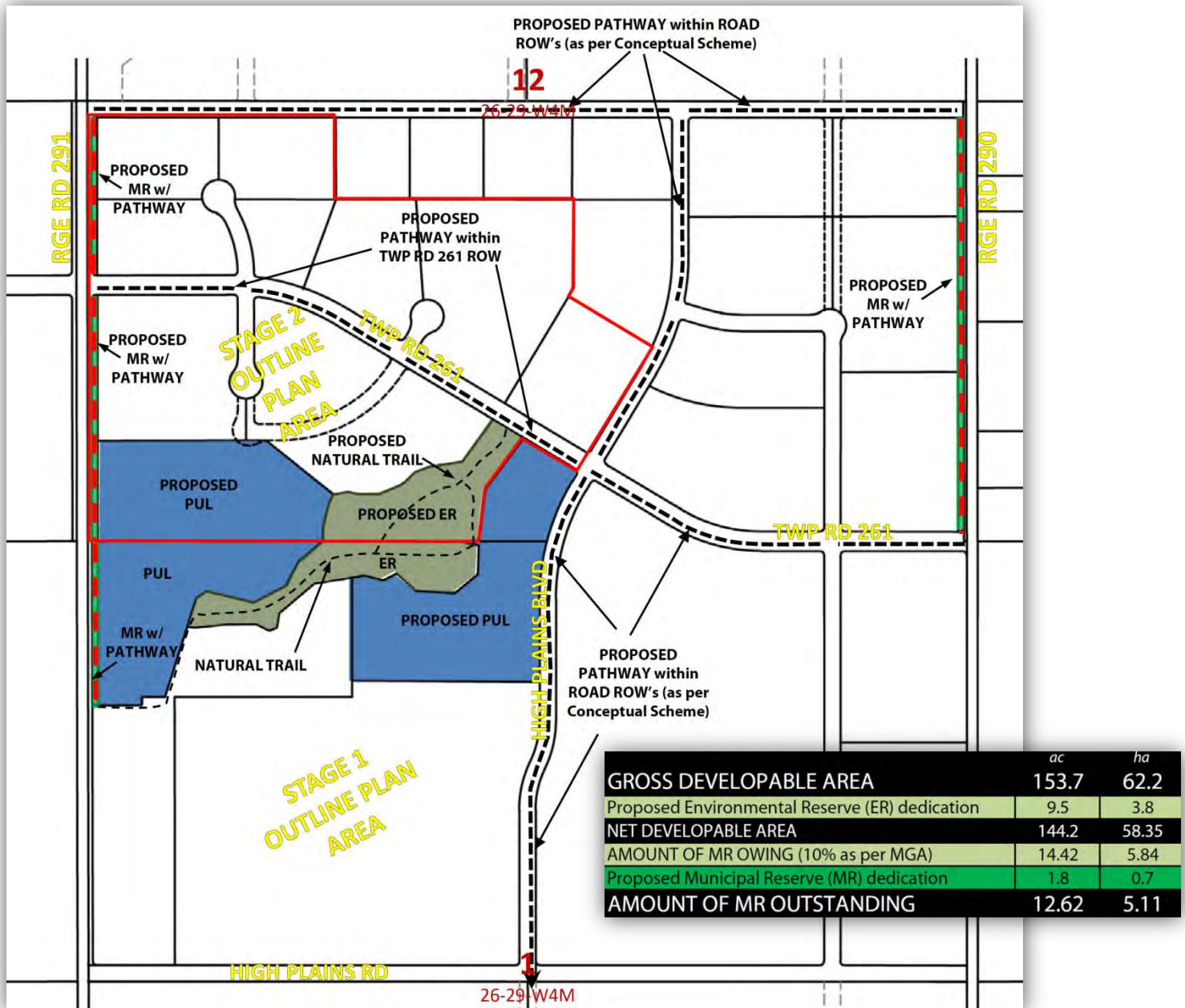
Twp Rd 261 will include a 36 m ROW. A divided median will be installed in the road ROW between Rge Rd 291 and the first internal subdivision road intersection in order to segregate east/west travelling lanes for emergency access purposes.

All internal subdivision roads will be constructed within 30 m ROW's.

A Future Road Acquisition Agreement will be registered against the affected titles to facilitate potential roadway construction in the event the initial parcel configuration may be intensified via future subdivision.

The segment of High Plains Blvd will be constructed by the developer with the Stage 2 transportation improvements.

Road ROW dedication (± 8 m) for widening purposes will be provided along the Rge Rd 291 frontage. The developer will provide the County's Transportation Off-Site Levy at the subdivision stage.



The Stage 2 Outline Plan Open Space Network will include a combination of Public Utility Lot (PUL), Environmental Reserve (ER) and Municipal Reserve (MR) as generally illustrated.

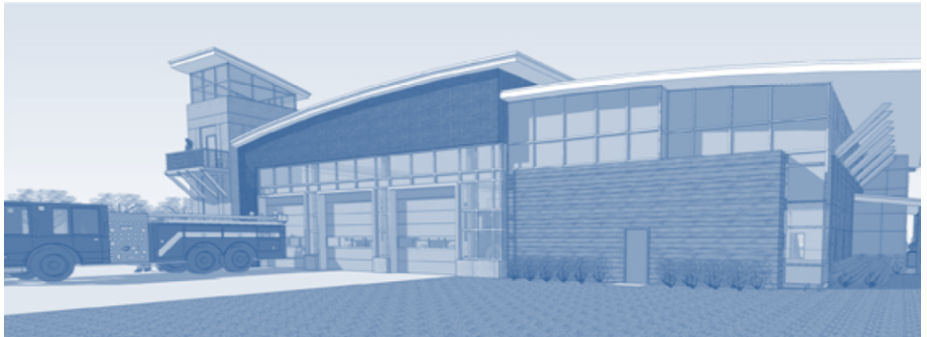
The PUL will contain the stormwater management facilities. The ER will preserve the site's existing wetlands and wooded areas. Existing rare plants and native grasses within the northern portion of the site will be removed and re-established within the ER. A linear MR will establish a landscaped buffer along the Rge Rd 291 frontage with a 2.5 m asphalt pathway to be constructed by the developer – to eventually connect with the regional trail along Hwy 566. A 2.5 m asphalt pathway will also be constructed by the developer along the south side of the Twp Rd 261 ROW and a natural trail within the ER in accordance with the provisions of the Development Agreement process.

Expected disposition of Municipal Reserve (MR) within the Stage 2 Outline Plan area is as per the chart illustrated. The amount of MR outstanding after dedication will be provided by payment of cash-in-lieu of land or deferred to other lands within the HPIP CS area.

EMERGENCY RESPONSE

Fire Service is expected from the recently-opened County's East Balzac Emergency Services located approximately 1 mile north of the Stage 2 Outline Plan area (within the RVC Municipal Campus site).

Police Service is expected from the RCMP Detachment in The City of Airdrie – with support from the RVC Community Peace Officers.



Ambulance Service is expected from EMS Facilities located in North Calgary and The City of Airdrie.

The Nexen Balzac Gas Plant has been safely processing sour gas for five decades. By Provincial regulation, Nexen is required to establish and maintain the Balzac Area Emergency Response Plan (BAERP) to implement protocols to notify and/or evacuate adjacent owners in the event of an accidental release. Nexen is required to compile and maintain a contact list of all landowners within the BAERP. If an incident at the Plant has potential to impact surrounding lands, Nexen would declare an emergency and contact all owners/residents within the Emergency Planning Zone to instruct evacuation.



The BAERP includes all lands within the East Balzac area situated east of McDonald Lake (including the Stage 2 Outline Plan area).

In 2011, Nexen decommissioned the facility, suspended well site operations and discontinued many of their pipelines within the surrounding area. Although gas is no longer flowing through the pipelines, the remaining lines will be re-licensed to a maximum H₂S concentration of 2% to accommodate future production opportunities. As a result of this change in scope, Nexen is revisiting the BAERP. However, it is expected that the BAERP will continue to apply within the affected portion of the East Balzac area.

As such, all future business lot owners within the Stage 2 Outline Plan will be added to Nexen's call-out list so that they will be contacted in the event of an incident at the Gas Plant that may require an evacuation or shelter in place.

HIGH PLAINS INDUSTRIAL PARK CONCEPTUAL SCHEME

STAGE 2 OUTLINE PLAN

	<i>ac</i>	<i>ha</i>	<i>%</i>
Industrial / Business Lots	110.1	44.6	72
Environmental Reserve (ER)	9.5	3.9	6
Municipal Reserve (MR)	1.8	0.7	1
Public Utility Lot (PUL)	17.8	7.2	12
Roads	12.9	5.2	8
Road ROW Widening	1.6	0.65	1
TOTALS	153.7	62.2	100

ARCHITECTURE GUIDELINES

Building Orientations

All site and building design layouts will conform to the regulations prescribed by the Rocky View County Land Use Bylaw's Industrial Activity District (I-IA) and the Commercial/Industrial/Office Design Guidelines.

Buildings will be oriented so that the main building entrance will face the public road that provides direct access to same.

The design of building facades facing the public roads will incorporate variations in finish and colour, roof-line offsets, and pedestrian scaling techniques designed to visually break the building mass and provide a high quality appearance.

The use of "green" building techniques which result in enhanced energy efficiencies will be encouraged.



Parking & Loading

Business-oriented public parking will be located near the front of each lot and will be situated in general proximity to the main entry of each building.

Large-scale parking, service and loading areas will be located at the sides and/or rear of the building and will be appropriately screened from adjacent properties and public roads.

Open outdoor storage areas will be screened from adjacent properties and public roads. Materials to be stored outside, not located within an enclosed building, will be maintained in an orderly fashion at all times.

Wherever possible, parking and outdoor storage areas will incorporate Low Impact Development (LID) stormwater management principles such as permeable pavement, on-site stormwater detention & treatment areas, rainwater capture/re-use and vegetated swales to implement 'source control' stormwater best management practices to reduce volume and improve surface drainage quality prior to its release into the roadside ditch system.



ARCHITECTURAL GUIDELINES

Fencing

Fencing at the front of buildings will be discouraged. If required for visual screening and/or security purposes, it will include a low-scale, decorative design and be located behind street-oriented landscaping in accordance with the requirements of the County's Land Use Bylaw.

Perimeter fencing along the side & rear property boundaries will be utilized to provide screening that reduces negative visual impacts of outdoor storage and large scale parking when viewed from public roads and adjacent parcels.

All fencing will include a design with high quality materials that will require limited maintenance.(likely black chain link) .

Lighting

Exterior lighting within each business lot will consider "dark sky" principles in order to minimize light pollution.

Lighting fixtures will include full cut-offs to direct light downward and prevent light spillage.

Signage

Lot and building signage will include high quality materials with a design that is compatible with each building façade treatment. The size and placement of all signage must be in accordance with the requirements of the County's Land Use Bylaw.

No back-lit signage will be permitted within the Stage 2 Outline Plan area.

Implementation

A Restrictive Covenant referencing the architectural guidelines will be registered as an encumbrance on each title with specific development criteria enforced by a Business Lot Owner's Association to be referred to as the High Plains Development Association (HPDA).

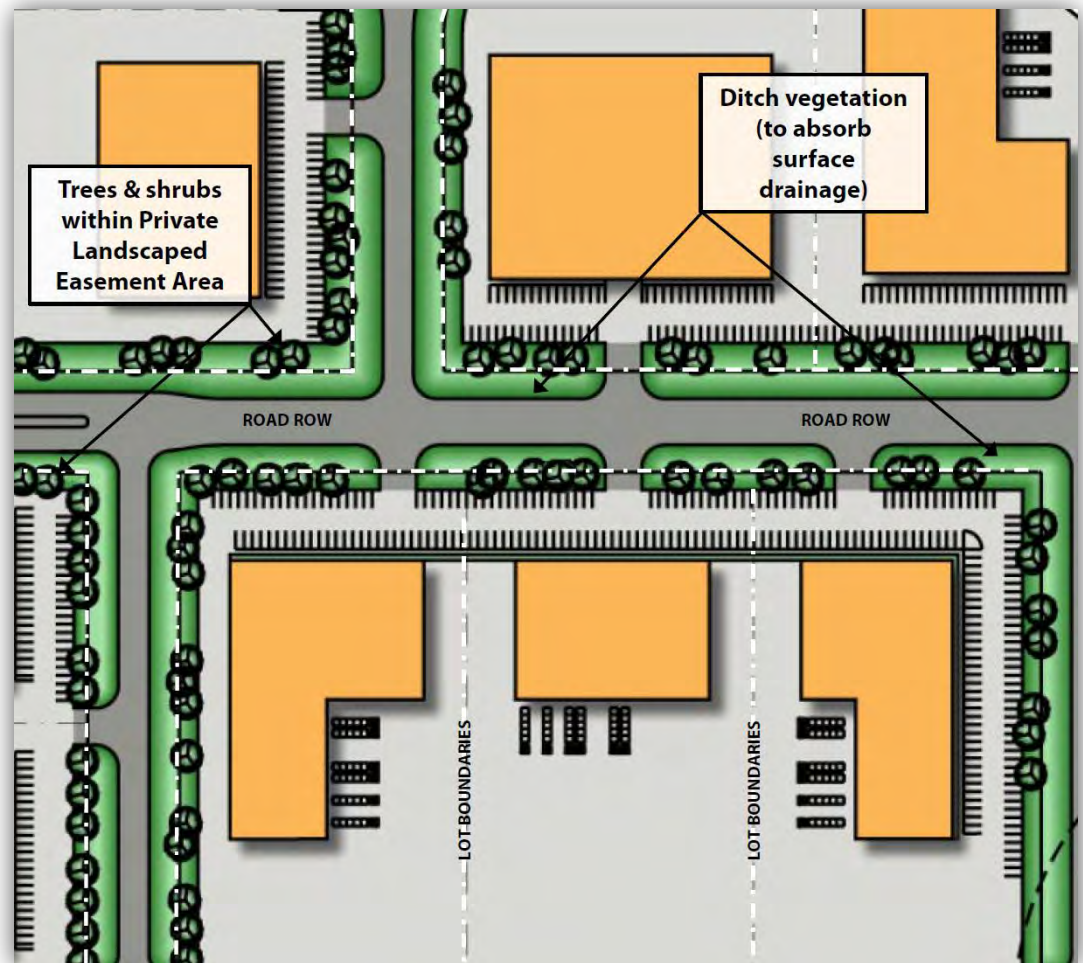
Lot owners will be required to submit site and building design plans to the HPDA for review and approval prior to seeking approvals from Rocky View County. Lot owners will be expected to provide Rocky View County with an endorsement from the HPDA with applications for development permit and/or a building permit.

LANDSCAPING CRITERIA

Landscaping treatments within the Stage 2 Outline Plan will be implemented in accordance with the general provisions established by the High Plains Industrial Park Conceptual Scheme.

A private Landscaped Easement area will be established within each lot abutting public roadways.

A Landscaping Plan will be prepared by a qualified professional as a condition of subdivision to detail the type and extent of plantings within the Landscaped Easement area. Plant materials must be appropriate for the Calgary region and include a uniform theme that is appropriate for the overall aesthetic of the industrial development. Plantings within the Landscaped Easement area will be provided by the developer in accordance with the County's Development Agreement process.

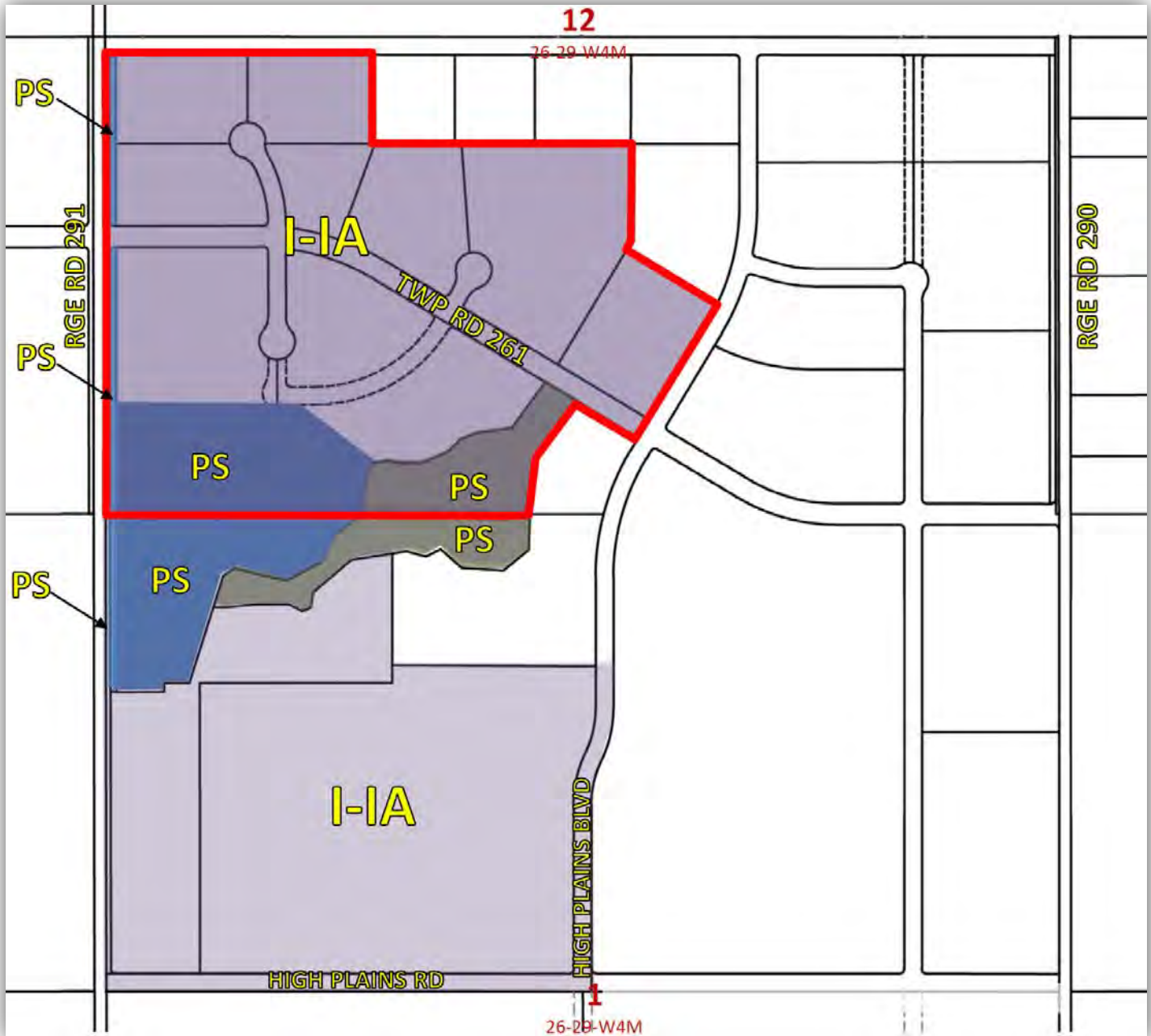


The Landscaped Easement area will be maintained by the High Plains Development Association (HPDA). Plantings within the Landscaped Easement area will be irrigated with treated stormwater from the facilities within the development's Public Utility Lots. Use of potable water to irrigate Landscaped Easement areas shall be prohibited.

As per the Balzac East Area Structure Plan and the Land Use Bylaw, a minimum of 10 % of each business lot must be landscaped. A detailed Landscaping Plan will be prepared by a qualified professional as a condition of development permit to establish how each lot-specific development plan will satisfy the 10 % landscaped requirement. The portion of each lot including the Landscaped Easement area above-described may be included in the calculation to determine the 10 % area requirement.

As previously discussed, pathways within the Municipal Reserve and Twp Rd 261 ROW will be constructed by the developer in accordance with the County's Development Agreement process. Once accepted as complete, all pathway infrastructure shall be maintained by the County.

PROPOSED LAND USE



Areas within the Stage 2 Outline Plan area proposed for business development are expected to be redesignated from Ranch & Farm District (RF) to Industrial – Industrial Activity District (I-IA). The purpose and intent of the I-IA District is to provide for a range of industrial activity, including industrial activity that may have off-site nuisance impacts, and the support services that may be associated with such activity.

The Environmental Reserve (ER), Municipal Reserve (MR) and Public Utility Lot (PUL) are expected to be redesignated from Ranch & Farm District (RF) to Public Service District (PS).

It is expected that the entire Stage 2 Outline Plan will be redesignated in accordance with a single land use amendment application.

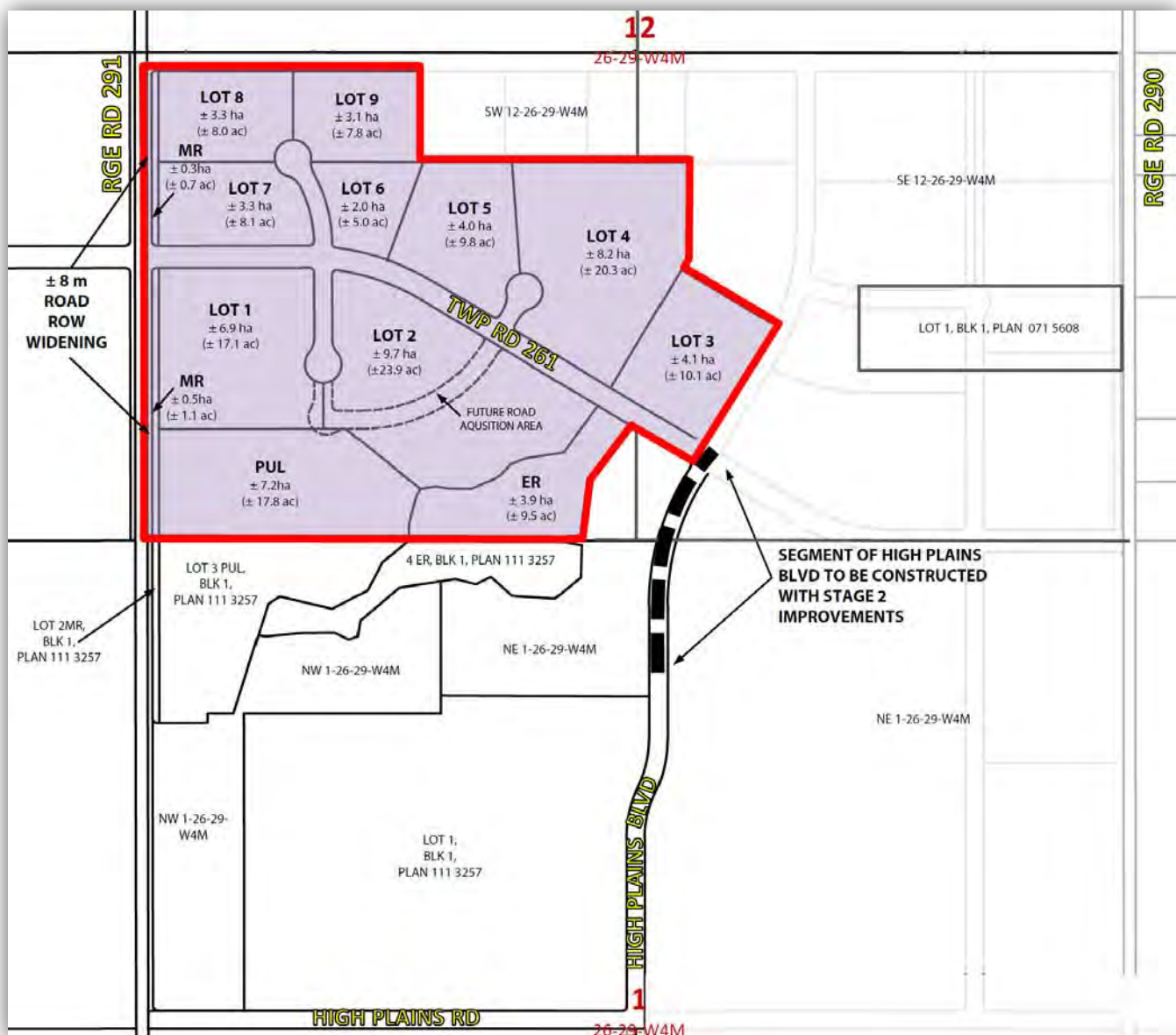
PROPOSED SUBDIVISION PLAN

Subdivision within the Stage 2 Outline Plan area is expected to include:

- Nine (9) business lots sized between ± 5 ac and ± 24 ac;
- A ± 9.5 ac Environmental Reserve (ER);
- A ± 18 ac Public Utility Lot (PUL); and
- A ± 1.6 ac Municipal Reserve (MR) dedicated in two parts.
- ± 8 m road ROW widening along the Rge Rd 291 frontage.

Subdivision is expected to be implemented in a single phase with all required transportation, utility servicing and recreation infrastructure constructed by the developer in accordance with the County's Development Agreement process.

Notwithstanding, the developer may decide to implement the subdivision in multiple phases as may be required to correspond with market conditions. If multiple phasing is required, the developer will be required to secure staged subdivision approval from the County. Appropriate infrastructure requirements will be established for each development stage.

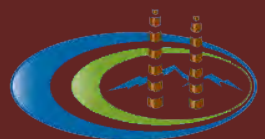


APPENDIX D

STAGE 3 OUTLINE PLAN

THE HIGH PLAINS INDUSTRIAL PARK STAGE 3 OUTLINE PLAN

ADOPTED JULY 31, 2013



**ROCKY VIEW COUNTY
BYLAW C-7286-2013**

A Bylaw of Rocky View County to amend Bylaw C-6930-2010, being the High Plains Industrial Park Conceptual Scheme

WHEREAS the Council deems it desirable to amend the said Bylaw, and

WHEREAS the Council of Rocky View County has received an application to amend Bylaw C-6930-2010 by appending the "Stage 3 Outline Plan." The amendment provides a framework for subsequent redesignation, subdivision and development within Lot 1, Block 2, Plan 111 3277 within the NE 1-26-29-W4M and a portion of the SE & SW 12-26-29-W4M as shown on the attached Schedule 'A', the "Stage 3 Outline Plan"; and

WHEREAS Council held a Public Hearing and has given consideration to the representations made to it in accordance with Section 692 of the Municipal Government Act, being Chapter M-26 of the Revised Statutes of Alberta, 2000, and all amendments thereto.

NOW THEREFORE the Council enacts the following:

1. That Bylaw C-6930-2010 be amended by appending the "Stage 3 Outline Plan," attached hereto as Schedule 'A', in order to provide a framework for subsequent redesignation, subdivision and development within Lot 1, Block 2, Plan 111 3277 within the NE 1-26-29-W4M and a portion of the SE & SW 12-26-29-W4M as shown on the attached Schedule 'A', forming part of this Bylaw; and
2. The Bylaw comes into effect upon the date of its third reading.

Division: 7

File: 06401004 / 06412001 / 002 / 006 – 2012-RV-074

First reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on June 25, 2013, on a motion by Councillor Solberg.

Second reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on July 30, 2013, on a motion by Councillor Habberfield.

Third reading passed in open Council, assembled in the City of Calgary, in the Province of Alberta, on July 30, 2013, on a motion by Councillor Solberg.

REEVE OR DEPUTY REEVE

MUNICIPAL CLERK

SCHEDULE 'A'

The Stage 3 Outline Plan for the High Plains Industrial Park is distributed under separate cover.

ACKNOWLEDGEMENTS

OWNERS/DEVELOPERS:



NORMAN DAMKAR



*GRANT & KATHERINE
SHUTTLEWORTH*

THE PROJECT TEAM:

B&A Planning Group
Bunt & Associates

IBI Group
Lawson Projects

Sweetgrass Consulting Ltd.
Westhoff Engineering Resources Inc.

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JULY 31, 2013

HIGH PLAINS INDUSTRIAL PARK

EAST BALZAC DEVELOPMENT CONTEXT



The High Plains Industrial Park Conceptual Scheme (HPIP CS) is a 'broad-based' policy that sets general expectations to guide implementation of comprehensively-planned industrial and commercial business developments within the eastern portion of the East Balzac area.

The HPIP CS establishes a general land use concept proposing industrial land uses within the southern portion of the site, a mix of industrial/business park uses within the central portion, and regional commercial developments within the northern portion (adjacent to Hwy 566).

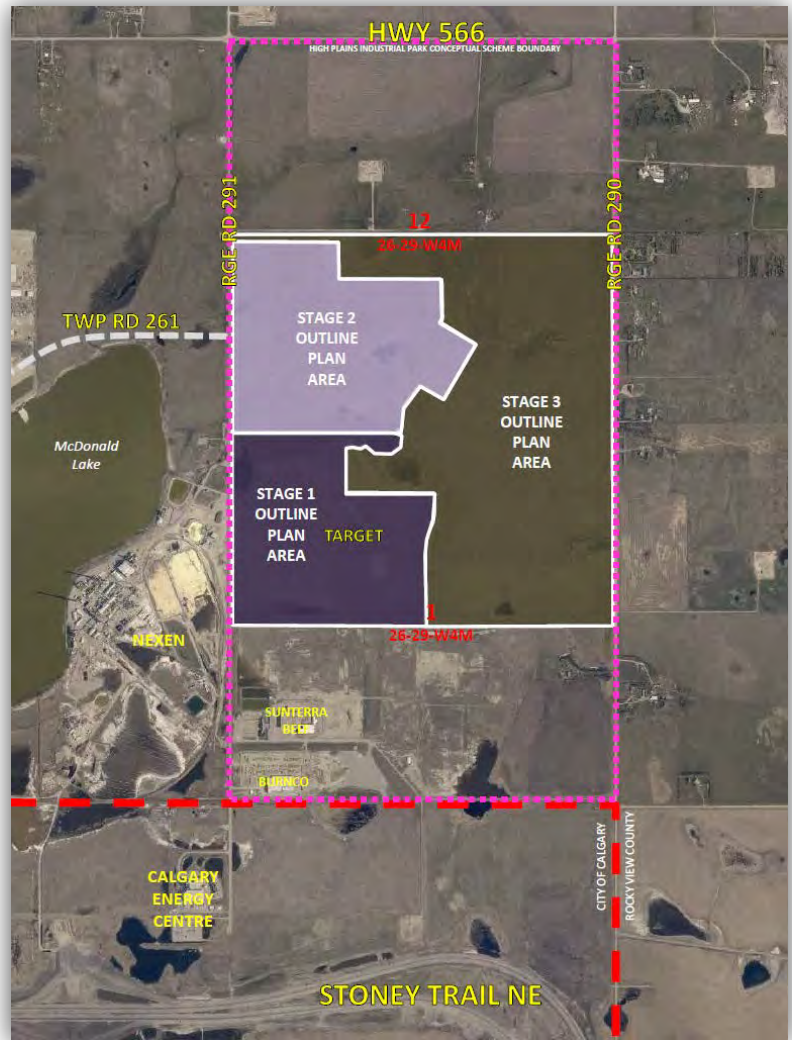
PURPOSE OF THE OUTLINE PLAN

As per the requirements of Section 10 of the High Plains Industrial Park Conceptual Scheme, an Outline Plan must be prepared at each development phase prior to Council approving land use redesignation and subsequent subdivision.

The Outline Plan must address the following:

- A Land use amendment required to implement development within the Outline Plan boundary;
- Anticipated phasing of subdivision within the Outline Plan including proposed parcel sizing & configurations;
- An update to the High Plains Industrial Park Master Utility Servicing Plan as required to describe proposed implementation of utility service infrastructure within the Outline Plan area;
- An update to the High Plains Industrial Park Traffic Impact Assessment to describe proposed implementation of local and regional roadway improvements as required within the Outline Plan area;
- An update to the High Plains Industrial Park Staged Master Drainage Plan to describe proposed implementation of stormwater infrastructure as required within the Outline Plan area;
- Where public recreation areas are proposed, a Landscaping Plan to detail how improvements will be implemented within Municipal Reserve parcels;
- Clarification of how emergency response considerations can be implemented within the Outline Plan area; and
- A description of performance standards and controls to implement a specific architectural theme, parking & loading considerations, fencing, screening & buffering considerations, signage & lighting elements, etc. within the Outline Plan area.

The Stage 3 Outline Plan addresses these development considerations pursuant to the requirements of the High Plains Industrial Park Conceptual Scheme.



EXISTING SITE CONDITIONS

The Stage 3 Outline Plan area includes ± 338.5 ac (± 137 ha) within SW/SE 12-26-29-W4M, Lot 1, Block 1, Plan 071 5608 and Lot 1, Block 2, Plan 111 3277 (NE and Pt. NW 1-26-29-W4M).

The site has generally flat to gently-rolling topography sloping from northeast to southwest.

The Plan area contains a number of existing wetlands and native/non-native grasslands. The developer acknowledges disturbance to identified wetlands must be endorsed by Alberta Sustainable Resource Development & Alberta Environment. Disturbances to native grasses will be mitigated by relocating top soil and vegetative materials into designated environmental areas within the Stage 1 & 2 Outline Plan areas.

The existing building site within the isolated parcel in SE 12 will be removed.

The Level 1 sour gas pipeline (Nexen) situated directly north of the Plan area does not present building setbacks for proposed development.

The Cell Tower Site is expected to remain (subject to Agreement with the Telecom provider) and will be integrated with the subdivision configuration as required.

The existing driveway extending from Rge Rd 290 to the abandoned Level 1 well-site within NW 1 will be removed by the developer.



STAGE 3 DEVELOPMENT CONCEPT

FUTURE OUTLINE PLAN AREA

The Stage 3 Outline Plan is expected to contain fifteen (15) industrial lots sized between ± 5 and ± 100 ac.

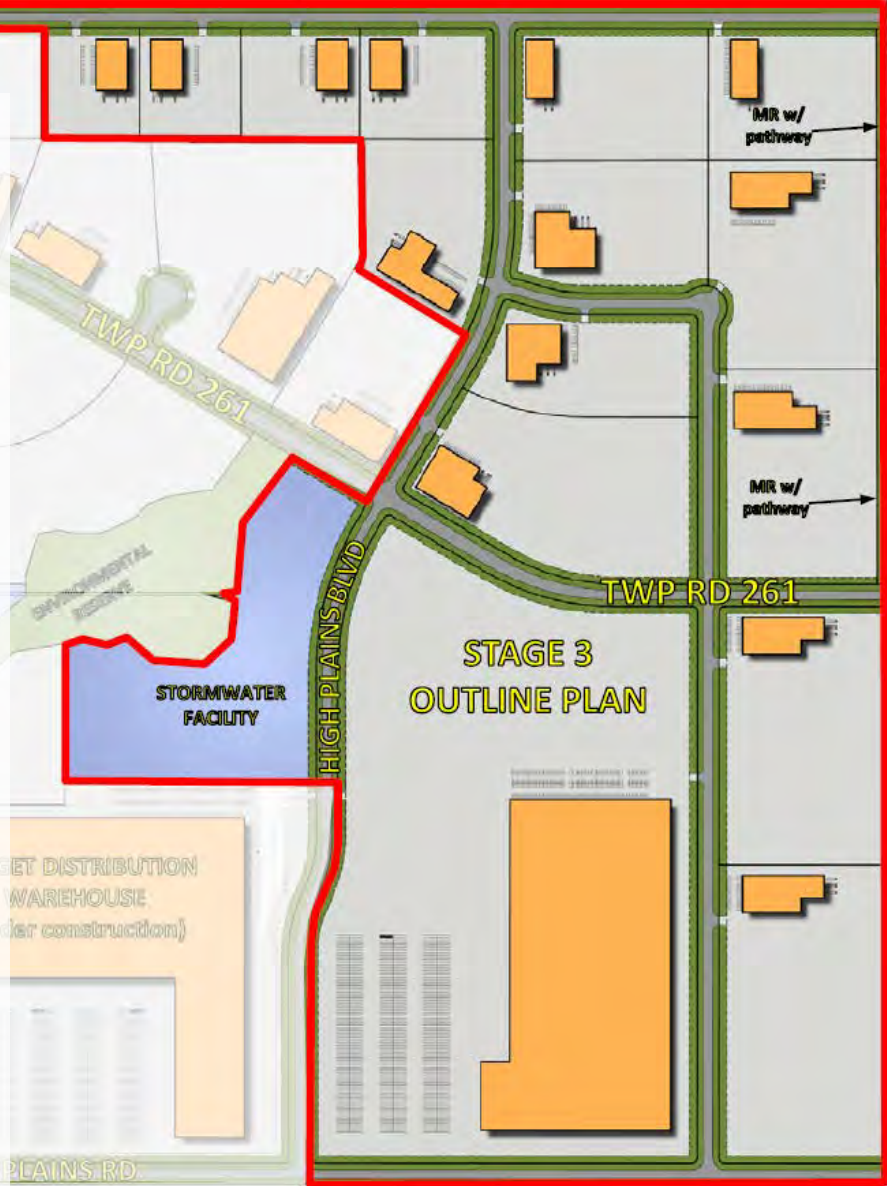
Primary access will be from Twp Rd 261 – a primary collector road to be constructed by the developer. An internal subdivision road network will be provided to access each individual lot.

Secondary access will be from Rge Rd 290 via three (3) intersections to be constructed by the developer as generally illustrated. The specific design and treatment of these intersections will be determined at the subdivision design stage.

The Plan area will be fully-served by the County's regional potable water & wastewater infrastructure. The developer will construct utility distribution infrastructure within the Outline Plan area as required to extend municipal services accordingly. The developer will also provide to the County all applicable infrastructure levies, assessments and contributions.

Stormwater management will be accommodated within one (1) Public Utility Lot (PUL) as generally illustrated. The drainage facilities within the Stage 3 Outline Plan will be integrated with similar facilities planned within the Stage 2 Outline Plan areas and under construction in the Stage 1 Outline Plan area.

A linear Municipal Reserve (MR) will be dedicated along the Rge Rd 290 frontage north of Twp Rd 261 and will include a pathway constructed by the developer at the subdivision stage.



FUTURE OUTLINE PLAN AREA

JULY 31, 2013

POTABLE WATER SERVICE

Potable water will be supplied by the East Rocky View Water system.

The developer will construct a potable water distribution network within the site as conceptually illustrated and further described by the **HPIP Stage 2/3 Outline Plan Utility Servicing Design Brief (IBI Group, March 2012)**.

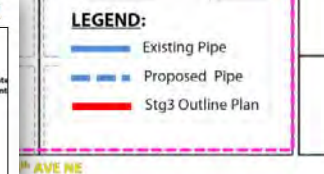
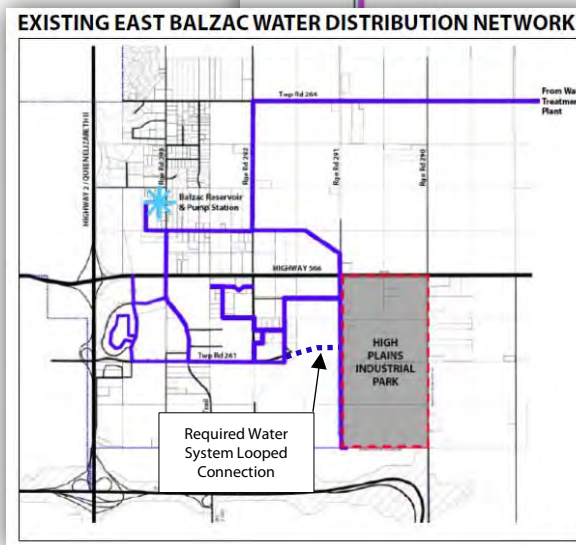
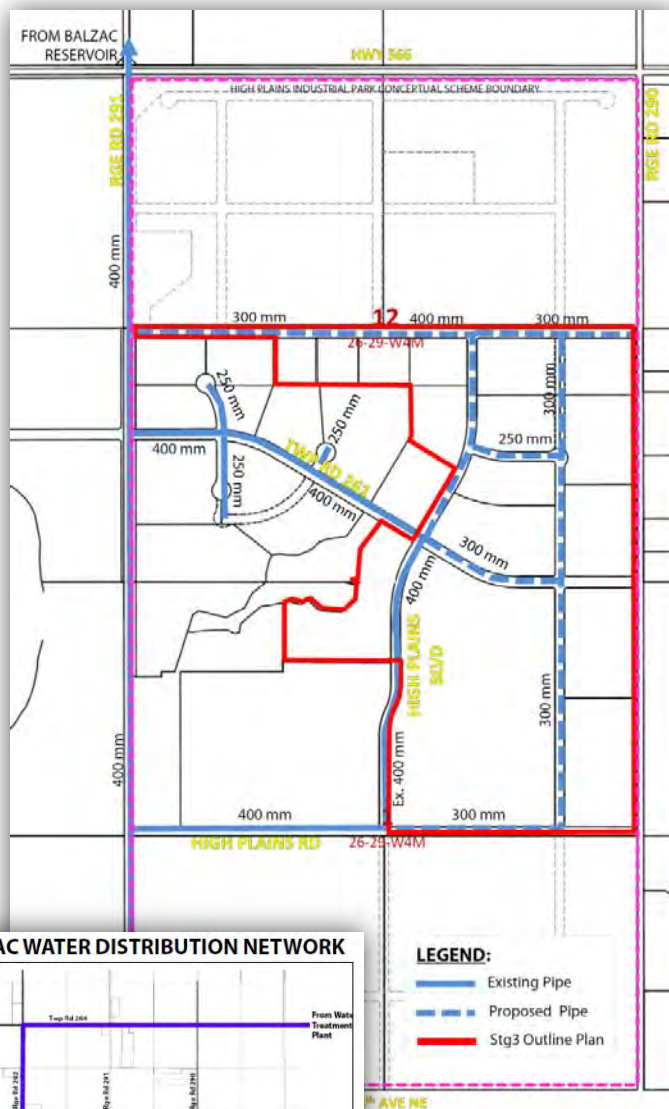
Water conservation will be implemented through metered consumption and mandatory potable water restrictions. The design of the water distribution network will accommodate fire suppression in accordance with applicable regulatory requirements.

The developer will contribute the applicable payment in accordance with the appropriate levies and/or water contribution agreements at the subdivision and/or development permit stage. Where the developer implements potable water infrastructure in support of the Stage 3 Outline Plan that benefits surrounding properties, the County's Infrastructure Cost Recovery policy will apply.

In order to provide the required level of water service to within the Stage 3 Outline Plan, the County has indicated the existing distribution network must be upgraded.

Specifically, existing mains situated within Rge Roads 291 and 292 must be interconnected. This water connection will be installed within the segment of Twp Rd 261 to be constructed by the County in 2013.

The developer is prepared to work with the County to complete this water system upgrade as component to development within the Stage 3 Outline Plan area. This upgrade will be subject to the County's Infrastructure Cost Recovery policy.



WASTEWATER SERVICE

Wastewater service will be provided by the East Rocky View Wastewater Transmission Line (ERVWWTL).

The HPIP Conceptual Scheme area includes two Sanitary Basins: the North Basin and the South Basin. The North Basin is further delineated into two (2) sub-basins and the South Basin into seven (7) sub-basins as illustrated.

Wastewater generated within the South Basin will be directed to the existing Rancher's Beef Lift Station.

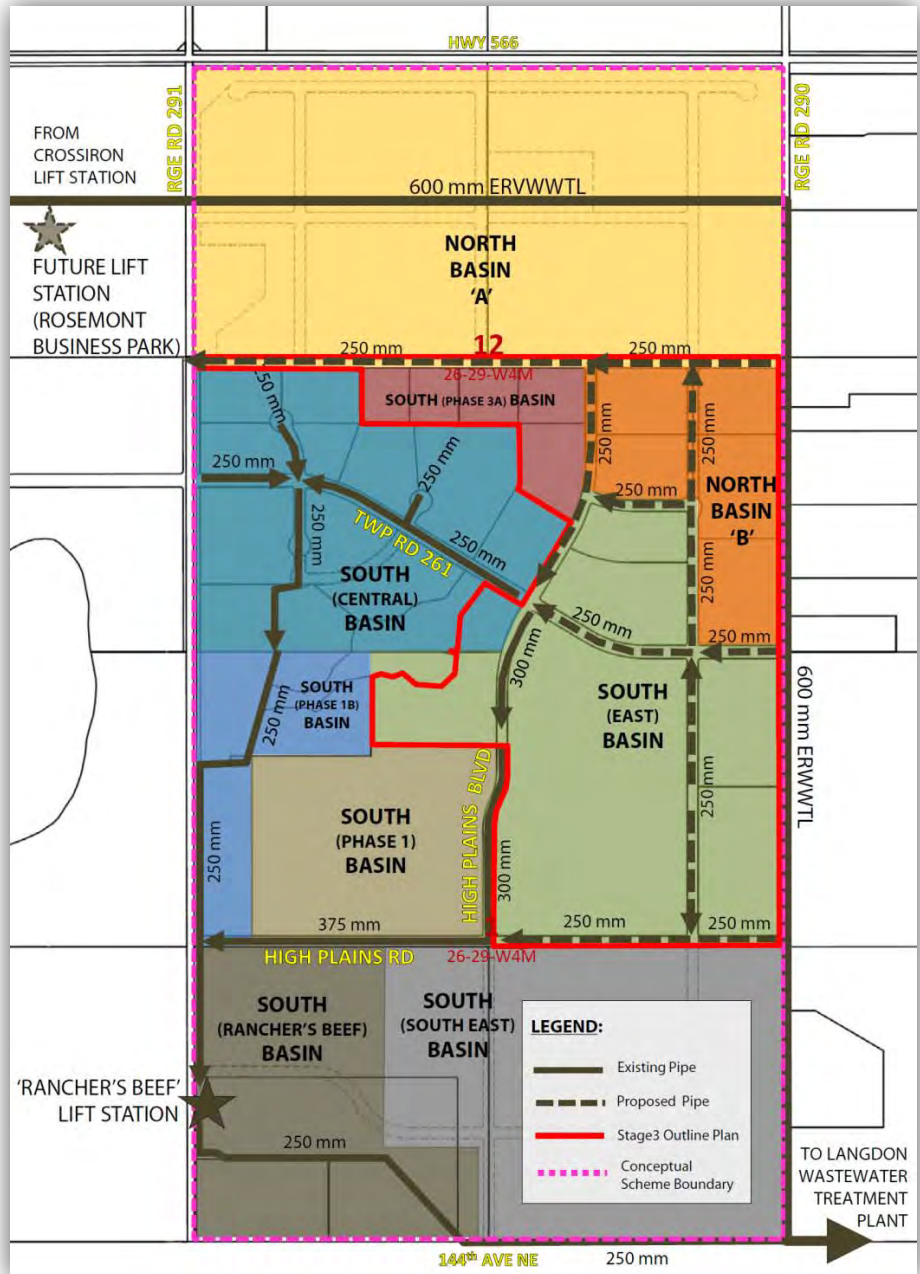
Wastewater in the North Basin will be directed to a Lift Station to be constructed in the Rosemont Business Park – which will be designed with capacity to service the North Basin and adjacent lands to the west of the HPIP CS area.

The developer will construct a wastewater collection network within the site as conceptually illustrated and further described by the **HPIP Stage 2/3 Outline Plan Utility Servicing Design Brief (IBI Group, March 2012)**.

A detailed sanitary analysis will be completed by the developer at the subdivision state to confirm capacity of the Rancher's Beef Lift Station. As required, any necessary upgrades will be completed by the developer.

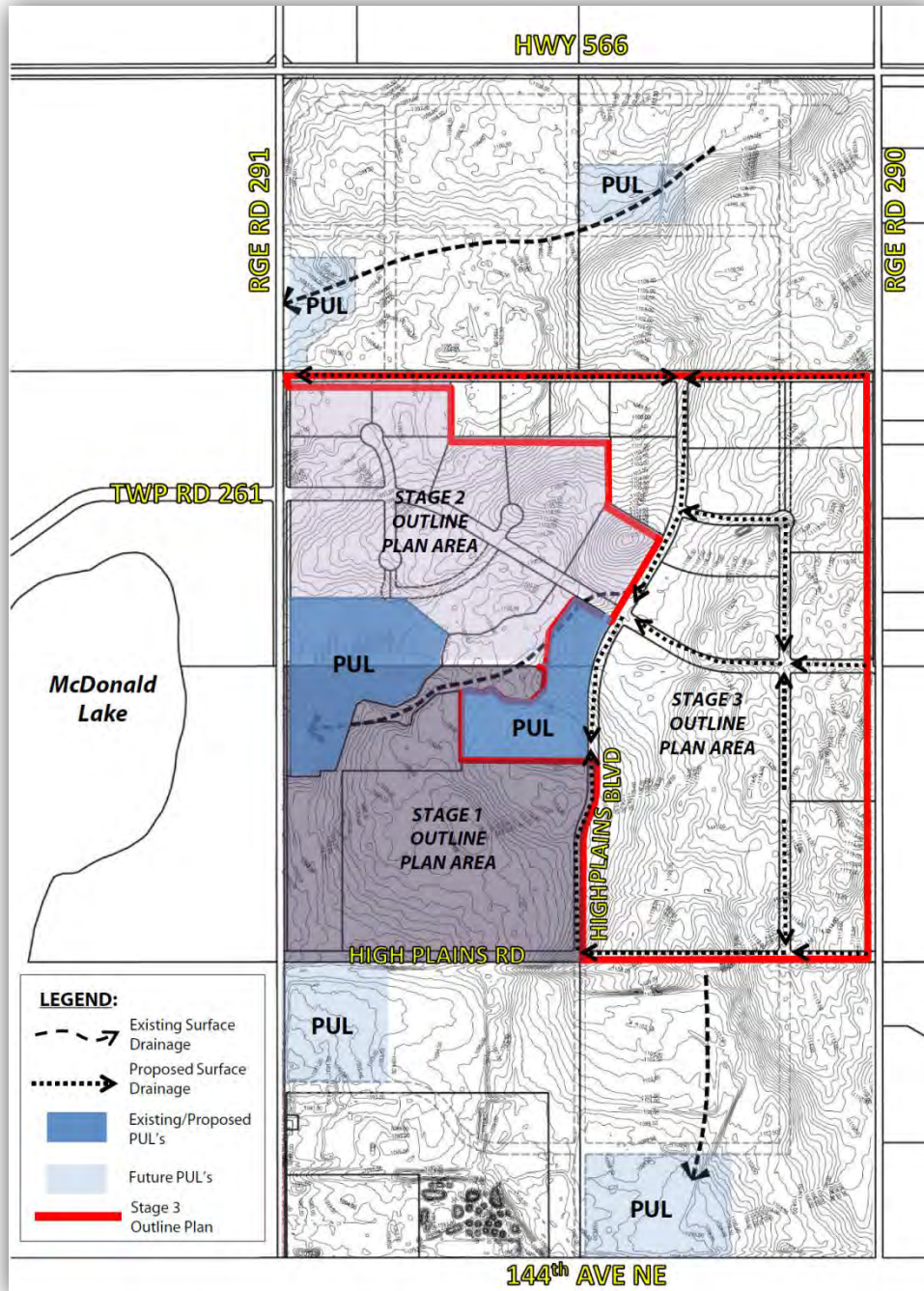
The developer will contribute the applicable payment in accordance with the appropriate levies and / or wastewater contribution agreements at the subdivision and/or development permit stage.

Where the developer implements wastewater infrastructure in support of the Stage 3 Outline Plan that benefits surrounding properties, the County's Infrastructure Cost Recovery policy will apply.



JULY 31, 2013

STORMWATER MANAGEMENT



Stormwater management in the Stage 3 Outline Plan Area will be accommodated by surface drainage facilities to be constructed by the developer as conceptually illustrated and further described by the **Stormwater Management Plan for the Stage 3 Outline Plan (Westhoff Engineering Resources Inc., May 2012)**.

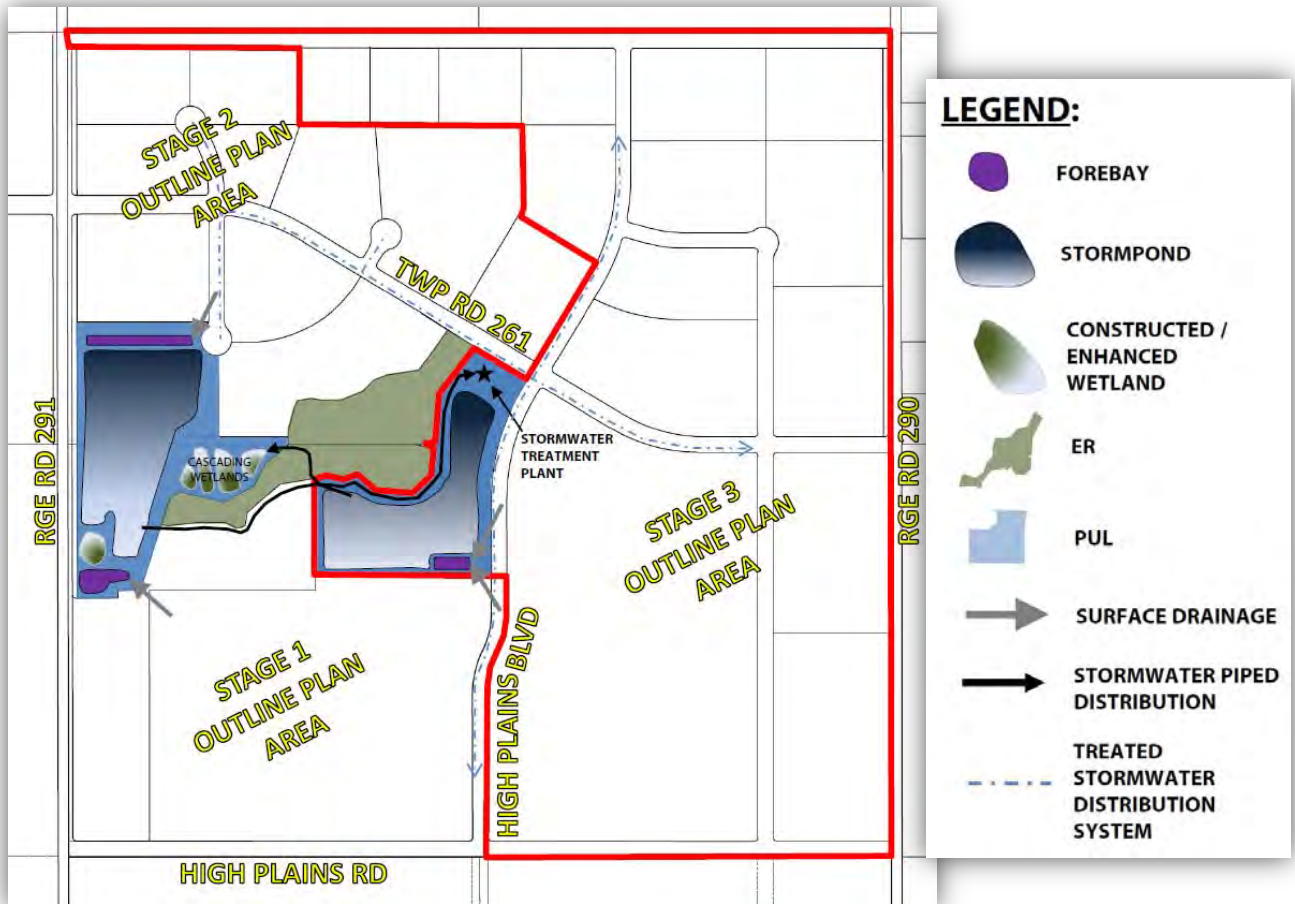
Surface drainage generated within the site will be conveyed to a Public Utility Lot (PUL) to be constructed in the southwest portion of the Outline Plan area.

The design of the PUL will include capacity to retain 100 % of surface drainage generated within the site without the requirement of a downstream discharge.

The stormwater management analysis assumes the occurrence of extended periods of precipitation leading to a greater than a 1:100 year storm event and was measured using a continuous modeling simulation.

In the event existing wetlands are disturbed within the site, approvals for same will be required from Alberta Environment and Alberta Sustainable Resource Development.

STORMWATER MANAGEMENT



Stormwater management facilities within the PUL will include a forebay, an enhanced wetland and a detention pond designed to implement a 'treatment train' intended to improve surface water quality via natural filtration processes. The Stage 3 Outline Plan's PUL will be combined with similar facilities being constructed in the Stage 1/2 Outline Plan areas (and with facilities to be constructed within future Outline Plan areas). Stormwater will be disposed of using operational considerations such as enhanced evaporation techniques and for irrigation of landscaped areas and adjacent agricultural lands. An internal distribution system will be installed by the developer to convey treated stormwater for these purposes.

As per the framework established by the HPIP CS, a mechanical treatment plant will be constructed by the developer within the Stage 3 Outline Plan area so that treated stormwater may be used to provide industrial lot owners with a non-potable water supply for processing activities, thereby augmenting the County's potable water system. A Utility Corporation will be established to own and manage the treated stormwater distribution system, to the satisfaction of the County and applicable regulatory authorities.

As described in the HPIP Conceptual Scheme, the developer is working with Olds College to implement a cold climate wetland research facility within the HPIP area. Specific operational details required to implement this research partnership will be established at the subdivision stage. The County reserves the right to participate as an active or passive member of this partnership.

TRANSPORTATION

EAST BALZAC TRANSPORTATION NETWORK



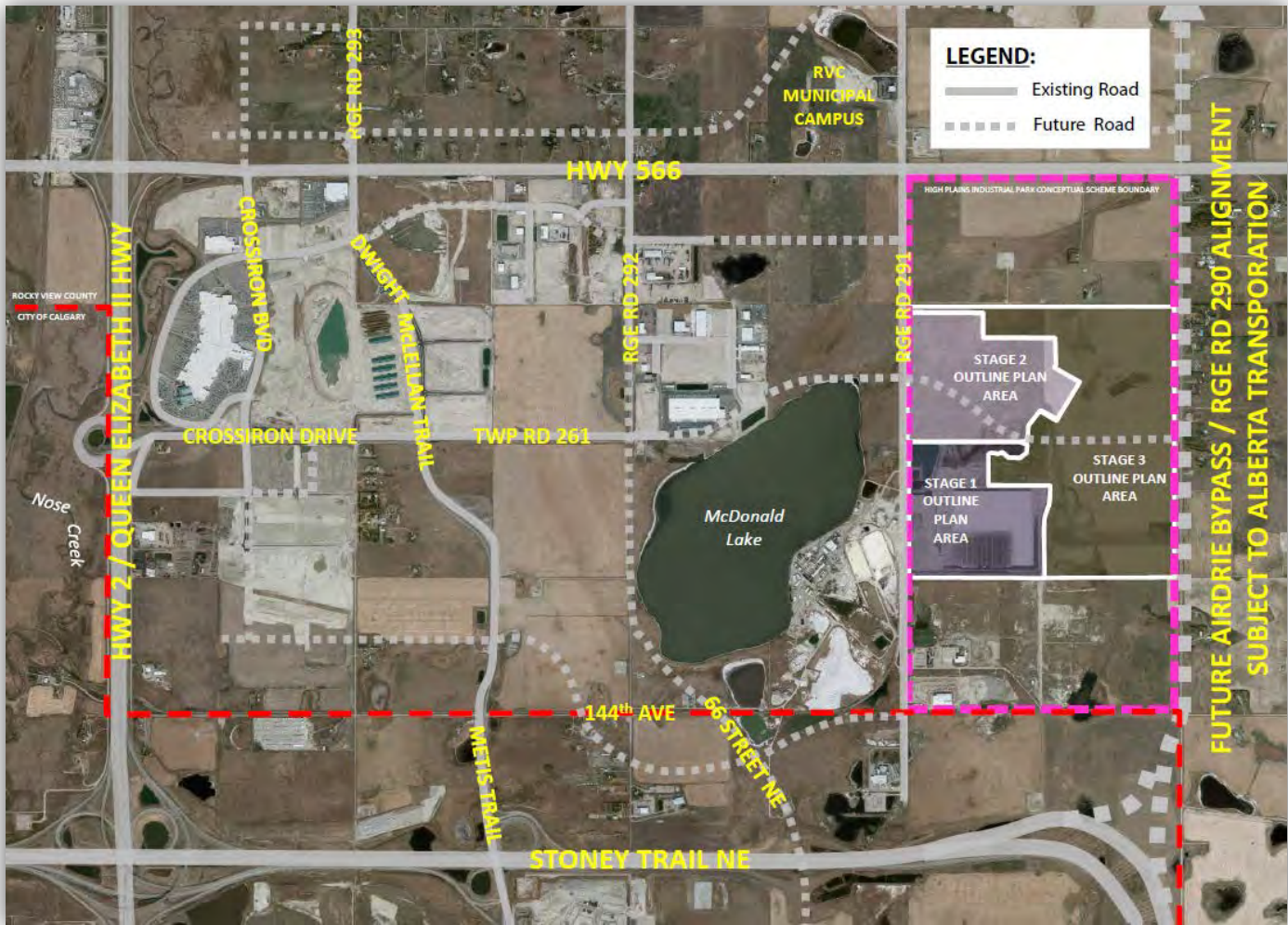
The East Balzac Transportation Network has been planned to facilitate access between the County's primary business and employment area and the surrounding regional road network.

The County has determined specific requirements for the East Balzac transportation network in accordance with a Global Traffic Impact Assessment (HDR | iTrans, December, 2010). All development in the High Plains Industrial Park (HPIP) must rationalize specific transportation requirements within the context of the County's Global TIA.

The Stage 3 Outline Plan area will be accessed primarily from Twp Rd 261 which will provide direct access to Hwy 2 and indirect access to Stoney Trail NE (via Metis Trail). The segment of Twp Rd 261 between Rge Rd 291 & 292 will be completed by the County during the 2013 and 2014 construction seasons.

TRANSPORTATION

AIRDRIE BYPASS ROAD / RGE RD 290 ALIGNMENT



Alberta Transportation (AT) has a long-term strategic plan to implement a regional transportation connection designed to route traffic from northeast Calgary and East Balzac onto Highway 2 at a connection point just north of The City of Airdrie. This proposed roadway is commonly referred to as 'The Airdrie Bypass Road'.

AT had originally requested the developer set aside land within the Conceptual Scheme area to accommodate ROW for this expected future roadway extension. However, AT has indicated that the Province does not anticipate construction of the Airdrie Bypass Road any time in the foreseeable future (in fact not likely within a 20 year horizon). As such, AT has indicated support for development proceeding within the Stage 3 Outline Plan area and will resolve the anticipated alignment of the Airdrie Bypass Road as part of future planning processes.

JULY 31, 2013

TRANSPORTATION

HIGH PLAINS INDUSTRIAL PARK CONCEPTUAL ROAD NETWORK

The High Plains Industrial Park Conceptual Scheme contemplates a transportation network to be implemented by the developer in accordance with a staged program of roadway improvements as directed by individual Outline Plans.

The developer is presently constructing segments of High Plains Rd & High Plains Blvd, upgrading the Hwy 566 / Rge Rd 291 intersection and widening portions Rge Rd 291 – as a condition of subdivision for the Stage 1 Outline Plan area.

The portion of Twp Rd 261 directly west of the Plan area will be constructed by the developer via implementation of the Stage 2 Outline Plan.

The **High Plains Industrial Park Stage 3 Traffic Impact Assessment (Bunt & Associates Engineering, 2012)** was prepared to support this Outline Plan. The traffic analysis was prepared assuming a 'worse-case scenario' that does not rely on access to/from existing and/or future roadways paralleling the east frontage of the Outline Plan area.

Under this 'worse-case' scenario, the TIA concludes that, with specific improvements to Rge Rd 291 and the intersection at Hwy 566, the road system within the Stage 3 Outline Plan will function within acceptable capacity parameters.

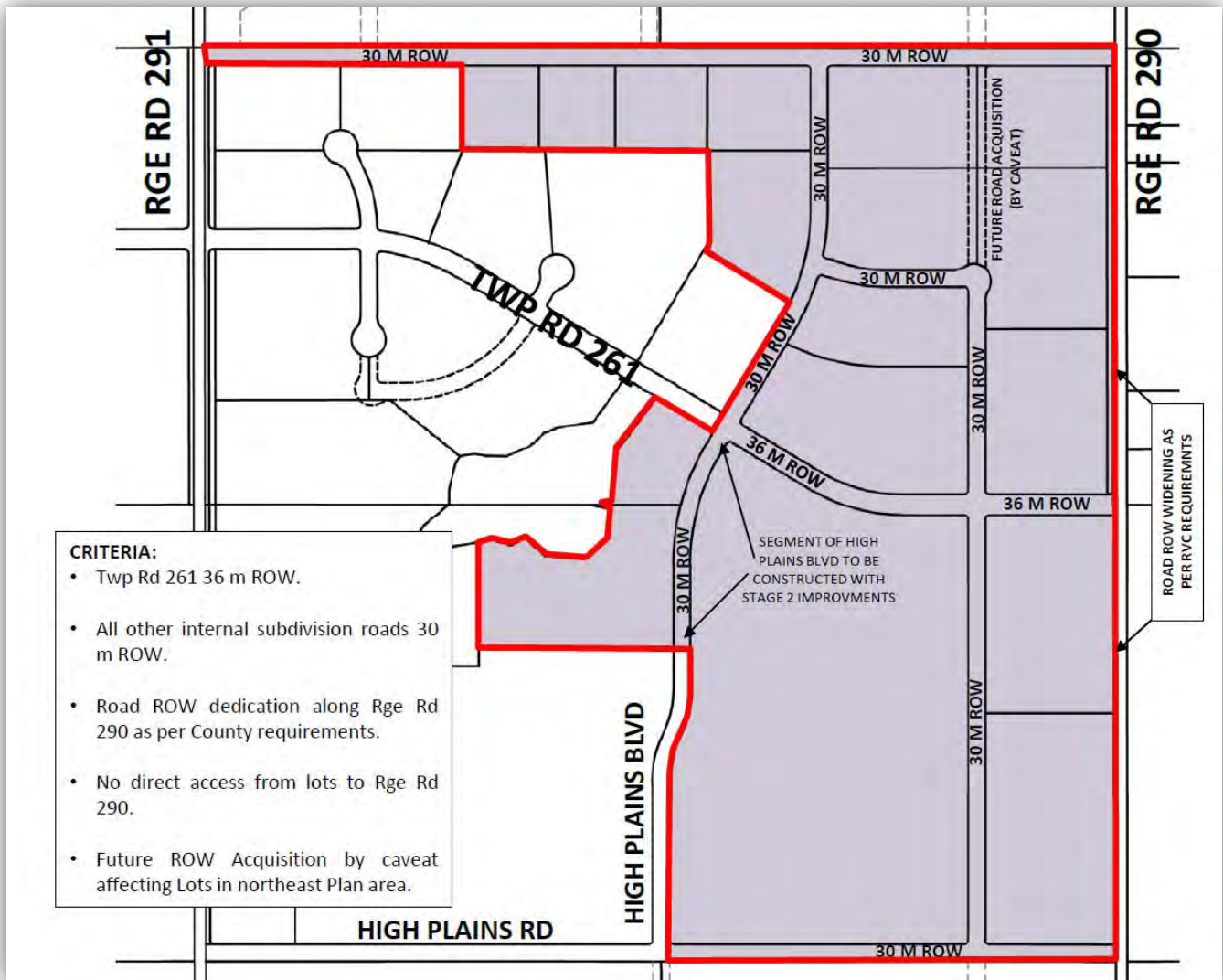
All roadways will be constructed by the developer in accordance with the County's engineering requirements. ROW's will include rural cross-sections with ditching to convey surface drainage.

Where transportation infrastructure constructed by the developer within the Stage 3 Outline Plan area is demonstrated to benefit surrounding lands, the County shall facilitate cost recoveries at such time the development occurs within the benefitting areas.



TRANSPORTATION

STAGE 3 OUTLINE PLAN ROAD NETWORK



The Stage 3 Outline Plan Area's road network will be constructed by the developer as generally illustrated. All internal roads will include a 30 m ROW, with exception of the Twp Rd 261 - which will include a 36 m. ROW.

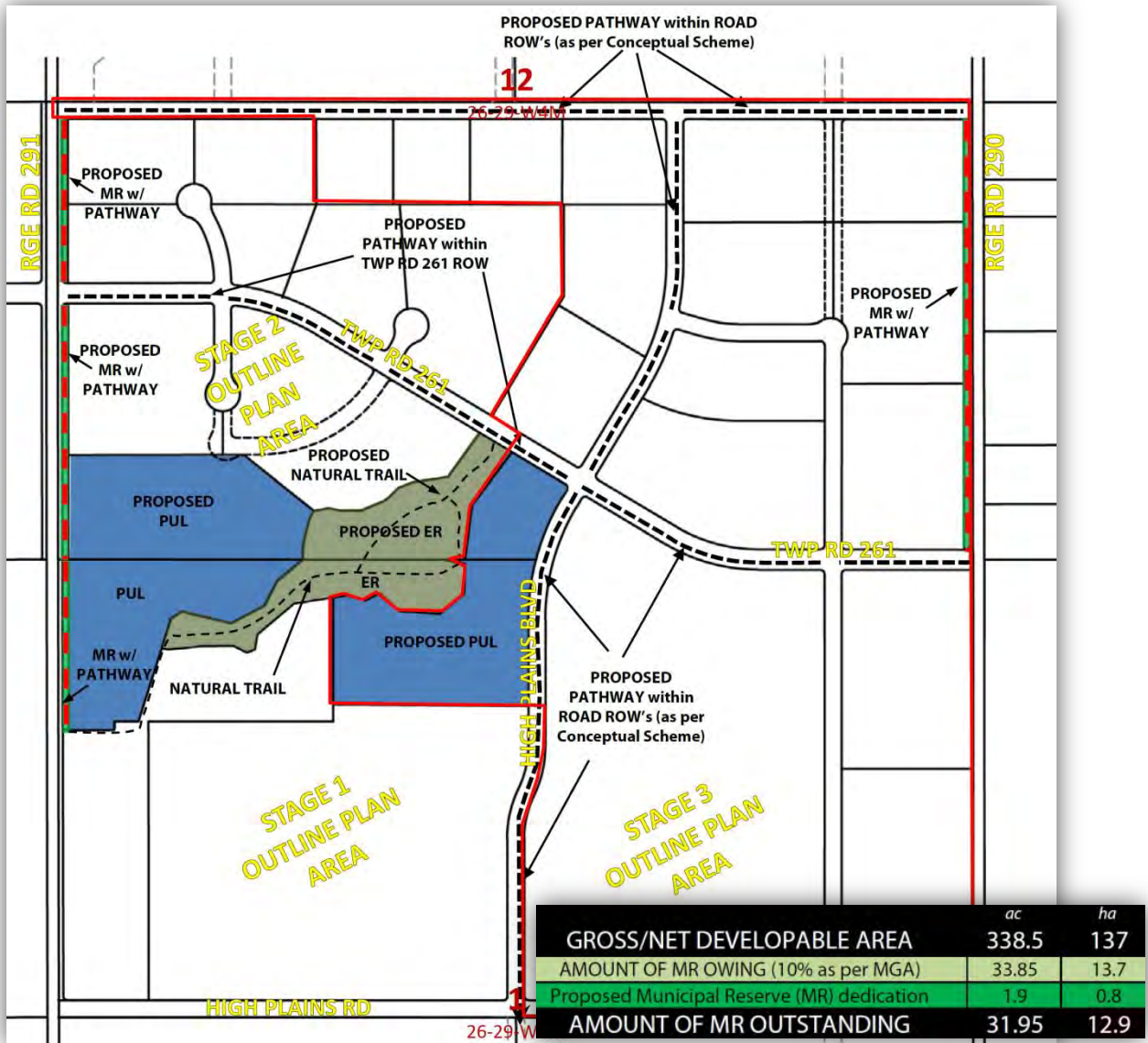
Road ROW dedication for widening purposes will be provided along the Rge Rd 290 frontage in accordance with the County's requirements. No direct access will be provided onto Rge Rd 290 from individual lots.

The developer will implement the internal road network in accordance with the County's Development Agreement process.

The County's Transportation Off-Site Levy will apply at the subdivision stage.

JULY 31, 2013

OPEN SPACE



The Open Space Network within the Stage 3 Outline Plan Area will include a Public Utility Lot (PUL) and Municipal Reserve (MR) as generally illustrated.

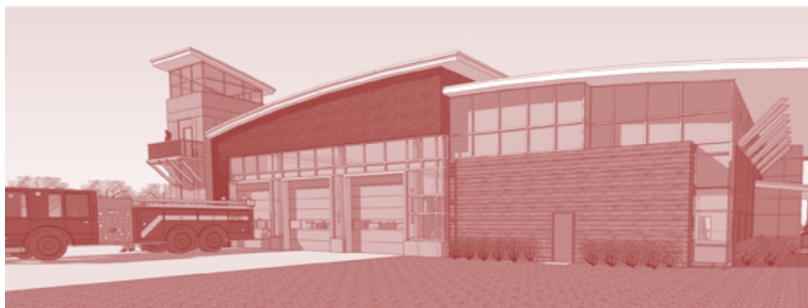
The PUL will contain the stormwater management facilities. Existing native grasses within the northern portion of the Plan Area will be relocated to the ER situated in the Stage 1 and 2 Outline Plan areas.

A linear MR will establish a landscaped buffer along the Rge Rd 290 frontage with a 2.5 m asphalt pathway to be constructed by the developer – to eventually connect with the regional trail along Hwy 566. A 2.5 m asphalt pathway will also be constructed by the developer within the road ROW's as illustrated in accordance with the provisions of the Development Agreement process.

The amount of MR outstanding after dedication shall be provided by payment of cash-in-lieu of land or deferred to other lands within the HPIP CS area.

EMERGENCY RESPONSE

Fire Service is expected from the recently-opened County's East Balzac Emergency Services located approximately 1 mile north of the Stage 3 Outline Plan area (within the RVC Municipal Campus site).



Police Service is expected from the RCMP Detachment in The City of Airdrie – with support from the RVC Community Peace Officers.

Ambulance Service is expected from EMS Facilities located in North Calgary and The City of Airdrie.

The Nexen Balzac Gas Plant has been safely processing sour gas for five decades. By Provincial regulation, Nexen is required to establish and maintain the Balzac Area Emergency Response Plan (BAERP) to implement protocols to notify and/or evacuate adjacent owners in the event of an accidental release. Nexen is required to compile and maintain a contact list of all landowners within the BAERP to instruct evacuation.



If an incident at the Plant has potential to impact surrounding lands, Nexen would declare an emergency and contact all owners/residents within the EPZ. The BAERP includes all lands within the East Balzac area situated east of McDonald Lake (including the Stage 3 Outline Plan area).

In 2011, Nexen decommissioned the facility, suspended well site operations and discontinued many of their pipelines within the surrounding area. Although gas is no longer flowing through the pipelines, the remaining lines will be re-licensed to a maximum H₂S concentration of 2% to accommodate future production opportunities. As a result of this change in scope, Nexen is revisiting the BAERP. However, it is expected that the BAERP will continue to apply within the affected portion of the East Balzac area.

As such, all future business lot owners within the Stage 3 Outline Plan will be added to Nexen's call-out list so that they will be contacted in the event of an incident at the Gas Plant that may require an evacuation or shelter in place.

HIGH PLAINS INDUSTRIAL PARK CONCEPTUAL SCHEME

STAGE 3 OUTLINE PLAN

	<i>ac</i>	<i>ha</i>	<i>%</i>
Industrial / Business Lots	270.5	109.5	80
Municipal Reserve (MR)	1.9	0.8	1
Public Utility Lot (PUL)	22.9	9.3	7
Roads	43.2	17.4	13
Rge Road 290 ROW Widening	TBD	TBD	TBD
TOTALS	338.5	137	100

ARCHITECTURE GUIDELINES

Building Orientations

All site and building design layouts will conform to the regulations prescribed by the Rocky View County Land Use Bylaw's Industrial Activity District (I-IA) and the Commercial/Industrial/Office Design Guidelines.

Buildings will be oriented so that the main building entrance will face the public road that provides direct access to same.

The design of building facades facing the public roads will incorporate variations in finish and colour, roof-line offsets, and pedestrian scaling techniques designed to visually break the building mass and provide a high quality appearance.

The use of "green" building techniques which result in enhanced energy efficiencies will be encouraged.



Parking & Loading

Business-oriented public parking will be located near the front of each lot and will be situated in general proximity to the main entry of each building.

Large-scale parking, service and loading areas will be located at the sides and/or rear of the building and will be appropriately screened from adjacent properties and public roads.

Open outdoor storage areas will be screened from adjacent properties and public roads. Materials to be stored outside, not located within an enclosed building, will be maintained in an orderly fashion at all times.

Wherever possible, parking and outdoor storage areas will incorporate Low Impact Development (LID) stormwater management principles such as permeable pavement, on-site stormwater detention & treatment areas, rainwater capture/re-use and vegetated swales to implement 'source control' stormwater best management practices to reduce volume and improve surface drainage quality prior to its release into the roadside ditch system.



ARCHITECTURAL GUIDELINES

Fencing

Fencing at the front of buildings will be discouraged. If required for visual screening and/or security purposes, it will include a low-scale, decorative design and be located behind street-oriented landscaping in accordance with the requirements of the County's Land Use Bylaw.

Perimeter fencing along the side & rear property boundaries will be utilized to provide screening that reduces negative visual impacts of outdoor storage and large scale parking when viewed from public roads and adjacent parcels.

All fencing will include a design with high quality materials that will require limited maintenance (likely black chain link) .

Lighting

Exterior lighting within each business lot will consider "dark sky" principles in order to minimize light pollution.

Lighting fixtures will include full cut-offs to direct light downward and prevent light spillage.

Signage

Lot and building signage will include high quality materials with a design that is compatible with each building façade treatment. The size and placement of all signage must be in accordance with the requirements of the County's Land Use Bylaw.

No back-lit signage will be permitted within the Stage 3 Outline Plan area.

Implementation

A Restrictive Covenant referencing the architectural guidelines will be registered as an encumbrance on each title with specific development criteria enforced by a Business Lot Owner's Association to be referred to as the High Plains Development Association (HPDA).

Lot owners will be required to submit site and building design plans to the HPDA for review and approval prior to seeking approvals from Rocky View County. Lot owners will be expected to provide Rocky View County with an endorsement from the HPDA with applications for development permit and/or a building permit.

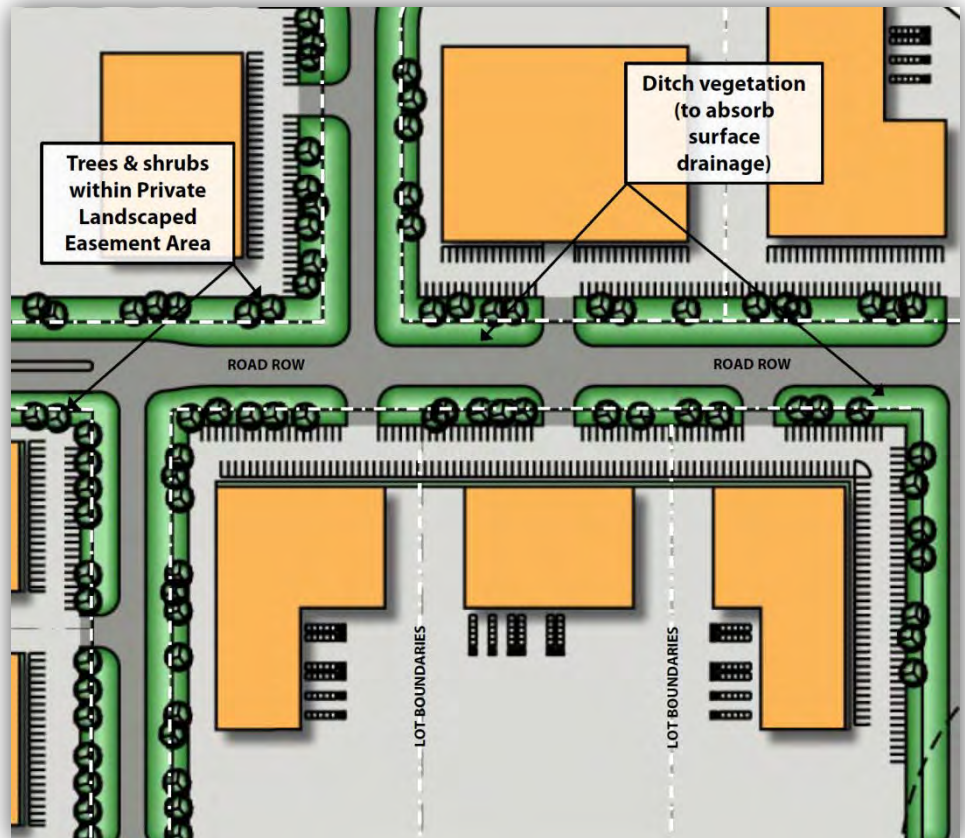
LANDSCAPING CRITERIA

Landscaping treatments within the Stage 3 Outline Plan will be implemented in accordance with the general provisions established by the High Plains Industrial Park Conceptual Scheme.

In similar fashion of the expectations established by the Stage 1 and 2 Outline Plans, a private Landscaped Easement area will be established within each lot abutting public roadways in the Stage 3 Outline Plan area.

A Landscaping Plan will be prepared by a qualified professional as a condition of subdivision to detail the type and extent of plantings within the Landscaped Easement area. Plant materials must be appropriate for the Calgary region and include a uniform theme that is appropriate for the overall aesthetic of the industrial development.

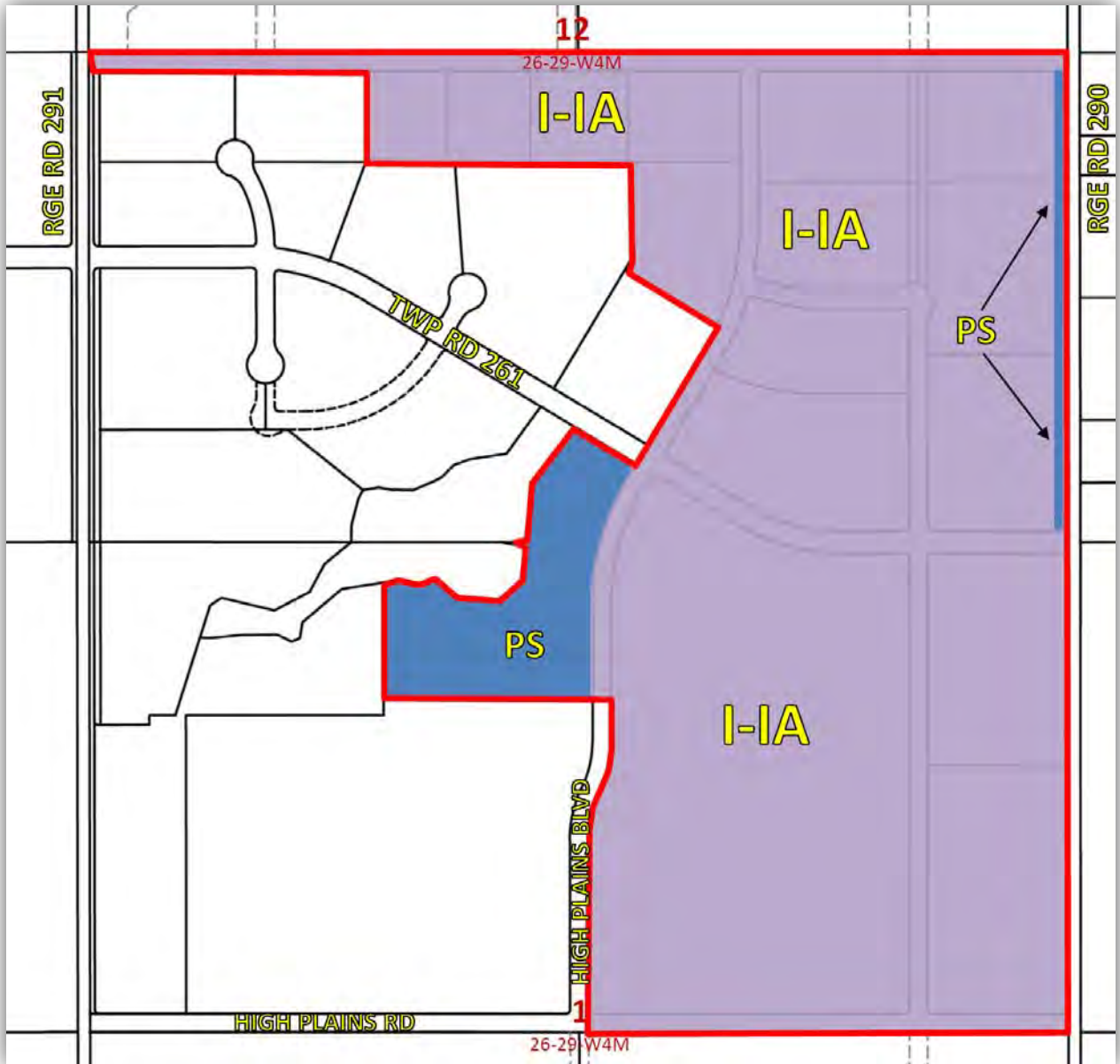
Plantings within the Landscaped Easement area will be provided by the developer in accordance with the County's Development Agreement process.



The Landscaped Easement area will be maintained by the High Plains Development Association (HPDA). Plantings within the Landscaped Easement area will be irrigated with treated stormwater from the facilities within the development's Public Utility Lots. Use of potable water to irrigate Landscaped Easement areas shall be prohibited.

As per the Balzac East Area Structure Plan and the Land Use Bylaw, a minimum of 10 % of each business lot must be landscaped. A detailed Landscaping Plan will be prepared by a qualified professional as a condition of development permit to establish how each lot-specific development plan will satisfy the 10 % landscaped requirement. The portion of each lot including the Landscaped Easement area above-described may be included in the calculation to determine the 10 % area requirement.

PROPOSED LAND USE



All areas within the Stage 3 Outline Plan area proposed for business development are expected to be designated Industrial Activity District (I-IA). The purpose and intent of the I-IA District is to provide for a range of industrial activity, including industrial activity that may have off-site nuisance impacts, and the support services that may be associated with such activity.

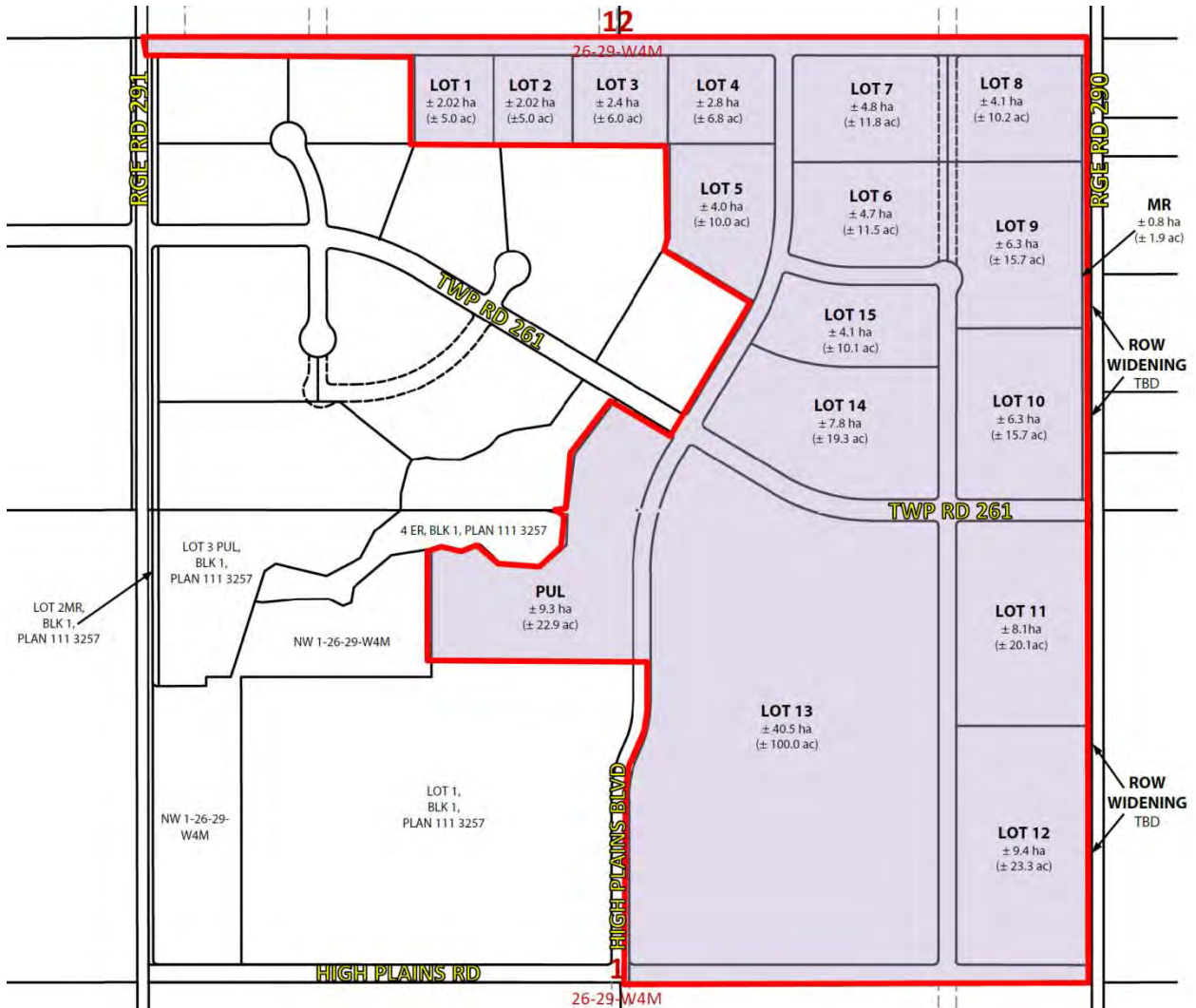
The Municipal Reserve (MR) and Public Utility Lot (PUL) are expected to be designated Public Service District (PS).

It is expected that the entire Stage 3 Outline Plan will be redesignated in accordance with a single land use amendment application.

PROPOSED SUBDIVISION PLAN

Subdivision within the Stage 3 Outline Plan area is expected to include:

- Fifteen (15) business lots sized between ± 5 ac and ± 100 ac;
- One (1) Public Utility Lots (PUL) sized at ± 22.9 ac; and
- A ± 1.9 ac linear Municipal Reserve (MR) to include a pathway.

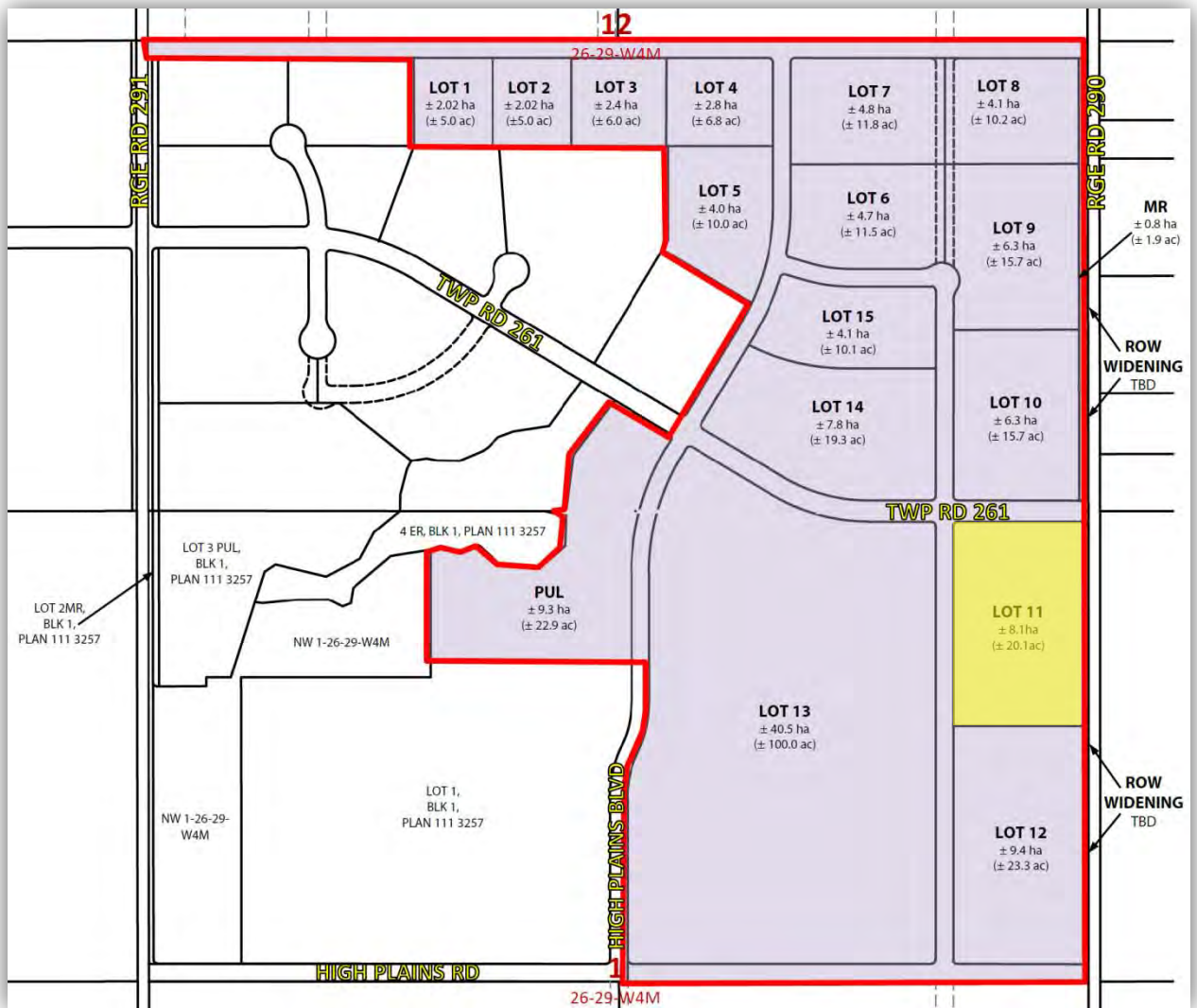


Subdivision is expected to proceed generally in two phases subject to the capacity of sanitary sewer infrastructure (i.e. the Rancher's Beef Lift Station). Generally, Lots 1-4 and 11-15 (situated within the South Sanitary Basin) are expected to be developed first while Lots 6-10 (situated within the North Basin) will be developed via future phasing at such time construction of the Rosemont Lift Station proceeds.

In all cases, all required transportation, utility servicing and recreation infrastructure will be implemented by the developer at each subdivision phase in accordance with the provisions of the County's Development Agreement process.

JULY 31, 2013

IMPLEMENTATION



As condition of land purchase agreement for NE 1-26-29-W4M, the High Plains Industrial Park developer and the previous owner (Shell Canada) agreed to terms outlining that a ± 20 ac parcel be isolated from the C of T and retained (by Shell) for future development purposes. As illustrated, proposed Lot 11 (containing ± 20 ac) has been identified for this purpose.

As such, the HPIIP developer will submit a subdivision application subsequent to Council granting land use redesignation within the site to isolate the ± 20 acre parcel as illustrated. Since Shell Canada has no immediate plans to develop this property, the HPIIP developer requests that all applicable RVC levies and assessments be deferred until such time the parcel is actually developed. Should Council approve the request and subdivision, the Applicant shall register a caveat against title to the proposed ±20 acre parcel to advise future landowners of their obligation to connect to municipal servicing and provide appropriate levies and assessments at the development permit stage. Deferral of the levies does not guarantee water or wastewater availability.

APPENDIX E

STAGE 6 OUTLINE PLAN

HIGH PLAINS INDUSTRIAL PARK STAGE 6 OUTLINE PLAN

Sunterra Beef Ltd.

Adopted September 29, 2015



ROCKY VIEW COUNTY
Cultivating Communities

BYLAW C-7515-2015

A Bylaw of Rocky View County pursuant to Division 12 of Part 17 of the Municipal Government Act to amend Bylaw C-6930-2010, known as the “High Plains Industrial Park Conceptual Scheme”

The Council of Rocky View County enacts as follows:

PART 1 - SHORT TITLE

1. The short title of this Bylaw is the “Bylaw C-7515-2015”.

PART 2 - DEFINITIONS

2. In this Bylaw the definitions and terms shall have the meanings given to them in the Land Use Bylaw C-4841-97 and the Municipal Government Act.

PART 3 - EFFECT OF BYLAW

THAT Bylaw C-6930-2010, known as the “High Plains Industrial Park Conceptual Scheme”, be amended to append the “Stage 6 Outline Plan” affecting Lot 1, Block 1, Plan 0612816 within SW-01-26-29-W04M; Lot A, Plan 0710376 within SW-01-26-29-W04M; and SW-01-26-29-W04M, as contained in Schedule ‘A’ attached to and forming part of this Bylaw; and

THAT The amendments contained in Schedule ‘A’ attached to and forming part of the Bylaw be adopted to provide a framework for subsequent redesignation, subdivision, and development within Lot 1, Block 1, Plan 0612816 within SW-01-26-29-W04M; Lot A, Plan 0710376 within SW-01-26-29-W04M; and SW-01-26-29-W04M, consisting of an area of approximately 56.41 hectares (\pm 139.39 acres) attached to and forming part of this Bylaw.

PART 4 - TRANSITIONAL

1. Bylaw C-7515-2015 is passed when it receives third reading, and is signed by the Reeve/Deputy Reeve and the CAO or Designate, as per the *Municipal Government Act*.

Division: 7

File: 06401006/07 and 36401004 – PL20140024

<i>PUBLIC HEARING WAS HELD IN COUNCIL this</i>	<i>September day of the 29th, 2015</i>
<i>READ A FIRST TIME IN COUNCIL this</i>	<i>September day of the 29th, 2015</i>
<i>READ A SECOND TIME IN COUNCIL this</i>	<i>September day of the 29th, 2015</i>
<i>UNANIMOUS PERMISSION FOR THIRD READING</i>	<i>September day of the 29th, 2015</i>
<i>READ A THIRD TIME IN COUNCIL this</i>	<i>September day of the 29th, 2015</i>

Reeve

CAO or Designate

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Introduction

CITYTREND, a local urban planning firm, was retained by Sunterra Beef Ltd in 2011 to prepare the design and process of an Outline Plan for the lands illustrated on Figure 1 and identified as the subject lands.

Sunterra Beef Ltd is a family owned, Alberta based company. Sunterra purchased the lands illustrated on Figure 4 in 2008 for the construction of a meat packaging plant. Sunterra Beef Ltd owns approximately 140 acres of land.

The subject lands are part of the High Plains Industrial Park Conceptual Scheme (CS) and are identified as Stage 6 of the CS. Planning, development and construction is currently underway in Stages 1 and 2 while land use has been obtained for Stage 3. The subdivision process for portions of stage 1, 2 and 3 is currently being reviewed by Rocky View County.

Existing Policies

The Outline Plan is prepared under the guidance of the Municipal Development Plan, The Balzac East Area Structure Plan and the policies of the High Plains Industrial Park Conceptual Scheme.

The Municipal Development Plan supports diverse business development in business parks and encourages diversity in business park developments.

The Balzac East Area Structure Plan ("BEASP") identifies the lands for future industrial development and requests the production of a Conceptual Scheme (CS) prior to subdivision. The High Plains Industrial Park Conceptual Scheme (CS) was adopted by Rocky View County Council on July 27, 2010. Figure 3 identifies the CS plan area, which consists of two full sections of land (Sec 1-26-29-W4M and Sec 12-26-29-W4M) and comprises seven (7) stages of development. The CS requires that an Outline Plan be approved by Rocky View County Council for each stage to ensure comprehensive development. Once approved, the Outline Plan will form part of the High Plains Industrial Park Conceptual Scheme. Stages 1, 2 and 4 have been adopted by Rocky View County Council.

Rocky View County Servicing Standards, the Land Use Bylaw and the current Direct Control bylaw (DC94) apply to the proposed development, as amended.

Purpose

The purpose of the Outline Plan is to provide direction on:

- Road connectivity between the stages;
- Subdivision design concept;
- Land use; and
- Utilities.

The High Plains Industrial Park Conceptual Scheme (CS) establishes criteria to review in the preparation of the Outline Plans.

The applicable criteria are as follows:

- An update to the Master Utility Servicing Plan to detail the specific mechanisms and expectations regarding implementation of utility service infrastructure in relation to the requirements of the overall CS area and surrounding lands;
- An update to the Traffic Impact Assessment to detail specific mechanisms and expectations required to implement local and regional roadway improvements in relation to the requirements of the overall CS area and surrounding lands;
- An update to the Stormwater Staged Master Drainage Plan to detail specific mechanisms and expectations regarding implementation of stormwater infrastructure in relation to the requirements of the overall CS area and surrounding lands;
- Identifying Municipal Reserve through a Landscaping and Public Amenities Plan that will detail the improvements made to public area along with expectations for use and maintenance responsibilities;
- Prepare an Emergency Response Plan that will address measures as they relate to emergency responses and/or evacuations in the event of a major industrial accident;
- Land use amendment criteria in relation to the subdivision and development phases; and
- Architectural controls and performance standard as may be required to implement development considerations.

Existing Conditions

Plan Area

Figure 1 illustrates that the plan area is located in the southwest corner of the High Plains Industrial Park Conceptual Scheme (CS) plan area. It is bounded by Range Road 291 to the west, High Plains Road to the north, undeveloped High Plains Boulevard to the east, and the undeveloped City of Calgary road allowance 144 Ave NE to the south.

The plan area includes 2 lots owned by Sunterra Beef Ltd. The total plan area is approximately 134.52 Acres or 54.41 Hectares.

Context

As illustrated on Figure 2, the Plan Area is located in the southwest corner of the CS. To the north is the approved Stage 1 Outline Plan area which consists of the Target distribution Centre and to the east is existing farmland; this land to the east has been designated as Natural Resource Industrial (NRI) District and is part of the CS area. The decommissioned Nexen Gas Plant is located to the west in the East half of 2-26-29-W4M and the City of Calgary's undeveloped 144 Ave NE is located to the south, further south is the City of Calgary's Transportation Utility Corridor. The parcel of land in the southwest corner of SW 1-26-29-W4M is owned by Ironwood III Assets Inc and is not part of the plan area.

Site Features

Existing Buildings

As illustrated on Figure 4, the site is predominantly undeveloped with the exception of 2 buildings in Lot 1 Block 1 Plan 0612816, a lift station in PLAN 0710376 and two existing stormwater catch basins.

As illustrated on Figure 4, Building 1 is an office building and is currently vacant. Building 2, a meat packaging facility, was built in 2007 and should return to operation in the spring of 2015.

A land use redesignation was approved in 2012 to allow for the use of building 1 for office purposes. The regulations are found under the Direct Control Bylaw (DC94).

Building 3 in PLAN 0710376, is a lift station that is owned and managed by Rocky View County.

Existing Stormwater Facilities

The existing stormwater facilities were developed to capture post development flow for the two buildings on site. The stormwater facilities will continue to operate until the storm pond, that form part of the Outline Plan, is built.

Pipelines and Right of Ways

Figure 1 shows that the site contains six (6) pipeline right-of-ways, three (3) of which are sour gas pipelines that lead to the abandoned Nexen Gas Plant to the west of the site, one (1) Rocky View County water line, one (1) ATCO gas pipeline, and one (1) Canadian Western Natural Gas Company pipeline. The Outline Plan has incorporated the pipelines into the design. It will see

many of the pipelines located to the front or back of a subdivided lots in order to maximize the developable area of each lot.

Three of the gas pipelines are operated by Nexen and are sour gas (502JK, 5819JK, and 4965JK). Pipeline 502JK is an abandoned pipeline while pipelines 5819 JK and 4965 JK have been decommissioned. Nexen is planning to abandon these facilities in the future and discussions have begun with Nexen on the abandonment and removal of the pipelines.

The Canadian Western Natural Gas Company Limited pipeline 951 2010 is located along the south boundary of the plan area. This right-of-way will be located to the back of the proposed lots 13, 14 and 15. The existing pipeline will not limit the development of the lots.

A Rocky View County water pipeline 0711197 is located to the north of the abandoned 502 JK pipeline and the decommissioned 4965 JK pipeline.

Appendix A comprises all the caveats related to the lands in the Outline Plan area.

SW 1-26-29-W4M

Ironwood III Assests Inc. has facilities located in the southwest corner of SW 1-26-29-W4M. The site is approximately 20 acres and is not part of the Outline Plan area. Access to the site is from Range Road 291. The site may become accessible from 144 Avenue NE. Access to this existing site will not be compromised by the Outline Plan and the services will remain unchanged.

Development Considerations

Stage 6 Outline Plan proposed the development of approximately 15 industrial lots with minimum parcel size of 2.75 ha (6.8 ac) and one Public Utility Lot. The plan and lot layout may change at the subdivision stage without the need for amendments to the Stage 6 Outline Plan.

Figure 5 illustrates the Outline Plan with all existing features, future proposed roads and proposed lot layout.

The lot design considers a number of existing site characteristics. The main design considerations are as follows:

- Existing facilities (office and meat packaging facility);
- Presence of sour gas pipelines;
- Presence of municipal services (sewer line);
- Existing development to the north; and
- Existing parcels (Lot 2, Block 1, PLAN 0711938, and PLAN 0710376).

Access

The main access to the plan area will be from an existing roadway to the north of the plan area: High Plains Road. Future access will also connect to High Plains Boulevard once the right of way is developed. The design of the Outline Plan will see that no access, with the exclusion of the existing access to lot 11, is created from Range Road 291.

Lots 1 to 4 will gain access directly to High Plains Roads, lots 5 to 10 will have double frontage and lot 12 to 15 will access the internal road system.

Double frontage lots adjacent to High Plains Boulevard should consider the facade facing this public right-of-way as the front yard.

Pipelines and Right of Ways

Figure 1 outlines the location of the utility right-of-way within the Plan Area. Figure 7 provides a more detailed illustration of the meat packaging facility, office, and the utility right-of-ways.

The location of the access into the plan area from Range Road 291 is restricted by the presence of existing buildings, sour gas pipeline right-of-ways, and a Rocky View County sewer line.

Subdivision and Phasing

The plan area is divided into 3 phases. Phase 1 consists of the lots immediately south of High Plains Road. It also includes subdivision of the meat packaging facility and the office building (lots 11 and 12) from Lot 1 Block 1 Plan 0612816. The north/south section of the internal road will be developed as part of phase one. A temporary cul-de-sac will placed at the end of the north/south internal road until a connection is provided to High Plains Boulevard.

The proposed phases are conceptual only and may change at the subdivision stage. No amendments will be required to the Outline Plan for phasing changes.

The subdivision layout and development details will require to meet the policies and bylaws

applicable at the time of subdivision. Modifications may be needed to meet the policies, bylaws and standards applicable and no amendments will be required to the Outline Plan.

Conceptual phasing plan is illustrated on Figure 6.

Development Statistics

Below are the proposed development statistics for Stage 6 Outline Plan. The development statistics are an approximation and will be refined at the subdivision stage.

Area	Acres (AC)	Hectares (HA)	Percentage (%)
Total Lot area	+/- 110.4	+/- 44.7	82
Road Widening	+/- 2.0	+/- 0.7	1.5
Roads	+/- 6.06	+/- 2.4	4.5
PUL	+/- 16.05	+/- 6.5	12
Total Site Area	134.52	54.41	100.00

Open Space

Figure 8 illustrates the open space for the plan area. The proposed open space plan produced for this Outline Plan accommodates the intention of the High Plains Industrial Park Conceptual Scheme. No Regional Pathway has been planned for this stage of development, and therefore, no Municipal Reserve has been planned for Stage 6 Outline Plan.

Municipal Reserve dedication will be achieved via cash-in-lieu compensation. The cash-in-lieu will be calculated according to the policies of Rocky View County at the time of subdivision.

Land Use

Figure 9 illustrates the existing land use for the plan area.

The current land uses for the subject lands are Natural Resource Industrial District (NRI) and Direct Control Bylaw (DC94). The Direct Control district was approved in 2004 and amended in 2012. DC94 allows for the meat packaging facility and accessory uses compatible with the operation, as well as office uses for the existing office building.

Section 3.0 of the Conceptual Scheme, under the land use concept section, states that the plan area lands will be used to accommodate a range of general industrial developments. Appendix B illustrates the General Land Use Concept for the Conceptual Scheme area. These developments tend to be heavier industrial processing activities and industrial developments with extensive outside storage. In order to achieve a range of general industrial uses, the plan area will need to comprise of a variety of lot sizes and this plan allows for that.

Figure 10 illustrates the proposed land uses for the plan area.

The stormwater pond to the north of the office will be redesignated from Natural Resource Industrial District (NRI) and Direct Control Bylaw (DC94) to Public Services District (PS).

The lands containing the office and meat packaging facilities (Lots 11 and 12) will remain with the Direct Control Bylaw (DC94) designation.

The remaining lands (Lots 1-10 and 13-15) will be redesignated from Direct Control Bylaw (DC94) and Natural Resource Industrial District (NRI) to Industrial - Industrial Activity District (I-IA). The purpose and intent of this district is to provide for a range of industrial activity, including industrial activity that may have off-site nuisance impacts, and the support services that may be associated with such activity.

Land Use redesignations will be applied and processed concurrently.

Infrastructure

Transportation

As per the criteria in the Conceptual Scheme, an update to the Traffic Impact Assessment is required for each Outline Plan. Bunt & Associates have completed the assessment for the roads in relation to the future uses for the plan area. The full report is provided under separate cover.

Direct access into the CS area (Figures 2 and 3) is by Highway 566 to the north, Range Road 291 to the west, Range Road 290 to the east. Main access to the CS area will be from Township Road 261, a continuous roadway between Highway 2 and Range Road 291.

High Plains Industrial Park Stages 1, 2 and 3 development programs each required improvements to the road network in the context of the overall long-term needs for the area. It is not expected that any additional improvements will be required for the offsite road network as a result of Stage 6. The need for offsite upgrades will be further evaluated at the Subdivision and Development Permit stages.

The results of the existing intersection capacity analysis confirmed that all offsite intersections are operating within acceptable capacity parameters. Offsite improvement, if required, will be in accordance with the approved TIA prepared at each stage of subdivision. No improvements are recommended at this time and further evaluation will take place at the Subdivision and Development Permit stages.

The analysis assumed full build-out of Stage 6 in 2017 and analyzed the following intersections:

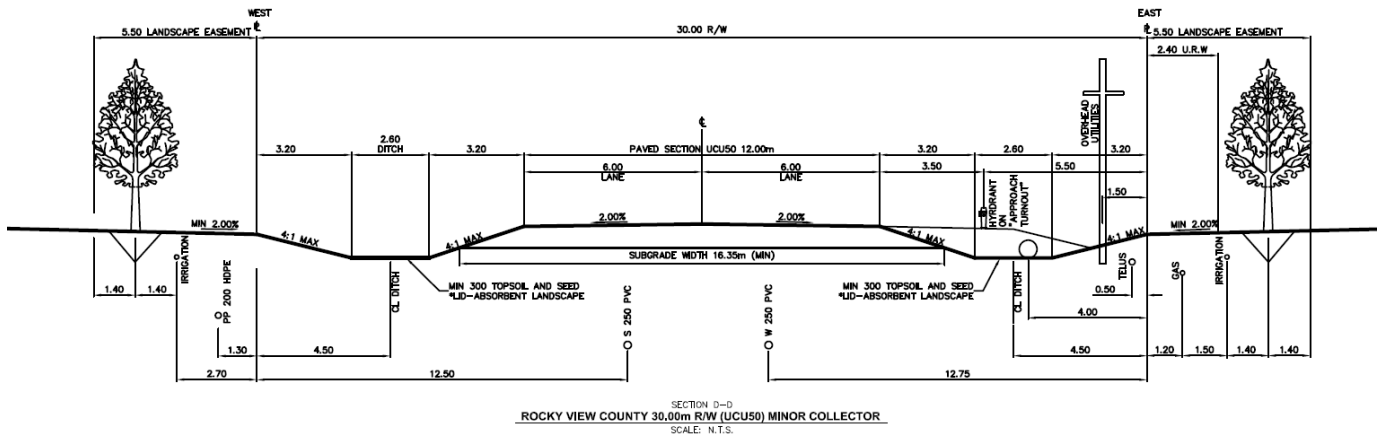
- Range Road 291 and Highway 566
- Range Road 291 and Township Road 261
- Township Road 261 and Dwight McLellan Trail
- Township Road 261 and CrossIron Blvd
- CrossIron Blvd and Highway 556 and
- Highway 566 at both Highway 2 connections (east and west)

Road Characteristics in Plan Area

For the matter of the Outline Plan area, access will mainly occur from High Plains Road to the north/south internal road to be constructed, and High Plains Boulevard.

The figure below illustrates a proposed roadway cross-section for the internal roads of the plan area. The road allowances within the plan area measure 30m and will allow for one lane of traffic in either direction.

All roadways within the Outline Plan area will be constructed by the developer and will be completed to the standards identified by the County's engineer. All cost recoveries will be facilitated by the County at the time development occurs.



Stormwater

Stormwater management for Stage 6 Outline Plan will be provided by surface drainage. A report outlining the details of the stormwater management strategies was produced by Westhoff Engineering Resources, Inc and is provided in support of the Stage 6 Outline Plan. The report was produced in December 2014.

The proposed stormwater management system embraces the Low Impact Development (LID) approach and places the focus on the capture of runoff within an existing self-contained sub catchment of the Nose Creek watershed.

The runoff from the lots and roads will be conveyed through roadside ditches and discharged into the stormwater storage facility, Pond 6. At the detailed design stage, details should be provided about the overland drainage conveyance and the use of the Stormwater.

The strategy envisions collected stormwater use. The use of stormwater will fall under 2 categories:

- Irrigating landscaped areas and wetting of impervious areas and;
- Using treated water for industrial uses.

Additional conclusions and recommendations are included in the report by Westhoff Engineering Resources, Inc., provided under separate cover.

Servicing

Water distribution system servicing

The proposed water distribution system in place will provide potable domestic water and fire flows to the Stage 6 development. Stormwater will be used for irrigation and as a source of processing water for industrial/commercial uses. The water distribution system will be designed according to the Rocky View County Servicing Standards and Alberta Environment Standards and Guidelines.

Sanitary servicing

The Outline Plan area will be serviced by gravity sewer lines within the development area which drain to an existing local Rocky View County lift station adjacent to Range Road 291. This lift station then connects via a 250mm force main to the existing 600mm East Rocky View Wastewater Transmission Main which transfers sewage from East Balzac to the Langdon Wastewater Treatment Plant. The capacity of the existing local lift station will be further analyzed through a sanitary servicing study which will be completed at the subdivision stage to identify potential facility upgrades and the timing related to those upgrades.

Shallow Utilities

In East Balzac, Fortis, ATCO Gas and Telus all have facilities in place. Capacity will be reviewed at the detail design stage and any necessary improvements will be identified.

Additional information on servicing is available under separate cover in the Stage 6 Utility Servicing Design Brief prepared by IBI Group.

All infrastructure upgrades required to accommodate the development of Stage 6 Outline Plan will be the responsibility of the Developer.

Additional Plans and Studies

At the subdivision stage and as part of each subdivision phase, the developer will provide the following documents:

- Construction Management Plan;
- Erosion Sedimentation Control Plan;
- Other documents as may be required by the County at the time of subdivision.

All documents will be prepared by the appropriate qualified professional.

Architectural Guidelines

The design guidelines for High Plains Industrial Park – Stage 2 will be applied to Stage 6. The full guidelines are provided in the appendices and a summary is included below.

Onsite parking and loading

Parking and loading will meet the requirement of the Rocky View Land Use Bylaw.

In addition, all parking, whether for visitors or employees, will be provided on site and quality directional signs will be provided.

Landscaping

Landscaping should be used to define areas such as entrances to buildings and parking areas, define plazas and amenity spaces, provide transition between neighbouring properties (buffering) and provide screening for outdoor storage, loading, and equipment areas. Landscaping will act as a continuous thread to tie together all phases of the High Plains Industrial Park development.

In addition to the requirements of the Rocky View County Land Use Bylaw, the criteria outlined in the appendix also applies.

Fencing

In addition to the requirements of the Rocky View County Land Use Bylaw, the following applies:

All fencing and screening will be designed and constructed with durable materials and integrated with the design of the building. Landscaped berms, mass planting of trees, shrubs and hedges should be used where appropriate to provide required screening.

All screening will be sufficient enough so as to visually screen parking, loading, storage areas and garbage areas from the roadways, pathways, outdoor amenity spaces, or adjacent lots.

Fences in highly visible locations such as facing public roadways, should be of a higher standard of fence, an example would be prefabricated back vinyl coated aluminum ornamental fencing, or black wrought iron fencing.

Signage

Signage is intended to help create a common element throughout the High Plains Industrial Park. The use of signage describing the tenant/owners and their business reinforces the identity of High Plains Industrial Park while other signage controls prevent unnecessary and unsightly signage from overpowering both individual buildings and their surroundings.

The requirements of the Rocky View County Land Use Bylaw and the criteria outlined in the appendix also apply.

Outside storage

All outdoor storage areas must be located away from the front entry of buildings and will be visually screened from neighbouring sites and public streets. The Rocky View Land Use Bylaw will also apply.

Lighting

On site lighting shall be designed to reduce spillage on adjacent properties and to protect the function of the Calgary International Airport. Lighting should only be used to highlight the area required to receive light.

Adequate lighting will be provided to ensure traffic safety at key conflict locales. In addition, all pedestrian/public areas are to be illuminated. Where appropriate, pedestrian scaled bollard lighting should be employed.

The requirements of the Rocky View Land Use Bylaw also apply.

Building Design

The plan area will consist mainly of industrial development. An effort should be made by designers to add articulations through the use of varied building materials and varied building heights. Building materials for the primary walls may consist of precast concrete or tilt up concrete. Other building materials may be approved at the discretion of the Developer. Building materials and colours shall be varied to create visual interest in building facades.

All colours, materials and finishes are to be coordinated on all exterior elevations of the buildings to achieve total continuity of design.

Colours proposed will be a major consideration of design approval. All colour schemes must be submitted for approval and shall include samples of finishes

All rooftop mechanical equipment that is located within 5.0m of the parapet wall shall be concealed by screening in a manner compatible with the architectural character of the building or concealed by incorporating it with the building roof so that it is not seen from ground level.

Any mechanical equipment (i.e. dust collection system) that sits outside of the building must be located at the back or side of the building.

Implementation

The developer should register the architectural guidelines on title and a proper process should be implemented to ensure compliance to these guidelines.

When reviewing an application the development authority of Rocky View County has the ability to use its discretion in the implementation of these guidelines and may grant variances where appropriate.

Emergency Response Plan

Nexen

The Conceptual Scheme discusses the proximity of the plan area to the Nexen Gas Plant. Due to its proximity and along with the nature of development that will be occurring within the Outline Plan area, an Emergency Response Plan is required.

The Gas Plant has been decommissioned since 2011 and is in the process of becoming abandoned. There is no sour gas flowing through the pipelines to the plant. The Nexen pipelines located on the Outline Plan lands have been abandoned and/or discontinued.

It is the responsibility of the developer to create an Emergency Response Plan that is comprehensive with the plans created by the neighbouring Outline plan areas and ensures that the proper steps are taken in the event of an industrial emergency.

Fire response

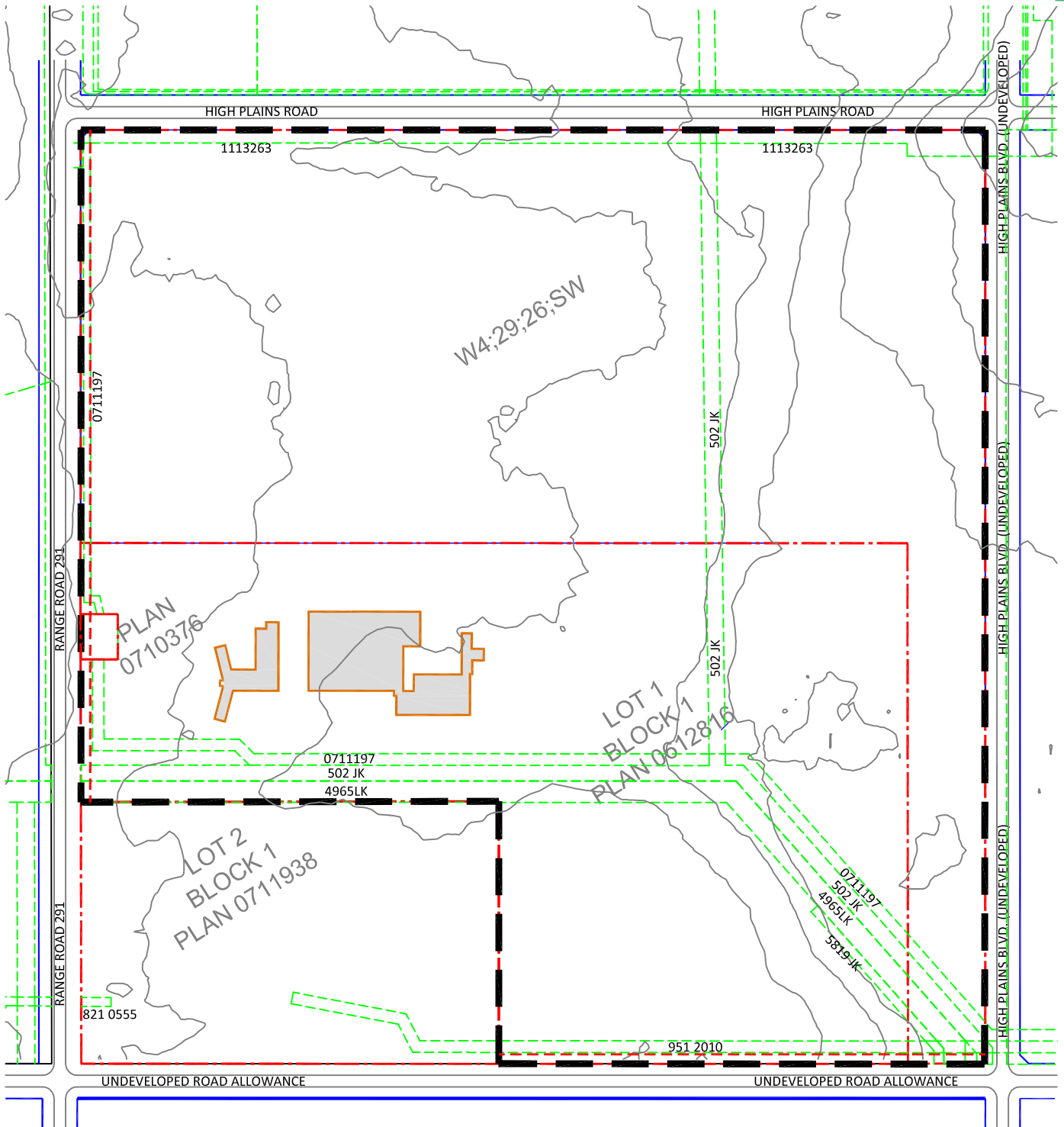
Fire Service to the area is expected to be from Balzac Fire Station 107. This station is located approximately 2.5 miles north of the Stage 4 Outline Plan area.

Police

Police Service is expected from an RCMP detachment in the City of Airdrie; the area will also be monitored by Rocky View County's Community Peace Officers.

EMS

Ambulance service is expected from EMS Facilities located in Calgary and Airdrie.



--- Plan Area (Subject Lands)

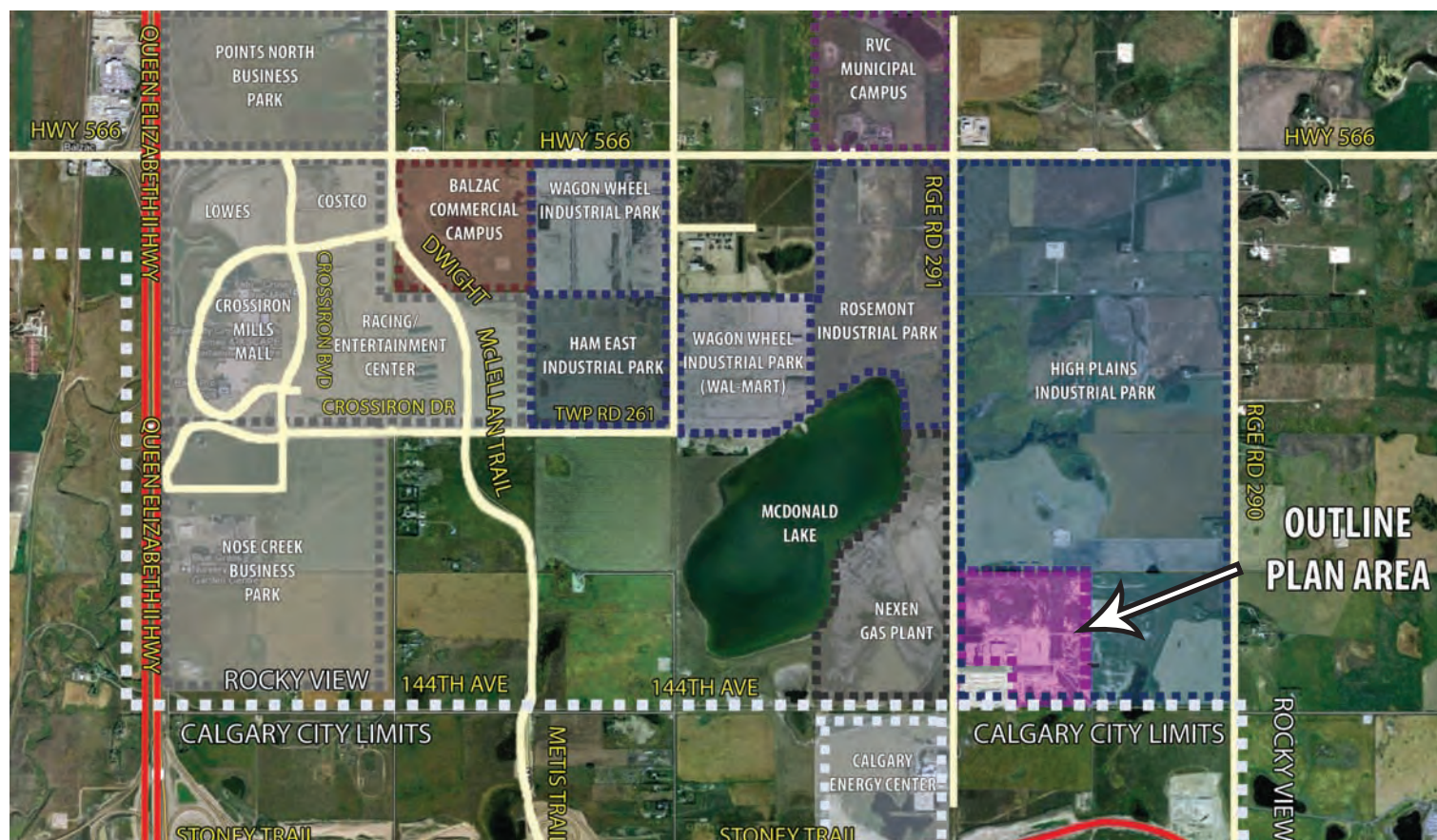
- - - Existing Property Lines

Figure 1 : Plan Area

Stage 6 Outline Plan
June 2015



NTS









-  Conceptual Scheme Boundary
-  Outline Plan Area
-  City of Calgary/Rocky View County Boundaries
-  Highways
-  Connection Roads
-  RVC Municipal Campus

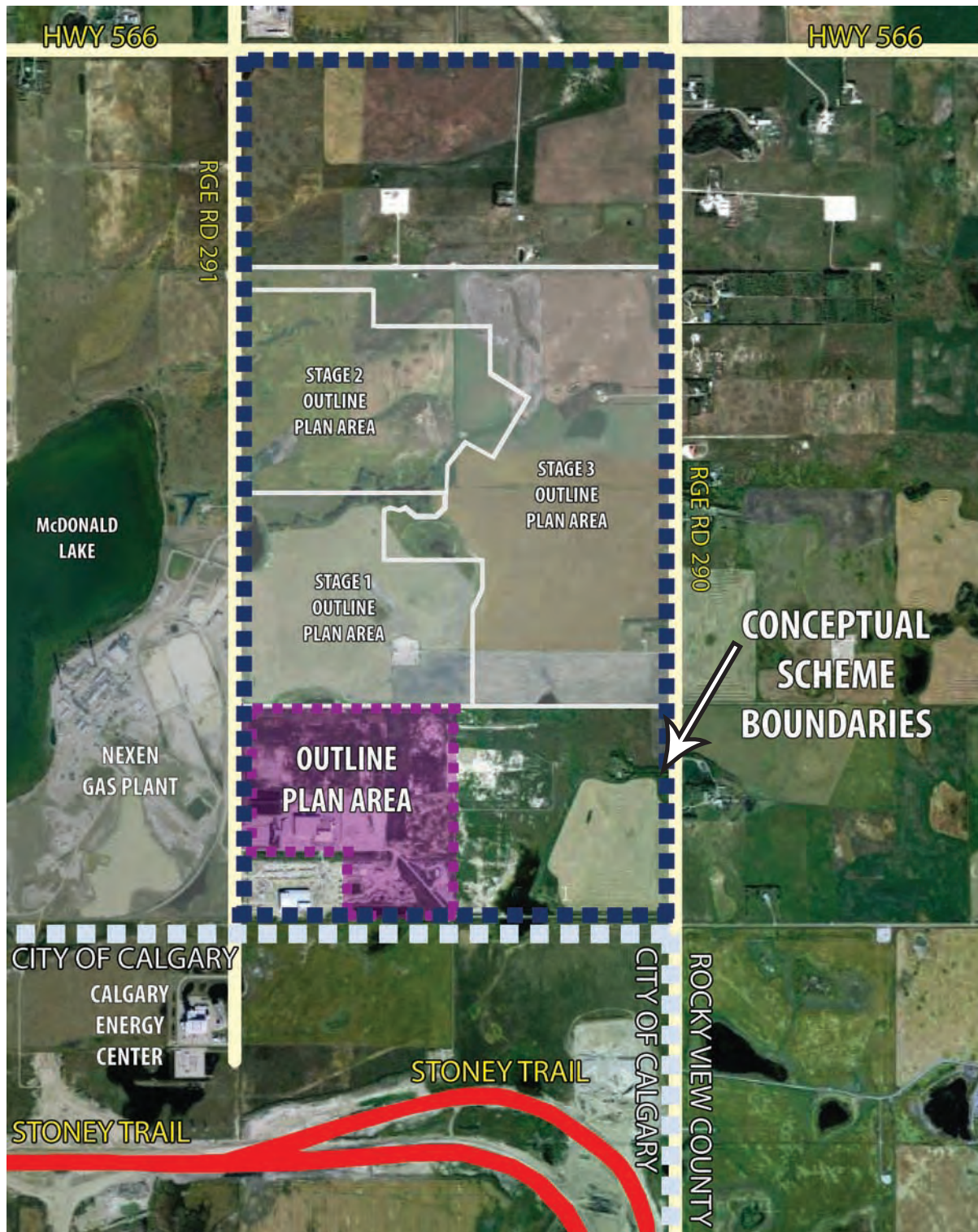
Figure 2: Context Map

Stage 6 Outline Plan
June 2015





NTS








-  Conceptual Scheme Boundaries
-  Outline Plan Area
-  City Of Calgary/ Rocky View Count Boundary
-  Connection Roads
-  High Plains Industrial Park Developemnt Stages

Figure 3: Conceptual Scheme Boundaries

Stage 6 Outline Plan
June 2015

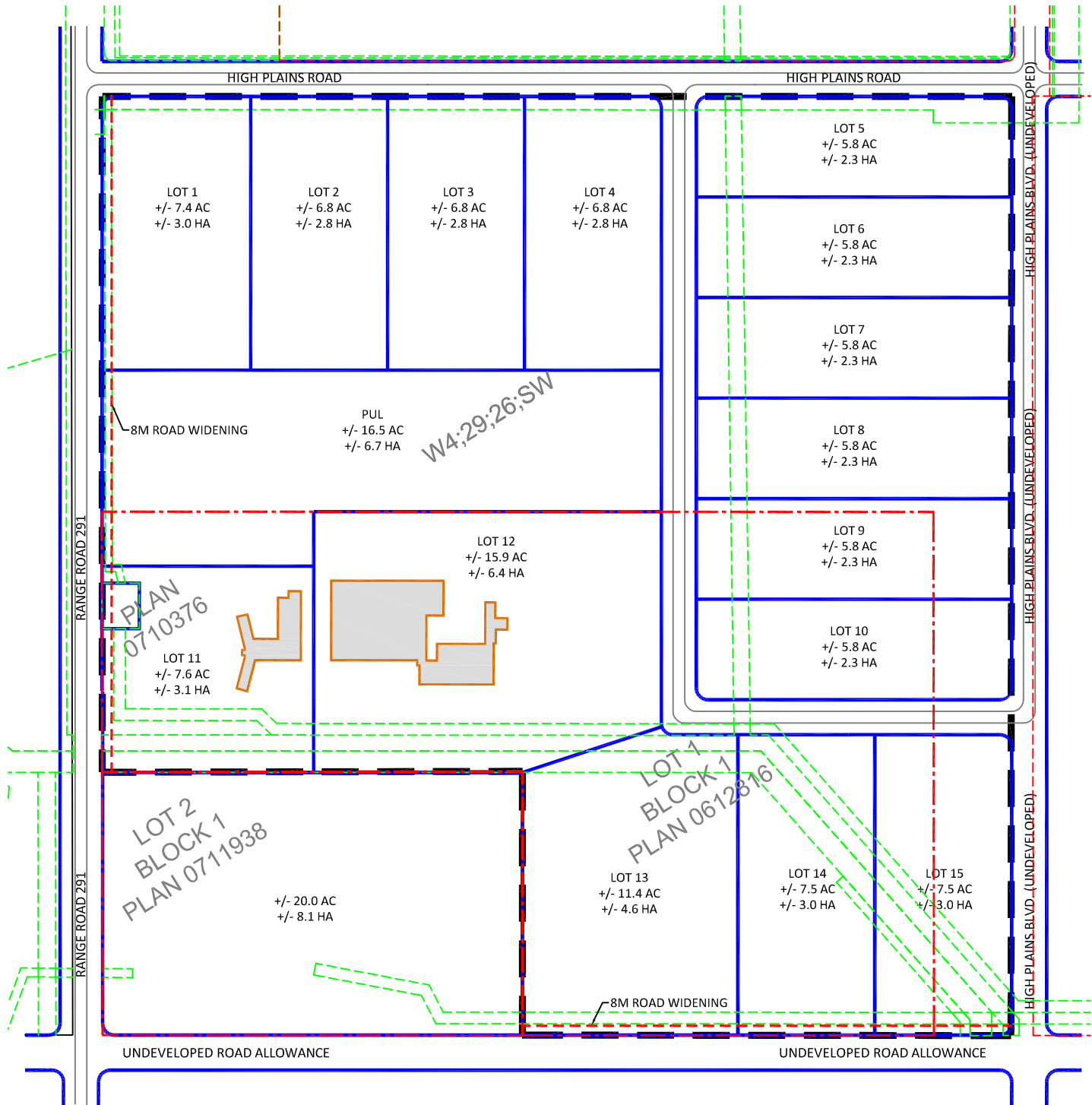




- ## RESULTS

RESULTS

RESULTS



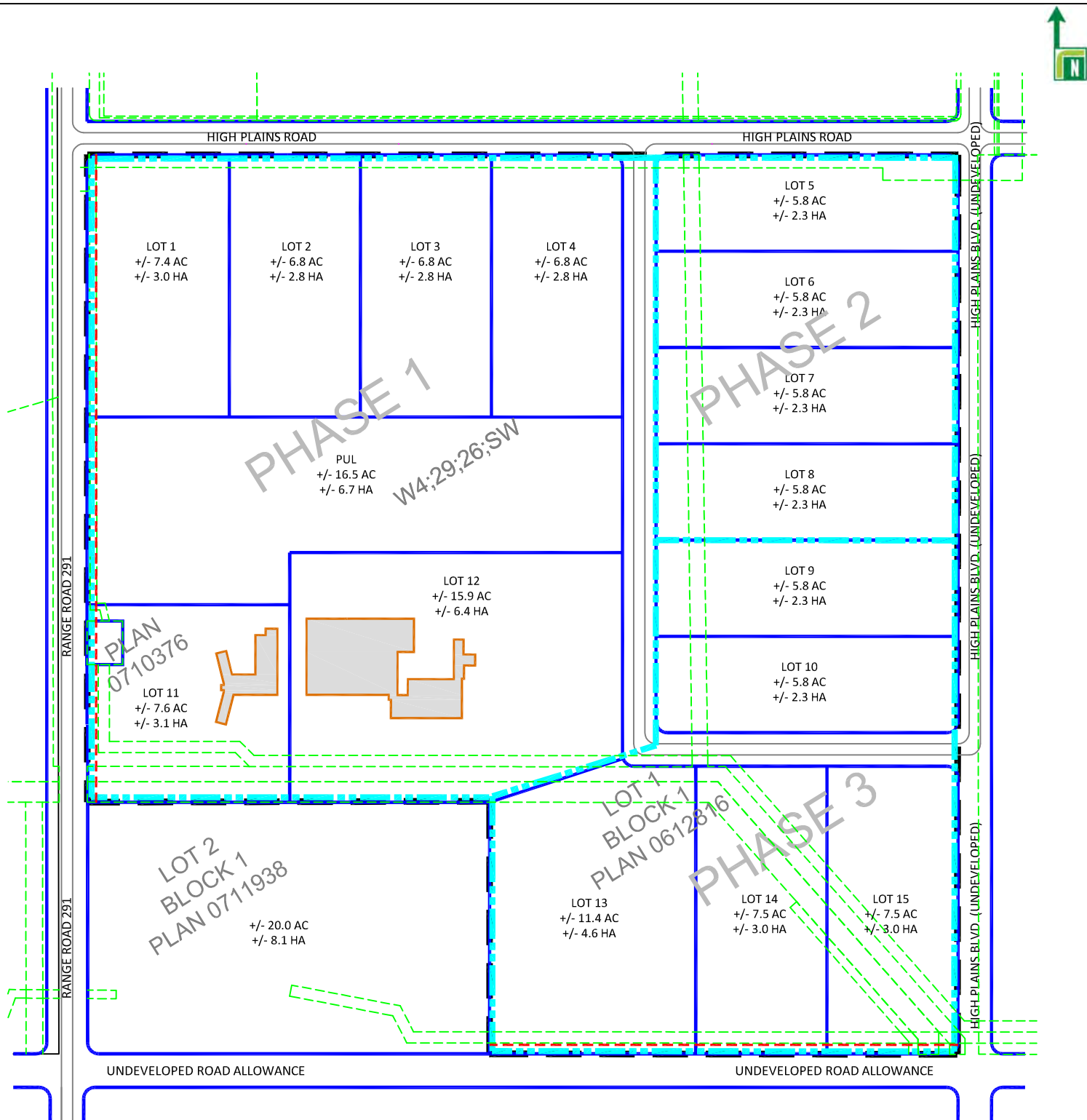
- Existing Property Lines
- Proposed Lot Lines
- ROW Boundaries
- Existing BLDG.
- Plan Area
- Road Widening (8m)

Note: Lot layout and specific lot dimensions to be defined at the subdivision stage.

Figure 5: Outline Plan

Stage 6 Outline Plan
June 2015



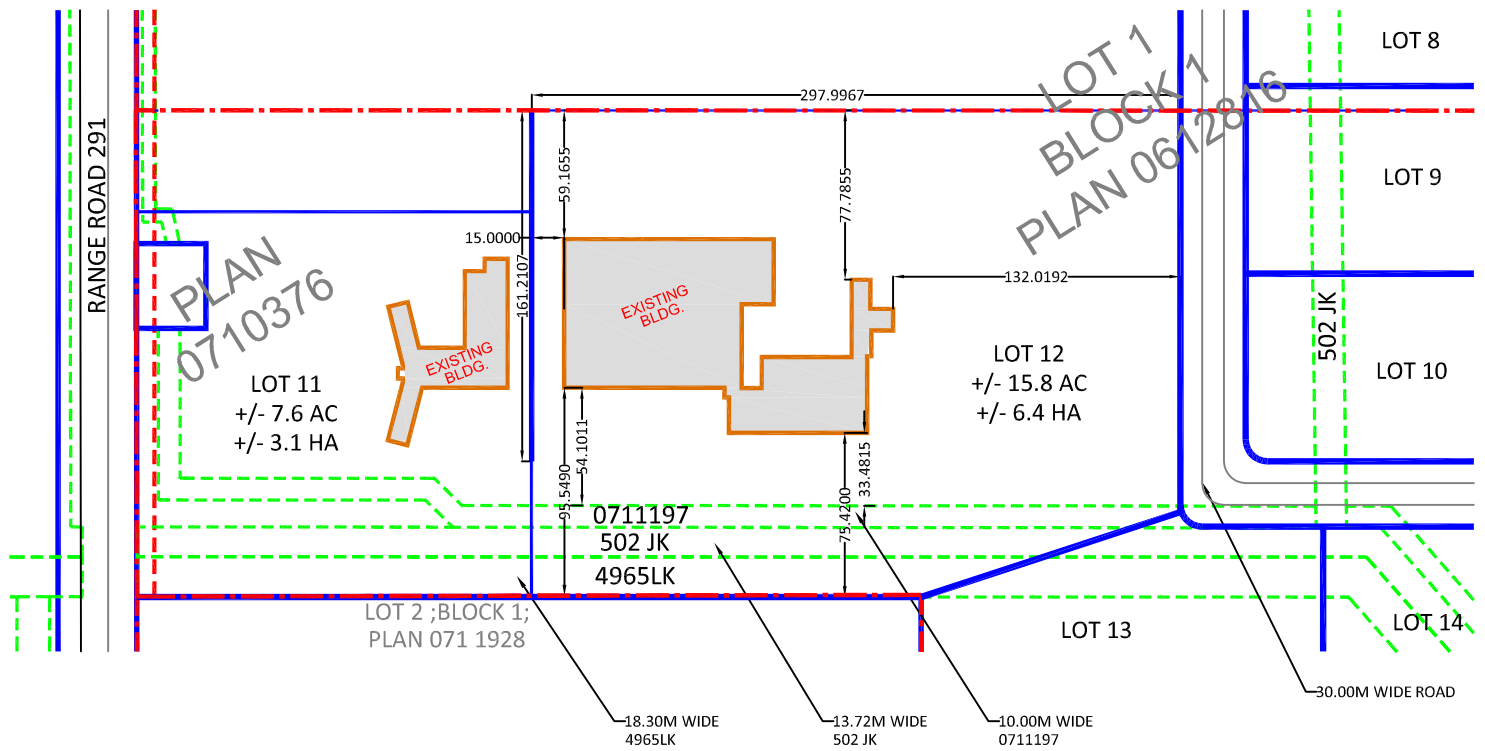


- Phasing
- Proposed Lot Lines
- ROW Boundaries
- Existing BLDG.
- Plan Area

Note: Lot layout and specific lot dimensions to be defined at the subdivision stage.

Figure 6: Phasing Plan

Stage 6 Outline Plan
June 2015



-  Existing Property Lines
 ROW Boundaries
 Proposed Lot Lines
 Existing Bldg.

Figure 7: Existing Facilities

Stage 6 Outline Plan

June 2015

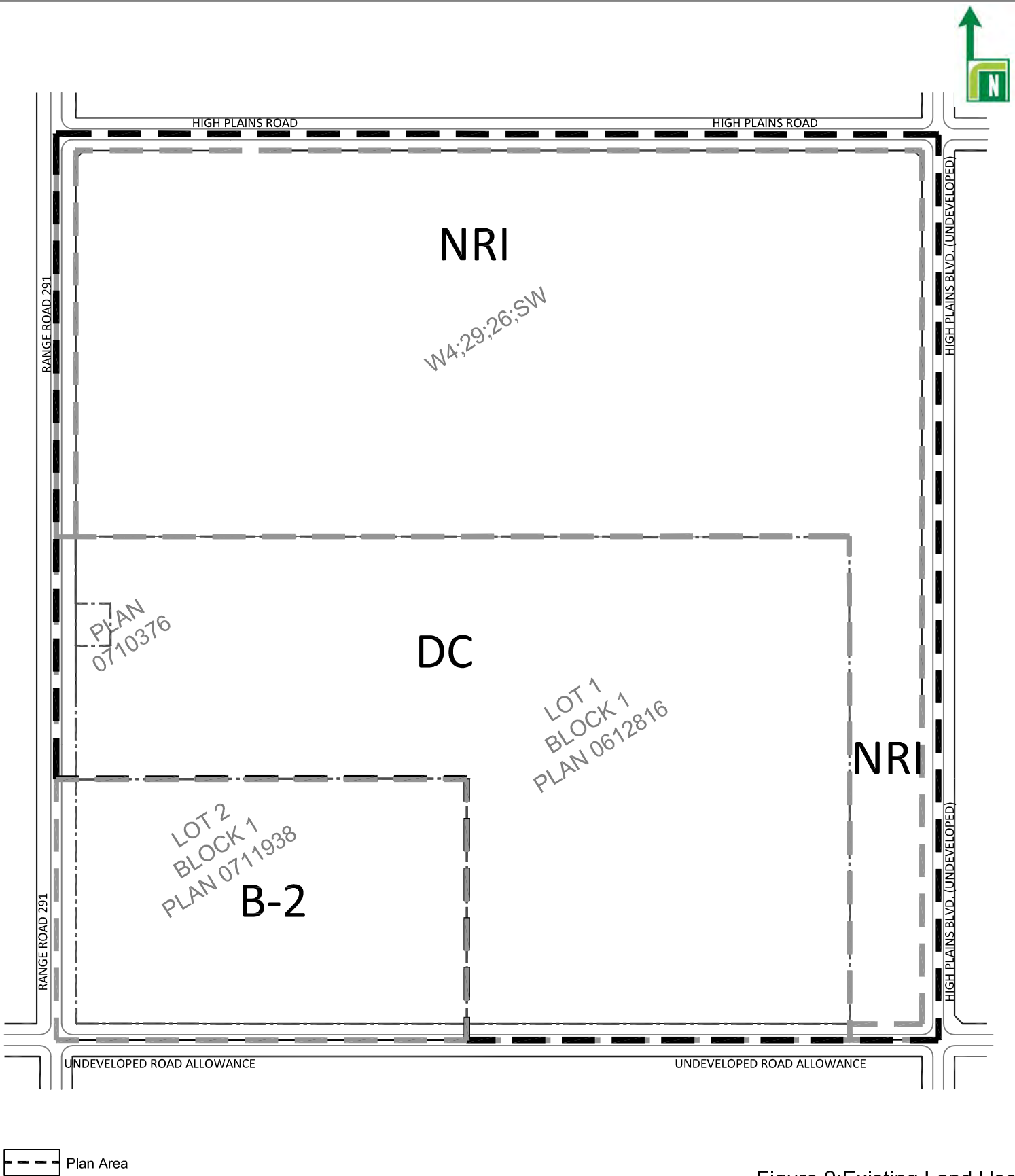


Figure 9:Existing Land Use

Stage 6 Outline Plan
June 2015

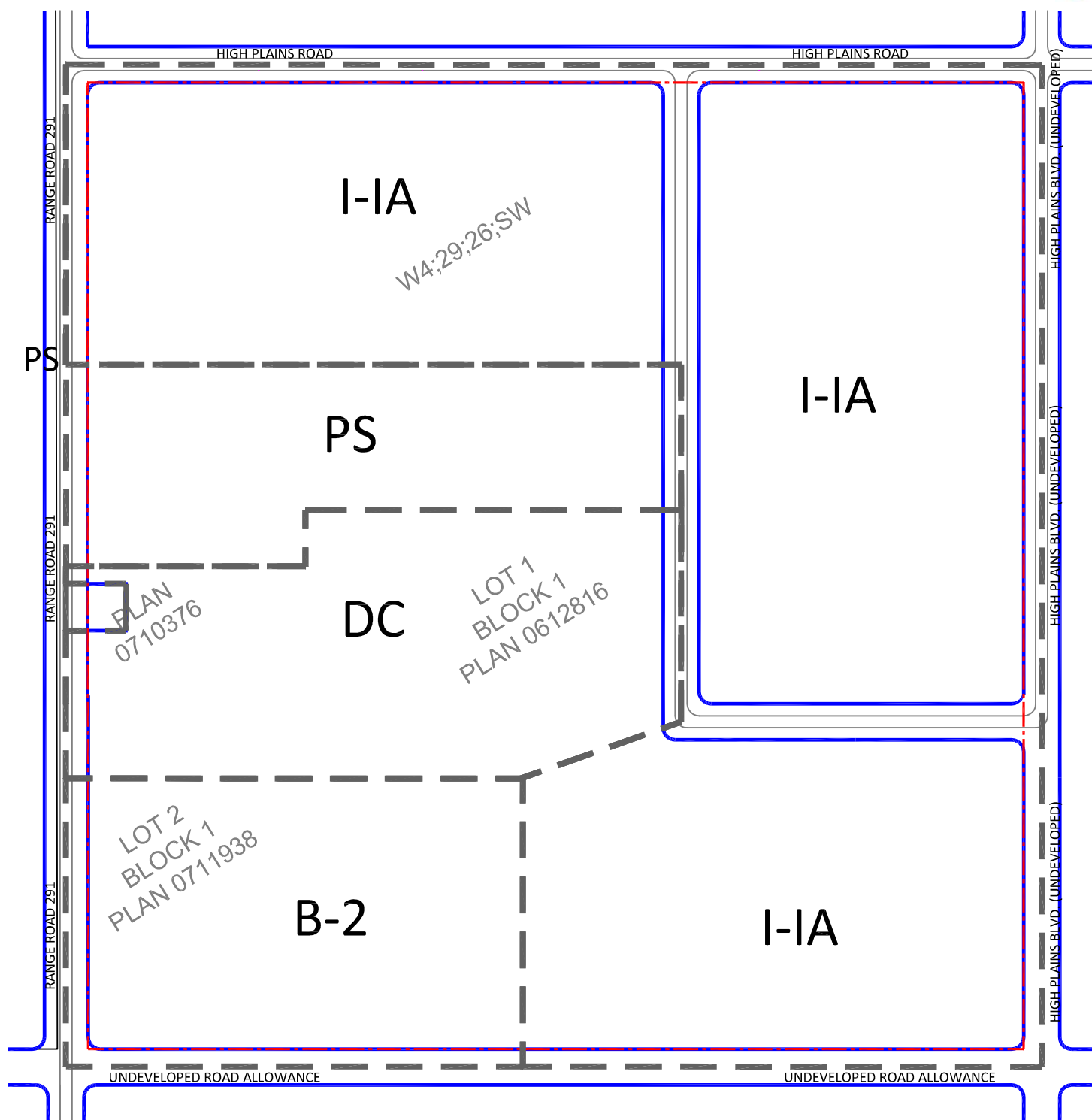
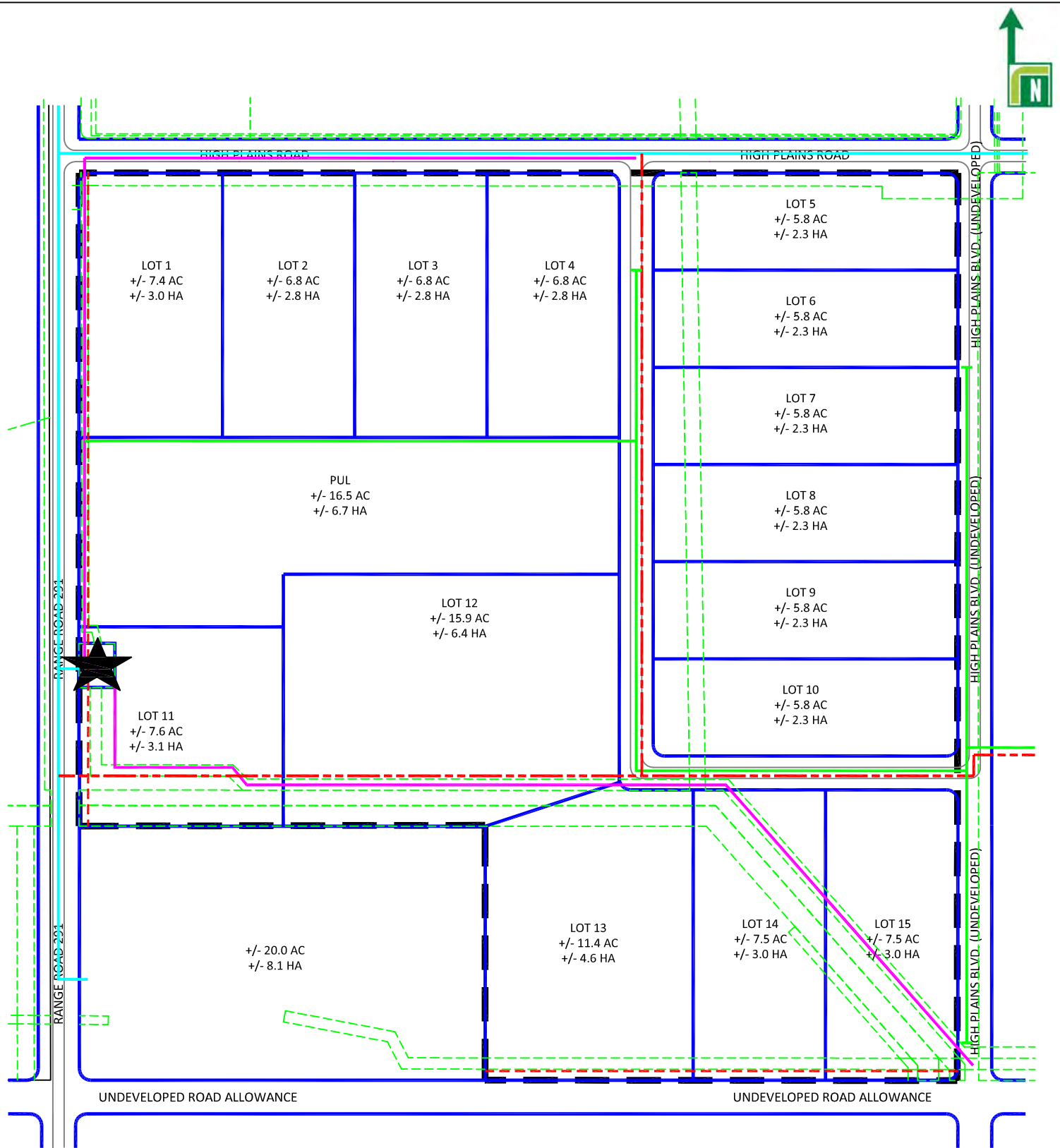








Figure 10: Land Use

Stage 6 Outline Plan
June 2015

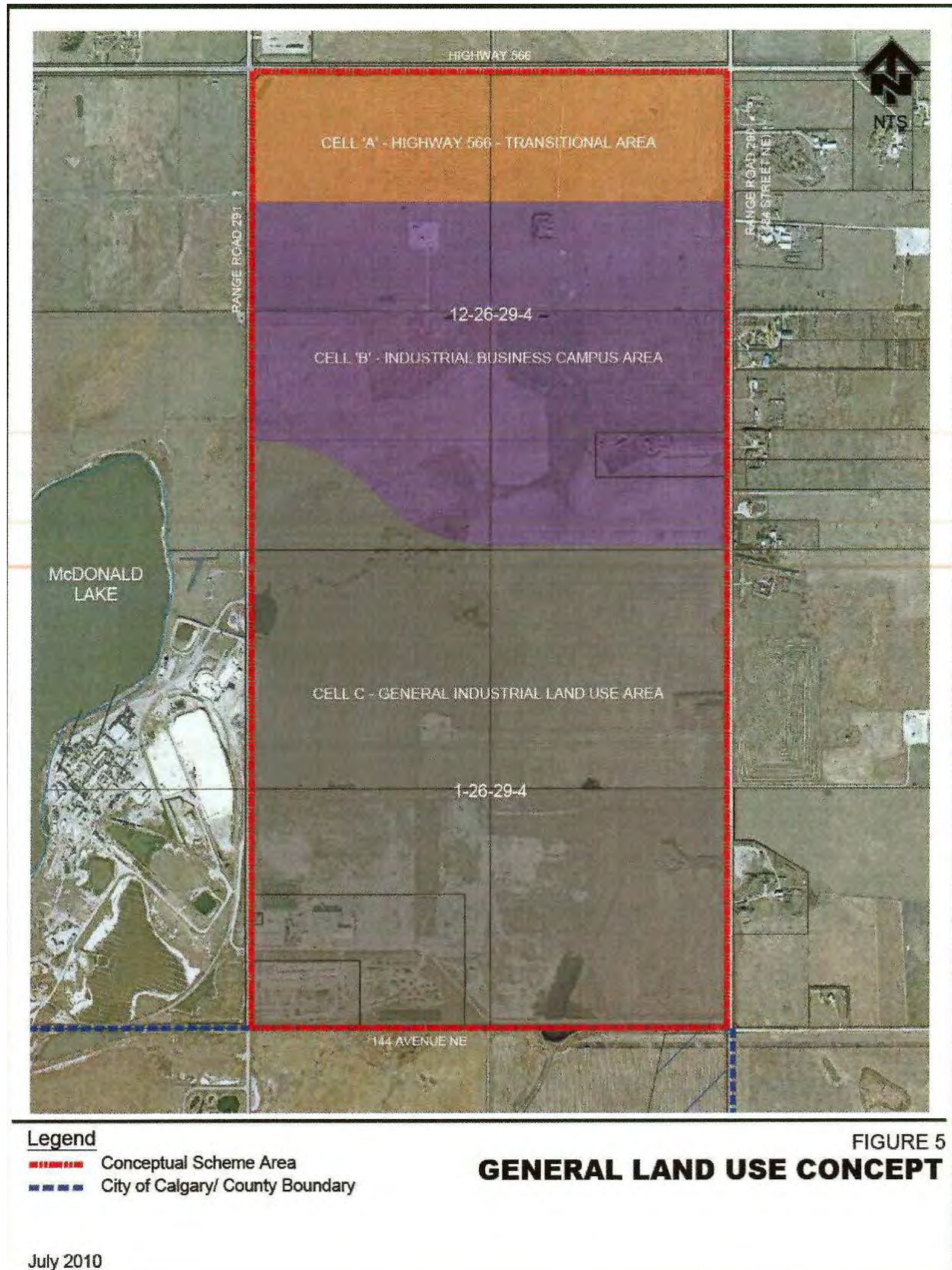


-  Lift Station
-  Proposed Water Main
-  Existing Water Main
-  Proposed Sanitary
-  Existing Sanitary
-  Plan Area

Note: Lot layout and specific lot dimensions to be defined at the subdivision stage.

Figure 11: Servicing
Stage 6 Outline Plan
June 2015

Appendix B: Conceptual Scheme General Land Use Concept



Appendix C: High Plains Industrial Park Design Guidelines

High Plains Industrial Park

1686825 Alberta Ltd., CLT Contracting Ltd., Highfield Stock Farm Inc. and Maxima Developments Inc.

HIGH PLAINS INDUSTRIAL PARK - STAGE 2

DESIGN GUIDELINES

APRIL 2014

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1. DESIGN GUIDELINES

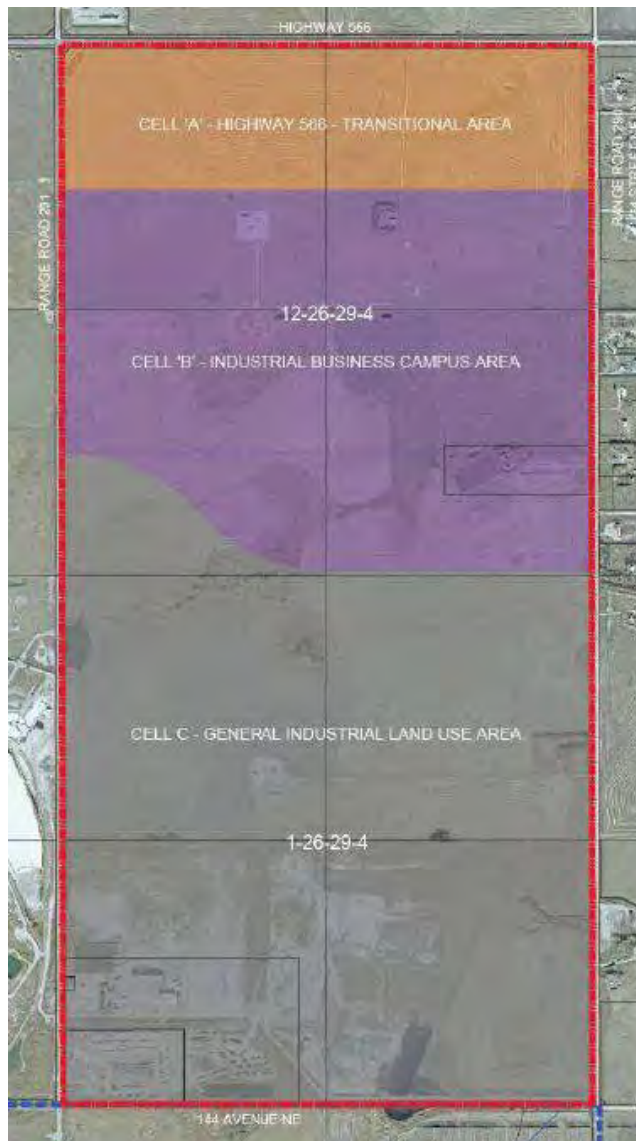
1.1 Purpose

The High Plains Industrial Park is located in Rocky View County Alberta and is part of the Balzac East Area Structure Plan. High Plains Industrial Park will be comprised of a comprehensive mix of business commercial and industrial land uses. This document will form the Design Guidelines for High Plains Industrial Park- Stage 2 which is located within the central portion of the site. Stage 2 is intended for general industrial uses and is zoned Industrial – Industrial Activity (I-IA).

The High Plains Industrial Park has easy access from Highway 566 and is in close proximity to Stoney Trail and the Queen Elizabeth Highway 2. These guidelines will form the framework for High Plains Industrial Park – Stage 2 and are intended to provide the appropriate criteria for design and construction. These guidelines are in no way intended to limit either design creativity or the functionality of the premises for their approved purposes.

The major design considerations of these guidelines are to:

- create an attractive environment characterized by quality in architectural design, selection of materials and colours, site layout and landscaping;
- ensure landscape design is integral to the overall site plan and provides consistency throughout the development including streetscape, parking areas and appropriate concealment of all service areas;
- ensure signage design is standardized to promote the development as one entity, to compliment the pedestrian-oriented environment. However, corporate identity/logos and suitable individual signs will be considered for buildings;



**Image from the High Plains
Conceptual Scheme**

- protect the owners, lessees and/or tenants of buildings within the development against improper and undesirable use of other building sites in High Plains Industrial Park, and to foster high quality developments;

These guidelines in no way limit the legal liability of the purchaser/applicant from compliance with the Alberta Building Standards, the Rocky View County Land Use Bylaw, the Rocky View County Commercial, Office & Industrial Guidelines and any other statutory requirements or regulations.

Furthermore, the Design Guidelines are meant to establish a framework around which individual landowners, builders, and/or architectural consultants can implement individual designs. This document is to serve as a guide to the Design Approval and the individual property owner / developer to effectively coordinate the visual and physical aspects of any new development.

2. GUIDELINE PROCEDURES

2.1 General

The Developer (1686825 Alberta Ltd., CLT Contracting Ltd., Highfield Stock Farm Inc., and Maxima Developments Inc.) will register these Design Guidelines as a Restrictive Covenant on the title of the land. It should be noted that the Developer is acting solely as the vendor of High Plains Industrial Park and in no capacity as the approving authority for Rocky View County. As noted in the Stage 2 Outline Plan for the High Plains Industrial Park, the Developer will be creating a design review committee.

It is the intention of the Developer to control and enhance development of High Plains Industrial Park by means of the restrictions and conditions of the Design Guidelines and to set these forth as the covenants registered against title. The covenants are intended to be common to all of the lots developed within High Plains Industrial Park to enhance and protect the value, market desirability, and aesthetic attractiveness of all of the lots to their mutual benefit.

It should be noted from the outset that these Design Guidelines are to be read in conjunction with the Rocky View County Land Use Bylaw and the Rocky View County Commercial, Office and Industrial Design Guidelines.

3. ZONING REGULATIONS

Stage 2 in High Plains Industrial Park is zoned I-IA which is Industrial- Industrial Activity. All rules pertaining to setbacks, site coverage and building height must be in accordance with section 73 – Industrial – Industrial Activity (I-IA) of the Rocky View County land use bylaw.

4. ON SITE PARKING AND LOADING

In addition to the requirements of the Rocky View County Land Use Bylaw

1. All parking, whether for visitors or employees, shall be provided on site.
2. Quality directional signs shall be provided.

5. LANDSCAPING

5.1 Intent

Landscaping should be used to define areas such as entrances to buildings and parking areas, define plazas and amenity areas, provide transition between neighbouring properties (buffering) and provide screening for outdoor storage, loading, and equipment areas. Landscaping shall be a major visual element within the overall development. Landscaping, in concert with the signage, is the continuous thread, which ties the High Plains Industrial Park development together.

A professionally designed landscape plan prepared by a landscape architect will be required for the architectural submission.

In addition to the requirements of Rocky View County Land Use Bylaw, the following applies:

1. All yards and all open spaces on the site, excluding parking stalls, onsite circulation, outdoor storage, display and service areas is required to be landscaped with trees, shrubs, sod or an approved suitable hard landscaping material.
2. Parking and Loading may encroach into the minimum front yard setbacks as long as the minimum required landscaping is provided as outlined in the Land Use Bylaw. This will be reviewed on an individual application basis by the Developer.
3. All areas of the site not otherwise developed, shall be soft landscaped, with naturalized grasses, sod or hydro-seed, tree and shrub beds, or hard landscaped with rock mulch, pavers, coloured concrete, concrete with pattern or other porous pavements.
4. Landscape materials shall consist of a variation of native and ornamental plantings capable of thriving in Rocky View County's climate. Trees, shrubs, ground covers, and naturalized grasses are preferred to flower beds, manicured sod, etc.
5. Winter appearance must be considered and material species chosen accordingly. Where landscape materials are used to provide screening, the coverage should be retained on a year round basis.
6. Trees shall be planted in the ratio of at least one coniferous tree to every four deciduous trees (20% coniferous to 80% deciduous). A maximum of 35% coniferous trees will be permitted.
7. The size requirement for deciduous trees in manicured planting areas is to be a minimum of 50% of the trees shall be 75mm calliper or larger and up to 50% may be less than 75mm calliper.
8. The size requirement for coniferous trees shall be 50% 2.5m in height or taller and up to 50% less than 2.5m m in height. Larch will be accepted as a coniferous tree but may not contribute to more than 20% of the total coniferous tree quantity.
9. Shrubs shall be planted in masses. These groups shall consist of no fewer than five plants of each species used.
10. All landscaped areas shall be finished with a minimum gradient of 2% to ensure positive drainage. A maximum gradient of 3:1 (33%) shall be permitted. Surface drainage shall be

directed to onsite drainage locations. A variety of topography, including berms, flat or sloped areas and depressions are encouraged.

11. The owner of the property, or their successor or assignees, shall be responsible for the proper maintenance of the landscape and any repairs to damage to the adjacent linear storm water system caused by run-off from the site.
12. No potable water shall be used for irrigation purposes. All irrigation water will be provided from the storm pond and metered. The rate is discounted at 15% of irrigation rates in the City of Calgary. There are no restrictions on quantity as there is excess storm water available.

6. FENCING AND SCREENING

In addition to the requirements of Rocky View County Land Use Bylaw, the following applies:

1. All fencing and screening shall be designed and constructed with durable materials and integrated with the design of the building. Landscaped berm's, mass planting of trees, shrubs and hedges should be used where appropriate to provide required screening.
2. All screening shall be sufficient enough so as to visually screen parking, loading, storage areas and garbage areas from the roadways, pathways, outdoor amenity spaces, or adjacent lots.
3. Black Vinyl Coated Chain link fencing at 2.44 m (8'-0") in height is the preferred fence within Stage 2. All posts, attachments, gates, chain link, and hardware shall be Black vinyl coated. Barbed wire may be used at the top of the fence for security purposes.
4. Black Vinyl Coated Chain link fencing higher than 2.44 m (8'-0") in height may be approved at the discretion of the Developer.
5. Fences in highly visible locations such as facing public roadways, should be of a higher standard of fence, an example would be prefabricated black vinyl coated aluminium ornamental fencing, or black wrought iron fencing.

7. SIGNAGE

7.1 Intent

Signage, along with landscaping, will help create a common element throughout the High Plains Industrial Park. The use of signage describing the tenant/owners and their business reinforces the identity of High Plains Industrial Park while other signage controls prevent unnecessary and unsightly signage from overpowering both individual buildings and their surroundings.

In addition to the requirements of the Rocky View County Land Use Bylaw, the following applies:

1. Directional-lit only signage will be permitted, all fixtures to include full cut-offs.
2. Pylon signage where deemed appropriate and approved by Rocky View County, approving authorities shall be in keeping with the architectural theme of the project. Pylon signs shall be located completely on private property.

3. All signage must be approved and shall conform to the requirements of the Rocky View County Land Use Bylaw as well as noted below:
 - a. Fascia signs will not be approved if they protrude above the roof level of a building to which they are attached.
 - b. No building shall rely on signage alone to distinguish individual entries. Signage shall only be used to identify a specific tenant, acting as a supplement to the entry design.
 - c. Signs affixed to the building may identify, by lettering and/or numbers the name, business and products of the owner(s) or occupant(s) of the premises. Signs may include the registered trademark or symbol if the signage complies with the intent and requirements set out in these guidelines. However, all such signage must be approved.

8. OUTSIDE STORAGE

In addition to the requirements of Rocky View County Land Use Bylaw, the following applies:

All outside storage areas must be located away from the front entry of the building and shall be visually screened from neighbouring sites and public streets.

9. LIGHTING

To help eliminate light pollution, extra care must be used in selecting the lighting for the buildings and for site. In addition to the requirements of the Rocky View County Land Use Bylaw, the following applies:

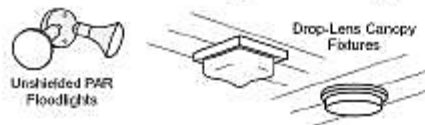
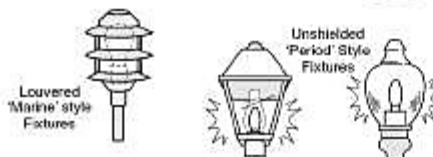
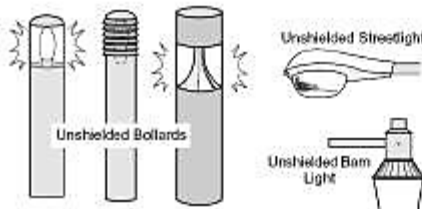
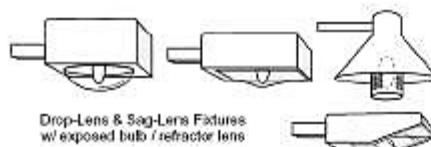
1. No external lighting fixtures shall be higher than the closest building parapet height when located adjacent to the building, parking lot lighting excluded.
2. Lighting should be used to highlight only the area required to receive light. Light spillage, particularly onto adjoining properties or into the sky, must be avoided.
3. Lighting design, which incorporates flood lighting of large areas of the site or building elevations will not be permitted.
4. Adequate lighting shall be provided to ensure traffic safety at key conflict locations.
5. All pedestrian/public areas are to be well illuminated. Where appropriate pedestrian scaled bollard lighting should be employed.
6. All light fixtures should be fully shielded to eliminate light pollution. The following exhibit outlines permitted and prohibited fixtures.

Better Lights for Better Nights

Help eliminate light pollution. Select the best fixture for your application using this guide. Use the lowest wattage bulb appropriate for the task and turn off the light when it's not being used.

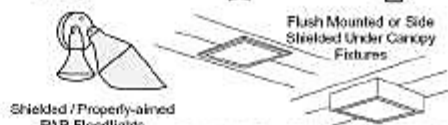
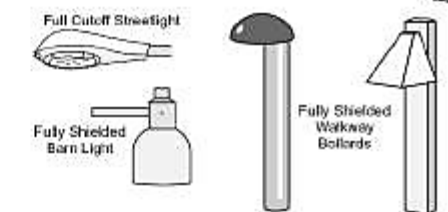
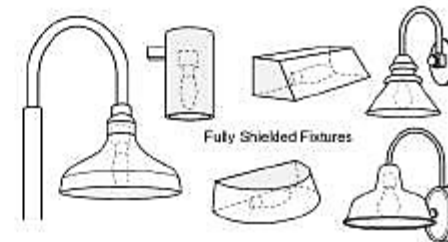
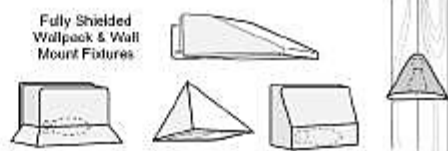
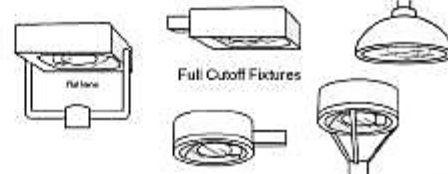
Unacceptable / Discouraged

Fixtures that produce glare and light trespass



Acceptable

Fixtures that shield the light source to minimize glare and light trespass and to facilitate better vision at night



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Dark Sky Society

www.darksksociety.org

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10. BUILDING DESIGN

Buildings in Stage 2 will be large format industrial buildings. An effort should be made by designers to add articulation to the elevations through the use of varied building materials and varied building heights. Building materials for the primary walls may consist of precast concrete or tilt up concrete. Other building materials may be approved at the discretion of the Developer. Building materials and colors shall be varied to create visual interest in building facades.

1. All colors, materials and finishes are to be coordinated on all exterior elevations of the buildings to achieve total continuity of design.
2. Colors proposed will be a major consideration of design approval. All color schemes must be submitted for approval and shall include samples of all finishes.
3. All rooftop mechanical equipment that is located within 5.0m of the parapet wall shall be concealed by screening in a manner compatible with the architectural character of the building or concealed by incorporating it with the building roof so that it is not seen from ground level.
4. Any mechanical equipment (i.e. dust collection system) that sits outside of the building must be located at the back or side of the building.

11. APPROVAL PROCESS

11.1 Intent

As noted in the Stage 2 Outline Plan for the High Plains Industrial Park, the Developer will be creating a design review committee. Lot owners will be required to submit site and building design plans to the Developer for review and approval prior to seeking approvals from Rocky View County. Submission requirements are identified in section 11.2, in addition the applicant shall ensure that all aspects of the proposed project meets the Rocky View Commercial, Office and Industrial Guidelines document prepared and available from Rocky View County. A Development Permit Checklist is also available from Rocky View County and all items requested on the check list shall be provided. Each submission must contain at least three copies of each drawing as the Developer shall each retain copies of all approved drawings and the applicant shall receive a record of each approved drawing duly stamped and noted.

11.2 Development Permit Application

Development permit application drawings must be approved by the Developer prior to submission to Rocky View County. The development permit drawings shall conform to the approved final design drawings and may be the same submission as the final design submission. All drawings submitted to Rocky View County must bear the approval of the Developer. Immediately upon release of a development permit from Rocky View County, the applicant shall provide the Developer with one copy of that approval including copies of any attached conditions. The applicant will advise the Developer of any Rocky View County recommendations for changes to the site or building plans.

APPENDIX F

STAGES 4 & 5 OUTLINE PLAN



HIGH PLAINS INDUSTRIAL PARK **STAGES 4 & 5** **OUTLINE PLAN**

FEBRUARY 2019

High Plains
Industrial Park



BYLAW C-7820-2018

A Bylaw of Rocky View County pursuant to Division 12 of Part 17 of the Municipal Government Act to amend Bylaw C-6930-2010, known as the "High Plains Industrial Park Conceptual Scheme".

The Council of Rocky View County enacts as follows:

PART 1 – TITLE

This bylaw shall be known as Bylaw C-7820-2018.

PART 2 – DEFINITIONS

In this bylaw, the definitions and terms shall have the meanings given to them in Land Use Bylaw C-4841-97 and the *Municipal Government Act*.

PART 3 – EFFECT OF BYLAW

THAT Bylaw C-6930-2010, known as the "High Plains Industrial Park Conceptual Scheme", be amended to append the "Stages 4 and 5 Outline Plan" affecting NW-12-26-29-W04M and NE-12-26-29-W04M, as contained in Schedules "A" and "B", attached to and forming part of this Bylaw; and

THAT The amendments contained in Schedules 'A' and 'B' attached to and forming part of the Bylaw to be adopted to provide a framework for subsequent redesignation, subdivision, and development within NW-12-26-29-W04M and NE-12-26-29-W04M, consisting of an area of approximately 127.89 hectares (316.02 acres) attached to and forming part of this Bylaw.

PART 4 – TRANSITIONAL

Bylaw C-7820-2018 is passed when it receives third reading, and is signed by the Reeve/Deputy Reeve and the Municipal Clerk, as per Section 189 of the *Municipal Government Act*.

Division: 7

File: 06412003/2004 – PL20180077

PUBLIC HEARING WAS HELD IN COUNCIL this

12th day of February, 2019

READ A FIRST TIME IN COUNCIL this

12th day of February, 2019

READ A SECOND TIME IN COUNCIL this

12th day of February, 2019

UNANIMOUS PERMISSION FOR THIRD READING

12th day of February, 2019

READ A THIRD TIME IN COUNCIL this

12th day of February, 2019



ROCKY VIEW COUNTY
Cultivating Communities

[Handwritten signature]

Reeve

[Handwritten signature]

CAO or Designate

[Handwritten date]

Date Bylaw Signed

SCHEDULE 'A'
FORMING PART OF BYLAW C-7820-2018

Schedule of Amendments to Bylaw C-6930-2010:

Amendment #1:

Amend the Table of Content by adding a reference to the Appendix and numbering accordingly:

APPENDIX 'F' – STAGES 4 AND 5 OUTLINE PLAN

Amendment #2:

Attach the "High Plains Industrial Park Stages 4 and 5 Outline Plan" as defined in Schedule 'B' attached to and forming part of this Bylaw.

Amendment #3:

Overall Abbreviations, Numbering, Grammar, Spelling and Punctuation

1. Renumbering the Bylaw as required.
2. Wherever the renumbering of the Bylaw affects a numbering reference elsewhere in the Bylaw, adjust the affected reference.
3. Italicize all definitions within the Bylaw.
4. Without changing the meaning or intent of the Bylaw, correct all grammatical, spelling, punctuations and spacing errors.

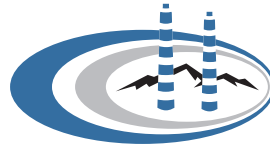
SCHEDULE 'B'
FORMING PART OF BYLAW C-7820-2018

An Outline Plan affecting the area within NW-12-26-29-W04M and NE-12-26-29-W04M, consisting of an area of approximately 127.89 hectares (316.02 acres), herein referred to as the "High Plains Industrial Park Stages 4 and 5 Outline Plan".

ACKNOWLEDGMENTS

PROJECT TEAM:





STAGES 4 & 5 OUTLINE PLAN

FEBRUARY 2019 | HIGHFIELD LAND MANAGEMENT INC.

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SECTION 1.0

Introduction

This Outline Plan for Stage 4 & 5 of the High Plains Industrial Park (HPIP) (“hereafter known as “The Outline Plan”) has been prepared by B&A Planning Group, with a team of technical experts, on behalf of Highfield Land Management (HLM) in support of the Land Use Redesignation for ± 127 hectares (± 316 acres) of land (hereafter known as the “Plan Area”). This Outline Plan reflects development Stage 4 & 5, supporting and rationalizing HLM’s proposal to redesignate the Plan Area from Ranch and Farm District (RF) to Industrial – Industrial Activity (I-IA). The proposal for the Plan Area builds upon the guiding principles and intentions of the County Plan, East Balzac Area Structure Plan, and HPIP Conceptual Scheme (CS).

The approved HPIP CS formalizes a comprehensive development framework to guide implementation and phasing within the East Balzac area, enabling a flexible approach that responds to market demand. Section 3.2 of the HPIP CS establishes a general land use concept for the area that proposes a mix of industrial and commercial uses.

HLM envisions Stage 4 & 5 of the Industrial Park to encompass a mix of employment generating commercial and industrial uses that respond to market demands; and Municipal Reserve (MR) in the form of landscape buffers, linear parks, and regional pathways.

This Outline Plan provides a rationale to support a minor amendment to the Balzac East Area Structure Plan as described in Section 13.0. The ASP Amendment proposal is consistent with Section 4.2.2 of the Calgary Metropolitan Region Board Interim Regional Evaluation Framework, and as such, does not require approval from the CRMB.

The Plan Area comprises ± 127 hectares (± 316 acres) and is located within the East Balzac portion of Rocky View County, east of Cross Iron Mills and various commercial-industrial business parks. The Balzac East Area Structure Plan (BEASP) anticipates an extension of similar uses into the Plan Area as appropriate and also identifies the southern portion of the Plan Area as Special Development Area (SDA) #5, which permits “heavier” uses such as General Industry Class III.

2.1 Existing Site Conditions

The Plan Area contains approximately ± 127 hectares (± 316 acres) of land within NW and NE 12-26-29-W4M and is owned by Highfield Investment Group Inc, CLT Contracting Ltd., CLT Developments Ltd., and Kidco Shares Ltd.

Three existing roadways currently provide direct access to the Plan Area. The roads include:

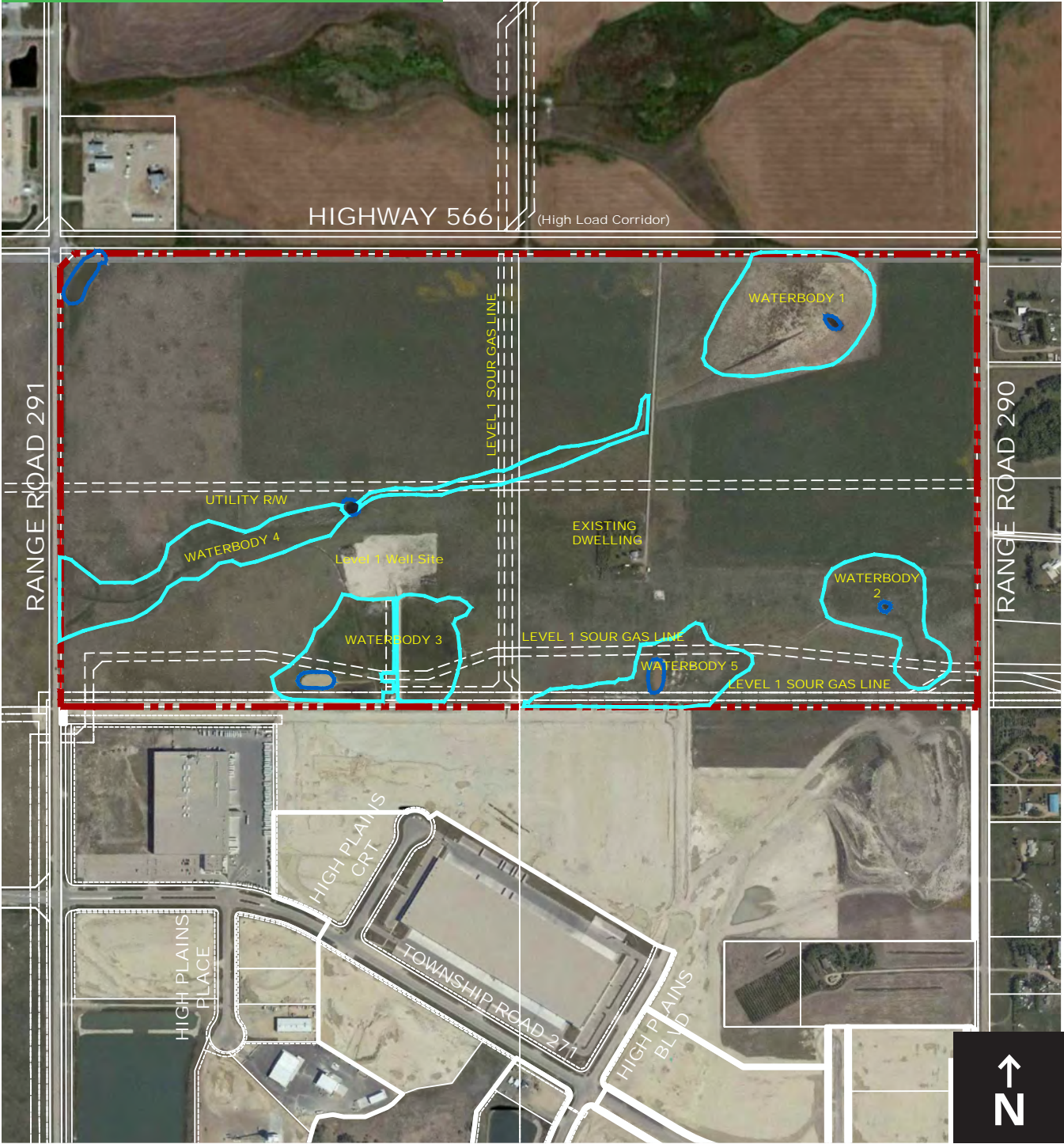
- **HIGHWAY 566 (NORTH BOUNDARY):** Alberta Transportation designates Highway 566 as a High Load Corridor running from Highway 772 NE of Calgary to Highway 9, East of the Plan Area. It is currently a two lane east-west, undivided, Secondary Highway with a paved surface treatment, which transitions into four lanes between the Highway 2 interchange and Dwight McLellan Trail. The current posted speed is 80 km/hour.
- **RANGE ROAD 291 (WEST BOUNDARY):** A two lane north-south, Regional Arterial roadway. The posted speed limit along this road is 60 km/hour with the majority of it paved, dead-ending north of Highway 566 and south of Township Road 261.
- **RANGE ROAD 290 (EAST BOUNDARY):** A two lane north-south Regional Arterial roadway with a posted speed limit of 60 km/hour. Range Road 290 is currently paved from Highway 566 south to 144 Avenue where it transitions to a gravel surface south to Country Hills Boulevard NW in the City of Calgary.

Figure 2 illustrates the existing site conditions. Local terrain is variable with surface drainage generally towards the southwest corner of the Site. As determined by the preliminary Wetland Assessment and Impact Report (WAIR) completed by Westhoff Engineering (2018), the majority of the lands have been cultivated with the remaining areas consisting of non-native grasslands.

Based on the WAIR, the Plan Area contains five (5) waterbodies that have been disturbed by agricultural practices from 1950 to present day. Follow-up field surveys later in the growing season will confirm the presence and extent of any classified wetlands. A submission to the Water Boundaries Division of Alberta Public Lands on March 6, 2018 confirms none of the waterbodies are Crown-claimed. The developer is not proposing to retain any wetlands and acknowledges that wetland loss will be mitigated by providing replacement (compensation) through existing Provincial approval processes under the Water Act and associated Alberta Wetland Policy.

In addition, the Plan Area includes an existing residence and a Level 1 abandoned well site in the central portions. Access to these sites is provided via Highway 566 and Range Road 291 respectively. Two level 1 sour gas pipelines run along the southern boundary of the Plan Area with an additional level 1 sour gas pipeline running north south through the centre. The pipeline operator (Nexen) is actively completing abandonment of the pipeline segments that dissect the Plan Area, anticipating full abandonment and removal by the end of 2018. An additional utility right-of-way runs east west through the centre of the Plan Area. The Plan Area includes a number of decommissioned Private Sewage Treatment Systems (PSTS) that will be removed and reclaimed at the subdivision stage.

Figure 2: Existing Site Conditions



- Legend
- Subject Lands
 - Dugouts
 - Wetlands
 - Utility / GasLine

2.2 Planning & Policy Context

2.2.1 THE COUNTY PLAN

The approved County Plan (Bylaw C-7280-2013) contains the overarching statutory policy framework and planning principles that guide development in Rocky View County.

The County Plan supports industrial and commercial expansion of East Balzac, including the Plan Area, and identifies it as a Regional Business Centre. The purpose of a Regional Business Centre is to provide regional and national business services, and local and regional employment opportunities. Regional Business Centres make a significant contribution in achieving the County's fiscal goals, while supporting growth from an agricultural to a mixed use industrial-commercial area.

2.2.2 BALZAC EAST AREA STRUCTURE PLAN (BEASP)

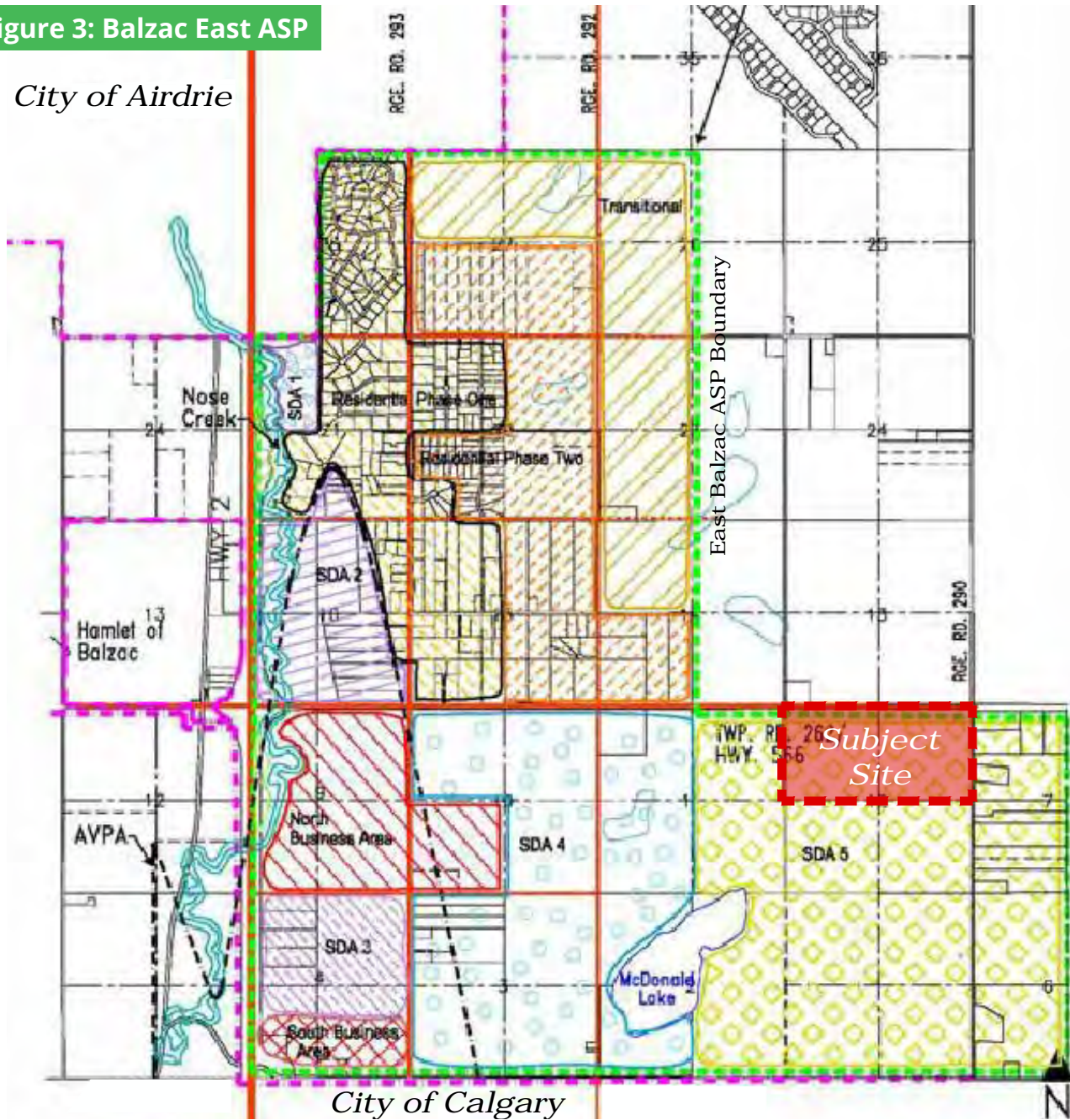
The BEASP (Bylaw C-5177-2000) applies to approximately 16.5 sections of land and provides an intermediary step in the planning process that offers policy guidance to Local Area Plans. The BEASP establishes development direction for the Plan Area and, in alignment with the County Plan's Regional Business Centre designation, devotes a significant area to industrial and commercial uses.

As shown on Figure 3, the BEASP's Land Use Plan identifies the Plan Area as Special Development Area 5. Being in close proximity to commercial and industrial uses to the west, the BEASP anticipates an extension of similar uses into the Plan Area as appropriate and also identifies the southern portion of the Plan Area as Special Development Area (SDA) #5, which permits "heavier" uses such as General Industry Class III.

A key aspect of the ASP as it applies to the Plan Area is a need to pay particular attention to the interface with the existing and future highway corridors as well as with adjacent residential and agricultural uses. The BEASP intends for uses within SDA #5 to complement business uses planned on other lands to the west.

Figure 3: Balzac East ASP

City of Airdrie



City of Calgary

- | | |
|---|---|
| Balzac East ASP Boundary | SDA Special Development Area 1 (SDA1) |
| Subject Site | SDA2 Special Development Area 2 (SDA2) |
| Municipal Boundary | SDA3 Special Development Area 3 (SDA3) |
| Residential Phase 1 | SDA4 Special Development Area 4 (SDA4) |
| Residential Phase 2 | Special Development Area 5 (SDA5) |
| Transitional | South Business Area |
| North Business Area | |

2.2.3 HIGH PLAINS INDUSTRIAL PARK CONCEPTUAL SCHEME

The HPIP Conceptual Scheme is a broad-based, non-statutory plan that sets general expectations to guide master planned industrial and commercial business developments within this portion of the East Balzac area. Refining the policies outlined in the BEASP, the CS establishes a general land use concept and, as Figure 4 identifies the north portion of the Plan Area as “Cell ‘A’ Highway 566 – Transitional Area” and the south portion as “Cell ‘B’ Industrial Business Campus Area.” Each area accommodates a variety of employment generating uses and requires development to maintain an attractive and cohesive built form with an enhanced “visual aesthetic. Cell ‘B’ permits uses that with the potential to create off-site impacts if the developer establishes the appropriate mitigation measures.

Given the relatively large area included within the Conceptual Scheme boundary, the specific illustration of anticipated lot layouts has not been defined. Instead, the detailed parcel configurations shall be established through subsequent phasing of development through the preparation of Outline Plans.

2.2.4 EXISTING AND ADJACENT LAND USES

The County Plan, BEASP, and HPIP CS designate the Plan Area as industrial and commercial uses for the purposes of generating employment and business activity.

The Rocky View Land Use Bylaw (LUB) C-4841-97 identifies the Plan Area as a Ranch and Farm District (RF). The purpose and intent of the RF District is to provide for agricultural activities as the primary land use on a quarter section of land or on large balance lands from a previous subdivision, or to provide for residential and associated minor agricultural pursuits on a small first parcel out. As such, this Outline Plan proposes redesignating the lands from RF to I-IA to align more closely with the intent of higher order plans and adjacent I-IA lands south of the Plan Area.

The LUB defines the lands east of the plan area as predominantly Residential (R-2, R-F2 and R-3) with small portions of Agricultural Holdings District (AH). The BEASP, however, identifies these as longer term business development lands (SDA #5) as illustrated in Figure 3. The LUB also defines lands to the west as a mix of industrial business uses (I-IA, B-IC and B-IC(s).

Lands to the north contain predominantly RF uses with a small portion identified as DC-30 which accommodates a crop production and farm equipment supplies. These lands do not fall within an approved Area Structure Plan boundary; therefore will likely remain undeveloped for the time being.

Figure 5: Existing Land Use highlights the existing zoning of the Plan Area and adjacent uses.

Figure 4: High Plains Industrial Park Conceptual Scheme

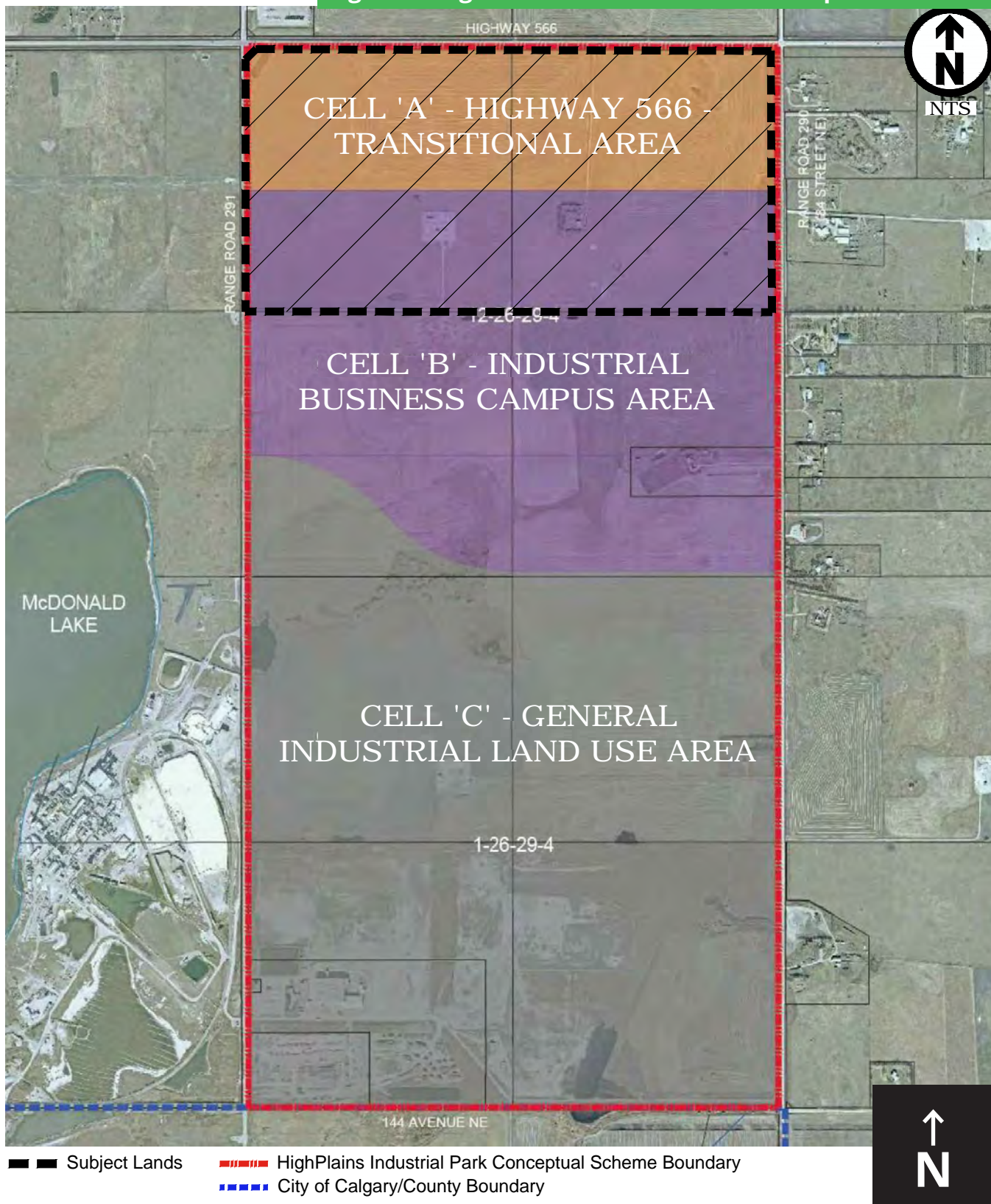
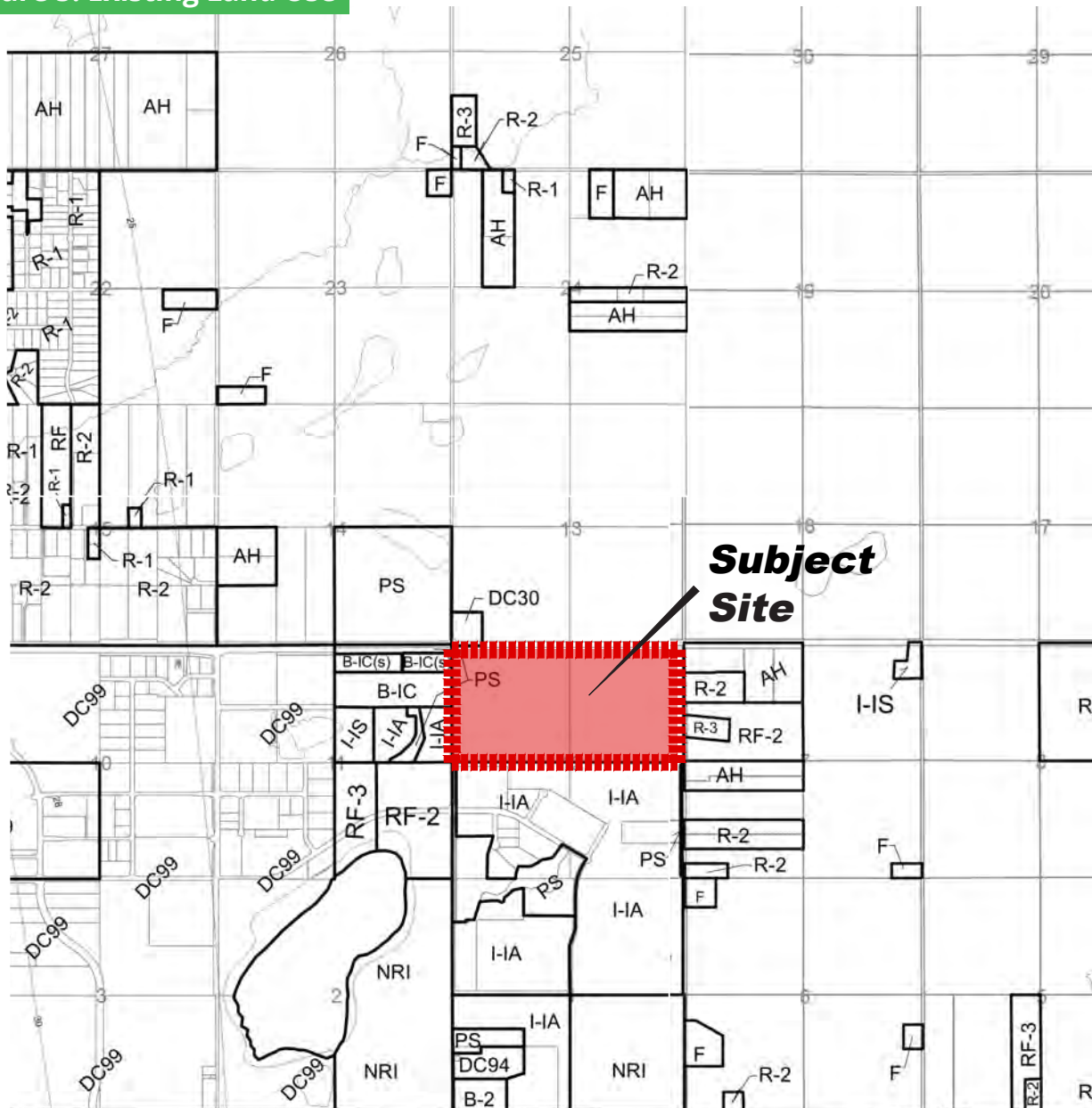


Figure 5: Existing Land Use



RANCH AND FARM DISTRICT	RF
SEE EXCEPTIONS LISTED WITH THIS DISTRICT	RF *
RANCH AND FARM TWO DISTRICT	RF-2
RANCH AND FARM THREE DISTRICT	RF-3
AGRICULTURAL HOLDING DISTRICT	AH
FARMSTEAD DISTRICT	F
RESIDENTIAL ONE DISTRICT	R-1
RESIDENTIAL TWO DISTRICT	R-2
RESIDENTIAL THREE DISTRICT	R-3
HIGHWAY BUSINESS DISTRICT	B-1
GENERAL BUSINESS DISTRICT	B-2
LIMITED BUSINESS DISTRICT	B-3
RECREATION BUSINESS DISTRICT	B-4
AGRICULTURAL BUSINESS DISTRICT	B-5
LOCAL BUSINESS DISTRICT	B-6
HIGHWAY FRONTAGE BUSINESS DISTRICT	B-HF
BUSINESS CAMPUS BUSINESS DISTRICT	B-BC
INDUSTRIAL CAMPUS BUSINESS DISTRICT	B-IC

RECREATION DESTINATION BUSINESS DISTRICT	B-RD
LEISURE AND RECREATION BUSINESS DISTRICT	B-LR
AGRICULTURAL SERVICES BUSINESS DISTRICT	B-AS
POINT COMMERCIAL DISTRICT	C-PT
VILLAGE CENTRE COMMERCIAL DISTRICT	C-VC
LOCAL COMMERCIAL DISTRICT	C-LC
REGIONAL COMMERCIAL DISTRICT	C-RC
INDUSTRIAL ACTIVITY DISTRICT	I-A
STORAGE AND SALES INDUSTRIAL DISTRICT	I-S
NATURAL RESOURCE INDUSTRIAL DISTRICT	NRI
HAMLET RESIDENTIAL SINGLE FAMILY DISTRICT	HR-1
HAMLET RESIDENTIAL (2) DISTRICT	HR-2
HAMLET COMMERCIAL DISTRICT	HC
HAMLET INDUSTRIAL DISTRICT	HI
PUBLIC SERVICES DISTRICT	PS
AIRPORT DISTRICT	AP
DIRECT CONTROL DISTRICT	DC



2.3 Purpose of the Outline Plan

Section 10 within the HPIP CS requires a developer to prepare an Outline Plan in support of each development phase, prior to Council approval of a land use redesignation; and subsequent subdivision and corresponding development permits. This Outline Plan forms the basis of Stage 4 & 5 of the High Plains Industrial Park, following consecutive approvals for Stages 1 through 3; and Stage 6.

As per the HPIP Conceptual Scheme this Outline Plan must address the following which details specific development criteria:

- Expected allocation of specific land use within the development phase;
- An illustration of anticipated lot configurations within the subdivision area;
- An update to the Master Utility Servicing Plan to detail the specific mechanisms and expectations regarding implementation of utility service infrastructure in relation to the requirements of the overall CS area and surrounding lands;
- An update to the Traffic Impact Assessment to detail the specific mechanisms and expectations required to implement local and regional roadway improvements in relation to the requirements of the overall CS area and surrounding lands;
- An update to the Stormwater Staged Master Drainage Plan to detail the specific mechanisms and expectations regarding implementation of stormwater infrastructure in relation to the requirements of the overall CS area and surrounding lands;
- Where dedication of Municipal Reserve is contemplated, a Landscaping & Public Amenities Plan to detail the anticipated public improvements therein including expectations for use and maintenance responsibilities;
- Specific criteria to inform the preparation of an Emergency Response Plan as may be required to ensure a strategy is put in place to address emergency responses and/or evacuations in the event of a major industrial accident;
- Specific criteria to inform the implementation of a land use amendment as may be required to implement the specific subdivision/development phase; and
- A summary of specific performance standards and architectural controls as may be required to implement development considerations such as architectural theme, parking & loading expectations, fencing and screening considerations, signage & lighting elements, etc.

This Outline Plan for Stage 4 & 5 addresses these development considerations pursuant to the requirements of the HPIP Conceptual Scheme, and will require adoption by RVC Council, and will be appended into the HPIP Conceptual Scheme via a statutory bylaw amendment process.

SECTION 3.0

Stage 4 & 5 Development Concept

The Stage 4 & 5 of the Industrial Park encompasses a mix of employment generating commercial and industrial uses that respond to market demands; and Municipal Reserve (MR) in the form of landscape buffers, linear parks, and regional pathways. The Stage 4 & 5 Outline Plan Area anticipates nineteen (19) industrial lots varying in size between ± 2 and ± 28 acres. However, the internal road network and lot configurations are subject to the demands of the market for industrial and commercial uses and shall be finalized at the subdivision stage.

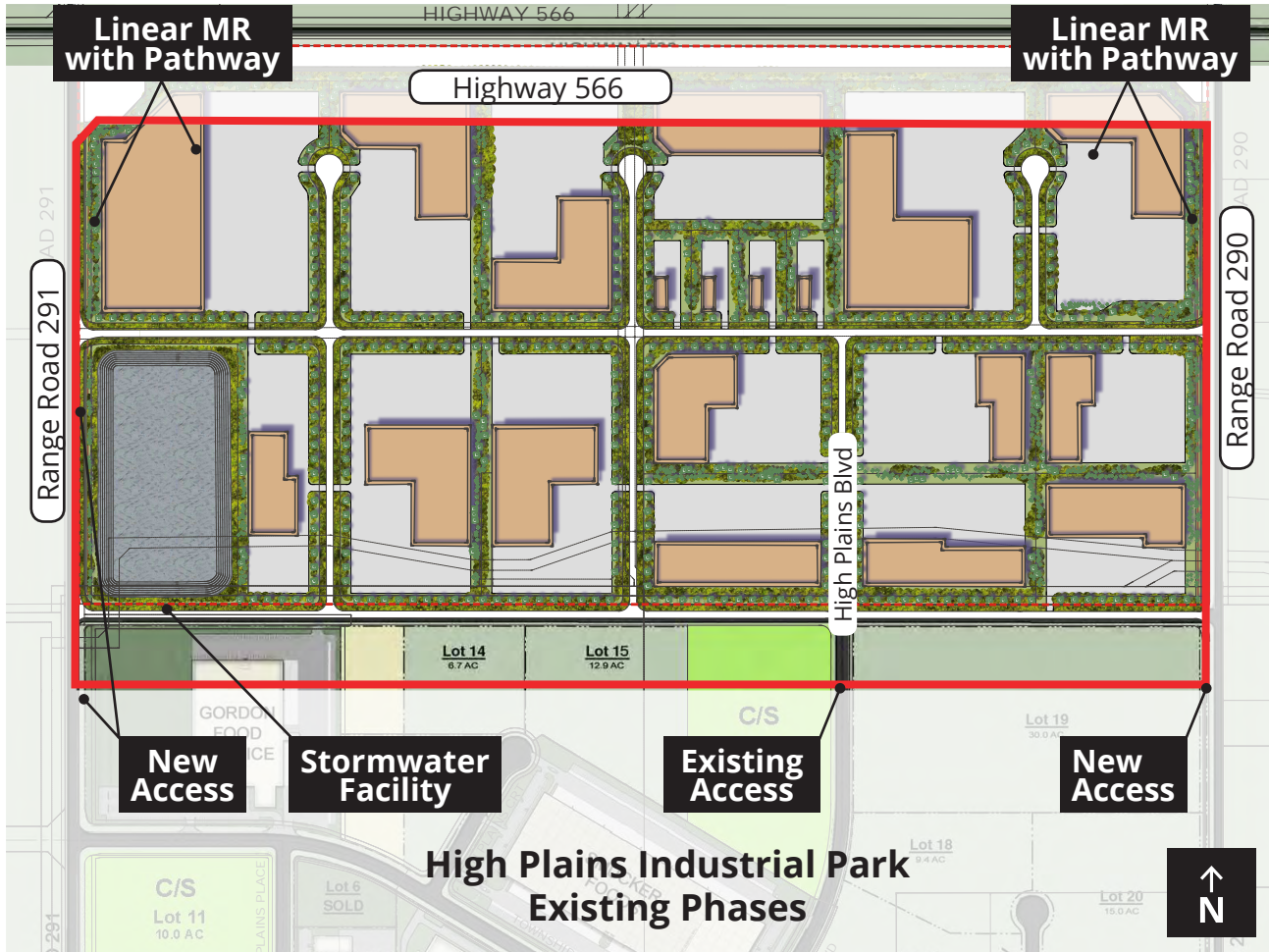
The Development Concept proposes vehicular access to the Plan Area from a combined total of four (4) all-turns accesses on Range Road 291 and Range Road 290. The specific design and treatment of these intersections will be determined at the subdivision design stage. An internal subdivision road network will provide access to each individual lot and restrict access to individual lots from the perimeter roads.

RVC's regional and potable water & wastewater infrastructure shall service the Plan Area. The developer shall be responsible for extending the appropriate level utility distribution infrastructure within the boundary of the Plan Area. The developer shall also provide applicable infrastructure levies, assessments and contributions.

The Plan Area manages stormwater within one (1) Public Utility Lot (PUL) as Figure 6 generally illustrates. The drainage facilities within the Stage 4 & 5 Outline Plan shall be integrated with facilities constructed as part of HPIP Stage 1, 2 and 3, as the proposed stormwater management pond is an extension of the existing ponds which have been in place for a number of years.

The Plan Area includes a linear Municipal Reserve (MR), along Highway 566 (20 metre buffer) and Range Roads 290 and 291 (10 metre buffer). The linear MR areas include a pathway that shall be constructed by the developer at the subdivision stage.

Figure 6: Development Concept



*Lot sizes, building sizes, and placement are conceptual only.

SECTION 4.0

Utility Servicing

4.1 Potable Water

The East Rocky View water system supplies the Plan Area with potable water, while the developer is responsible for constructing a potable water distribution network within the Plan Area. Water metering and mandatory potable water restrictions for irrigation and architectural controls may assist with water conservation. The design of the water distribution network shall accommodate fire suppression in accordance with applicable regulatory requirements.

The developer shall contribute the applicable payment in accordance with the Water and Wastewater Off-site Levy Bylaw (C-7273-2013) at the subdivision stage and complete any required upgrades. RVC's Infrastructure Cost Recovery Policy shall apply in situations where the developer implements potable water infrastructure, in support of the Stage 4 & 5 Outline Plan, that benefits surrounding properties.

The developer shall construct a potable water distribution network within the site as conceptually illustrated by Figure 7: Potable Water Servicing and further described by the HPIP Stage 4 & 5 Outline Plan Utility Servicing Design Brief (CIMA, November 2018).

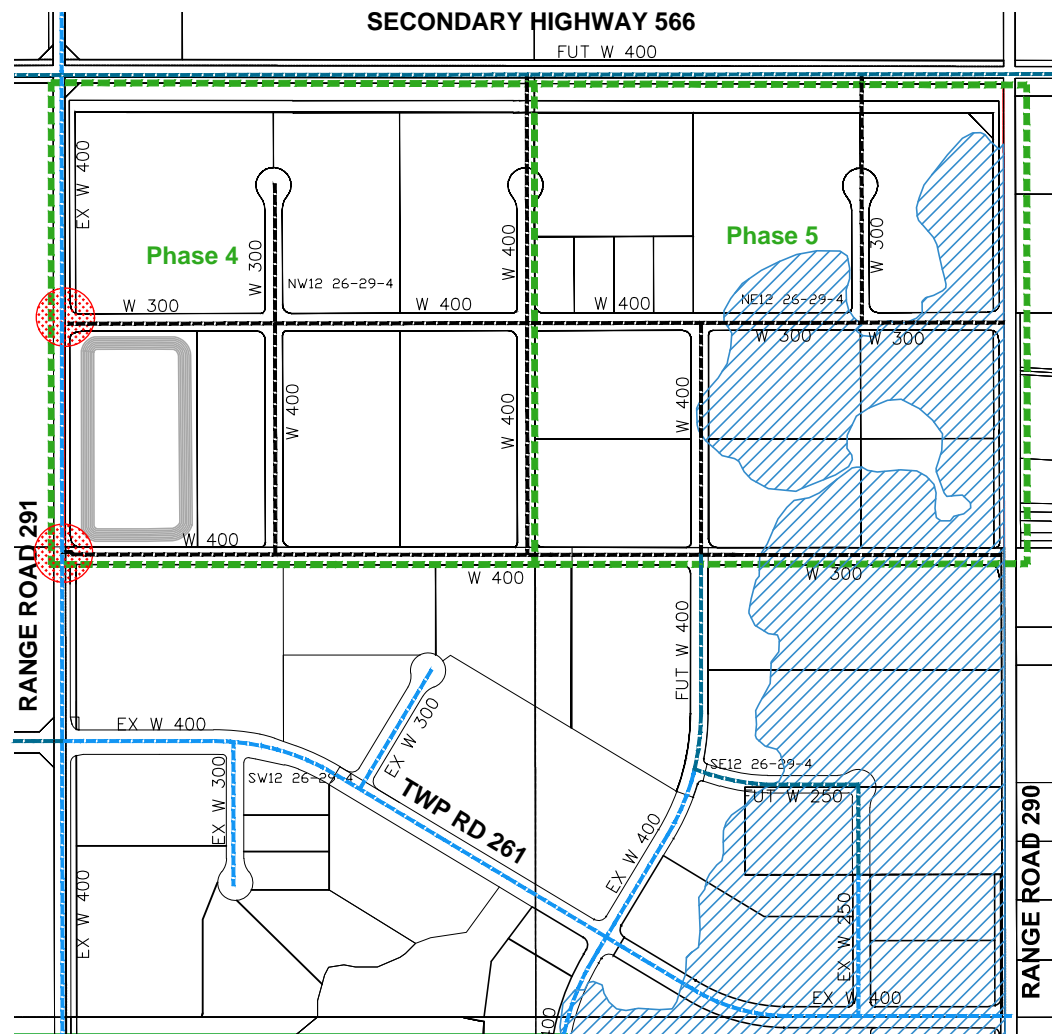


Figure 7: Potable Water Servicing

4.2 Wastewater

The East Rocky View Wastewater Transmission Line (ERVWWTL) services the Plan Area, while the developer shall be responsible for constructing the sanitary network within the site boundary. The HPIP Conceptual Scheme area includes two Sanitary Basins: the North Basin and the South Basin. The North Basin is further delineated into two (2) sub-basins which make-up the Plan Area.

The Stage 4 and 5 Outline Plan proposes to connect to the existing lift station that services the County's Municipal Campus. An analysis was completed by CIMA+ titled Municipal Campus Lift Station Analysis (November 2018) which demonstrates that the municipal campus has available capacity to service the Plan Area. A more detailed sanitary servicing study, to be completed at the detailed engineering stage of each subdivision phase within the outline plan area, will investigate the available capacity of the existing RVC

municipal campus lift station as well as confirm connection details, locations, and possible upgrades if required.

The developer shall construct a wastewater collection network within the site as conceptually illustrated by Figure 8: Wastewater Servicing and further described by the HPIP Stage 4 & 5 Outline Plan Utility Servicing Design Brief (CIMA, November 2018). The developer shall contribute the applicable payment in accordance with the Water and Wastewater Off-site Levy Bylaw (C-7273-2013) at the subdivision stage and complete any required upgrades.

RVC's Infrastructure Cost Recovery Policy shall apply in situations where the developer support of the Stage 4 & 5 Outline

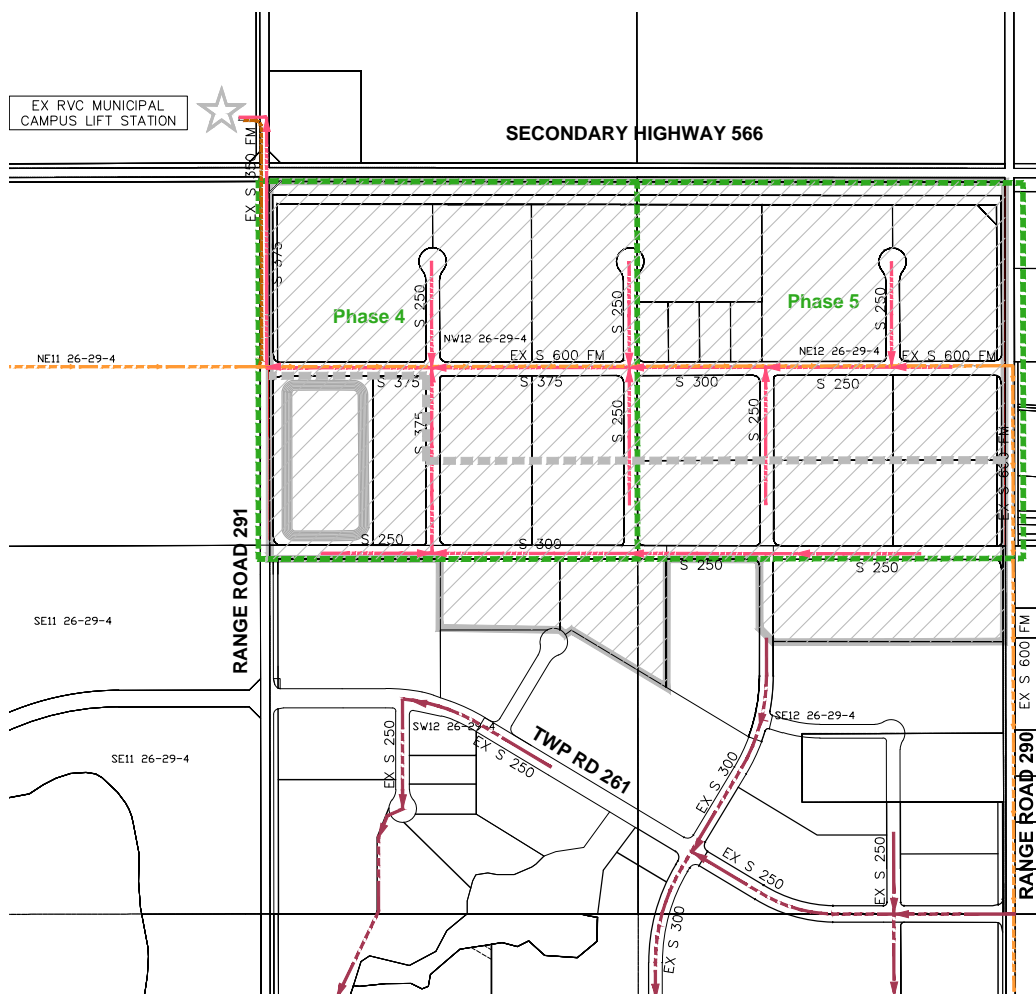
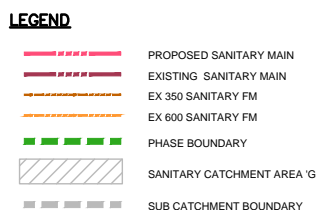


Figure 8: Wastewater Servicing



implements wastewater infrastructure, in Plan, that benefits surrounding properties.

4.3 Stormwater Management

Westhoff Engineering Resources Inc. (Westhoff) prepared the original Staged Master Drainage Plan (SMDP) in support of the HPIP CS. The original SMDP contemplated two stormwater retention ponds within the Plan Area. Upon further analysis in support of the Stage 4 & 5 Outline Plan, Westhoff determines one pond, connecting to the comprehensive pond system, is appropriate. As such, the developer proposes to construct a Public Utility Lot (PUL) in the southwest portion of the Plan Area to accommodate surface drainage. In addition, the runoff from the external roads of Highway 566, Range Road 290 and 291 and High Plains Drive drains to this pond. A Staged Master Drainage Plan, completed by Westhoff, has been submitted as part of this application.

The design of the pond includes capacity to retain 100% of surface drainage generated within the Plan Area as the High Plains Industrial Park is a non-tributary development. The stormwater management analysis uses recorded precipitation data and a continuous simulation technique to size the stormwater storage facility to a greater than 1:100 year run-off event.

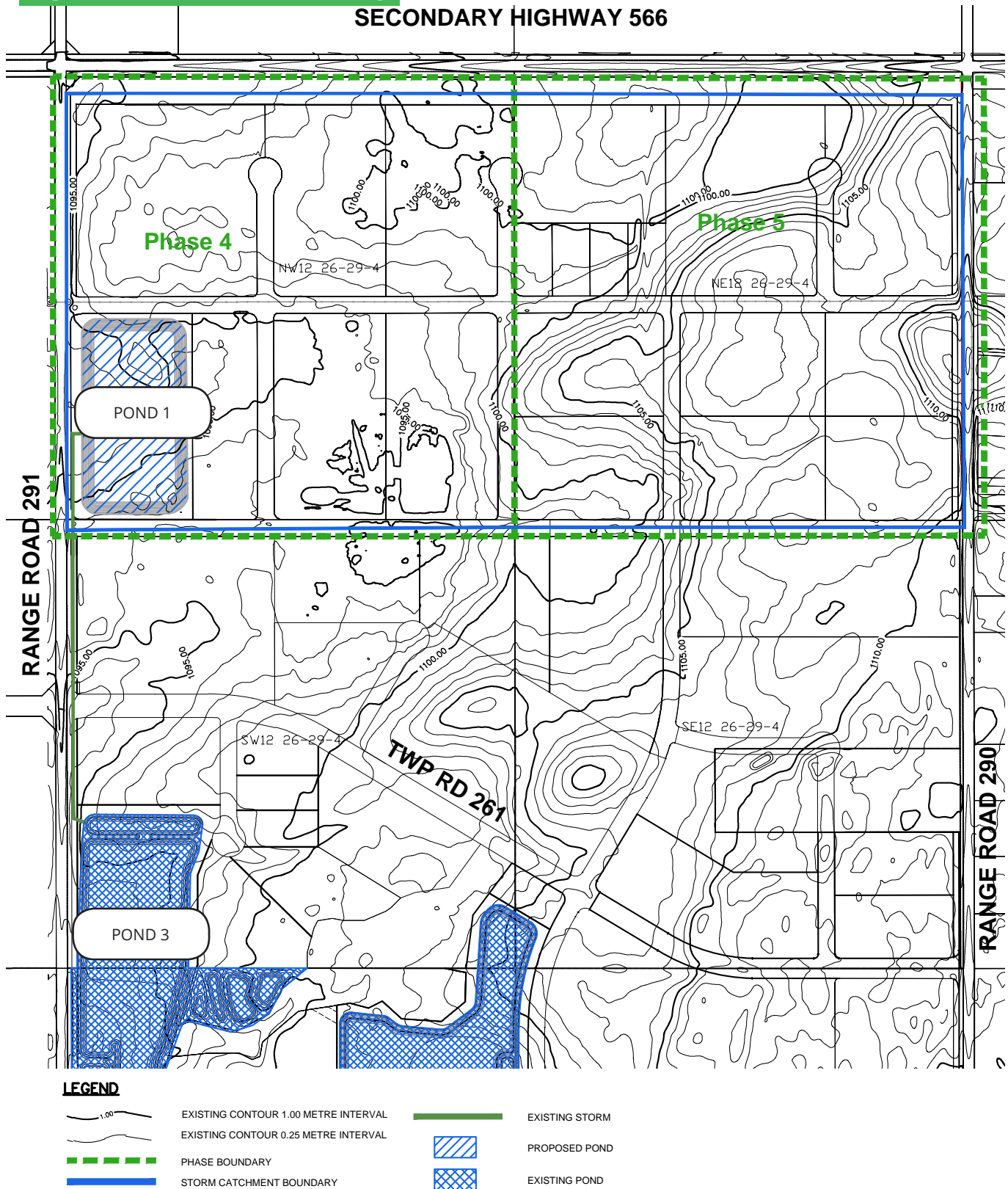
The proposed drainage system comprises:

- A major overland conveyance system, generally following the road network system leading to the stormwater storage facility;
- Treatment of stormwater using a treatment train approach including on-site treatment at the lot level, conveyance by grassed ditches and forebays, at the storage facilities and the stormwater facility itself;
- A stormwater storage facility;
- An existing 1,050mm concrete pipe from the proposed pond (Pond 1) to the existing pond in the HPIP (Pond 3);
- An existing pump station located at existing Pond 3 south of the Plan Area, where runoff is pumped for use of irrigation, dust control and eventually other users when approved by AEP;
- The stormwater re-distribution pipe in Stage 1, 2 and 3 of HPIP shall extend to service Stage 4 & 5 from the existing pump stations located at existing Pond 3 and existing Pond 5.

Details of the pond, including forebay, geotechnical consideration and lining requirements are to be submitted as part of the detailed design at the subdivision stage.

As described in the HPIP Conceptual Scheme, the developer is working with Olds College to implement a cold climate wetland research facility within the HPIP area. Specific operational details required to implement this research partnership will be established at the subdivision stage. The County reserves the right to participate as an active or passive member of this partnership.

Figure 9: Stormwater Servicing

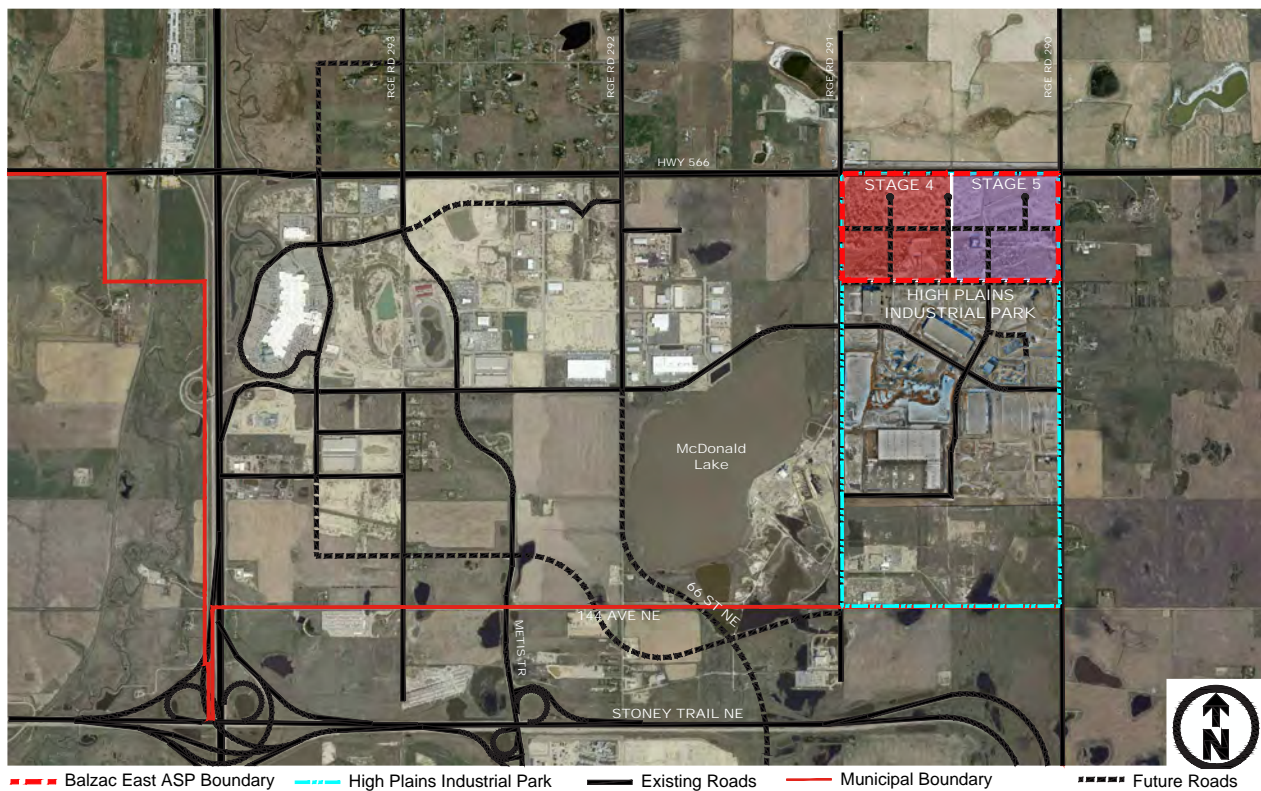


SECTION 5.0

Transportation

The East Balzac Transportation Network has been planned to facilitate access between the County's primary business and employment area and the surrounding regional road network. All developments within the HPIP must rationalize specific transportation requirements within the context of the County's Global TIA. Bunt and Associates (Bunt) completed a Traffic Impact Assessment in support of this Outline Plan application.

Alberta Transportation (AT) has a long-term strategic plan to implement a regional transportation connection designed to route traffic from northeast Calgary and East Balzac onto Highway 2 at a connection point just north of The City of Airdrie. This proposed roadway is commonly referred to as 'The Airdrie Bypass Road'. AT had originally requested the developer set aside land within the Conceptual Scheme area to accommodate ROW for this expected future roadway extension. Bunt, however, confirmed through correspondence with AT, that the Province is not budgeting or planning regional upgrades relating to the 'Airdrie Bypass Road.' As such, AT has not identified any regional concerns or implications relating to this Outline Plan application.



A number of sites within the HPIP have been constructed and are fully operational since the last submission in 2015. The analysis supporting this Outline Plan revisits and assesses the original data, redeveloping background volumes by completing existing counts, while including any additional approved but not yet constructed development in the area.

RVC also requires the analysis include consideration of the current forecast model outputs as provided by Watt Consulting Group (Watt). The forecasts are comprehensive, and include differing levels of detail and assumptions than what would typically be required using the Alberta Transportation (AT) methodology. For this reason, the TIA assesses the impacts of the proposed development in two ways. The main analysis is based on the standard AT methodology for TIA completion; and a sensitivity analysis was then undertaken using the forecast model outputs provided by Watt, on behalf of RVC.

Outputs from these two methods were then compared to determine the expected improvement program for the area and are further detailed within the TIA. Bunt recommends that the network should be monitored over time to confirm if the distribution and volumes outlined in the RVC model occur, which could then trigger the need for some or all of the additional improvements. The improvements from the Forecast Model Methodology include all the improvements noted in Table 1 for the AT methodology and the additional improvements noted in Table 2. The Developer shall be responsible for all internal roads, while Table 1 and 2 recommends improvements, which may be financed by a combination of levies and/or cost recoveries if deemed to be necessary.

(1) Summary of Recommended Improvements – AT Methodology

Intersection / Road Link	Interim (2030)		Long Term (2040)	
	Background	Post Development	Background	Post Development
Highway 566	Widen to four lanes from Dwight McLellan Trail to Range Road 291	-	-	-
Township Road 261	Widen to four-lane cross-section west of Range Road 291	-	-	-
Range Road 291/ Highway 566	-	Add protected/ permissive phases to signal timing	-	-
Range Road 291/ Township Road 261	-	Add protected/ permissive phases to signal timing	-	-
Range Road 290/Highway 566	Signalize and construct northbound right turn bay	Construct eastbound and westbound left turn bay and add protected/ permissive phase to signal timing	Construct eastbound and westbound left turn bay	-

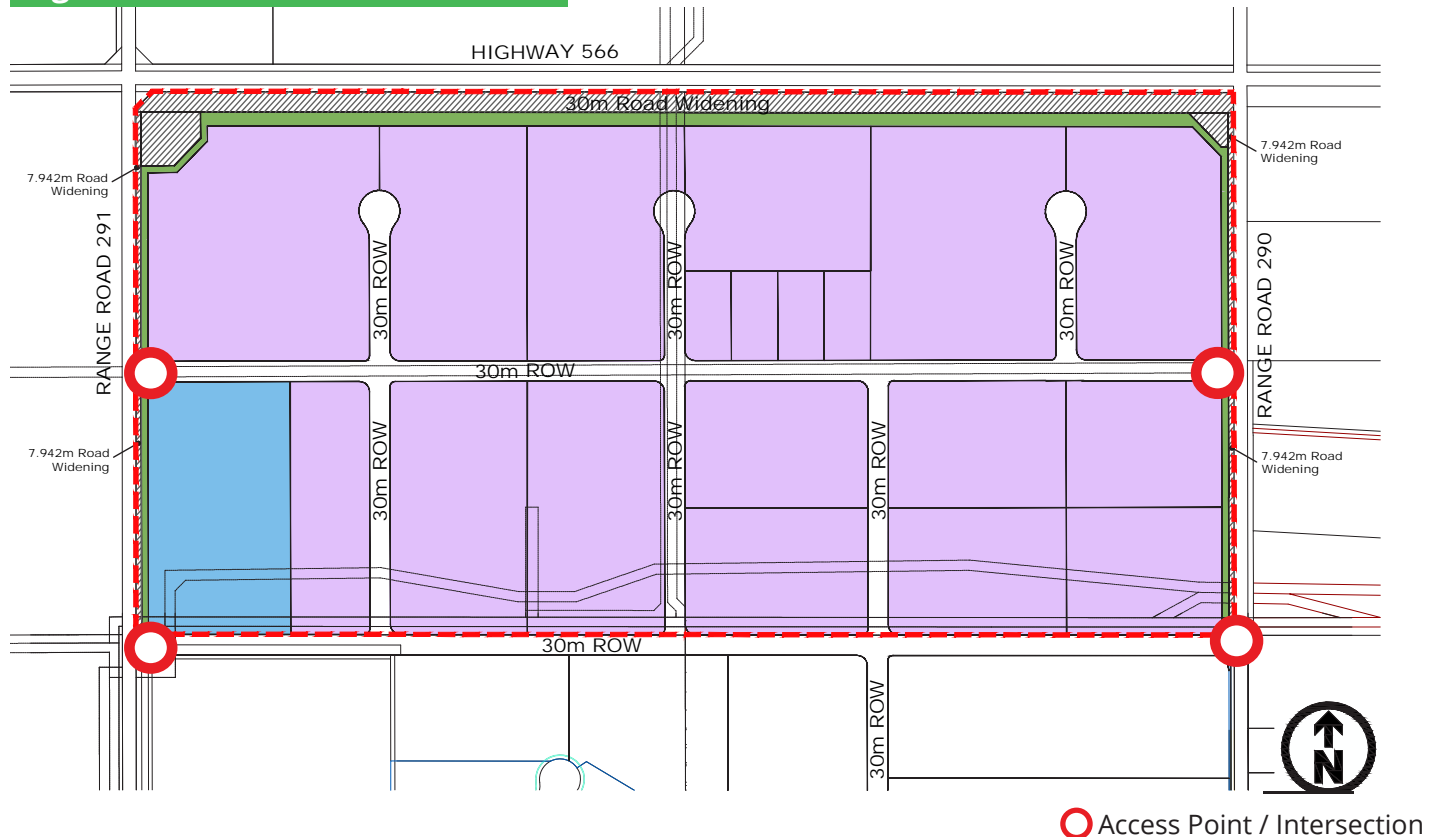
(2) Summary of Recommended Improvements – Forecast Model Methodology

Intersection / Road Link	Forecast (Year 2030)	Forecast (Year 2040)
Range Road 291	-	Widen to four-lane cross-section Highway 566 to Township Road 261
Range Road 291 / Highway 566	-	Construct an eastbound right turn bay
Range Road 291 / High Plains Drive	Signalize	Construct a westbound right turn bay
Range Road 290 / Highway 566	Construct northbound left turn bay	-
Range Road 290 / High Plains Drive	-	Monitor for future signalization

5.2 Internal Transportation Network

Range Road 291 and Range Road 290 (via the future High Plains Boulevard) provide access to the Plan Area through one of four (4) intersections. These points provide direct access to regional network via Highway 566, Queen Elizabeth II (Highway 2) and Stoney Trail NE.

Figure 10: Internal Road Network



The developer shall construct the internal road network, as Figure 10 illustrates, and be built to a 30 metre ROW.

In alignment with the HPIP CS, the HPIP Stage 4 & 5 Outline Plan proposes the following road ROW dedication for the purposes of widening:

- ± 8 metres along Range Roads 291 and 290; and
- ± 30.0 metres along Highway 566.

The plan restricts direct access to Range Road 290 & 291 or Highway 566 from individual lots. The developer shall implement the internal road network in accordance with the County's Development Agreement process. The County's Transportation Off-Site Levy will apply at the subdivision stage and the County will also require a road naming application.

SECTION 6.0

Open Space & Municipal Reserve

The Outline Plan proposes an open space network comprising a Public Utility Lot (PUL) in the form of storm pond and Municipal Reserve (MR) in the form of linear parks.

The Outline Plan proposes linear MR along the north, east and west boundaries of the Plan Area in the form of landscape buffers, including a regional pathway, reflecting the intent of the County Plan, BEASP and the HPIP CS. Pathway connections within the Plan Area facilitate pedestrian mobility to the East Balzac Regional Trail System by trails constructed within MR's adjacent to Range Roads 290 & 291 and Highway 566; and by a local pathway system. Section 6.1 conceptually illustrates the buffer areas.

The developer shall construct the pathways and MR which connect to greater HPIP. The developer shall also construct a 2.5 m asphalt pathway within the internal road ROW's as the cross sections illustrate in accordance with the provisions of the Development Agreement process.

The Outline Plan proposes the balance of Municipal Reserve as follows:

Municipal Reserve Calculation	± HECTARES	± ACRES	PERCENT OF GDA
Gross Developable Area (GDA)	120.98	298.95	-
Municipal Reserve Owing (10% as per MGA)	12.09	29.89	-
Proposed Municipal Reserve Dedication	4.39	10.86	3.6%
Municipal Reserve Outstanding (cash-in-lieu)	7.70	19.03	6.4%
TOTAL	12.09	29.89	10%

The developer shall provide outstanding MR, after dedication, through cash-in-lieu of land at the subdivision stage.

SECTION 7.0

Emergency Response

7.1 Nexen Site

From 1961 to 2011, Nexen operated the Balzac sour gas processing plant (Balzac gas plant) located west of the HPIP and ceased operations in 2011. While remediation of the plan continues, the Balzac Power Station remains operational. By Provincial regulation, Nexen was required to establish and maintain the Balzac Area Emergency Response Plan (BAERP) to implement protocols to notify and/or evacuate adjacent owners in the event of an accidental release.

In 2011, Nexen decommissioned the facility, suspended well site operations and discontinued many of their pipelines within the surrounding area. None of the pipeline segments within the Plan Area shall be re-licensed and Nexen anticipates full abandonment and removal by the end of 2018. As a result of this change in scope, the Balzac Area Emergency Response Plan was revised, and the Emergency Planning Zone (EPZ) no longer applies. Residences and businesses within the planning zone were notified of the change.

Any remaining operations shall be included in the Plant Decommissioning Area Emergency Response Plan (ERP) and nearby land owners will no longer receive correspondence unless there is activity in close proximity.



7.2 Emergency Services

RVC's East Balzac Emergency Services are located approximately 600 metres from the Plan Area, north of Highway 566 along Range Road 291, and will service the entire High Plains Industrial Park. The RCMP Detachment in the City of Airdrie – with support from the RVC Community Peace Offices will Police the area, while emergency ambulance services will be provided by EMS Facilities located in North Calgary and within the City of Airdrie.



SECTION 8.0

Outline Plan Development Statistics

The HPIP Stage 4 & 5 Outline Plan proposes the following statistics which the developer will refine in more detail at the subdivision stage.

High Plains Stage 4 & 5 Outline Plan - Statistics				
	LAND USE DISTRICT	AREA (±)		% OF GDA
		HECTARES	ACRE	
NW & NE ¼ SEC. 12-26-29-W4M		127.77	315.73	
<i>LESS:</i>				
<i>30.0m Road Widening (Highway 566)</i>		<i>5.56</i>	<i>13.75</i>	
<i>8.0m Road Widening (Range Rd 290)</i>		<i>0.61</i>	<i>1.51</i>	
<i>8.0m Road Widening (Range Rd 291)</i>		<i>0.62</i>	<i>1.52</i>	
GROSS DEVELOPABLE AREA (GDA)		120.98	298.95	100.0%
INDUSTRIAL ACTIVITY DISTRICT	I-IA	97.99	242.14	81.0%
PUBLIC SERVICE DISTRICT - PUL	PS(PUL)	7.70	19.03	6.4%
PUBLIC SERVICE DISTRICT - MR	PS(MR)	4.39	10.86	3.6%
INDUSTRIAL/COMMERCIAL (I/C) ROAD (10.0m / 30.0m)		10.89	26.92	9.0%

* Statistics and total areas are conceptual and will be subject to detailed design at the subdivision stage.

SECTION 9.0

Architectural Guidelines

The Developer shall provide high-quality architectural features, suitable to industrial park context to enhance the overall development, attract investment, and add value to the existing High Plains Industrial Park and create a unique identity within the area of East Balzac. The Stage 4 & 5 Outline Plan maintains the cohesive built-form established in Stages 1 through 3 and aligns with the policies and guidelines outlined within the County's planning documents. Doing so will be beneficial to both the Developer and the County in terms of creating a visually-appealing development that adds value, is competitive and respond appropriately to market demands.



9.1 Building Orientation

The Outline Plan ensures that the sides of structures facing Highway 566, Range Road 291 and Range Road 290 are attractive. All site and building layouts will conform to the regulations prescribed by the Rocky View County Land Use Bylaw's Industrial Activity District (I-IA) and the Commercial, Office and Industrial Design Guidelines. The Developer should orient buildings so that the main entrance will face the public road that provides direct access to same.

The design of building facades facing the public roads may incorporate variations in finish and colour, roofline offsets, and pedestrian scaling techniques designed to visually break the building mass and provide a high quality appearance. The use of "green" building techniques which result in enhanced energy efficiencies may be encouraged.

9.2 Lighting

Exterior lighting within each business lot should consider "dark sky" principles in order to minimize light pollution. Lighting fixtures should include full cut-offs to direct light downward and prevent light spillage. Lighting shall be sensitive to the adjacent residential uses east of the Plan Area.





9.3 Parking and Loading

Developments should orient public parking to the front of each lot in general proximity to the main entry of each building. Large-scale parking, service and loading areas should be located at the sides and/or rear of the building and should be appropriately screened from adjacent properties and public roads. Particular attention should be paid to the landscaping along Highway 566, Range Road 291 and Range Road 290.

Each development shall screen outdoor storage from adjacent properties and public roads, while the HPLOA will ensure that industrial uses maintain outdoor storage in an orderly fashion.

Parking and outdoor storage areas incorporate Low Impact Development (LID) stormwater management principles where possible and include features such as permeable pavement, on-site stormwater detention & treatment areas, rainwater capture/re-use and vegetated swales to implement 'source control' stormwater best management practices to reduce volume and improve surface drainage quality prior to its release into the roadside ditch system.

9.4 Fencing

The developer will discourage fencing in the front portion of the lots. If required for visual screening and/or security purposes, it should include a low-scale, decorative design and be located behind street-oriented landscaping in accordance with the requirements of the County's Land Use Bylaw.

Perimeter fencing along the side & rear property boundaries should be utilized to provide screening that reduces negative visual impacts of outdoor storage and large scale parking when viewed from public roads and adjacent parcels.

All fencing should include a design with high quality materials that will require limited maintenance.



9.5 Signage

Lot and building signage should include high quality materials with a design that is compatible with each building façade treatment. The size and placement of all signage must be in accordance with the requirements of the County's Land Use Bylaw. Signage should be sensitive to the adjacent residential uses east of the Plan Area. No back-lit signage is permitted within the Stage 4 & 5 Outline Plan Area.

9.6 Implementation

As per the HPIP CS, the developer shall implement the Architectural Guidelines within this Plan at the subdivision and/or development permit stage.

A Restrictive Covenant referencing the architectural guidelines shall be registered as an encumbrance on each title with specific development criteria enforced by a Business Lot Owner's Association to be referred to as the HPLOA.

Lot owners are required to submit site and building design plans to the HPLOA for review and approval prior to seeking approvals from Rocky View County. Lot owners shall provide Rocky View County with an endorsement from the HPLOA with applications for development permit and/or a building permit.

SECTION 10.0

Landscape Criteria

As per the HPIP CS the High Plains Industrial Parks project goal is to not only create an innovative and aesthetically appealing development, but to create a top-quality, environmentally sensitive and sustainable industrial park. The developer shall implement landscaping treatments within the Plan Area according to the general provisions established by the HPIP CS to enhance the visual appearance and aid in the management of surface drainage. Landscaping treatments should follow similar features approved and constructed in Outline Plan Stages 1 through 3, meaning a private Landscaped Easement shall be established within each lot abutting public roadway.

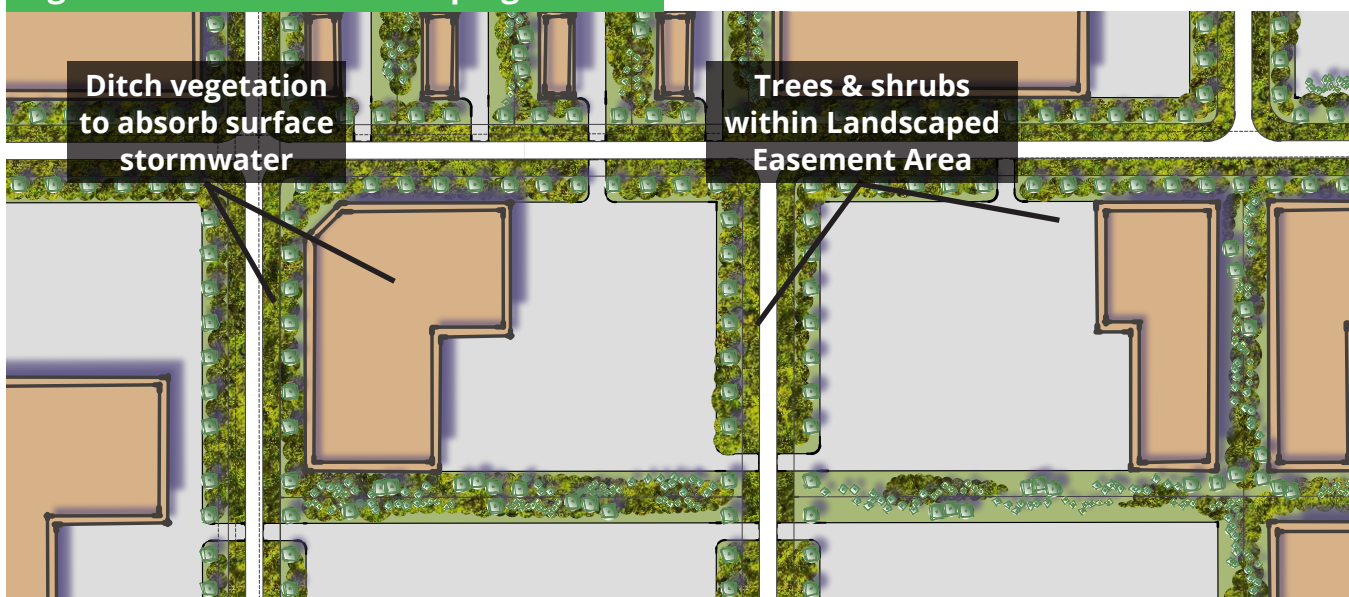
A qualified professional shall prepare a Landscaping Plan as a condition of subdivision to detail the type and extent of plantings within the Landscaped Easement area.. Plant materials must be appropriate for the Calgary region and include a uniform theme that is appropriate for the overall aesthetic that reflects the industrial nature of this development. Plantings within the Landscaped Easement area shall be provided by the developer in accordance with the County's Development Agreement process.

The Landscaped Easement is maintained by the HPLOA. Plantings within the Landscaped Easement area may be irrigated with treated stormwater from the facilities within the HPIP's integrated Public Utility Lots. Use of potable water to irrigate Landscaped Easement areas shall be prohibited.

As per the BEASP and the Land Use Bylaw, a minimum of 10% of each business lot must be landscaped. A detailed Landscaping Plan shall be prepared by a qualified professional as a condition of development permit to establish how each lot-specific development plan will satisfy the 10 % landscaped requirement.

The portion of each lot including the Landscaped Easement area may be included in the calculation to determine the 10 % area requirement.

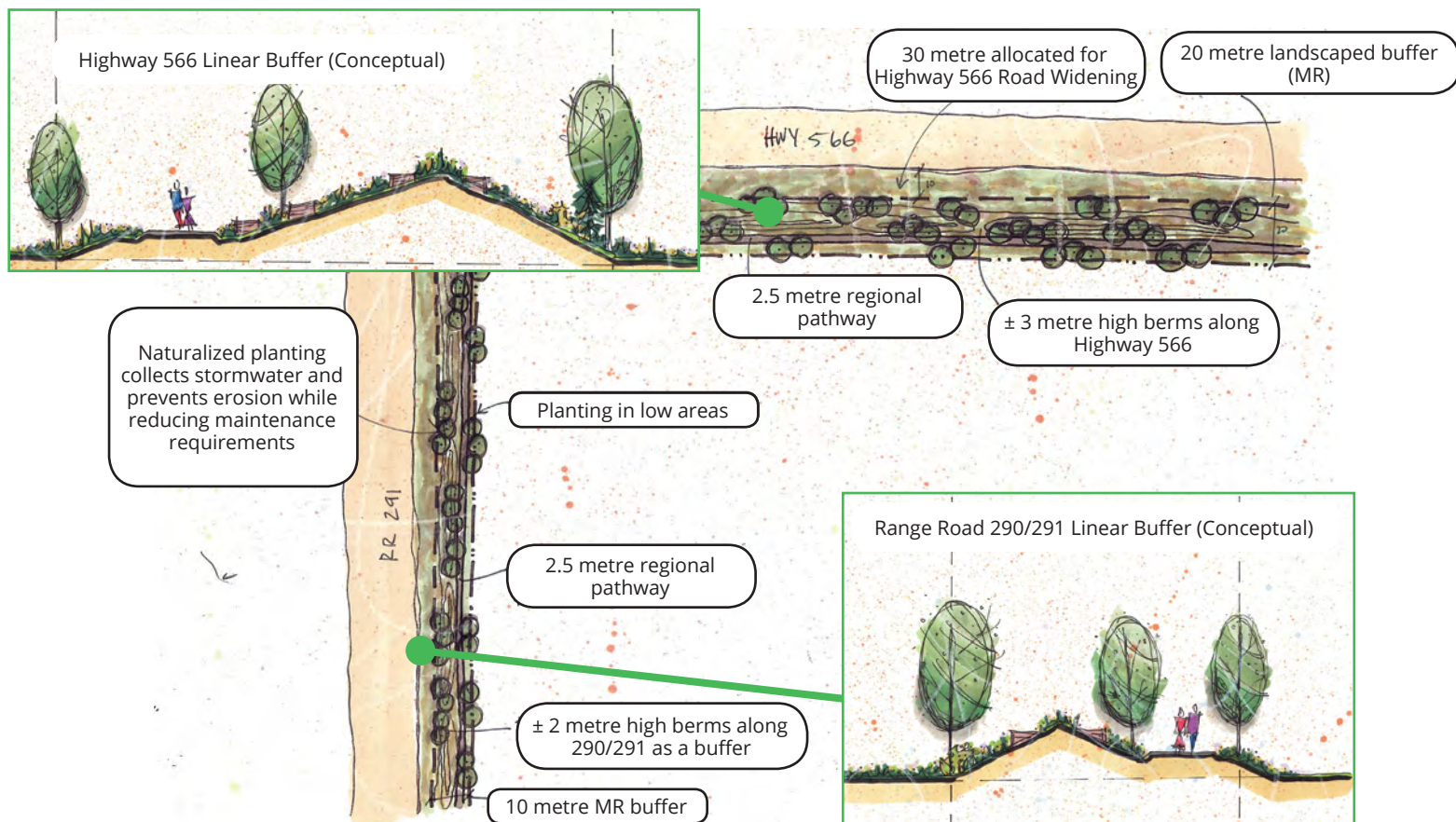
Figure 11: Internal Landscaping Criteria



10.1 Landscape Buffering

The Plan Area include a linear Municipal Reserve (MR), along Highway 566 (20 metre buffer) and Range Roads 290 and 291 (10 metre buffer). The linear MR areas include a regional pathway that will be constructed by the developer at the subdivision stage. Figure 12 conceptually illustrates how the landscape buffer could be provided along Highway 566 and Range Road 291/290.

Figure 12: Buffering Concepts



**Note: these are conceptual and will be finalized at the time of detailed design. Range Road 290 has not been shown as it is intended to be the exact same as Range Road 291.*

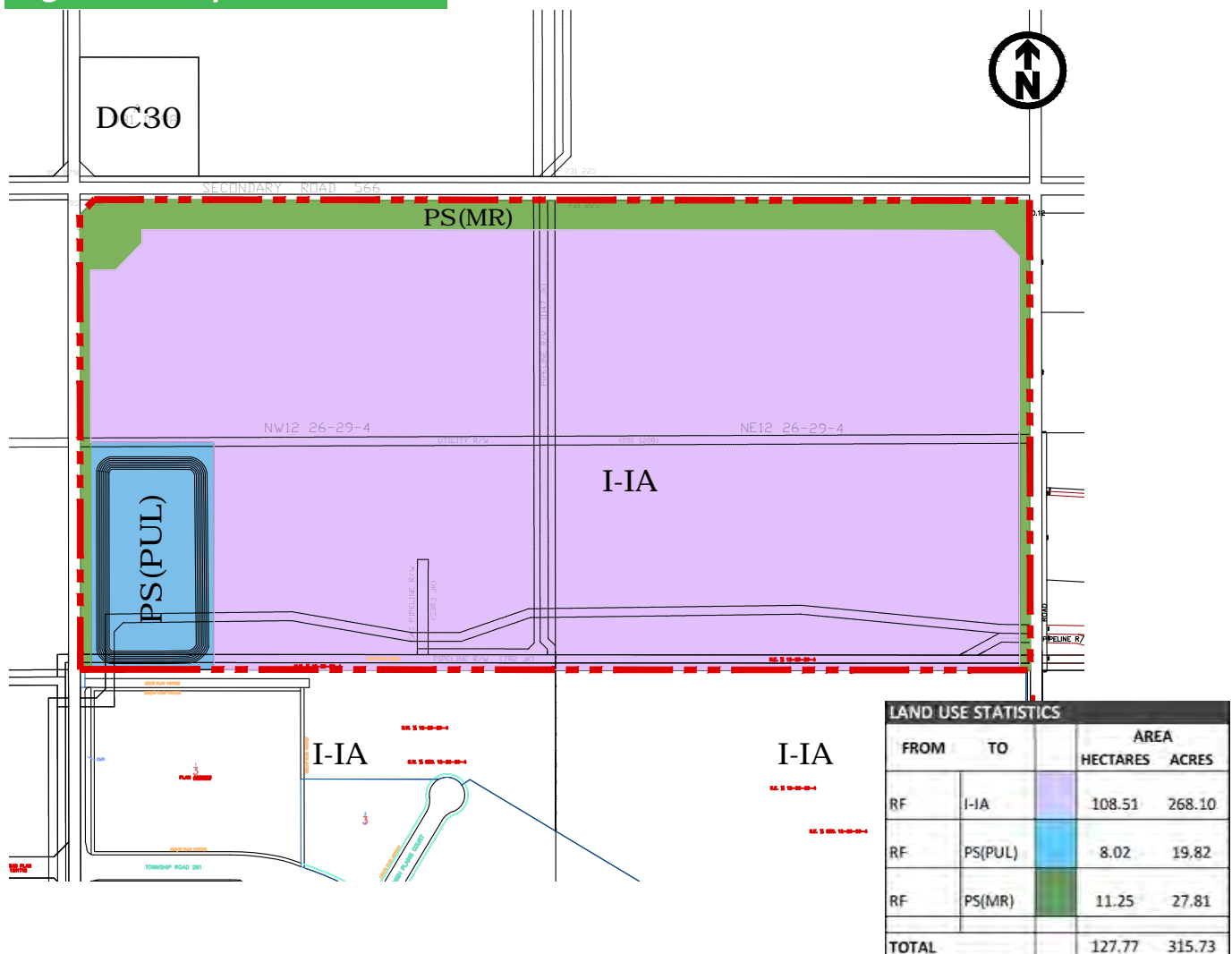
SECTION 11.0

Proposed Land Use

Stage 4 & 5 of the High Plains Industrial Park will encompass a mix of employment generating commercial and industrial uses that respond to market demands; and Municipal Reserve (MR) in the form of landscape buffers, linear parks, and regional pathways. As such, this Outline Plan proposes to redesignate the Plan Area from Ranch and Farm District (RF) to Industrial – Industrial Activity (I-IA) to align more closely with the intent of higher order plans and adjacent I-IA lands south of the Plan Area.

Figure 12: illustrates the land uses proposed land uses and the corresponding statistics.

Figure 13: Proposed Land Use



SECTION 12.0

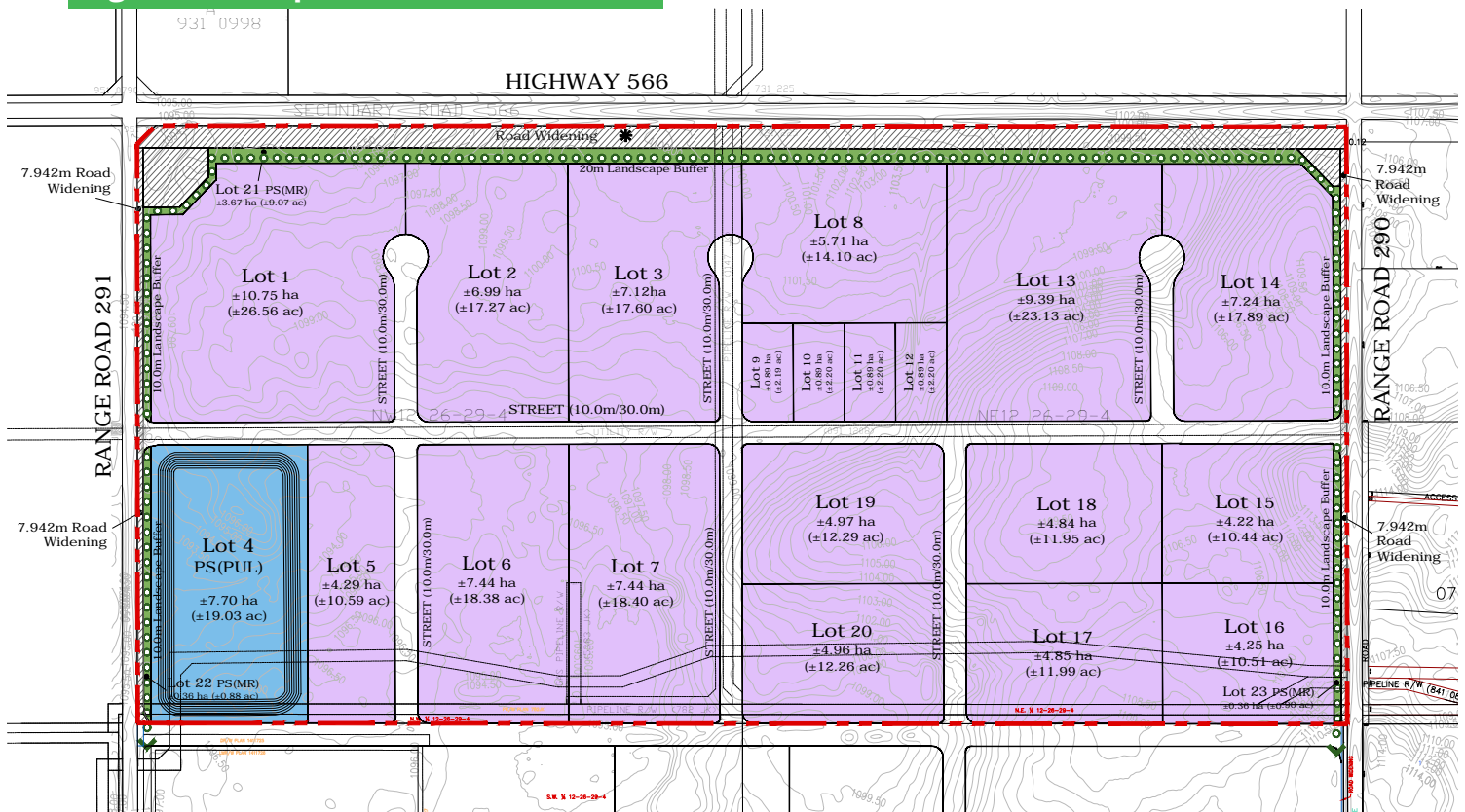
Proposed Subdivision Plan

As Figure 13: Proposed Subdivision Plan illustrates, subdivision of the Stage 4 & 5 Outline Plan will include approximately:

- Nineteen (19) Industrial Activity (I-IA) lots varying in size between ± 2 and ± 28 acres, totaling $244 \pm$ acres of employment-generating uses;
- One (1) ± 19 acre Public Utility Lot;
- ± 10.86 acres of Municipal Reserve (MR) dedicate along the north, east and west perimeters, in the form of linear park spaces, that include regional pathways; and
- ± 16.7 acres of road ROW set to accommodate widening of Range Roads 291 & 290; and Highway 566.

The developer expects the market to dictate lot sizes, the phasing of subdivision with transportation, utility servicing and recreation construction occurring in accordance with the County's Development Agreement process. The developer understands that it may be required to secure staged subdivision approval from the County to align with infrastructure requirements established at each development phase.

Figure 14: Proposed Subdivision Plan



SECTION 13.0

Proposed Amendment to the Balzac East Area Structure Plan

Based on previous experience and an understanding of existing market dynamics, HLM identifies the Industrial Activity (I-IA) Land Use District to be the most flexible Industrial Land Use District in terms of accommodating market demand for a variety of employment-generating commercial and industrial uses. As such, the High Plains Industrial Park Stage 4 & 5 Outline Plan proposes the I-IA District as a means to provide a flexible response to accommodate new business opportunities; a goal and vision of the BEASP.

This Outline Plan aligns with the intent of the BEASP to accommodate a diverse offering of business uses within the BEASP Area. The BEASP, however, includes two prescriptive regulation-based policies that restricts the developer's ability to successfully implement the Industrial – Industrial Activity (I-IA) within the Plan Area. These policies include:

- *Policy 4.7.5 (i): Cell A is located along the northerly and easterly boundary of SDA#5 and along Range Road 290. Uses that are compatible with adjacent residential and other uses may be permitted in this Cell. Landscaped buffers are mandatory. Development heights, parking and landscaping will be sensitive to the adjacent residential uses. General Industry Class III uses are not permitted in Cell A. Architectural guidelines prepared for Cell A will ensure that the sides of structures facing Highway 566 and Range Road 290 are attractive **and no outside storage is permitted in the yards facing these highway corridors.** Signage and lighting must also be sensitive to the adjacent residential uses. and;*
- *Policy 4.7.5 (m): within Cell A of SDA #5, the maximum height of a principal building shall be 10 metres. The minimum building setback from Highway 566 shall be 50 metres.*

Policy 4.7.5(i) contains language that restricts the ability to provide outside storage along Highway 566 or Range Road 290. The Developer must be able to ensure that lot layouts are flexible in order to respond to varying market requirements. This Outline Plan proposes architectural guidelines and landscape buffering techniques, regulated by Section 26 of the Land Use Bylaw, that will mitigate concerns relating to outside storage along Highway 566 and Range Road 290, without compromising the intent of the BEASP. As such, to support the development concept proposed by this Stage 4 & 5 Outline Plan, a concurrent application to amend the BEASP has been submitted in order adjust policy 4.7.5(i) and remove the wording "...and no outside storage is permitted in the yards facing these highway corridors."

In addition, Policy 4.7.5 (m) prevents successful implementation of the Industrial Activity (I-IA) land use district which allows a maximum building height of 20.00 metres; and minimum setbacks of 15.0 metres (front, side, and rear). As such, to support the development concept proposed by this Stage 4 & 5 Outline Plan, a concurrent application to amend the BEASP has been submitted in order to delete policy 4.7.5(m).

The Minor ASP amendment maintains the intent of the BEASP while enabling the developer to implement an Industrial Land Use District, within the Stage 4 and 5 Outline Plan Area, which remains flexible and responsive to the market realities and leverages the developer's experience with the I-IA land use in stages 1 through 3 of the HPIP CS area.